

**JESSE VIERTEL MEMORIAL AIRPORT (VER)
BOONVILLE, MISSOURI**

MoDOT PROJECT NO. 23-039A-1

**BASE BID
Reconstruct North T-Hangar Taxilanes**

**ADD ALTERNATE NO. 1
Rehabilitate Shade Hangar Taxilane**

**ADD ALTERNATE NO. 2
Rehabilitate South T-Hangar Taxilanes and South Connecting Taxiway**

ADDENDUM NO. 1

March 25th, 2024



TO ALL PROSPECTIVE BIDDERS:

- A. You are hereby notified of the following amendments to the Construction Plans for the subject project. Revised versions of the referenced Plan Sheets are included with this addendum for reference.**
1. Sheet No. 0.0, Title Sheet. The sheet list table has been modified to incorporate new Plan Sheets 1.5 and 5.1A that are part of Addendum No. 1.
Revised Sheet No. 0.0, Title Sheet is included with this addendum for reference.
 2. New Sheet No. 1.5, Construction Safety and Phasing Plan Details. Fence removal and replacement is required to install and remove the stabilized construction exit. This plan sheet provides a detail for the fence that shall be replaced after the removal of the stabilized construction exit. Material and labor required to remove and re-install the fence as part of the construction, maintenance, and removal of the stabilized construction exit shall be considered subsidiary under Base Bid Item "Stabilized Construction Exit".
New Sheet No. 1.5, Construction Safety and Phasing Plan Details is included with this addendum for reference.
 3. Sheet No. 2.0, Summary of Quantities. This sheet has been revised to reflect up-to-date quantities resulting from this addendum affecting Base Bid Items 2, 25, 26, and 27.
Revised Sheet No. 2.0, Summary of Quantities is included with this addendum for reference.
 4. New Sheet No. 5.1A, Grading Plan. The Contractor has the option to remove the excess excavation off-site or to place it on-site. This sheet details the placement and grading of the excess excavation and seeding, as well as the installation of the erosion control.
New Plan Sheet No. 5.1A, Grading Plan is included with this addendum for reference.

B. You are hereby notified of the following amendments to the Contract Documents/Specifications (Book 1 of 2) for the subject project.

1. Section 1, Notice to Bidders. This section has been revised to reflect up-to-date quantities resulting from this addendum affecting Base Bid Items 2, 25, 26, and 27.

Revised Section 1, Notice to Bidders is included with this addendum for reference.

2. Proposal Form. This section has been revised to reflect up-to-date quantities resulting from this addendum affecting Base Bid Items 2, 25, 26, and 27.

Revised Proposal Form is not included with this addendum for reference.

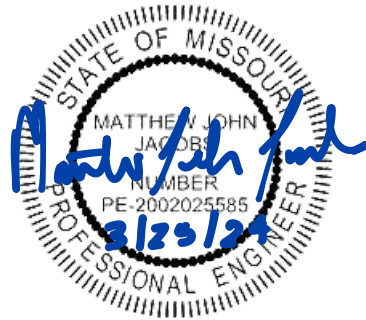
C. You are hereby notified of the following amendments to the Official Bid Form for the subject project.

1. Pages 1 and 2 have been revised to reflect up-to-date quantities resulting from this addendum affecting Base Bid Items 2, 25, 26, and 27.

Revised Page 2 of the Official Bid Form is included with this addendum. Please utilize these revised pages for preparing your bid.

D. A copy of the pre-bid meeting minutes and attendees list is included with this addendum for reference.

E. All bidders must acknowledge receipt of this addendum in the space provided on Page 5 of the Official Bid Form packet. Failure to acknowledge receipt of an addendum may be cause for rejection of the bid.





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CITY OF BOONVILLE, MISSOURI

JESSE VIERTEL MEMORIAL AIRPORT (VER)

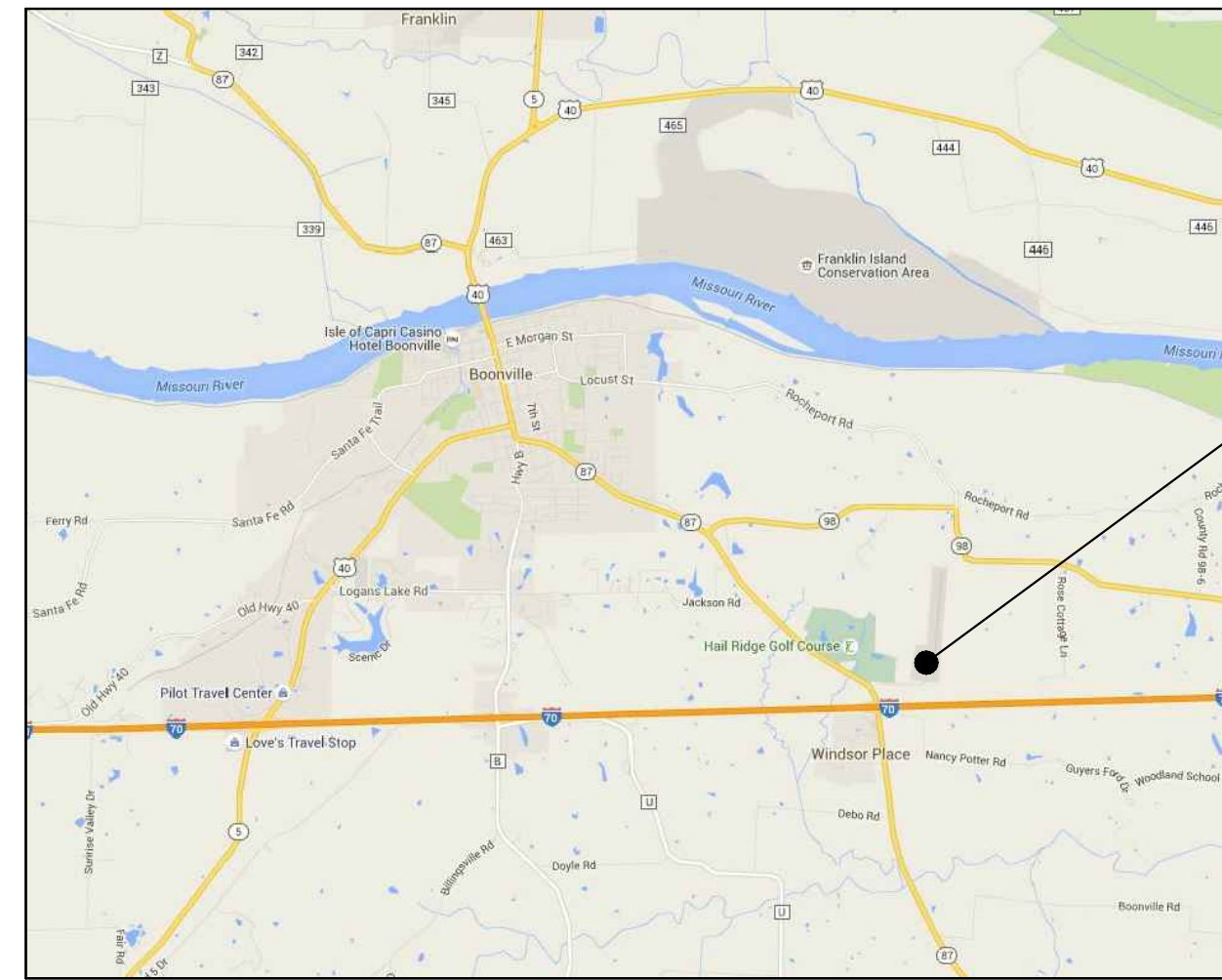
BOONVILLE, MISSOURI

PROJECT NO.	18627 TO01
DRAWN BY	REM/JDR
DATE	09/22/2023
CHECKED BY	BRE
DATE	09/25/2023
DESIGNED BY	REM
DATE	09/22/2023
REVISIONS	
ADDENDUM No. 1	03/25/2024

TITLE SHEET

0.0

VICINITY MAP



NO SCALE

Jesse Viertel Memorial Airport (VER)

CITY OF BOONVILLE, MISSOURI

CONSTRUCTION PLANS

FOR

JESSE VIERTEL MEMORIAL AIRPORT (VER)

MoDOT PROJECT NO. 23-039A-1

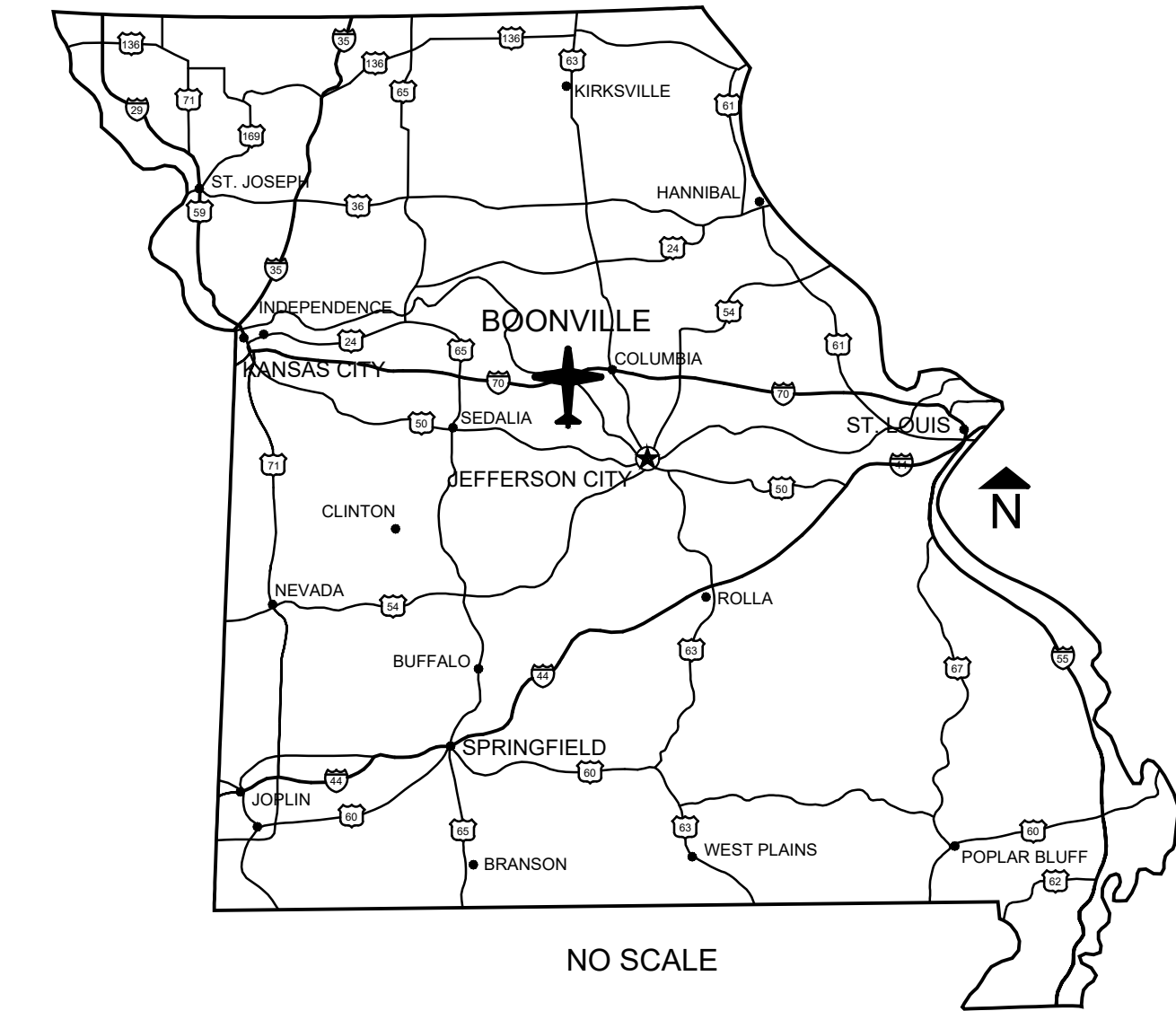
BASE BID
RECONSTRUCT NORTH T-HANGAR TAXILANES

ADD ALTERNATE NO. 1
REHABILITATE SHADE HANGAR TAXILANE

ADD ALTERNATE NO. 2
REHABILITATE SOUTH T-HANGAR TAXILANES
AND SOUTH CONNECTING TAXIWAY

MARCH 2024

LOCATION MAP



NO SCALE

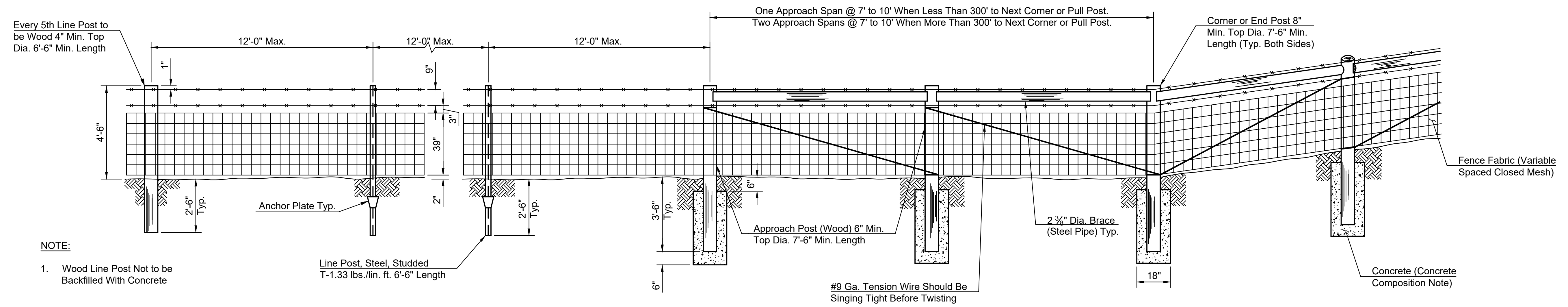
Sheet List Table	
Sheet Number	Sheet Title
0.0	TITLE SHEET
0.1	GENERAL LAYOUT
0.2	AIRPORT LAYOUT DRAWING
1.0 - 1.3	CONSTRUCTION SAFETY AND PHASING PLAN
1.4 - 1.5	CONSTRUCTION SAFETY AND PHASING PLAN DETAILS
2.0	SUMMARY OF QUANTITIES
2.1	PAY ITEM NOTES
2.2	TYPICAL SECTIONS
2.3	BORING LOG
2.4 - 2.5	BORING LOG DETAILS
3.0 - 3.1	DEMOLITION PLAN
4.0	WEST TAXILANE PLAN AND PROFILE
4.1	CENTER TAXILANE PLAN AND PROFILE
4.2	EAST TAXILANE PLAN AND PROFILE
4.3	NORTH TAXILANE PLAN AND PROFILE
4.4	DRAINAGE DETAILS
5.0	SPOT ELEVATIONS
5.1 - 5.1A	GRADING PLAN
5.2	EROSION CONTROL DETAILS
6.0	JOINT PLAN
6.1	JOINT DETAILS
7.0	ADD ALTERNATE NO. 1 PAVEMENT REHABILITATION PLAN
7.1 - 7.3	ADD ALTERNATE NO. 2 PAVEMENT REHABILITATION PLAN
7.4 - 7.5	PAVEMENT REHABILITATION DETAILS
8.0	BASE BID PAVEMENT MARKING PLAN
8.1 - 8.2	ADD ALTERNATE PAVEMENT MARKING PLAN
9.0 - 9.1	WEST TAXILANE CROSS SECTIONS
9.2 - 9.3	CENTER TAXILANE CROSS SECTIONS
9.4 - 9.6	EAST TAXILANE CROSS SECTIONS
9.7 - 9.9	NORTH TAXILANE CROSS SECTIONS

