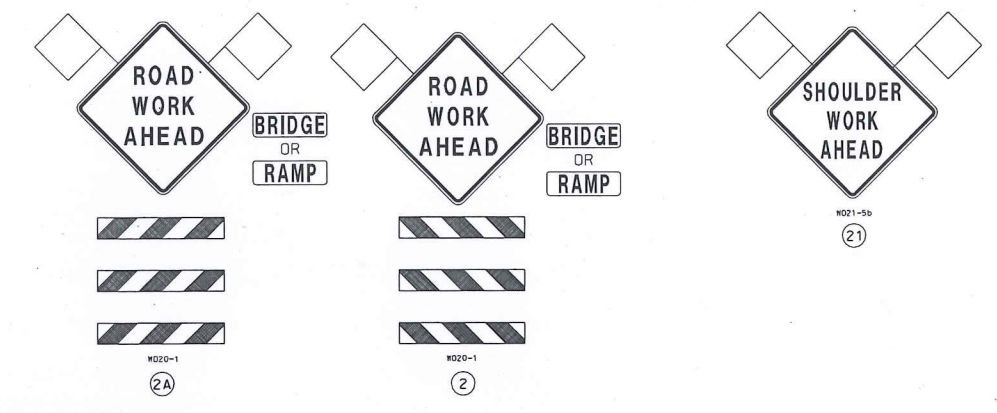
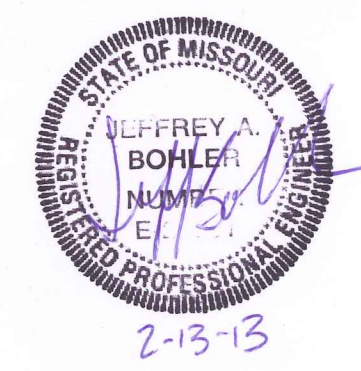


SHOULDER CLOSURE



GENERAL NOTES:

1. EXISTING SIGNS SHALL BE COVERED DURING WORKING HOURS ONLY IF IN CONFLICT WITH TRAFFIC CONTROL PLANS.
2. NO DIRECT PAYMENT WILL BE MADE FOR RELOCATING, COVERING, UNCOVERING OR REMOVING SIGNS.
3. CONTRACTOR SHALL NOT ASSUME THE EXISTING SHOULDER WIDTH WILL BE WIDE ENOUGH TO ACCOMMODATE CONSTRUCTION VEHICLES.