NOTE:

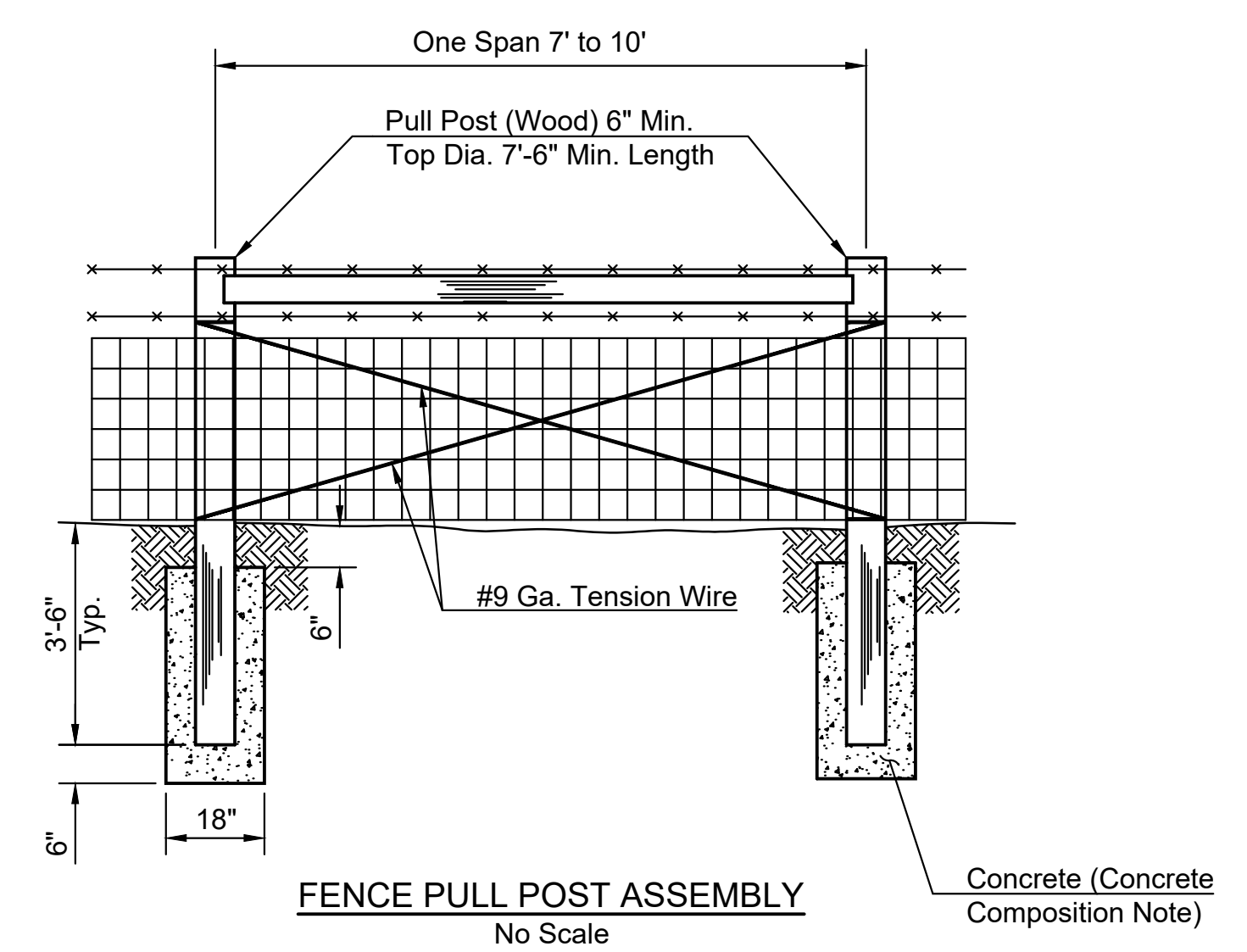
This plan set has been drawn in color to better depict the proposed construction at the airport. For clarity, please print the entire plan set in color.



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ADDENDUM No. 1		03/25/2024	



PERIMETER FENCE REPLACEMENT DETAIL
No Scale



FENCE PULL POST ASSEMBLY
No Scale

NOTE:

1. Post Holes shall be at least 6" larger than the diameter of the post and shall be backfilled with concrete.
2. Wood line post not to be backfilled with concrete.
3. Steel line posts shall be of an approved "U", "Y", "T", or channel section, notched or studded with an anchor plate. Post punched with holes or self fastening lugs will not be permitted.
4. Staples shall be screw shank type or equivalent (1-1/4" minimum length)
5. Stretched fabric and barbed wire on outside of post on corners and curves.
6. Attachment of fabric to steel line posts in accordance with manufacturer's recommendations.
7. Pull post to be installed at 500' intervals.
8. Pull post assembly two 6" top diameter post, one 4" diameter brace post, 9 gauge tension wire twisted.
9. Material and labor required to remove and re-install fence as part of the construction, maintenance, and removal of the Stabilized Construction Exit shall be considered subsidiary under Base Bid Item "Stabilized Construction Exit".

1

This sheet added per Addendum No. 1

SUMMARY OF QUANTITIES					
ITEM NO.	SPEC	ITEM DESCRIPTION	UNIT	QUANTITY	
				ESTIMATED	AS-CONST.
BASE BID					
RECONSTRUCT NORTH T-HANGAR TAXILANES					
1	C-100	Contractor Quality Control Program (CQCP)	L.S.	1	
2	C-102	Erosion Control Barrier (Silt Fence) (Revised per Addendum No. 1)	L.F.	760	
3	C-102	Erosion Control Barrier (Straw Wattle)	L.F.	15	
4	C-102	Stabilized Construction Exit	L.S.	1	
5	C-105	Mobilization (NTE 10% of Total Bid Amount)	L.S.	1	
6	TEMP	Temporary Marking, Lighting, and Barricades	L.S.	1	
7	P-101	Remove Pavement and Aggregate Base Course	S.Y.	6,439	
8	P-101	Saw Cut (Full Depth)	L.F.	1,802	
9	P-101	Remove Underdrain System	L.S.	1	
10	P-152	Unclassified Excavation	C.Y.	1,499	
11	P-152	Unsuitable Subgrade Removal and Replacement	C.Y.	400	
12	P-154	Aggregate Subbase Course (10")	S.Y.	6,524	
13	P-208, P-209, or P-219	Aggregate Base Course (4")	S.Y.	6,524	
14	P-501	P.C.C. Pavement (6")	S.Y.	6,458	
15	P-620	Surface Preparation, Pavement Marking Removal	S.F.	845	
16	P-620	Permanent Reflectorized Pavement Marking (Yellow)	S.F.	1,098	
17	P-620	Permanent Non-Reflectorized Pavement Marking (Black)	S.F.	2,196	
18	P-620	Temporary Non-Reflectorized Pavement Marking (Yellow)	S.F.	1,098	
19	D-705	Perforated Underdrain (Schedule 40) (4")	L.F.	458	
20	D-705	Perforated Underdrain (Schedule 80) (4")	L.F.	560	
21	D-705	Non-Perforated Outlet Pipe (Schedule 40) (4")	L.F.	19	
22	D-705	Non-Perforated Outlet Pipe (Schedule 80) (4")	L.F.	313	
23	D-705	Underdrain Cleanout Riser	Each	7	
24	D-705	Splash Pad	Each	2	
25	TREC	Erosion Control Blanket, Type 2C (Revised per Addendum No. 1)	S.Y.	3,974	
26	T-901	Temporary Seeding (Revised per Addendum No. 1)	S.Y.	3,974	
27	T-901	Permanent Seeding (Revised per Addendum No. 1)	S.Y.	3,974	

SUMMARY OF QUANTITIES					
ITEM NO.	SPEC	ITEM DESCRIPTION	UNIT	QUANTITY	
				ESTIMATED	AS-CONST.
ADD ALTERNATE NO. 1					
REHABILITATE SHADE HANGAR TAXILANE					
1	P-101	Clean and Seal Joints and Cracks in Asphalt Pavement	L.F.	500	
2	P-101	Route, Clean, and Seal Cracks in Concrete Pavement	L.F.	150	
3	P-620	Temporary Non-Reflectorized Pavement Marking (Yellow)	S.F.	134	
4	P-620	Permanent Reflectorized Pavement Marking (Yellow)	S.F.	134	
5	P-620	Permanent Non-Reflectorized Pavement Marking (Black)	S.F.	268	
6	P-631	Refined Coal Tar Emulsion with Additives for Slurry Seal	S.Y.	578	

ITEM NO.	SPEC	ITEM DESCRIPTION	UNIT	QUANTITY	
				ESTIMATED	AS-CONST.
ADD ALTERNATE NO. 2					
REHABILITATE SOUTH T-HANGAR TAXILANES AND SOUTH CONNECTING TAXIWAY					
1	P-101	Full Depth Concrete Pavement Removal (Full and Partial Panel) (6")	S.Y.	369	
2	P-101	Unsuitable Base Course Removal (4")	S.Y.	30	
3	P-101	Clean and Reseal Existing Concrete Joints	L.F.	20,433	
4	P-101	Joint Spall Repair	C.F.	5	
5	P-101	Concrete Pavement Pop-Outs	L.S.	1	
6	P-208, P-209, or P-219	Aggregate Base Course (4")	S.Y.	37	
7	P-501	Full and Partial Panel Material (Concrete)	C.Y.	62	
8	P-501	Full and Partial Panel Placement (Concrete)	S.Y.	369	
9	P-620	Surface Preparation, Pavement Marking Removal	S.F.	3,159	
10	P-620	Permanent Reflectorized Pavement Marking (Yellow)	S.F.	1,156	
11	P-620	Permanent Non-Reflectorized Pavement Marking (Black)	S.F.	2,262	

EARTHWORK - BASE BID				
AREA	ON-SITE EXCAVATION (CU YDS.) (1) (3)		COMPACTED EMBANKMENT (CU YDS.) (2)	
	UNCLASSIFIED EXCAVATION	REVISED	COMMON	REVISED
Project Grading Limits	1,499		161	
Excess Excavation Material Placement	0		1,338	
Total	1,499		1,499	

NOTES:

- The quantities for Unclassified Excavation were calculated based on the volume of material required between the existing surface with pavement and base removed and proposed datum surfaces.
- No shrinkage allowance is included with this quantity
- Excess excavation shall be placed on site as shown on the grading plans.



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CITY OF BOONVILLE, MISSOURI

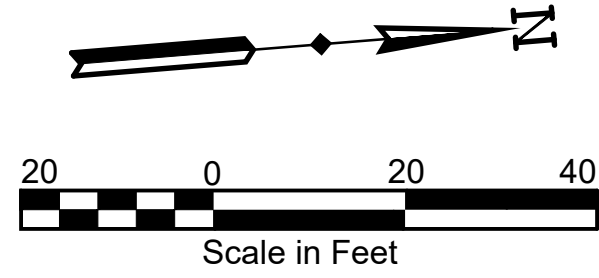
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BOONVILLE, MISSOURI

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SUMMARY OF QUANTITIES

2.0

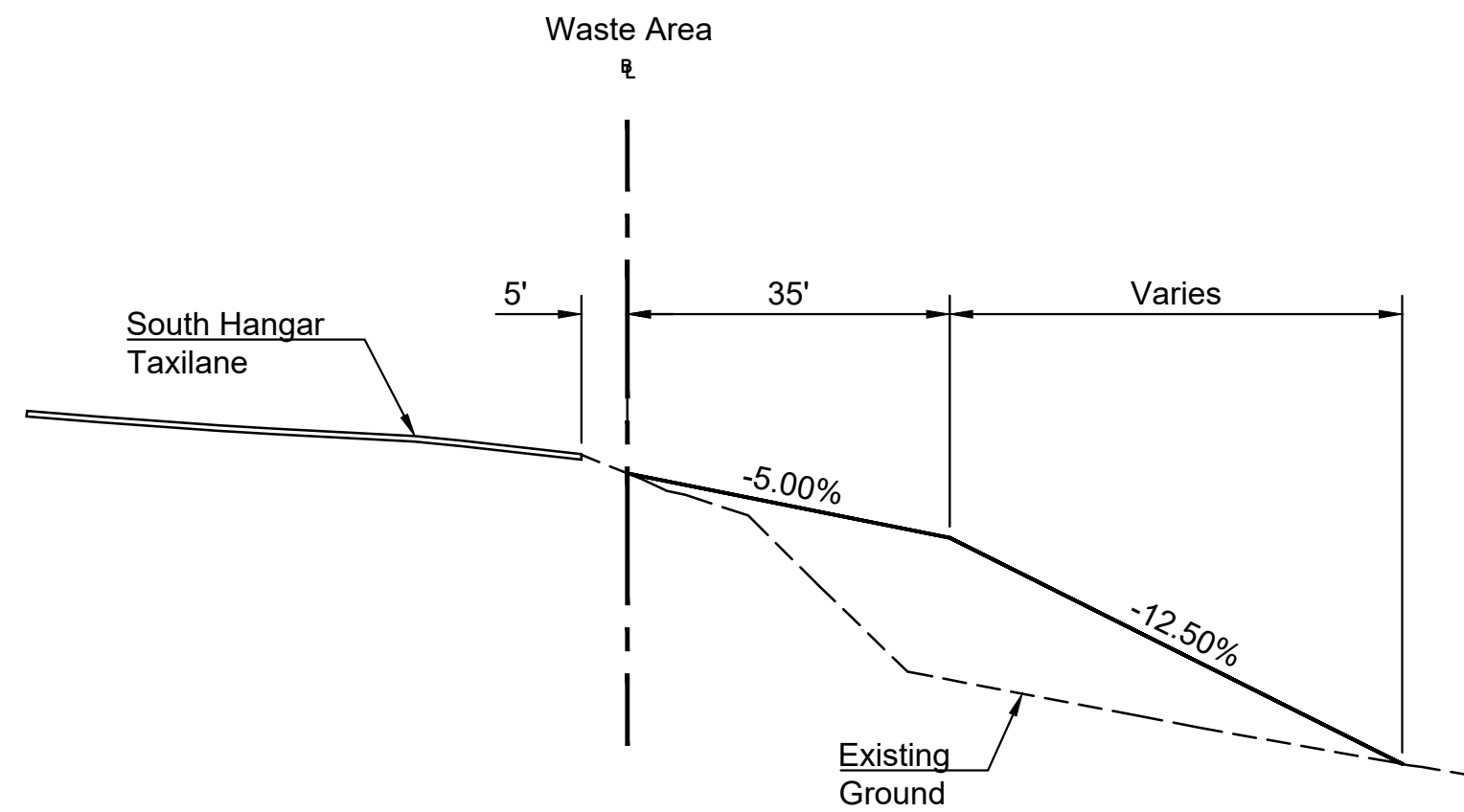


LEGEND

	715	Proposed 1' Contour
	715.5	Proposed 0.5' Contour
	715	Existing 1' Contour
	715.5	Existing 0.5' Contour
		Proposed Silt Fence
		Proposed Straw Wattle
		Grading, Seeding, and Erosion Control Blanket Limits
		Proposed Erosion Control Blanket, Type 2C

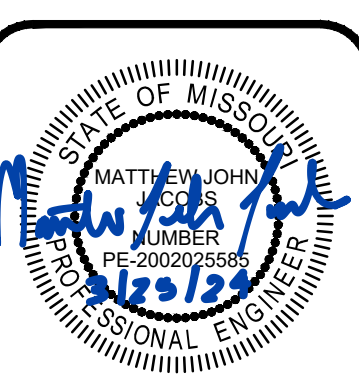
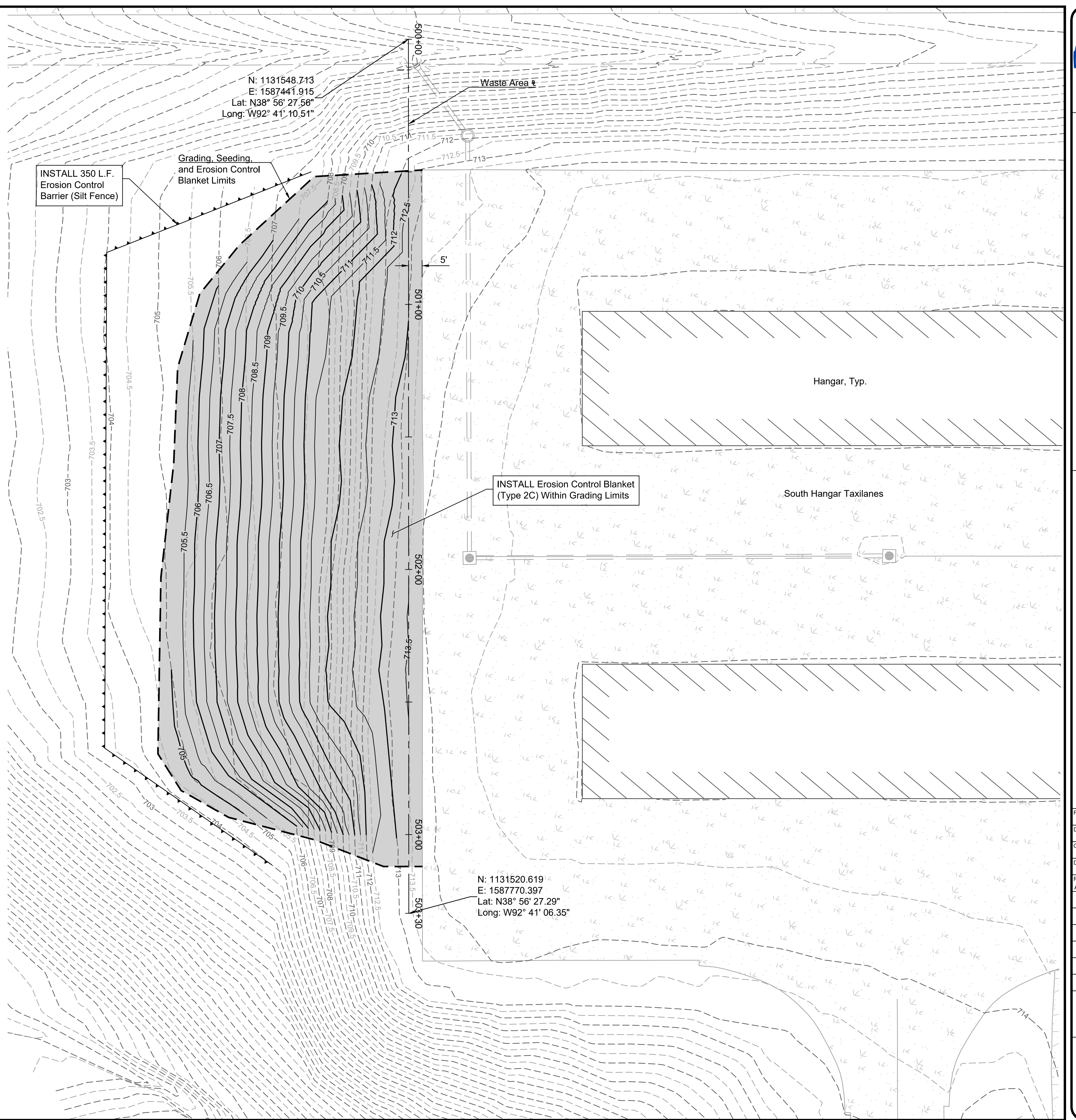
SEEDING AND EROSION CONTROL NOTES:

- All disturbed areas within the Grading, Seeding, and Erosion Control Limits shall be subject to Seeding per FAA Specification T-901 CRM and Erosion Control Blanket (Type 2C) installation per Specification TREC.
- The implementation and compliance with the Storm Water Pollution Prevention Plan (SWPPP) will not be paid for directly but shall be considered subsidiary to other items in the contract. This includes all labor, equipment, and material to complete the described work in the SWPPP.



WASTE AREA TYPICAL SECTION
Sta. 500+50.00 to Sta. 503+00
No Scale

1 This Sheet Added Per Addendum No. 1



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CITY OF BOONVILLE, MISSOURI

JESSE VIERTEL MEMORIAL AIRPORT (VER)
BOONVILLE, MISSOURI

PROJECT NO.	18627 TO01
DRAWN BY	REM/JDR DATE 09/22/2023
CHECKED BY	BRE DATE 09/25/2023
DESIGNED BY	REM DATE 09/22/2023
REVISIONS	DATE
ADDENDUM No. 1	03/25/2024

GRADING PLAN

5.1A

**SECTION 1
NOTICE TO BIDDERS**

**CITY OF BOONVILLE, MISSOURI
JESSE VIERTEL MEMORIAL AIRPORT (VER)
State Block Grant Project No. 23-039A-1**

Sealed bids subject to the conditions and provisions presented herein will be received until **10:00 A.M. (CDT), Tuesday, April 2nd, 2024**, and then publicly opened and read immediately after at the **Council Chambers, 525 E. Spring**, for furnishing all labor, materials, equipment and performing all work necessary to:

Base Bid

Reconstruct North T-Hangar Taxilanes

Add Alternate No. 1

Rehabilitate Shade Hangar Taxilane

Add Alternate No. 2

Rehabilitate South T-Hangar Taxilanes and South Connecting Taxiway

Copies of the bid documents including project drawings and technical specifications are on file and may be inspected at:

**Boonville City Hall
401 Main Street
Boonville, MO 65233**

**Lochner
15717 College Boulevard
Lenexa, KS 66219**

A digital download of construction plans, project specifications, contract documents, the Official Bid Form, and other related contract information may be ordered online at www.QuestCDN.com. These documents may be downloaded for \$30.00. Payment must be made directly to Quest CDN. There will be no refunds.

An **In-Person** pre-bid meeting will be on **Wednesday, March 20th, 2024 at 2:00 P.M. (CDT)** at the **Council Chambers, 525 E. Spring**. Following the pre-bid meeting, prospective bidders will have the opportunity to tour the project site. This is the only time prospective bidders will have to tour the project site.

Contract Work Items. This project will involve the following work items and estimated quantities. Prospective bidders are hereby advised that the quantities indicated herein are approximate and are subject to change.

ITEM NO.	SPEC.	ITEM DESCRIPTION	QTY.	UNIT
MoDOT PROJECT NO. 23-039A-1				
BASE BID				
RECONSTRUCT NORTH T-HANGAR TAXILANES				
1	C-100	Contractor Quality Control Program (CQCP)	1	L.S.
2	C-102	Erosion Control Barrier (Silt Fence) <i>(Revised per Addendum No. 1)</i>	760	L.F.
3	C-102	Erosion Control Barrier (Straw Wattle)	15	L.F.
4	C-102	Stabilized Construction Exit	1	L.S.
5	C-105	Mobilization (NTE 10% of Total Bid Amount)	1	L.S.
6	TEMP	Temporary Marking, Lighting, and Barricades	1	L.S.
7	P-101	Remove Pavement and Aggregate Base Course	6,439	S.Y.
8	P-101	Saw Cut (Full Depth)	1,802	L.F.
9	P-101	Remove Underdrain System	1	L.S.

ITEM NO.	SPEC.	ITEM DESCRIPTION	QTY.	UNIT
10	P-152	Unclassified Excavation	1,499	C.Y.
11	P-152	Unsuitable Subgrade Removal and Replacement	400	C.Y.
12	P-154	Aggregate Subbase Course (10")	6,524	S.Y.
13	P-208, P-209, or P-219	Aggregate Base Course (4")	6,524	S.Y.
14	P-501	P.C.C. Pavement (6")	6,458	S.Y.
15	P-620	Surface Preparation, Pavement Marking Removal	845	S.F.
16	P-620	Permanent Reflectorized Pavement Marking (Yellow)	1,098	S.F.
17	P-620	Permanent Non-Reflectorized Pavement Marking (Black)	2,196	S.F.
18	P-620	Temporary Non-Reflectorized Pavement Marking (Yellow)	1,098	S.F.
19	D-705	Perforated Underdrain (Schedule 40) (4")	458	L.F.
20	D-705	Perforated Underdrain (Schedule 80) (4")	560	L.F.
21	D-705	Non-Perforated Outlet Pipe (Schedule 40) (4")	19	L.F.
22	D-705	Non-Perforated Outlet Pipe (Schedule 80) (4")	313	L.F.
23	D-705	Underdrain Cleanout Riser	7	Each
24	D-705	Splash Pad	2	Each
25	TREC	Erosion Control Blanket, Type 2C (<i>Revised per Addendum No. 1</i>)	3,974	S.Y.
26	T-901	Temporary Seeding (<i>Revised per Addendum No. 1</i>)	3,974	S.Y.
27	T-901	Permanent Seeding (<i>Revised per Addendum No. 1</i>)	3,974	S.Y.

ITEM NO.	SPEC.	ITEM DESCRIPTION	QTY.	UNIT
ADD ALTERNATE NO. 1				
REHABILITATE SHADE HANGAR TAXILANE				
1	P-101	Clean and Seal Joints and Cracks in Asphalt Pavement	500	L.F.
2	P-101	Route, Clean, and Seal Cracks in Concrete Pavement	150	L.F.
3	P-620	Temporary Non-Reflectorized Pavement Marking (Yellow)	134	S.F.
4	P-620	Permanent Reflectorized Pavement Marking (Yellow)	134	S.F.
5	P-620	Permanent Non-Reflectorized Pavement Marking (Black)	268	S.F.
6	P-631	Refined Coal Tar Emulsion with Additives for Slurry Seal	578	S.Y.

ITEM NO.	SPEC.	ITEM DESCRIPTION	QTY.	UNIT
ADD ALTERNATE NO. 2				
REHABILITATE SOUTH T-HANGAR TAXILANES AND SOUTH CONNECTING TAXIWAY				
1	P-101	Full Depth Concrete Pavement Removal (Full and Partial Panel) (6")	369	S.Y.
2	P-101	Unsuitable Base Course Removal (4")	30	S.Y.
3	P-101	Clean and Reseal Existing Concrete Joints	20,433	L.F.
4	P-101	Joint Spall Repair	5	C.F.
5	P-101	Concrete Pavement Pop-Outs	1	L.S.
6	P-208, P-209, or P-219	Aggregate Base Course (4")	37	S.Y.
7	P-501	Full and Partial Panel Material (Concrete)	62	C.Y.
8	P-501	Full and Partial Panel Placement (Concrete)	369	S.Y.
9	P-620	Surface Preparation, Pavement Marking Removal	3,159	S.F.
10	P-620	Permanent Reflectorized Pavement Marking (Yellow)	1,156	S.F.
11	P-620	Permanent Non-Reflectorized Pavement Marking (Black)	2,262	S.F.

Contract Time. The owner has established a contract performance time of **seventy-four (74)** calendar days from the date of the Notice-to-Proceed. **Phase 1 shall be completed in thirty (30) calendar days or less, Phase 2 shall be completed in forty (40) calendar days or less, Phase 3 shall be completed in two (2) calendar days or less, and Phase 4 shall be completed in two (2) calendar days or less.** All project work shall be substantially completed within the stated timeframe. This project is subject to liquidated damages as prescribed in the project manual. **The phases may not run concurrently and shifting of contract time between phases will not be permitted.**

Bid Security. No bid will be considered unless accompanied by a certified check or cashier's check on any bank or trust company insured by the Federal Deposit Insurance Corporation, payable to the **City of Boonville, Missouri**, for not less than five (5) percent of the total amount of the bid, or by a bid bond secured by an approved surety or sureties, payable to the owner, for not less than five (5) percent of the total amount of the bid.

Bonding Requirements. The successful bidder will be required to furnish separate performance and payment bonds each in an amount equal to 100% of the contract price at the time of contract execution.

Award of Contract. All proposals submitted in accordance with the instructions presented herein will be subject to evaluation. Bids may be held by the **City of Boonville** for a period not to exceed **ninety (90)** calendar days from the date of the bid opening for the purpose of conducting the bid evaluation.

Award of contract will be based on the lowest aggregate sum proposal submitted from those bidders that are confirmed as being responsive and responsible for the following scenarios:

Base Bid

Base Bid + Add Alternate No. 1

Base Bid + Add Alternate No. 2

Base Bid + Add Alternate No. 1 + Add Alternate No. 2

The owner reserves the right to select any one of the combinations of the base bid(s) and alternate bid(s), which in the judgment of the owner, best serves the owner's interest. The right is reserved, as the **City of Boonville** may require, to reject any bid and all bids.

Award of contract is contingent upon the owner receiving Federal-funding assistance under the State Block Grant Program.

Notice-To-Proceed

It is the intent of the Owner to issue the Notice-To-Proceed (NTP) as soon as practical after the Award of Contract. The anticipated early NTP date is **July 2024** and the anticipated late NTP is **April 2025**.

Federal Provisions. This project is subject to the following Federal provisions, statutes and regulations:

Equal Employment Opportunity - Executive Order 11246 and 41 CFR Part 60: The Bidder's attention is called to the "Equal Opportunity Clause" and the "Standard Federal Equal Employment Opportunity Construction Contract Specifications" set forth within the supplementary provisions. The successful Bidder shall not discriminate against any employee or applicant for employment because of race, color, religion, sex, or national origin. The Contractor will take affirmative action to ensure that applicants are employed, and that employees are treated during employment without regard to their race, color, religion, sex, or national origin.

Notice of Requirement for Affirmative Action to Ensure Equal Employment Opportunity:

1. The Bidder's attention is called to the "Equal Opportunity Clause" and the "Standard Federal Equal Employment Opportunity Construction Contract Specifications" set forth within the supplementary provisions.

2. The goals and timetables for minority and female participation, expressed in percentage terms for the contractor's aggregate workforce in each trade on all construction work in the covered area, are as follows:

Timetables

Goals for minority participation for each trade: **4.0% (Cooper County)**

Goals for female participation in each trade: 6.9%.

These goals are applicable to all of the contractor's construction work (whether or not it is Federal or federally-assisted) performed in the covered area. If the contractor performs construction work in a geographical area located outside of the covered area, it shall apply the goals established for such geographical area where the work is actually performed. With regard to this second area, the contractor is also subject to the goals for both its federally involved and non-federally involved construction.

The contractor's compliance with the Executive Order and the regulations in 41 CFR Part 60-4 shall be based on its implementation of the Equal Opportunity Clause, specific affirmative action obligations required by the specifications set forth in 41 CFR 60-4.3(a), and its efforts to meet the goals. The hours of minority and female employment and training must be substantially uniform throughout the length of the contract, and in each trade, and the contractor shall make a good faith effort to employ minorities and women evenly on each of its projects. The transfer of minority or female employees or trainees from Contractor to Contractor or from project to project for the sole purpose of meeting the Contractor's goals shall be a violation of the contract, the Executive Order and the regulations in 41 CFR Part 60-4. Compliance with the goals will be measured against the total work hours performed.