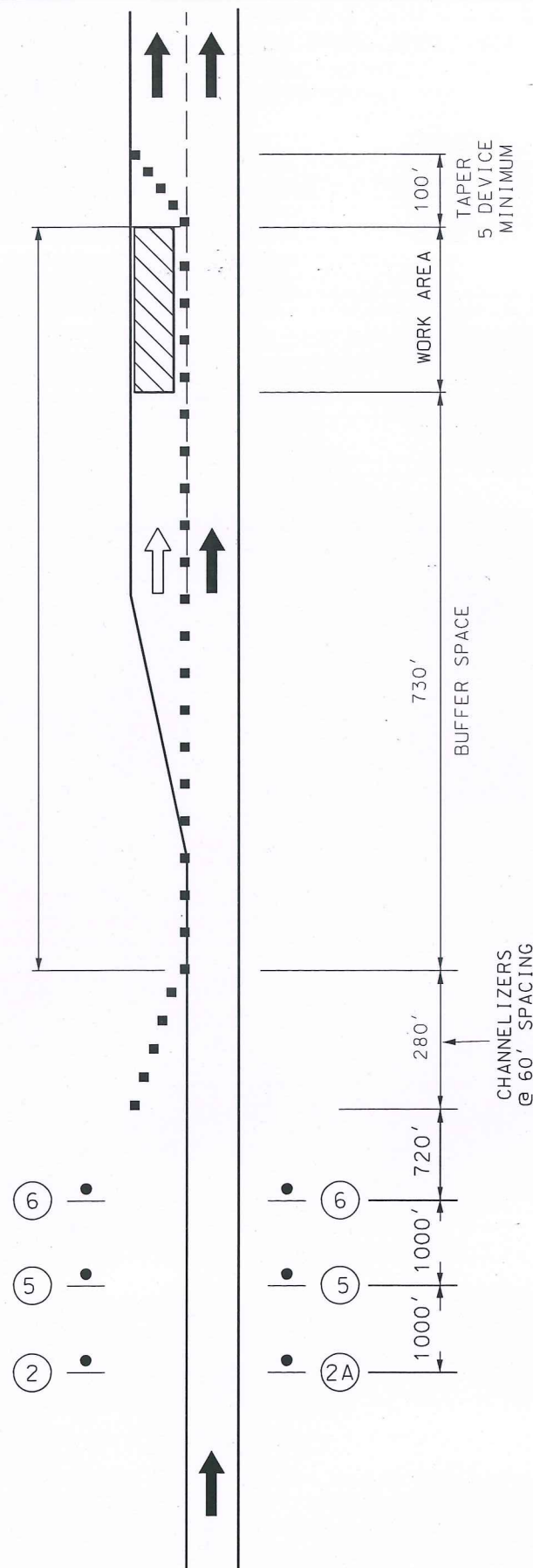
TRAFFIC CONTROL LEGEND

- WORK ZONE
- TRIMLINE CHANNELIZER
- SIGN
- FLASHING ARROW PANEL

NOT TO SCALE

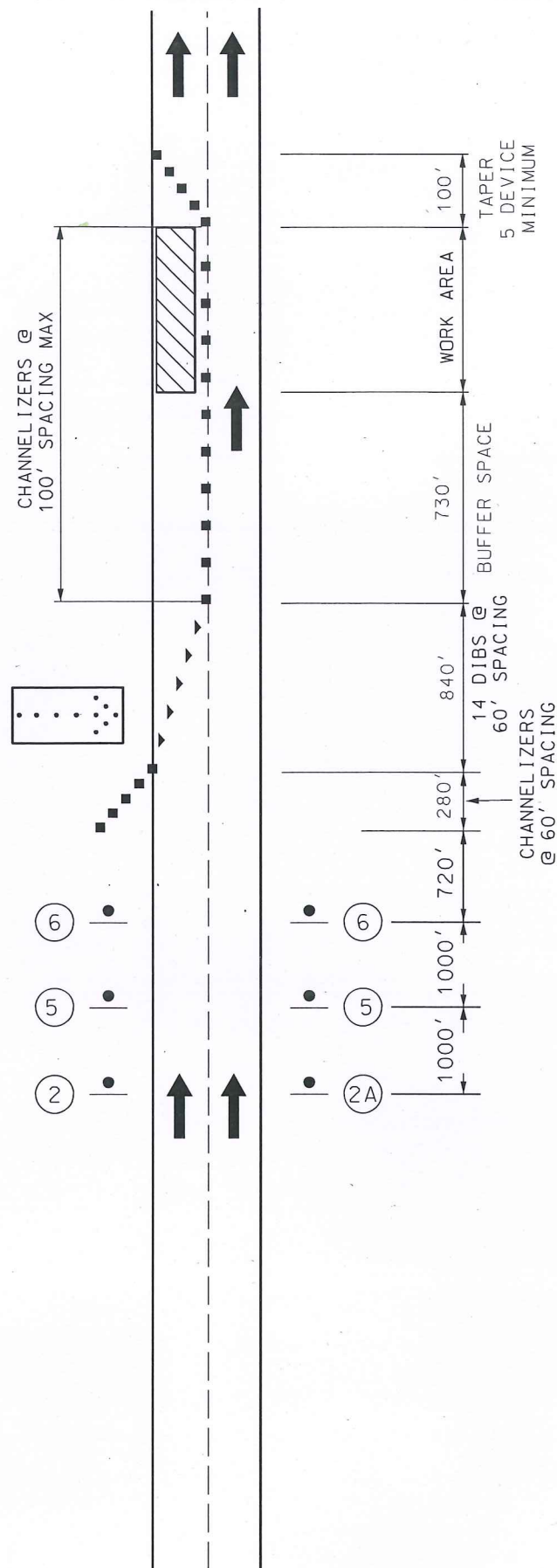
DISCLAIMER
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CHANNELIZERS @
100' SPACING MAX