3. The Contractor shall provide written notification to the Director of the Office of Federal Contract Compliance Programs (OFCCP) within 10 working days of award of any construction subcontract in excess of \$10,000 at any tier for construction work under the contract resulting from this solicitation. The notification shall list the name, address, and telephone number of the subcontractor; employer identification number of the subcontractor; estimated dollar amount of the subcontract; estimated starting and completion dates of the subcontract; and the geographical area in which the subcontract is to be performed.

4. As used in this notice and in the contract resulting from this solicitation, the "covered area" is **Jesse Viertel Memorial Airport, City of Boonville, Cooper County, Missouri.**

Certification of Nonsegregated Facilities – 41 CFR Part 60: A certification of Nonsegregated Facilities must be submitted prior to the award of a federally-assisted construction contract exceeding \$10,000 which is not exempt from the provisions of the Equal Opportunity Clause.

Contractors receiving federally assisted construction contract awards exceeding \$10,000 which are not exempt from the provisions of the Equal Opportunity Clause will be required to provide for the forwarding of the notice to prospective subcontractors for supplies and construction contracts where the subcontracts exceed \$10,000 and are not exempt from the provisions of the Equal Opportunity Clause. The penalty for making false statements in offers is prescribed in 18 U.S.C. 1001.

Disadvantaged Business Enterprise – 49 CFR Part 26: The requirements of 49 CFR Part 26, Regulations of the U.S. Department of Transportation, apply to this contract. It is the policy of MoDOT and the City to practice nondiscrimination based on race, color, sex or national origin in the award or performance of this contract. All firms qualifying under this solicitation are encouraged to submit bids/proposals regardless of their business size or ownership. Awards of this contract will be conditioned upon satisfying the requirements of this section. These requirements apply to all bidders, including those who qualify as a DBE. The owner's award of this contract is condition upon the bidder satisfying the good faith effort requirements of 49 CFR §26.53. A DBE contract goal of 4.0 percent has been established for this contract. The *non-DBE* bidder shall subcontract 4.0 percent of the dollar value of the base bid(s), excluding any additive alternates, to disadvantaged business enterprises (DBE) or make good faith efforts to meet the DBE contract goal. *The bidder and any subcontractor who qualifies as a DBE who subcontracts work to another non-DBE firm must subtract the amount of the non-DBE contract from the total DBE work counted toward the goal, as defined in 49 CFR Part 26.55.* **The DBE Goal is applicable only to the Base Bid portion of the project.**

The apparent successful competitor will be required to submit the following information as a condition of bid responsiveness: (1) the names and addresses of DBE firms that will participate in the contract; (2) a description of the work that each DBE firm will perform; (3) the dollar amount of the participation of each DBE firm participating; (4) written statement from bidder that attests their commitment to use the DBE firm(s) listed under (1) above to meet the owner's project goal; and (5) if the contract goal is not met, evidence of good faith efforts undertaken by the bidder, as described in Appendix A to 49 CFR Part 26.

The apparent successful competitor must provide written confirmation of participation from each of the DBE firms listed in their commitment with the proposal documents as a condition of bid responsiveness.

Davis-Bacon Act, as amended – 29 CFR Part 5: The Contractor is required to comply with wage and labor provisions and to pay minimum wages in accordance with the current schedule of wage rates established by the United States Department of Labor included in the supplementary provisions.

In addition, the contractor will also be required to comply with the wage and labor requirements and pay minimum wages in accordance with the schedule of wage rates established by the Missouri Division of Labor Standards included in the Supplementary Provisions.

The highest rate between the two (Federal and State) for each job classification shall be considered the prevailing wage.

Debarment, Suspension, Ineligibility and Voluntary Exclusion – 49 CFR Part 29: The bidder certifies, by submission of a proposal or acceptance of a contract, that neither it nor its principals are presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded from participation in this transaction by any Federal department or agency.

Foreign Trade Restriction – 49 CFR Part 30: The Bidder and Bidder's subcontractors, by submission of an offer and/or execution of a contract, is required to certify that it:

- a. is not owned or controlled by one or more citizens of a foreign country included in the list of countries that discriminate against U.S. firms published by the Office of the United States Trade Representative (USTR);

b. has not knowingly entered into any contract or subcontract for this project with a person that is a citizen or national of a foreign country on said list, or is owned or controlled directly or indirectly by one or more citizens or nationals of a foreign country on said list; or

c. has not procured any product nor subcontracted for the supply of any product for use on the project that is produced in a foreign country on said list.

Buy American Certificate – Aviation Safety and Capacity Act of 1990: This contract is subject to the “Buy American Preferences” of the Aviation Safety and Capacity Act of 1990. Prospective Bidders are required to certify that steel and manufactured products have been produced in the United States and to clearly identify those items produced or manufactured outside of the United States.

Additional Provisions:

Modification to the project documents may only be made by written addendum by the Owner or Owner’s authorized Representative.

The proposal must be made on the **Official Bid Form provided separate from the bound project manual.** Bidders must supply all required information prior to the time of bid opening.

SECTION 16

ITEM P-501 CRM

CEMENT CONCRETE PAVEMENT

DESCRIPTION

501-1.1 This work shall consist of pavement composed of cement concrete **with reinforcement** constructed on a prepared underlying surface in accordance with these specifications and shall conform to the lines, grades, thickness, and typical cross-sections shown on the plans. The terms cement concrete, hydraulic cement concrete, and concrete are interchangeable in this specification.

MATERIALS

501-2.1 Aggregates.

a. Reactivity. Fine and Coarse aggregates to be used in PCC on this project shall be tested and evaluated by the Contractor for alkali-aggregate reactivity in accordance with both ASTM C1260 and ASTM C1567. Tests must be representative of aggregate sources which will be providing material for production. ASTM C1260 and ASTM C1567 tests may be run concurrently.

- **(1)** Test coarse aggregate and fine aggregate separately, in accordance with ASTM C1260; however, extend the length of the test to 28 days (30 days from casting). Complete the tests within six months of the date of the concrete submittal. If expansion of either the coarse or fine aggregate exceeds 0.10% at 28 days, limit the alkali loading of the concrete to be less than or equal to 3.0 lb per cubic yard, calculated in accordance with Engineering Brief 106.

- **(2)** The combined coarse and fine aggregate shall be tested in accordance with ASTM C1567, modified for combined aggregates, using the proposed mixture design proportions of aggregates, cementitious materials, and/or specific reactivity reducing chemicals. If the expansion does not exceed 0.10% at 28 days, the proposed combined materials will be accepted. If the expansion is greater than 0.10% at 28 days, the aggregates will not be accepted unless adjustments to the combined materials mixture can reduce the expansion to less than 0.10% at 28 days, or new aggregates shall be evaluated and tested.

- **(3)** If lithium nitrate is proposed for use with or without supplementary cementitious materials, the aggregates shall be tested in accordance with Corps of Engineers (COE) Concrete Research Division (CRD) C662 in lieu of ASTM C1567. If lithium nitrate admixture is used, it shall be nominal 30% ±0.5% weight lithium nitrate in water. If the expansion does not exceed 0.10% at 28 days, the proposed combined materials will be accepted. If the expansion is greater than 0.10% at 28 days, the aggregates will not be accepted unless adjustments to the combined materials mixture can reduce the expansion to less than 0.10% at 28 days, or new aggregates shall be evaluated and tested.

b. Fine aggregate. Grading of the fine aggregate, as delivered to the mixer, shall conform to the requirements of ASTM C33 and the parameters identified in the fine aggregate material requirements below. Fine aggregate material requirements and deleterious limits are shown in the table below.

Fine Aggregate Material Requirements		
Soundness of Aggregates by Use of Sodium Sulfate or Magnesium Sulfate	Loss after 5 cycles: 10% maximum using Sodium sulfate - or - 15% maximum using magnesium sulfate	ASTM C88
Sand Equivalent	45 minimum	ASTM D2419
Fineness Modulus (FM)	$2.50 \leq FM \leq 3.40$	ASTM C136
Limits for Deleterious Substances in Fine Aggregate for Concrete		
Clay lumps and friable particles	1.0% maximum	ASTM C142
Coal and lignite	0.5% using a medium with a density of Sp. Gr. of 2.0	ASTM C123
Total Deleterious Material	1.0% maximum	

c. Coarse aggregate. The maximum size coarse aggregate shall be **3/4-inch**.

Aggregates delivered to the mixer shall be clean, hard, uncoated aggregates consisting of crushed stone, crushed or uncrushed gravel, air-cooled iron blast furnace slag, crushed recycled concrete pavement, or a combination. The aggregates shall have no known history of detrimental pavement staining. Steel blast furnace slag shall not be permitted. Coarse aggregate material requirements and deleterious limits are shown in the table below; washing may be required to meet aggregate requirements.

Coarse Aggregate Material Requirements

Material Test	Requirement	Standard
Resistance to Degradation	Loss: 40% maximum	ASTM C131
Soundness of Aggregates by Use of Sodium Sulfate or Magnesium Sulfate	Loss after 5 cycles: 12% maximum using Sodium sulfate - or - 18% maximum using magnesium sulfate	ASTM C88
Flat, Elongated, or Flat and Elongated Particles	8% maximum, by weight, of flat, elongated, or flat and elongated particles at 5:1 for any size group coarser than 3/8 (9.5 mm) sieve ¹	ASTM D4791
Bulk density of slag ²	Weigh not less than 70 pounds per cubic foot (1.12 Mg/cubic meter)	ASTM C29
D-cracking (Freeze-Thaw)³	Durability factor ≥ 95	ASTM C666

¹ A flat particle is one having a ratio of width to thickness greater than five (5); an elongated particle is one having a ratio of length to width greater than five (5).

² Only required if slag is specified.

³ *Coarse aggregate may only be accepted from sources that have a 20-year service history for the same gradation to be supplied with no history of D-Cracking. The aggregate supplier must certify the **20-year service history and include surveillance documentation from a State DOT for at least 20 years. Documentation from the DOT should include an aggregate gradation that matches the gradation used in the project mix design.** Aggregates that do not have a 20-year record of service free from major repairs (less than 5% of slabs replaced) in similar conditions without D-cracking shall not be used unless the material currently being*

produced has a durability factor greater than or equal to 95 per ASTM C666. The Contractor shall submit a current certification and test results to verify the aggregate acceptability. Test results will only be accepted from a State Department of Transportation (DOT) materials laboratory or an accredited laboratory. Certification and test results which are not dated or which are over one (1) year old or which are for different gradations will not be accepted. **State DOT tests are not considered to be a sufficient substitution for ASTM 666.**"

The amount of deleterious material in the coarse aggregate shall not exceed the following limits:

Limits for Deleterious Substances in Coarse Aggregate

Deleterious material	ASTM	Percentage by Mass
Clay Lumps and friable particles	ASTM C142	1.0
Material finer than No. 200 sieve (75 µm)	ASTM C117	1.0 ¹
Lightweight particles	ASTM C123 using a medium with a density of Sp. Gr. of 2.0	0.5
Chert ² (less than 2.40 Sp Gr.)	ASTM C123 using a medium with a density of Sp. Gr. of 2.40)	0.1

¹ The limit for material finer than 75-µm is allowed to be increased to 1.5% for crushed aggregates consisting of dust of fracture that is essentially free from clay or shale. Test results supporting acceptance of increasing limit to 1.5% with statement indicating material is dust of fracture must be submitted with Concrete mix. Acceptable techniques to characterizing these fines include methylene blue adsorption or X-ray diffraction analysis.

² Chert and aggregates with less than 2.4 specific gravity.

³ The limit for chert may be increased to 1.0 percent by mass in areas not subject to severe freeze and thaw.

d. Combined aggregate gradation. This specification is targeted for a combined aggregate gradation developed following the guidance presented in United States Air Force Engineering Technical Letter (ETL) 97-5: Proportioning Concrete Mixtures with Graded Aggregates for Rigid Airfield Pavements. Base the aggregate grading upon a combination of all the aggregates (coarse and fine) to be used for the mixture proportioning. Three aggregate sizes may be required to achieve an optimized combined gradation that will produce a workable concrete mixture for its intended use. Use aggregate gradations that produce concrete mixtures with well-graded or optimized aggregate combinations. The Contractor shall submit complete mixture information necessary to calculate the volumetric components of the mixture. The combined aggregate grading shall meet the following requirements:

- **(1)** The materials selected and the proportions used shall be such that when the Coarseness Factor (CF) and the Workability Factor (WF) are plotted on a diagram as described in paragraph 501-2.1d(4) below, the point thus determined shall fall within the parallelogram described therein.
- **(2)** The CF shall be determined from the following equation:

$$CF = \frac{\text{(cumulative percent retained on the 3/8 in. (9.5 mm) sieve)}(100)}{\text{(cumulative percent retained on the No. 8 (2.36 mm) sieve)}}$$
- **(3)** The WF is defined as the percent passing the No. 8 (2.36 mm) sieve based on the combined gradation. However, WF shall be adjusted, upwards only, by 2.5 percentage points for each 94 pounds (42 kg) of cementitious material per cubic meter yard greater than 564 pounds per cubic yard (335 kg per cubic meter).

- **(4)** A diagram shall be plotted using a rectangular scale with WF on the Y-axis with units from 20 (bottom) to 45 (top), and with CF on the X-axis with units from 80 (left side) to 30 (right side). On this diagram a parallelogram shall be plotted with corners at the following coordinates (CF-75, WF-28), (CF-75, WF-40), (CF-45, WF-32.5), and (CF-45, WF-44.5). If the point determined by the intersection of the computed CF and WF does not fall within the above parallelogram, the grading of each size of aggregate used and the proportions selected shall be changed as necessary. The point determined by the plotting of the CF and WF may be adjusted during production ± 3 WF and ± 5 CF. Adjustments to gradation may not take the point outside of the parallelogram.

e. Contractors combined aggregate gradation. The Contractor shall submit their combined aggregate gradation using the following format:

Contractor’s Combined Aggregate Gradation

Sieve Size	Contractor’s Concrete mix Gradation (Percent passing by weight)
2 inch (50 mm)	*
1-1/2 inch (37.5 mm)	*
1 inch (25.0 mm)	*
3/4 inch (19.0 mm)	*
1/2 inch (12.5 mm)	*
3/8 inch (9.5 mm)	*
No. 4 (4.75 mm)	*
No. 8 (2.36 mm)	*
No. 16 (1.18 mm)	*
No. 30 (600 μ m)	*
No. 50 (300 μ m)	*
No. 100 (150 μ m)	*

501-2.2 Cement. Cement shall conform to the requirements of ASTM C150, Type I, II, or V; ASTM C595, Type IS, IP, IL or IT.

501-2.3 Cementitious materials.

a. Fly ash. Fly ash shall meet the requirements of ASTM C618, with the exception of loss of ignition, where the maximum shall be less than 6%. Fly ash shall have a Calcium Oxide (CaO) content of less than 15% and a total alkali content less than 3% per ASTM C311. The Contractor shall furnish the previous three most recent, consecutive ASTM C618 reports for each source of fly ash proposed in the concrete mix, and shall furnish each additional report as they become available during the project. The reports can be used for acceptance or the material may be tested independently by the Resident Project Representative (RPR).

b. Slag cement (ground granulated blast furnace (GGBF)). Slag cement shall conform to ASTM C989, Grade 100 or Grade 120. Slag cement shall be used only at a rate between 25% and 55% of the total cementitious material by mass.

c. Raw or calcined natural pozzolan. Natural pozzolan shall be raw or calcined and conform to ASTM C618, Class N, including the optional requirements for uniformity and effectiveness in controlling

Alkali-Silica reaction and shall have a loss on ignition not exceeding 6%. Class N pozzolan for use in mitigating Alkali-Silica Reactivity shall have a total available alkali content less than 3%.

d. Ultrafine fly ash and ultrafine pozzolan. UltraFine Fly Ash (UFFA) and UltraFine Pozzolan (UFP) shall conform to ASTM C618, Class F or N, and the following additional requirements:

(1) The strength activity index at 28 days of age shall be at least 95% of the control specimens.