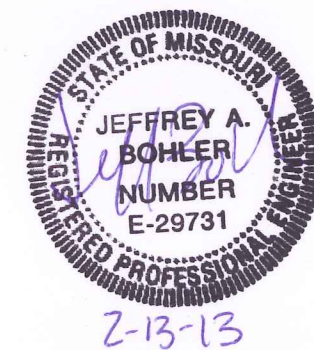


LANE CLOSURE FOR RAMP
ONE-TWO LANE ADDITION

CHANNELIZERS @
100' SPACING MAX

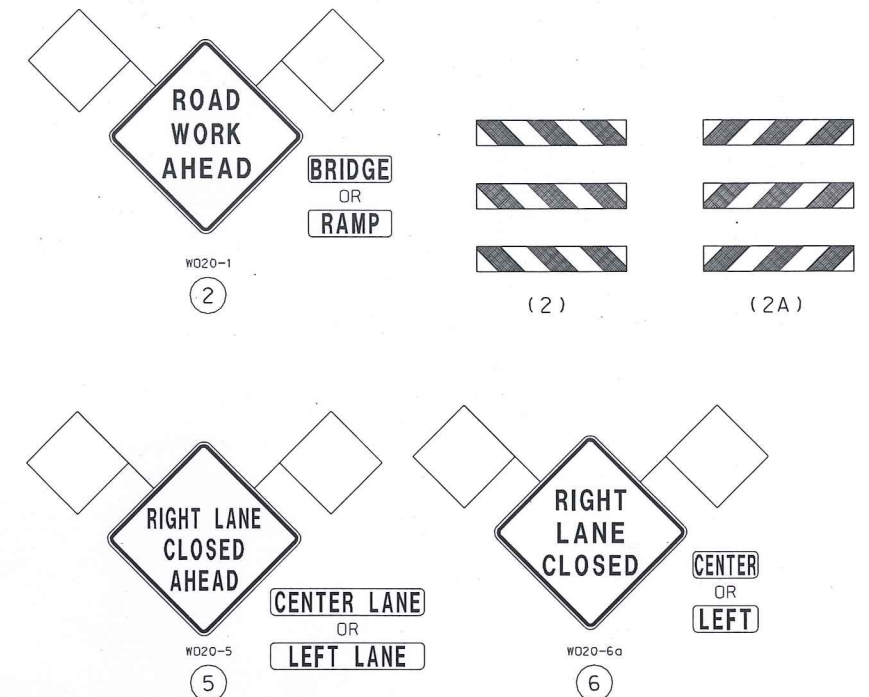


LANE CLOSURE FOR RAMP



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NOT TO SCALE



- NOTES:
1. SPACINGS MAY BE ADJUSTED AS NECESSARY TO MEET FIELD CONDITIONS.
 2. ANY EXISTING SIGNING THAT CONFLICTS WITH TRAFFIC CONTROL WILL BE REMOVED OR COVERED.

TRAFFIC CONTROL LEGEND

- WORK ZONE
- TRIMLINE CHANNELIZER
- DIRECTIONAL INDICATOR BARRICADE
- SIGN
- FLASHING ARROW PANEL

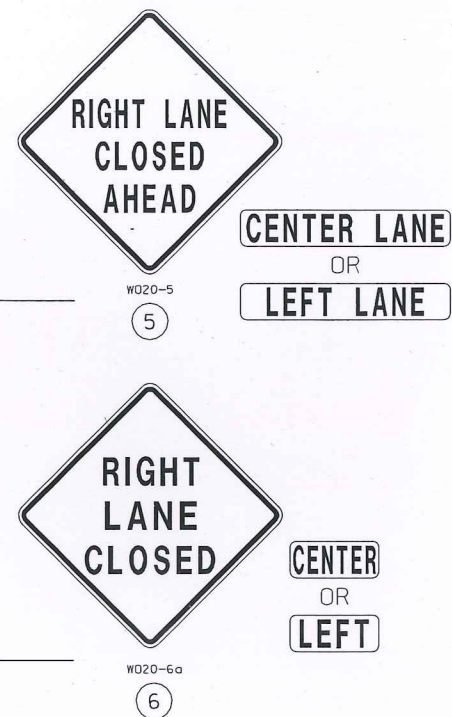
EXHIBIT C
TRAFFIC CONTROL
SHEET 2 OF 4

PROTECTIVE TRUCK WITH
WORK SIGN, FLASHING
ARROW PANEL AND TRUCK
MOUNTED ATTENUATOR.