(2) The average particle size shall not exceed 6 microns.

501-2.4 Joint seal. The joint seal for the joints in the concrete pavement shall meet the requirements of **Item P-605** and shall be of the type specified in the plans.

501-2.5 Isolation joint filler. Premolded joint filler for isolation joints shall conform to the requirements of ASTM D1751 or ASTM D1752 and shall be where shown on the plans. The filler for each joint shall be furnished in a single piece for the full depth and width required for the joint, unless otherwise specified by the RPR. When the use of more than one piece is required for a joint, the abutting ends shall be fastened securely and held accurately to shape by stapling or other positive fastening means satisfactory to the RPR.

501-2.6 Steel reinforcement. Reinforcing shall consist of **welded wire fabric** conforming to the requirements of **ASTM A1064**.

501-2.7 Dowel and tie bars. Dowel bars shall be plain steel bars conforming to ASTM A615 and shall be free from burring or other deformation restricting slippage in the concrete.

a. Dowel Bars. Before delivery to the construction site each dowel bar shall be epoxy coated per ASTM A1078, Type 1, with a coating thickness after curing greater than 10 mils. Patched ends are not required for Type 1 coated dowels. The dowels shall be coated with a bond-breaker recommended by the manufacturer. Dowel sleeves or inserts are not permitted. Grout retention rings shall be fully circular metal or plastic devices capable of supporting the dowel until the grout hardens.

b. Tie Bars. Tie bars shall be deformed steel bars and conform to the requirements of ASTM A615. Tie bars designated as Grade 60 in ASTM A615 or ASTM A706 shall be used for construction requiring bent bars.

501-2.8 Water. Water used in mixing or curing shall be potable. If water is taken from other sources considered non-potable, it shall meet the requirements of ASTM C1602.

501-2.9 Material for curing concrete. Curing materials shall conform to one of the following specifications:

a. Liquid membrane-forming compounds for curing concrete shall conform to the requirements of ASTM C309, Type 2, Class A, or Class B.

b. White polyethylene film for curing concrete shall conform to the requirements of ASTM C171.

c. White burlap-polyethylene sheeting for curing concrete shall conform to the requirements of ASTM C171.

d. Waterproof paper for curing concrete shall conform to the requirements of ASTM C171.

501-2.10 Admixtures. Admixtures shall conform to the following specifications:

a. Air-entraining admixtures. Air-entraining admixtures shall meet the requirements of ASTM C260 and shall consistently entrain the air content in the specified ranges under field conditions. The air-entraining agent and any water reducer admixture shall be compatible.

b. Water-reducing admixtures. Water-reducing admixture shall meet the requirements of ASTM C494, Type A, B, or D.

c. Other admixtures. The use of set retarding and set-accelerating admixtures shall be approved by the RPR prior to developing the concrete mix. Retarding admixtures shall meet the requirements of ASTM C494, Type A, B, or D and set-accelerating admixtures shall meet the requirements of ASTM C494, Type C. Calcium chloride and admixtures containing calcium chloride shall not be used.

d. Lithium Nitrate. The lithium admixture shall be a nominal 30% aqueous solution of Lithium Nitrate, with a density of 10 pounds/gallon (1.2 kg/L), and shall have the approximate chemical form as shown below:

Lithium Admixture

Constituent	Limit (Percent by Mass)
LiNO ₃ (Lithium Nitrate)	30 ±0.5
SO ₄ (Sulfate Ion)	0.1 (max)
Cl (Chloride Ion)	0.2 (max)
Na (Sodium Ion)	0.1 (max)
K (Potassium Ion)	0.1 (max)

The lithium nitrate admixture dispensing and mixing operations shall be verified and certified by the lithium manufacturer's representative.

501-2.11 Epoxy-resin. All epoxy-resin materials shall be two-component materials conforming to the requirements of ASTM C881, Class as appropriate for each application temperature to be encountered, except that in addition, the materials shall meet the following requirements:

a. Material for use for embedding dowels and anchor bolts shall be Type IV, Grade 3.

b. Material for use as patching materials for complete filling of spalls and other voids and for use in preparing epoxy resin mortar shall be Type III, Grade as approved.

c. Material for use for injecting cracks shall be Type IV, Grade 1.

d. Material for bonding freshly mixed Portland cement concrete or mortar or freshly mixed epoxy resin concrete or mortar to hardened concrete shall be Type V, Grade as approved.

501-2.12 Bond Breaker. Not required.

CONCRETE MIX

501-3.1. General. No concrete shall be placed until an acceptable concrete mix has been submitted to the RPR for review and the RPR has taken appropriate action. The RPR's review shall not relieve the Contractor of the responsibility to select and proportion the materials to comply with this section.

501-3.2 Concrete Mix Laboratory. The laboratory used to develop the concrete mix shall be accredited in accordance with ASTM C1077. The laboratory accreditation must be current and listed on the accrediting authority's website. All test methods required for developing the concrete mix must be included in the lab accreditation. A copy of the laboratory's current accreditation and accredited test methods shall be submitted to the RPR prior to start of construction.

501-3.3 Concrete Mix Proportions. Develop the mix using the procedures contained in Portland Cement Association (PCA) publication, "Design and Control of Concrete Mixtures." Concrete shall be

proportioned to achieve a ~~28-day flexural strength~~ *(56-day flexural strength for ASTM C595, Type II)* that meets or exceeds the acceptance criteria contained in paragraph 501-6.6 for a flexural strength of ~~620 psi per ASTM C78~~. *28-day compressive strength that meets or exceeds the acceptance criteria for a compressive strength of 4,400 psi per ASTM C39. (Revised per Addendum No. 1)*

The minimum cementitious material shall be adequate to ensure a workable, durable mix. The minimum cementitious material (cement plus fly ash, or slag cement) shall be **564** pounds per cubic yard. The ratio of water to cementitious material, including free surface moisture on the aggregates but not including moisture absorbed by the aggregates shall be between 0.38 – 0.45 by weight.

Flexural strength test specimens shall be prepared in accordance with ASTM C192 and tested in accordance with ASTM C78. At the start of the project, the Contractor shall determine an allowable slump as determined by ASTM C143 not to exceed 2 inches (50 mm) for slip-form placement. For fixed-form placement, the slump shall not exceed 3 inches (75 mm). For hand placement, the slump shall not exceed 4 inches (100 mm).

The results of the concrete mix shall include a statement giving the maximum nominal coarse aggregate size and the weights and volumes of each ingredient proportioned on a one cubic yard (meter) basis. Aggregate quantities shall be based on the mass in a saturated surface dry condition.

If a change in source(s) is made, or admixtures added or deleted from the mix, a new concrete mix must be submitted to the RPR for approval.

The RPR may request samples at any time for testing, prior to and during production, to verify the quality of the materials and to ensure conformance with the applicable specifications.

501-3.4 Concrete Mix submittal. The concrete mix shall be submitted to the RPR at least **30** days prior to the start of operations. The submitted concrete mix shall not be more than 180 days old and must use the materials to be used for production for the project. Production shall not begin until the concrete mix is approved in writing by the RPR.

Each of the submitted concrete mixes (i.e, slip form, side form machine finish and side form hand finish) shall be stamped or sealed by the responsible professional Engineer of the laboratory and shall include the following items and quantities as a minimum:

- Certified material test reports for aggregate in accordance with paragraph 501-2.1. Certified reports must include all tests required; reporting each test, test method, test result, and requirement specified (criteria).
- Combined aggregate gradations and analysis; and including plots of the fine aggregate fineness modulus.
- Reactivity Test Results.
- Coarse aggregate quality test results, including deleterious materials.
- Fine aggregate quality test results, including deleterious materials.
- Mill certificates for cement and supplemental cementitious materials.
- Certified test results for all admixtures, including Lithium Nitrate if applicable.
- Specified flexural strength, slump, and air content.
- Recommended proportions/volumes for proposed mixture and trial water-cementitious materials ratio, including actual slump and air content.
- Flexural and compressive strength summaries and plots, including all individual beam and cylinder breaks.

- Correlation ratios for acceptance testing and Contractor QC testing, when applicable.
- Historical record of test results documenting production standard deviation, when applicable.
- Alkali loading contributed by the cement per cubic yard, calculated in accordance with Engineering Brief 106.

501-3.5 Cementitious materials.

a. Fly ash. When fly ash is used as a partial replacement for cement, the replacement rate shall be determined from laboratory trial mixes, and shall be between 20 and 30% by weight of the total cementitious material. If fly ash is used in conjunction with slag cement the maximum replacement rate shall not exceed 10% by weight of total cementitious material.

b. Slag cement (ground granulated blast furnace (GGBF)). Slag cement may be used. The slag cement, or slag cement plus fly ash if both are used, may constitute between 25 to 55% of the total cementitious material by weight.

c. Raw or calcined natural pozzolan. Natural pozzolan may be used in the concrete mix. When pozzolan is used as a partial replacement for cement, the replacement rate shall be determined from laboratory trial mixes, and shall be between 20 and 30% by weight of the total cementitious material. If pozzolan is used in conjunction with slag cement the maximum replacement rate shall not exceed 10% by weight of total cementitious material.

d. Ultrafine fly ash (UFFA) and ultrafine pozzolan (UFP). UFFA and UFP may be used in the concrete mix with the RPR's approval. When UFFA and UFP is used as a partial replacement for cement, the replacement rate shall be determined from laboratory trial mixes, and shall be between 7% and 16% by weight of the total cementitious material.

501-3.6 Admixtures.

a. Air-entraining admixtures. Air-entraining admixture are to be added in such a manner that will ensure uniform distribution of the agent throughout the batch. The air content of freshly mixed air-entrained concrete shall be based upon trial mixes with the materials to be used in the work adjusted to produce concrete of the required plasticity and workability. The percentage of air in the mix shall be 5.0%. Air content shall be determined by testing in accordance with ASTM C231 for gravel and stone coarse aggregate and ASTM C173 for slag and other highly porous coarse aggregate.

b. Water-reducing admixtures. Water-reducing admixtures shall be added to the mix in the manner recommended by the manufacturer and in the amount necessary to comply with the specification requirements. Tests shall be conducted with the materials to be used in the work, in accordance with ASTM C494.

c. Other admixtures. Set controlling, and other approved admixtures shall be added to the mix in the manner recommended by the manufacturer and in the amount necessary to comply with the specification requirements. Tests shall be conducted with the materials to be used in the work, in accordance with ASTM C494.

d. Lithium nitrate. Lithium nitrate shall be added to the mix in the manner recommended by the manufacturer and in the amount necessary to comply with the specification requirements in accordance with paragraph 501-2.10d.

CONSTRUCTION METHODS

501-4.1 Control Strip. The control strip(s) shall be to the next planned joint after the initial 250 feet (75 m) of each type of pavement construction (slip-form pilot lane, slip-form fill-in lane, or fixed form). The

Contractor shall demonstrate, in the presence of the RPR, that the materials, concrete mix, equipment, construction processes, and quality control processes meet the requirements of the specifications. The concrete mixture shall be extruded from the paver meeting the edge slump tolerance and with little or no finishing. Pilot, fill-in, and fixed-form control strips will be accepted separately. Minor adjustments to the mix design may be required to place an acceptable control strip. The production mix will be the adjusted mix design used to place the acceptable control strip. Upon acceptance of the control strip by the RPR, the Contractor must use the same equipment, materials, and construction methods for the remainder of concrete paving. Any adjustments to processes or materials must be approved in advance by the RPR. The acceptable control strip shall be paid for in accordance with paragraph 501-6.6.

501-4.2 Equipment. The Contractor is responsible for the proper operation and maintenance of all equipment necessary for handling materials and performing all parts of the work to meet this specification.

a. Plant and equipment. The plant and mixing equipment shall conform to the requirements of ASTM C94 and/or ASTM C685. Each truck mixer shall have attached in a prominent place a manufacturer's nameplate showing the capacity of the drum in terms of volume of mixed concrete and the speed of rotation of the mixing drum or blades. The truck mixers shall be examined daily for changes in condition due to accumulation of hard concrete or mortar or wear of blades. The pickup and throwover blades shall be replaced when they have worn down 3/4 inch (19 mm) or more. The Contractor shall have a copy of the manufacturer's design on hand showing dimensions and arrangement of blades in reference to original height and depth.

Equipment for transferring and spreading concrete from the transporting equipment to the paving lane in front of the finishing equipment shall be provided. The equipment shall be specially manufactured, self-propelled transfer equipment which will accept the concrete outside the paving lane and will spread it evenly across the paving lane in front of the paver and strike off the surface evenly to a depth which permits the paver to operate efficiently.

b. Finishing equipment.

- **(1) Slip-form.** The standard method of constructing concrete pavements shall be with an approved slip-form paving equipment designed and operated to spread, consolidate, screed, and finish the freshly placed concrete in one complete pass of the machine so that the end result is a dense and homogeneous pavement which is achieved with a minimum of hand finishing. The paver-finisher shall be a heavy duty, self-propelled machine designed specifically for paving and finishing high quality concrete pavements.

- **(2) Fixed-form.** On projects requiring less than **5,000 cubic yards** of concrete pavement or irregular areas at locations inaccessible to slip-form paving equipment, concrete pavement may be placed with equipment specifically designed for placement and finishing using stationary side forms. Methods and equipment shall be reviewed and accepted by the RPR. Hand screeding and float finishing may only be used on small irregular areas as allowed by the RPR.

c. Vibrators. Vibrator shall be the internal type. The rate of vibration of each vibrating unit shall be sufficient to consolidate the pavement without segregation or voids. The number, spacing, and frequency shall be as necessary to provide a dense and homogeneous pavement and meet the recommendations of American Concrete Institute (ACI) 309R, Guide for Consolidation of Concrete. Adequate power to operate all vibrators shall be available on the paver. The vibrators shall be automatically controlled so that they shall be stopped as forward motion ceases. The Contractor shall provide an electronic or mechanical means to monitor vibrator status. The checks on vibrator status shall occur a minimum of two times per day or when requested by the RPR.

Hand held vibrators may only be used in irregular areas and shall meet the recommendations of ACI 309R, Guide for Consolidation of Concrete.

d. Concrete saws. The Contractor shall provide sawing equipment adequate in number of units and power to complete the sawing to the required dimensions. The Contractor shall provide at least one standby saw in good working order and a supply of saw blades at the site of the work at all times during sawing operations.

e. Fixed forms. Straight side fixed forms shall be made of steel and shall be furnished in sections not less than 10 feet (3 m) in length. Forms shall be provided with adequate devices for secure settings so that when in place they will withstand, without visible spring or settlement, the impact and vibration of the consolidating and finishing equipment. Forms with battered top surfaces and bent, twisted or broken forms shall not be used. Built-up forms shall not be used, except as approved by the RPR. The top face of the form shall not vary from a true plane more than 1/8 inch (3 mm) in 10 feet (3 m), and the upstanding leg shall not vary more than 1/4 inch (6 mm). The forms shall contain provisions for locking the ends of abutting sections together tightly for secure setting. Wood forms may be used under special conditions, when approved by the RPR. The forms shall extend the full depth of the pavement section.