WORK UNIT OR
WORK AREA






MOVING OPERATION ON MULTI-LANE RAMP OPEN TO TRAFFIC

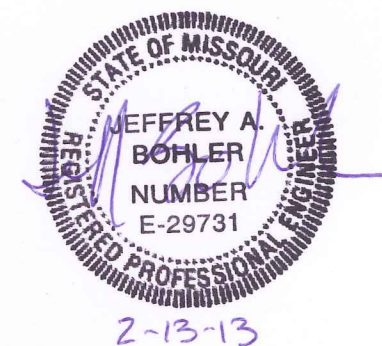
Right or Left
Lane



- NOTES:
1. SPACINGS MAY BE ADJUSTED AS NECESSARY TO MEET FIELD CONDITIONS.
 2. ANY EXISTING SIGNING THAT CONFLICTS WITH TRAFFIC CONTROL WILL BE REMOVED OR COVERED.

TRAFFIC CONTROL LEGEND

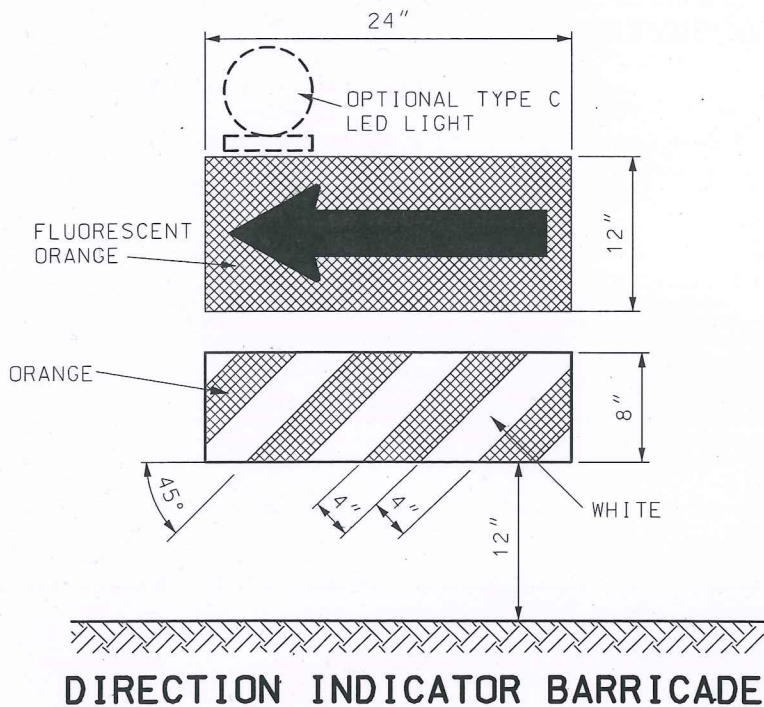
-  WORK ZONE
-  TRIMLINE CHANNELIZER
-  DIRECTIONAL INDICATOR BARRICADE
-  SIGN
-  FLASHING ARROW PANEL



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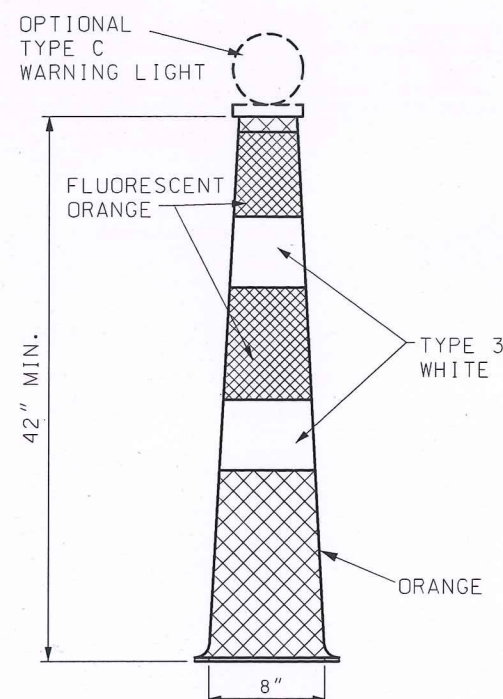
EXHIBIT C
TRAFFIC CONTROL
SHEET 3 OF 4



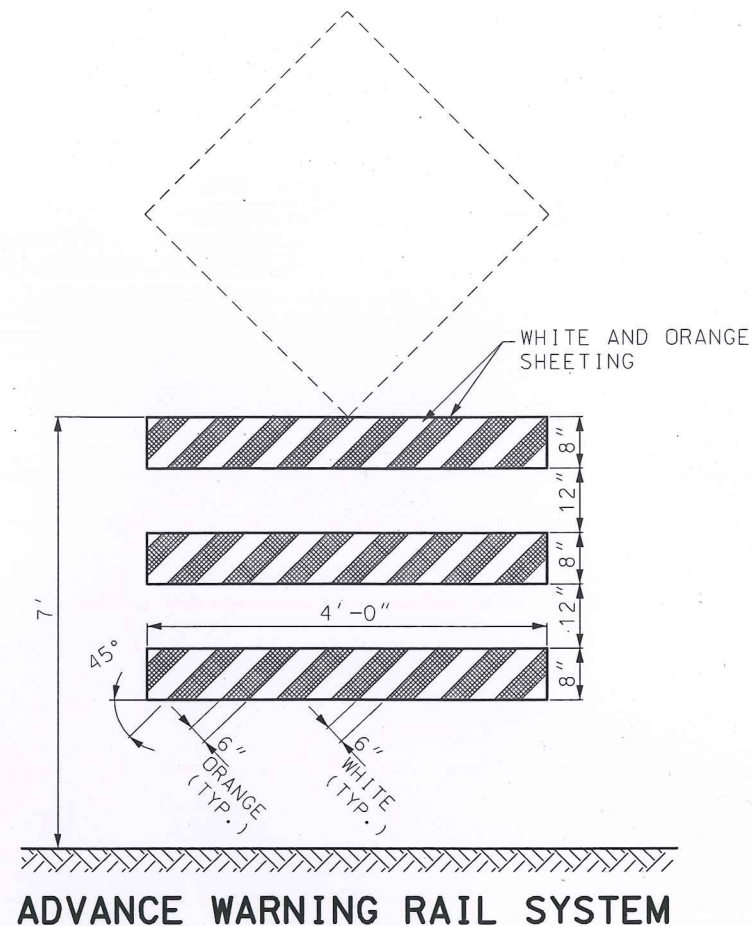
VERTICAL DIMENSIONS DO NOT INCLUDE PROJECTIONS DESIGNED FOR EASE OF HANDLING.

DIRECTION INDICATOR BARRICADES SHALL NOT BE USED IN SHIFTING TAPERS UNLESS SHOWN ON THE PLANS.

THE PANELS SHALL BE SECURELY ATTACHED TO A SUPPORT THAT IS PORTABLE, CAPABLE OF REMAINING UPRIGHT AND ENTIRELY FREE STANDING.



REFLECTIVE SHEETING APPLIED TO CHANNELIZERS SHALL BE REBOUNDABLE MEETING ASTM D 4956. STRIPES ON TRIM-LINE CHANNELIZERS SHALL BE 6" TO 8".

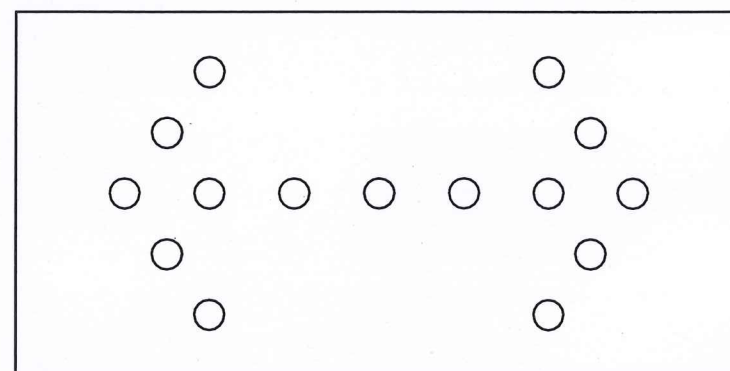


MAXIMUM WEIGHT OF SIGN SHALL NOT EXCEED 25 LBS.

THE SIGN AND RAIL SYSTEM MAY BE MOUNTED AS TWO SEPARATE CRASHWORTHY DEVICES. THE RAIL SYSTEM SHALL BE LOCATED DIRECTLY IN FRONT OF THE SIGN WITH 7 TO 10 FEET SEPARATING THE TWO DEVICES.