501-4.3 Form setting. Forms shall be set to line and grade as shown on the plans, sufficiently in advance of the concrete placement, to ensure continuous paving operation. Forms shall be set to withstand, without visible spring or settlement, the impact and vibration of the consolidating and finishing equipment. Forms shall be cleaned and oiled prior to the concrete placement.

501-4.4 Base surface preparation prior to placement. Any damage to the prepared base, subbase, and subgrade shall be corrected full depth by the Contractor prior to concrete placement. The underlying surface shall be entirely free of frost when concrete is placed. The prepared grade shall be moistened with water, without saturating, immediately ahead of concrete placement to prevent rapid loss of moisture from concrete.

501-4.5 Handling, measuring, and batching material. Aggregate stockpiles shall be constructed and managed in such a manner that prevents segregation and intermixing of deleterious materials. Aggregates from different sources shall be stockpiled, weighed and batched separately at the concrete batch plant. Aggregates that have become segregated or mixed with earth or foreign material shall not be used. All aggregates produced or handled by hydraulic methods, and washed aggregates, shall be stockpiled or binned for draining at least 12 hours before being batched. Store and maintain all aggregates at a uniform moisture content prior to use. A continuous supply of materials shall be provided to the work to ensure continuous placement.

501-4.6 Mixing concrete. The concrete may be mixed at the work site, in a central mix plant or in truck mixers. The mixer shall be of an approved type and capacity. Mixing time shall be measured from the time all materials are placed into the drum until the drum is emptied into the truck. All concrete shall be mixed and delivered to the site in accordance with the requirements of ASTM C94 or ASTM C685.

Mixed concrete from the central mixing plant shall be transported in truck mixers, truck agitators, or non-agitating trucks. The elapsed time from the addition of cementitious material to the mix until the concrete is discharged from the truck should not exceed 30 minutes when the concrete is hauled in non-agitating trucks, nor 90 minutes when the concrete is hauled in truck mixers or truck agitators. In no case shall the temperature of the concrete when placed exceed 90°F (32°C). Retempering concrete by adding water or by other means will not be permitted. With transit mixers additional water may be added to the batch materials and additional mixing performed to increase the slump to meet the specified requirements provided the addition of water is performed within 45 minutes after the initial mixing operations and provided the water/cementitious ratio specified is not exceeded.

501-4.7 Weather Limitations on mixing and placing. No concrete shall be mixed, placed, or finished when the natural light is insufficient, unless an adequate and approved artificial lighting system is operated.

a. Cold weather. Unless authorized in writing by the RPR, mixing and concreting operations shall be discontinued when a descending air temperature in the shade and away from artificial heat reaches 40°F (4°C) and shall not be resumed until an ascending air temperature in the shade and away from artificial heat reaches 35°F (2°C).

The aggregate shall be free of ice, snow, and frozen lumps before entering the mixer. The temperature of the mixed concrete shall not be less than 50°F (10°C) at the time of placement. Concrete shall not be placed on frozen material nor shall frozen aggregates be used in the concrete.

When concreting is authorized during cold weather, water and/or the aggregates may be heated to not more than 150°F (66°C). The apparatus used shall heat the mass uniformly and shall be arranged to preclude the possible occurrence of overheated areas which might be detrimental to the materials.

Curing during cold weather shall be in accordance with paragraph 501-4.13d.

b. Hot weather. During periods of hot weather when the maximum daily air temperature exceeds 85°F (30°C), the following precautions shall be taken.

The forms and/or the underlying surface shall be sprinkled with water immediately before placing the concrete. The concrete shall be placed at the coolest temperature practicable, and in no case shall the temperature of the concrete when placed exceed 90°F (32°C). The aggregates and/or mixing water shall be cooled as necessary to maintain the concrete temperature at or not more than the specified maximum.

The concrete placement shall be protected from exceeding an evaporation rate of 0.2 psf (0.98 kg/m² per hour) per hour. When conditions are such that problems with plastic cracking can be expected, and particularly if any plastic cracking begins to occur, the Contractor shall immediately take such additional measures as necessary to protect the concrete surface. If the Contractor's measures are not effective in preventing plastic cracking, paving operations shall be immediately stopped.

Curing during hot weather shall be in accordance with paragraph 501-4.13e.

c. Temperature management program. Prior to the start of paving operation for each day of paving, the Contractor shall provide the RPR with a Temperature Management Program for the concrete to be placed to assure that uncontrolled cracking is avoided. (Federal Highway Administration HIPERPAV 3 is one example of a temperature management program.) As a minimum, the program shall address the following items:

- (1) Anticipated tensile strains in the fresh concrete as related to heating and cooling of the concrete material.
- (2) Anticipated weather conditions such as ambient temperatures, wind velocity, and relative humidity; and anticipated evaporation rate using Figure 19-9, PCA, Design and Control of Concrete Mixtures.
- (3) Anticipated timing of initial sawing of joint.
- (4) Anticipated number and type of saws to be used.

d. Rain. The Contractor shall have available materials for the protection of the concrete during inclement weather. Such protective materials shall consist of rolled polyethylene sheeting at least 4 mils (0.1 mm) thick of sufficient length and width to cover the plastic concrete slab and any edges. The sheeting may be mounted on either the paver or a separate movable bridge from which it can be unrolled without dragging over the plastic concrete surface. When rain appears imminent, all paving operations shall stop and all available personnel shall begin covering the surface of the unhardened concrete with the protective covering.

501-4.8 Concrete Placement. At any point in concrete conveyance, the free vertical drop of the concrete from one point to another or to the underlying surface shall not exceed 3 feet (1 m). The finished concrete product must be dense and homogeneous, without segregation and conforming to the standards

in this specification. Backhoes and grading equipment shall not be used to distribute the concrete in front of the paver. Front end loaders will not be used. All concrete shall be consolidated without voids or segregation, including under and around all load-transfer devices, joint assembly units, and other features embedded in the pavement. *For public and construction traffic after placement and before acceptance see 501-4.17 Protection of pavement and 501-4.18 Opening to construction traffic.*

The Contractor shall have available materials for the protection of the concrete during cold, hot and/or inclement weather in accordance with paragraph 501-4.7.

a. Slip-form construction. The concrete shall be distributed uniformly into final position by a self-propelled slip-form paver without delay. The alignment and elevation of the paver shall be regulated from outside reference lines established for this purpose. The paver shall vibrate the concrete for the full width and depth of the strip of pavement being placed and the vibration shall be adequate to provide a consistency of concrete that will stand normal to the surface with sharp well-defined edges. The sliding forms shall be rigidly held together laterally to prevent spreading of the forms. The plastic concrete shall be effectively consolidated by internal vibration with transverse vibrating units for the full width of the pavement and/or a series of equally placed longitudinal vibrating units. The space from the outer edge of the pavement to longitudinal unit shall not exceed 9 inches (23 cm) for slipform and at the end of the dowels for the fill-in lanes. The spacing of internal units shall be uniform and shall not exceed 18 inches (0.5 m).

The term internal vibration means vibrating units located within the specified thickness of pavement section.

The rate of vibration of each vibrating unit shall be sufficient to consolidate the pavement without, segregation, voids, or vibrator trails and the amplitude of vibration shall be sufficient to be perceptible on the surface of the concrete along the entire length of the vibrating unit and for a distance of at least one foot (30 cm). The frequency of vibration or amplitude should be adjusted proportionately with the rate of travel to result in a uniform density and air content. The paving machine shall be equipped with a tachometer or other suitable device for measuring and indicating the actual frequency of vibrations.

The concrete shall be held at a uniform consistency. The slip-form paver shall be operated with as nearly a continuous forward movement as possible and all operations of mixing, delivering, and spreading concrete shall be coordinated to provide uniform progress with stopping and starting of the paver held to a minimum. If for any reason, it is necessary to stop the forward movement of the paver, the vibratory and tamping elements shall also be stopped immediately. No tractive force shall be applied to the machine, except that which is controlled from the machine.

When concrete is being placed adjacent to an existing pavement, that part of the equipment which is supported on the existing pavement shall be equipped with protective pads on crawler tracks or rubber-tired wheels on which the bearing surface is offset to run a sufficient distance from the edge of the pavement to avoid breaking the pavement edge.

Not more than 15% of the total free edge of each 500-foot (150 m) segment of pavement, or fraction thereof, shall have an edge slump exceeding 1/4 inch (6 mm), and none of the free edge of the pavement shall have an edge slump exceeding 3/8 inch (9 mm). (The total free edge of 500 feet (150 m) of pavement will be considered the cumulative total linear measurement of pavement edge originally constructed as nonadjacent to any existing pavement; that is, 500 feet (150 m) of paving lane originally constructed as a separate lane will have 1,000 feet (300 m) of free edge, 500 feet (150 m) of fill-in lane will have no free edge, etc.). The area affected by the downward movement of the concrete along the pavement edge shall be limited to not more than 18 inches (0.5 m) from the edge.

When excessive edge slump cannot be corrected before the concrete has hardened, the area with excessive edge slump will be removed the full width of the slip form lane and replaced at the expense of the Contractor as directed by the RPR.

b. Fixed-form construction. Forms shall be drilled in advance of being placed to line and grade to accommodate tie bars / dowel bars where these are specified.

Immediately in advance of placing concrete and after all subbase operations are completed, side forms shall be trued and maintained to the required line and grade for a distance sufficient to prevent delay in placing.

Side forms shall remain in place at least 12 hours after the concrete has been placed, and in all cases until the edge of the pavement no longer requires the protection of the forms. Curing compound shall be applied to the concrete immediately after the forms have been removed.

Side forms shall be thoroughly cleaned and coated with a release agent each time they are used and before concrete is placed against them.

Concrete shall be spread, screed, shaped and consolidated by one or more self-propelled machines. These machines shall uniformly distribute and consolidate concrete without segregation so that the completed pavement will conform to the required cross-section with a minimum of handwork.

The number and capacity of machines furnished shall be adequate to perform the work required at a rate equal to that of concrete delivery. The equipment must be specifically designed for placement and finishing using stationary side forms. Methods and equipment shall be reviewed and accepted by the RPR.

Concrete for the full paving width shall be effectively consolidated by internal vibrators. The rate of vibration of each vibrating unit shall be sufficient to consolidate the pavement without segregation, voids, or leaving vibrator trails.

Power to vibrators shall be connected so that vibration ceases when forward or backward motion of the machine is stopped.

c. Consolidation. Concrete shall be consolidated with the specified type of lane-spanning, gang-mounted, mechanical, immersion type vibrating equipment mounted in front of the paver, supplemented, in rare instances as specified, by hand-operated vibrators. The vibrators shall be inserted into the concrete to a depth that will provide the best full-depth consolidation but not closer to the underlying material than 2 inches (50 mm). Vibrators shall not be used to transport or spread the concrete. For each paving train, at least one additional vibrator spud, or sufficient parts for rapid replacement and repair of vibrators shall be maintained at the paving site at all times. Any evidence of inadequate consolidation (honeycomb along the edges, large air pockets, or any other evidence) or over-consolidation (vibrator trails, segregation, or any other evidence) shall require the immediate stopping of the paving operation and adjustment of the equipment or procedures as approved by the RPR.

If a lack of consolidation of the hardened concrete is suspected by the RPR, referee testing may be required. Referee testing of hardened concrete will be performed by the RPR by cutting cores from the finished pavement after a minimum of 24 hours curing. The RPR shall visually examine the cores for evidence of lack of consolidation. Density determinations will be made by the RPR based on the water content of the core as taken. ASTM C642 shall be used for the determination of core density in the saturated-surface dry condition. When required, referee cores will be taken at the minimum rate of one for each 500 cubic yards (382 m²) of pavement, or fraction. The Contractor shall be responsible for all referee testing cost if they fail to meet the required density.

The average density of the cores shall be at least 97% of the original concrete mix density, with no cores having a density of less than 96% of the original concrete mix density. Failure to meet the referee tests will be considered evidence that the minimum requirements for vibration are inadequate for the job

conditions. Additional vibrating units or other means of increasing the effect of vibration shall be employed so that the density of the hardened concrete conforms to the above requirements.

501-4.9 Strike-off of concrete and placement of reinforcement. Following the placing of the concrete, it shall be struck off to conform to the cross-section shown on the plans and to an elevation that when the concrete is properly consolidated and finished, the surface of the pavement shall be at the elevation shown on the plans. When reinforced concrete pavement is placed in two layers, the bottom layer shall be struck off to such length and depth that the sheet of reinforcing steel fabric or bar mat may be laid full length on the concrete in its final position without further manipulation. The reinforcement shall then be placed directly upon the concrete, after which the top layer of the concrete shall be placed, struck off, and screed. If any portion of the bottom layer of concrete has been placed more than 30 minutes without being covered with the top layer or if initial set has taken place, it shall be removed and replaced with freshly mixed concrete at the Contractor's expense. When reinforced concrete is placed in one layer, the reinforcement may be positioned in advance of concrete placement or it may be placed in plastic concrete by mechanical or vibratory means after spreading.

Reinforcing steel, at the time concrete is placed, shall be free of mud, oil, or other organic matter that may adversely affect or reduce bond. Reinforcing steel with rust, mill scale or a combination of both will be considered satisfactory, provided the minimum dimensions, weight, and tensile properties of a hand wire-brushed test specimen are not less than the applicable ASTM specification requirements.

501-4.10 Joints. Joints shall be constructed as shown on the plans and in accordance with these requirements. All joints shall be constructed with their faces perpendicular to the surface of the pavement and finished or edged as shown on the plans. Joints shall not vary more than 1/2-inch (12 mm) from their designated position and shall be true to line with not more than 1/4-inch (6 mm) variation in 10 feet (3 m). The surface across the joints shall be tested with a 12-foot (3 m) straightedge as the joints are finished and any irregularities in excess of 1/4 inch (6 mm) shall be corrected before the concrete has hardened. All joints shall be so prepared, finished, or cut to provide a groove of uniform width and depth as shown on the plans.

a. Construction. Longitudinal construction joints shall be slip-formed or formed against side forms as shown in the plans.

Transverse construction joints shall be installed at the end of each day's placing operations and at any other points within a paving lane when concrete placement is interrupted for more than 30 minutes or it appears that the concrete will obtain its initial set before fresh concrete arrives. The installation of the joint shall be located at a planned contraction or expansion joint. If placing of the concrete is stopped, the Contractor shall remove the excess concrete back to the previous planned joint.

b. Contraction. Contraction joints shall be installed at the locations and spacing as shown on the plans. Contraction joints shall be installed to the dimensions required by forming a groove or cleft in the top of the slab while the concrete is still plastic or by sawing a groove into the concrete surface after the concrete has hardened. When the groove is formed in plastic concrete the sides of the grooves shall be finished even and smooth with an edging tool. If an insert material is used, the installation and edge finish shall be according to the manufacturer's instructions. The groove shall be finished or cut clean so that spalling will be avoided at intersections with other joints. Grooving or sawing shall produce a slot at least 1/8 inch (3 mm) wide and to the depth shown on the plans.

c. Isolation (expansion). Isolation joints shall be installed as shown on the plans. The premolded filler of the thickness as shown on the plans, shall extend for the full depth and width of the slab at the joint. The filler shall be fastened uniformly along the hardened joint face with no buckling or debris between the filler and the concrete interface, including a temporary filler for the sealant reservoir at the top of the slab. The edges of the joint shall be finished and tooled while the concrete is still plastic

d. Dowels and Tie Bars for Joints

(1) Tie bars. Tie bars shall consist of deformed bars installed in joints as shown on the plans. Tie bars shall be placed at right angles to the centerline of the concrete slab and shall be spaced at intervals shown on the plans. They shall be held in position parallel to the pavement surface and in the middle of the slab depth and within the tolerances in paragraph 501-4.10(f). When tie bars extend into an unpaved lane, they may be bent against the form at longitudinal construction joints, unless threaded bolt or other assembled tie bars are specified. Tie bars shall not be painted, greased, or enclosed in sleeves. When slip-form operations call for tie bars, two-piece hook bolts can be installed.