WHERE MARKING IS NOT PROVIDED ON THE BACKSIDE, STRIPS OF 3" WIDE MODOT TYPE 7 ORANGE SHEETING MAY BE APPLIED TO THE ENDS OF EACH RAIL TO HELP DELINEATE THE DEVICE.

WHITE AND ORANGE REFLECTIVE SHEETING SHALL BE IN ACCORDANCE WITH SEC 1042.2.7.3.



PANEL MOUNTING HEIGHT SHALL BE AT LEAST 7 FEET FROM THE ROADWAY SURFACE TO THE LOWEST POINT ON THE PANEL. THE BOTTOM OF THE PANEL SHALL BE RELATIVELY LEVEL WHEN IN USE.

GENERAL NOTES:

FLUORESCENT ORANGE REFLECTIVE SHEETING SHALL IN ACCORDANCE WITH SEC 1042.2.7.3.

BALLAST FOR TRAFFIC CONTROL DEVICES SHALL CONFORM TO MANUFACTURERS' RECOMMENDATION FOR FIELD CONDITIONS WHEN APPLICABLE.

WHITE AND ORANGE REFLECTIVE SHEETING SHALL BE IN ACCORDANCE WITH SEC 1042.2.7.2.

IF REQUIRED BY THE ENGINEER OR SPECIFIED ON THE PLANS, EACH DIRECTION INDICATOR BARRICADE, DRUMLIKE CHANNELIZER, AND VERTICAL PANEL SHALL BE EQUIPPED WITH ONE TYPE C LED PORTABLE LIGHT UNIT. IF USED, THE LIGHT UNIT AND BATTERY COMPARTMENT SHALL BE FURNISHED BY THE DEVICE MANUFACTURER OR OTHERWISE MEET THE MANUFACTURER'S RECOMMENDATIONS FOR DESIGN AND WILL BE REQUIRED ON ALL DEVICES IN THE SERIES.

UPON APPROVAL OF THE ENGINEER, THE CONTRACTOR MAY, AT NO ADDITIONAL COST, USE DRUM-LIKE CHANNELIZERS IN LIEU OF TRIM-LINE CHANNELIZERS TO PROVIDE LONGITUDINAL CHANNELIZATION WITHIN THE ACTIVITY AREA WHERE NO RAMPS, INTERSECTIONS OR LIMITED LATERAL CLEARANCE EXISTS.

UPON APPROVAL OF THE ENGINEER, THE CONTRACTOR MAY, AT NO ADDITIONAL COST, USE DIRECTION INDICATOR BARRICADES IN LIEU OF TRIM-LINE CHANNELIZERS IN MERGING TAPERS.

UPON APPROVAL OF THE ENGINEER, THE CONTRACTOR MAY, AT NO ADDITIONAL COST, USE VERTICAL PANELS IN LIEU OF TRIM-LINE CHANNELIZERS TO PROVIDE LONGITUDINAL CHANNELIZATION WITHIN THE ACTIVITY AREA.

UPON APPROVAL OF THE ENGINEER, THE CONTRACTOR MAY, AT NO ADDITIONAL COST, USE CONES IN LIEU OF TRIM-LINE CHANNELIZERS DURING DAYTIME OPERATIONS ON MINOR ROUTES.

PANEL AND RAIL MARKINGS FOR TRAFFIC DELINEATION SHALL SLOPE DOWNWARD TOWARD THE INTENDED DIRECTION OF TRAVEL. ILLUSTRATIONS SHOWN ARE FOR INSTANCES WHERE TRAFFIC MOVES TO THE LEFT, REVERSE CONFIGURATIONS SHALL BE USED FOR TRAFFIC MOVEMENTS TO THE RIGHT. MARKINGS SHALL ONLY BE APPLIED TO THE FRONT OF EACH RAIL OR PANEL, OR MAY BE APPLIED TO BOTH THE FRONT AND BACK PROVIDING THE MARKING ON THE BACK DOES NOT CONFLICT WITH INTENDED OPPOSING TRAFFIC MOVEMENT.



EXHIBIT C
TRAFFIC CONTROL
SHEET 4 OF 4