- **(2) Dowel bars.** Dowel bars shall be placed across joints in the proper horizontal and vertical alignment as shown on the plans. The dowels shall be coated with a bond-breaker or other lubricant recommended by the manufacturer and approved by the RPR. Dowel bars at longitudinal construction joints shall be bonded in drilled holes.

- **(3) Placing dowels and tie bars.** Horizontal spacing of dowels shall be within a tolerance of $\pm 3/4$ inch (19 mm). The vertical location on the face of the slab shall be within a tolerance of $\pm 1/2$ inch (12 mm). The method used to install dowels shall ensure that the horizontal and vertical alignment will not be greater than $1/4$ inch per foot (6 mm per 0.3 m), except for those across the crown or other grade change joints. Dowels across crowns and other joints at grade changes shall be measured to a level surface. Horizontal alignment shall be checked perpendicular to the joint edge. The portion of each dowel intended to move within the concrete or expansion cap shall be wiped clean and coated with a thin, even film of lubricating oil or light grease before the concrete is placed. Dowels shall be installed as specified in the following subparagraphs.

(a) Contraction joints. Dowels and tie bars in longitudinal and transverse contraction joints within the paving lane shall be held securely in place by means of rigid metal frames or basket assemblies of an approved type. The basket assemblies shall be held securely in the proper location by means of suitable pins or anchors. Do not cut or crimp the dowel basket tie wires.

At the Contractor's option, dowels and tie bars in contraction joints may be installed by insertion into the plastic concrete using approved equipment and procedures per the paver manufacturer's design. Approval of installation methods will be based on the results of the control strip showing that the dowels and tie bars are installed within specified tolerances as verified by cores or non-destructive rebar location devices approved by the RPR.

(b) Construction joints. Install dowels and tie bars by the cast-in-place or the drill-and-dowel method. Installation by removing and replacing in preformed holes will not be permitted. Dowels and tie bars shall be prepared and placed across joints where indicated, correctly aligned, and securely held in the proper horizontal and vertical position during placing and finishing operations, by means of devices fastened to the forms.

(c) Joints in hardened concrete. Install dowels in hardened concrete by bonding the dowels into holes drilled into the concrete. The concrete shall have cured for seven (7) days or reached a minimum **flexural strength of 450 psi** before drilling begins. Holes $1/8$ inch (3 mm) greater in diameter than the dowels shall be drilled into the hardened concrete using rotary-core drills. Rotary-percussion drills may be used, provided that excessive spalling does not occur. Spalling beyond the limits of the grout retention ring will require modification of the equipment and operation. Depth of dowel hole shall be within a tolerance of $\pm 1/2$ inch (12 mm) of the dimension shown on the drawings. On completion of the drilling operation, the dowel hole shall be blown out with oil-free, compressed air. Dowels shall be bonded in the drilled holes using epoxy resin. Epoxy resin shall be injected at the back of the hole before installing the dowel and extruded to the collar during insertion of the dowel so as to completely fill the void around the dowel. Application by buttering the dowel will not be permitted. The dowels shall be held in alignment at the collar of the hole by means of a suitable metal or plastic grout retention ring fitted around the dowel.

e. Sawing of joints. Sawing shall commence, without regard to day or night, as soon as the concrete has hardened sufficiently to permit cutting without chipping, spalling, or tearing and before uncontrolled shrinkage cracking of the pavement occurs and shall continue without interruption until all joints have been sawn. All slurry and debris produced in the sawing of joints shall be removed by vacuuming and washing. Curing compound or system shall be reapplied in the initial saw-cut and maintained for the remaining cure period.

Joints shall be cut in locations as shown on the plans. The initial joint cut shall be a minimum 1/8 inch (3 mm) wide and to the depth shown on the plans. Prior to placement of joint sealant or seals, the top of the joint shall be widened by sawing as shown on the plans.

501-4.11 Finishing. Finishing operations shall be a continuing part of placing operations starting immediately behind the strike-off of the paver. Initial finishing shall be provided by the transverse screed or extrusion plate. The sequence of operations shall be transverse finishing, longitudinal machine floating if used, straightedge finishing, edging of joints, and then texturing. Finishing shall be by the machine method. The hand method shall be used only on isolated areas of odd slab widths or shapes and in the event of a breakdown of the mechanical finishing equipment. Supplemental hand finishing for machine finished pavement shall be kept to an absolute minimum. Any machine finishing operation which requires appreciable hand finishing, other than a moderate amount of straightedge finishing, shall be immediately stopped and proper adjustments made or the equipment replaced. Equipment, mixture, and/or procedures which produce more than 1/4 inch (6 mm) of mortar-rich surface shall be immediately modified as necessary to eliminate this condition or operations shall cease. Compensation shall be made for surging behind the screeds or extrusion plate and settlement during hardening and care shall be taken to ensure that paving and finishing machines are properly adjusted so that the finished surface of the concrete (not just the cutting edges of the screeds) will be at the required line and grade. Finishing equipment and tools shall be maintained clean and in an approved condition. At no time shall water be added to the surface of the slab with the finishing equipment or tools, or in any other way. Fog (mist) sprays or other surface applied finishing aids specified to prevent plastic shrinkage cracking, approved by the RPR, may be used in accordance with the manufacturers requirements.

a. Machine finishing with slipform pavers. The slipform paver shall be operated so that only a very minimum of additional finishing work is required to produce pavement surfaces and edges meeting the specified tolerances. Any equipment or procedure that fails to meet these specified requirements shall immediately be replaced or modified as necessary. A self-propelled non-rotating pipe float may be used while the concrete is still plastic, to remove minor irregularities and score marks. Only one pass of the pipe float shall be allowed. Equipment, mixture, and/or procedures which produce more than 1/4 inch (6 mm) of mortar-rich surface shall be immediately modified as necessary to eliminate this condition or operations shall cease. Remove excessive slurry from the surface with a cutting straightedge and wipe off the edge. Any slurry which does run down the vertical edges shall be immediately removed by hand, using stiff brushes or scrapers. No slurry, concrete or concrete mortar shall be used to build up along the edges of the pavement to compensate for excessive edge slump, either while the concrete is plastic or after it hardens.

b. Machine finishing with fixed forms. The machine shall be designed to straddle the forms and shall be operated to screed and consolidate the concrete. Machines that cause displacement of the forms shall be replaced. The machine shall make only one pass over each area of pavement. If the equipment and procedures do not produce a surface of uniform texture, true to grade, in one pass, the operation shall be immediately stopped and the equipment, mixture, and procedures adjusted as necessary.

c. Other types of finishing equipment. Clary screeds, other rotating tube floats, or bridge deck finishers are not allowed on mainline paving, but may be allowed on irregular or odd-shaped slabs, and near buildings or trench drains, subject to the RPR's approval.

Bridge deck finishers shall have a minimum operating weight of 7500 pounds (3400 kg) and shall have a transversely operating carriage containing a knock-down auger and a minimum of two immersion vibrators. Vibrating screeds or pans shall be used only for isolated slabs where hand finishing is permitted as specified, and only where specifically approved.

d. Hand finishing. Hand finishing methods will not be permitted, except under the following conditions: (1) in the event of breakdown of the mechanical equipment, hand methods may be used to finish the concrete already deposited on the grade and (2) in areas of narrow widths or of irregular dimensions where operation of the mechanical equipment is impractical.

e. Straightedge testing and surface correction. After the pavement has been struck off and while the concrete is still plastic, it shall be tested for trueness with a 12-foot (3.7-m) finishing straightedge swung from handles capable of spanning at least one-half the width of the slab. The straightedge shall be held in contact with the surface in successive positions parallel to the centerline and the whole area gone over from one side of the slab to the other, as necessary. Advancing shall be in successive stages of not more than one-half the length of the straightedge. Any excess water and laitance in excess of 1/8 inch (3 mm) thick shall be removed from the surface of the pavement and wasted. Any depressions shall be immediately filled with freshly mixed concrete, struck off, consolidated, and refinished. High areas shall be cut down and refinished. Special attention shall be given to assure that the surface across joints meets the smoothness requirements. Straightedge testing and surface corrections shall continue until the entire surface is found to be free from observable departures from the straightedge and until the slab conforms to the required grade and cross-section. The use of long-handled wood floats shall be confined to a minimum; they may be used only in emergencies and in areas not accessible to finishing equipment.

501-4.12 Surface texture. The surface of the pavement shall be finished as designated below for all newly constructed concrete pavements. It is important that the texturing equipment not tear or unduly roughen the pavement surface during the operation. The texture shall be uniform in appearance and approximately 1/16 inch (2 mm) in depth. Any imperfections resulting from the texturing operation shall be corrected to the satisfaction of the RPR.

a. Brush or broom finish. Shall be applied when the water sheen has practically disappeared. The equipment shall operate transversely across the pavement surface.

b. Burlap drag finish. Burlap, at least 15 ounces per square yard (555 grams per square meter), will typically produce acceptable texture. To obtain a textured surface, the transverse threads of the burlap shall be removed approximately one foot (30 cm) from the trailing edge. A heavy buildup of grout on the burlap threads produces the desired wide sweeping longitudinal striations on the pavement surface.

c. Artificial turf finish. Shall be applied by dragging the surface of the pavement in the direction of concrete placement with an approved full-width drag made with artificial turf. The leading transverse edge of the artificial turf drag will be securely fastened to a lightweight pole on a traveling bridge. At least 2 feet (60 cm) of the artificial turf shall be in contact with the concrete surface during dragging operations. Approval of the artificial turf will be done only after it has been demonstrated by the Contractor to provide a satisfactory texture. One type that has provided satisfactory texture consists of 7,200 approximately 0.85-inch-long polyethylene turf blades per square foot.

Immediately after finishing operations are completed and bleed water is gone from the surface, all exposed surfaces of the newly placed concrete shall be cured for a 7-day cure period in accordance with one of the methods below. Failure to provide sufficient cover material of whatever kind the Contractor may elect to use, or lack of water to adequately take care of both curing and other requirements, shall be cause for immediate suspension of concreting operations. The concrete shall not be left exposed for more than 1/2 hour during the curing period.

When a two-saw-cut method is used to construct the contraction joint, the curing compound shall be applied to the saw-cut immediately after the initial cut has been made. The sealant reservoir shall not be sawed until after the curing period has been completed. When the one cut method is used to construct the contraction joint, the joint shall be cured with wet rope, wet rags, or wet blankets. The rags, ropes, or blankets shall be kept moist for the duration of the curing period.

a. Impervious membrane method. Curing with liquid membrane compounds should not occur until bleed and surface moisture has evaporated. All exposed surfaces of the pavement shall be sprayed uniformly with white pigmented curing compound immediately after the finishing of the surface and before the set of the concrete has taken place. The curing compound shall not be applied during rainfall. Curing compound shall be applied by mechanical sprayers under pressure at the rate of one gallon (4 liters) to not more than 150 square feet (14 sq m). The spraying equipment shall be of the fully atomizing type equipped with a tank agitator. At the time of use, the compound shall be in a thoroughly mixed condition with the pigment uniformly dispersed throughout the vehicle. During application, the compound shall be stirred continuously by mechanical means. Hand spraying of odd widths or shapes and concrete surfaces exposed by the removal of forms will be permitted. When hand spraying is approved by the RPR, a double application rate shall be used to ensure coverage. Should the film become damaged from any cause, including sawing operations, within the required curing period, the damaged portions shall be repaired immediately with additional compound or other approved means. Upon removal of side forms, the sides of the exposed slabs shall be protected immediately to provide a curing treatment equal to that provided for the surface.

b. White burlap-polyethylene sheets. The surface of the pavement shall be entirely covered with the sheeting. The sheeting used shall be such length (or width) that it will extend at least twice the thickness of the pavement beyond the edges of the slab. The sheeting shall be placed so that the entire surface and both edges of the slab are completely covered. The sheeting shall be placed and weighted to remain in contact with the surface covered, and the covering shall be maintained fully saturated and in position for seven (7) days after the concrete has been placed.

c. Water method. The entire area shall be covered with burlap or other water absorbing material. The material shall be of sufficient thickness to retain water for adequate curing without excessive runoff. The material shall be kept wet at all times and maintained for seven (7) days. When the forms are stripped, the vertical walls shall also be kept moist. It shall be the responsibility of the Contractor to prevent ponding of the curing water on the subbase.

d. Concrete protection for cold weather. Maintain the concrete at a temperature of at least 50°F (10°C) for a period of 72 hours after placing and at a temperature above freezing for the remainder of the 7-day curing period. The Contractor shall be responsible for the quality and strength of the concrete placed during cold weather; and any concrete damaged shall be removed and replaced at the Contractor's expense.

e. Concrete protection for hot weather. Concrete should be continuous moisture cured for the entire curing period and shall commence as soon as the surfaces are finished and continue for at least 24 hours. However, if moisture curing is not practical beyond 24 hours, the concrete surface shall be protected from drying with application of a liquid membrane-forming curing compound while the surfaces are still damp. Other curing methods may be approved by the RPR.

501-4.14 Removing forms. Unless otherwise specified, forms shall not be removed from freshly placed concrete until it has hardened sufficiently to permit removal without chipping, spalling, or tearing. After the forms have been removed, the sides of the slab shall be cured in accordance with paragraph 501-4.13.

If honeycombed areas are evident when the forms are removed, materials, placement, and consolidation methods must be reviewed and appropriate adjustments made to assure adequate consolidation at the edges of future concrete placements. Honeycombed areas that extend into the slab less than

approximately 1 inch (25 mm), shall be repaired with an approved grout, as directed by the RPR. Honeycombed areas that extend into the slab greater than a depth of 1 inch (25 mm) shall be considered as defective work and shall be removed and replaced in accordance with paragraph 501-4.19.

501-4.15 Saw-cut grooving. If shown on the plans, grooved surfaces shall be provided in accordance with the requirements of Item P-621.

501-4.16 Sealing joints. The joints in the pavement shall be sealed in accordance with Item **P-605**.

501-4.17 Protection of pavement. *The Contractor shall protect the pavement and its appurtenances against both public traffic and traffic caused by the Contractor's employees and agents until accepted in writing by the RPR. This shall include watchmen to direct traffic and the erection and maintenance of warning signs, lights, pavement bridges, crossovers, and protection of unsealed joints from intrusion of foreign material, etc. Any damage to the pavement occurring prior to final acceptance shall be replaced at the Contractor's expense. Slabs shall be removed to the full depth, width, and length of the slab. The replacement slabs shall be doweled and tied at the direction of the RPR.*

Aggregates, rubble, or other similar construction materials shall not be placed on airfield pavements.

Hauling equipment or other mechanical equipment can be permitted on adjoining previously constructed pavement when the concrete strength reaches a flexural strength of 550 psi, based on the average of four field cured specimens per 2,000 cubic yards (1,530 cubic meters) of concrete placed. The Contractor is responsible for the verification testing and costs associated with that testing. The Contractor must determine, with the use of FAARFIELD, that the above minimum strengths are adequate for the protection of the pavement from overloads due to the construction equipment proposed for the project. The testing results and the overload protection calculations shall be approved in writing by the RPR prior to any traffic being allowed on new pavement.

In paving intermediate lanes between newly paved pilot lanes, operation of the hauling and paving equipment will be permitted on the new pavement only after the pavement has met the above criteria, and the slab edge is protected."

501-4.18 Opening to construction traffic. *The pavement shall not be opened to any traffic until the pavement has met the requirements of 4.17. If such tests are not conducted, the pavement shall not be opened to traffic until accepted by the RPR. Prior to opening the pavement to any traffic, all joints shall either be sealed or protected from damage to the joint edge and intrusion of foreign materials into the joint. As a minimum, backer rod or tape may be used to protect the joints from foreign matter intrusion. All new and existing pavement carrying any traffic or equipment shall be kept clean and spillage of concrete and other materials shall be cleaned up immediately."*

501-4.19 Repair, removal, or replacement of slabs. New pavement slabs that are broken or contain cracks or are otherwise defective or unacceptable as defined by acceptance criteria in paragraph 501-6.6 shall be removed and replaced or repaired, as directed by the RPR, at the Contractor's expense. Spalls along joints shall be repaired as specified. Removal of partial slabs is not permitted. Removal and replacement shall be full depth, shall be full width of the slab, and the limit of removal shall be normal to the paving lane and to each original transverse joint. The RPR will determine whether cracks extend full depth of the pavement and may require cores to be drilled on the crack to determine depth of cracking. Such cores shall have a diameter of 2 inches (50 mm) to 4 inches (100 mm), shall be drilled by the Contractor and shall be filled by the Contractor with a well consolidated concrete mixture bonded to the walls of the hole with a bonding agent, using approved procedures. Drilling of cores and refilling holes shall be at no expense to the Owner. Repair of cracks as described in this section shall not be allowed if in the opinion of the RPR the overall condition of the pavement indicates that such repair is unlikely to achieve an acceptable and durable finished pavement. No repair of cracks shall be allowed in any panel

that demonstrates segregated aggregate with an absence of coarse aggregate in the upper 1/8 inch (3 mm) of the pavement surface.

a. Shrinkage cracks. Shrinkage cracks which do not exceed one-third of the pavement depth shall be cleaned and either high molecular weight methacrylate (HMWM) applied; or epoxy resin (Type IV, Grade 1) pressure injected using procedures recommended by the manufacturer and approved by the RPR. Sandblasting of the surface may be required following the application of HMWM to restore skid resistance. Care shall be taken to ensure that the crack is not widened during epoxy resin injection. All epoxy resin injection shall take place in the presence of the RPR. Shrinkage cracks which exceed one-third the pavement depth shall be treated as full depth cracks in accordance with paragraphs 501-4.19b and 501-19c.

b. Slabs with cracks through interior areas. Interior area is defined as that area more than 6 inches (150 mm) from either adjacent original transverse joint. The full slab shall be removed and replaced at no cost to the Owner, when there are any full depth cracks, or cracks greater than one-third the pavement depth, that extend into the interior area.

c. Cracks close to and parallel to joints. All full-depth cracks within 6 inches (150 mm) either side of the joint and essentially parallel to the original joints, shall be treated as follows.

(1) Full depth cracks and original joint not cracked. The full-depth crack shall be treated as the new joint and the original joint filled with an epoxy resin.

i. Full-depth crack. The joint sealant reservoir for the crack shall be formed by sawing to a depth of 3/4 inches (19 mm), $\pm 1/16$ inch (2 mm), and to a width of 5/8 inch (16 mm), $\pm 1/8$ inch (3 mm). The crack shall be sawed with equipment specially designed to follow random cracks. Any equipment or procedure which causes raveling or spalling along the crack shall be modified or replaced to prevent raveling or spalling. The joint shall be sealed with sealant in accordance with P-605 or as directed by the RPR.

ii. Original joint. If the original joint sealant reservoir has been sawed out, the reservoir and as much of the lower saw cut as possible shall be filled with epoxy resin, Type IV, Grade 2, thoroughly tooled into the void using approved procedures.

If only the original narrow saw cut has been made, it shall be cleaned and pressure injected with epoxy resin, Type IV, Grade 1, using approved procedures.

Where a parallel crack goes part way across paving lane and then intersects and follows the original joint which is cracked only for the remained of the width, it shall be treated as specified above for a parallel crack, and the cracked original joint shall be prepared and sealed as originally designed.

• **(2) Full depth cracks and original joint cracked.** If there is any place in the lane width where a parallel crack and a cracked portion of the original joint overlap, the entire slab containing the crack shall be removed and replaced.

d. Removal and replacement of full slabs. Make a full depth cut perpendicular to the slab surface along all edges of the slab with a concrete saw cutting any dowels or tie-bars. Remove damaged slab protecting adjacent pavement from damage. Damage to adjacent slabs may result in removal of additional slabs as directed by the RPR at the Contractor's expense.

The underlying material shall be repaired, re-compacted and shaped to grade.

Dowels of the size and spacing specified for other joints in similar pavement on the project shall be installed along all four (4) edges of the new slab in accordance with paragraph 501-4.10d.

Placement of concrete shall be as specified for original construction. The joints around the new slab shall be prepared and sealed as specified for original construction.

e. Spalls along joints.

- (1) Spalls less than one inch wide and less than the depth of the joint sealant reservoir, shall be filled with joint sealant material.
- (2) Spalls larger than one inch and/or deeper than the joint reservoir, but less than ½ the slab depth, and less than 25% of the length of the adjacent joint shall be repaired as follows:
 - i. Make a vertical saw cut at least one inch (25 mm) outside the spalled area and to a depth of at least 2 inches (50 mm). Saw cuts shall be straight lines forming rectangular areas surrounding the spalled area.
 - ii. Remove unsound concrete and at least 1/2 inch (12 mm) of visually sound concrete between the saw cut and the joint or crack with a light chipping hammer.
 - iii. Clean cavity with high-pressure water jets supplemented with compressed air as needed to remove all loose material.
 - iv. Apply a prime coat of epoxy resin, Type III, Grade I, to the dry, cleaned surface of all sides and bottom of the cavity, except any joint face.
 - v. Fill the cavity with low slump concrete or mortar or with epoxy resin concrete or mortar.
 - vi. An insert or other bond-breaking medium shall be used to prevent bond at all joint faces.
 - vii. A reservoir for the joint sealant shall be sawed to the dimensions required for other joints, or as required to be routed for cracks. The reservoir shall be thoroughly cleaned and sealed with the sealer specified for the joints.
- (3) Spalls deeper than 1/2 of the slab depth or spalls longer than 25% of the adjacent joint require replacement of the entire slab.

f. Diamond grinding of Concrete surfaces. Diamond grinding shall be completed prior to pavement grooving. Diamond grinding of the hardened concrete should not be performed until the concrete is at least 14 days old and has achieved full minimum strength. Equipment that causes ravels, aggregate fractures, spalls or disturbance to the joints will not be permitted. The depth of diamond grinding shall not exceed 1/2 inch (13 mm) and all areas in which diamond grinding has been performed will be subject to the final pavement thickness tolerances specified.

Diamond grinding shall be performed with a machine specifically designed for diamond grinding capable of cutting a path at least 3 feet (0.9 m) wide. The saw blades shall be 1/8-inch (3-mm) wide with sufficient number of flush cut blades that create grooves between 0.090 and 0.130 inches (2 and 3.5 mm) wide; and peaks and ridges approximately 1/32 inch (1 mm) higher than the bottom of the grinding cut. The Contractor shall determine the number and type of blades based on the hardness of the aggregate. Contractor shall demonstrate to the RPR that the grinding equipment will produce satisfactory results prior to making corrections to surfaces.

Grinding will be tapered in all directions to provide smooth transitions to areas not requiring grinding. The slurry resulting from the grinding operation shall be continuously removed and the pavement left in a clean condition. All grinding shall be at the expense of the Contractor.

CONTRACTOR QUALITY CONTROL (CQC)

501-5.1 Quality control program. The Contractor shall develop a Quality Control Program in accordance with Item C-100. No partial payment will be made for materials that are subject to specific quality control requirements without an approved quality control program.

501-5.2 Contractor Quality Control (CQC). The Contractor shall provide or contract for testing facilities in accordance with Item C-100. The RPR shall be permitted unrestricted access to inspect the Contractor's QC facilities and witness QC activities. The RPR will advise the Contractor in writing of any noted deficiencies concerning the QC facility, equipment, supplies, or testing

personnel and procedures. When the deficiencies are serious enough to be adversely affecting the test results, the incorporation of the materials into the work shall be suspended immediately and will not be permitted to resume until the deficiencies are satisfactorily corrected.

501-5.3 Contractor QC testing. The Contractor shall perform all QC tests necessary to control the production and construction processes applicable to this specification and as set forth in the CQCP. The testing program shall include, but not necessarily be limited to, tests for aggregate gradation, aggregate moisture content, slump, and air content. A QC Testing Plan shall be developed and approved by the RPR as part of the CQCP.

The RPR may at any time, notwithstanding previous plant acceptance, reject and require the Contractor to dispose of any batch of concrete mixture which is rendered unfit for use due to contamination, segregation, or improper slump. Such rejection may be based on only visual inspection. In the event of such rejection, the Contractor may take a representative sample of the rejected material in the presence of the RPR, and if it can be demonstrated in the laboratory, in the presence of the RPR, that such material was erroneously rejected, payment will be made for the material at the contract unit price.

a. Fine aggregate.

(1) Gradation. A sieve analysis shall be made at least twice daily in accordance with ASTM C136 from randomly sampled material taken from the discharge gate of storage bins or from the conveyor belt.

(2) Moisture content. If an electric moisture meter is used, at least two direct measurements of moisture content shall be made per week to check the calibration. If direct measurements are made in lieu of using an electric meter, two tests shall be made per day. Tests shall be made in accordance with ASTM C70 or ASTM C566.

(3) Deleterious substances. Fine aggregate as delivered to the mixer shall be tested for deleterious substances in fine aggregate for concrete as specified in paragraph 501-2.1b, prior to production of the control strip, and a minimum of every 30-days during production or more frequently as necessary to control deleterious substances.

b. Coarse Aggregate.

(1) Gradation. A sieve analysis shall be made at least twice daily for each size of aggregate. Tests shall be made in accordance with ASTM C136 from randomly sampled material taken from the discharge gate of storage bins or from the conveyor belt.

(2) Moisture content. If an electric moisture meter is used, at least two direct measurements of moisture content shall be made per week to check the calibration. If direct measurements are made in lieu of using an electric meter, two tests shall be made per day. Tests shall be made in accordance with ASTM C566.

(3) Deleterious substances. Coarse aggregate as delivered to the mixer shall be tested for deleterious substances in coarse aggregate for concrete as specified in paragraph 501-2.1c, prior to production of the control strip, and a minimum of every 30-days during production or more frequently as necessary to control deleterious substances.

c. Slump. One test shall be made for each subplot. Slump tests shall be performed in accordance with ASTM C143 from material randomly sampled from material discharged from trucks at the paving site. Material samples shall be taken in accordance with ASTM C172.

d. Air content. One test shall be made for each subplot. Air content tests shall be performed in accordance with ASTM C231 for gravel and stone coarse aggregate and ASTM C173 for slag or other porous coarse aggregate, from material randomly sampled from trucks at the paving site. Material samples shall be taken in accordance with ASTM C172.

e. Unit weight and Yield. One test shall be made for each subplot. Unit weight and yield tests shall be in accordance with ASTM C138. The samples shall be taken in accordance with ASTM C172 and at the same time as the air content tests.

f. Temperatures. Temperatures shall be checked at least four times per lot at the job site in accordance with ASTM C1064.

g. Smoothness for Contractor Quality Control.

The Contractor shall perform smoothness testing in transverse and longitudinal directions daily to verify that the construction processes are producing pavement with variances less than ¼ inch in 12 feet, identifying areas that may pond water which could lead to hydroplaning of aircraft. If the smoothness criteria is not met, appropriate changes and corrections to the construction process shall be made by the Contractor before construction continues

The Contractor may use a 12-foot (3.7 m) straightedge, a rolling inclinometer meeting the requirements of ASTM E2133 or rolling external reference device that can simulate a 12-foot (3.7 m) straightedge approved by the RPR. Straight-edge testing shall start with one-half the length of the straightedge at the edge of pavement section being tested and then moved ahead one-half the length of the straightedge for each successive measurement. Testing shall be continuous across all joints. The surface irregularity shall be determined by placing the freestanding (unleveled) straightedge on the pavement surface and allowing it to rest upon the two highest spots covered by its length, and measuring the maximum gap between the straightedge and the pavement surface in the area between the two high points. If the rolling inclinometer or external reference device is used, the data may be evaluated using the FAA profile program, ProFAA, or FHWA ProVal, using the 12-foot straightedge simulation function.

Smoothness readings shall not be made across grade changes or cross slope transitions. The transition between new and existing pavement shall be evaluated separately for conformance with the plans.

(1) Transverse measurements. Transverse measurements shall be taken for each day's production placed. Transverse measurements shall be taken perpendicular to the pavement centerline each 50 feet (15 m) or more often as determined by the RPR. The joint between lanes shall be tested separately to facilitate smoothness between lanes.

(2) Longitudinal measurements. Longitudinal measurements shall be taken for each day's production placed. Longitudinal tests shall be parallel to the centerline of paving; at the center of paving lanes when widths of paving lanes are less than 20 feet (6 m); and at the third points of paving lanes when widths of paving lanes are 20 ft (6 m) or greater. When placement abuts previously placed material the first measurement shall start with one half the length of the straight edge on the previously placed material.

Deviations on the final surface course in either the transverse or longitudinal direction that will trap water greater than 1/4 inch (6 mm) shall be corrected with diamond grinding per paragraph 501-4.19f or by removing and replacing the surface course to full depth. Grinding shall be tapered in all directions to provide smooth transitions to areas not requiring grinding. All areas in which diamond grinding has been performed shall be subject to the final pavement thickness tolerances specified in paragraph 501-6.6.

Control charts shall be kept to show area of each day's placement and the percentage of corrective grinding required. Corrections to production and placement shall be initiated when corrective grinding is required. If the Contractor's machines and/or methods produce significant areas that need corrective actions in excess of 10 percent of a day's production, production shall be stopped until corrective measures are implemented by the Contractor.

h. Grade. Grade will be evaluated prior to and after placement of the concrete surface.

Measurements will be taken at appropriate gradelines (as a minimum at center and edges of paving lane) and longitudinal spacing as shown on cross-sections and plans. The final surface of the pavement will not vary from the gradeline elevations and cross-sections shown on the plans by more than 1/2 inch (12 mm) vertically **and 0.1 feet laterally**. The documentation will be provided by the Contractor to the RPR **within 48 hours**.

Areas with humps or depression that that exceed grade or smoothness and that retain water on the surface must be ground off provided the course thickness after grinding is not more than 1/2 inch (12 mm) less than the thickness specified on the plans. If these areas cannot be corrected with grinding then the slabs that are retaining water must be removed and replaced in accordance with paragraph 501-4.19d. Grinding shall be in accordance with paragraph 501-4.19f. All corrections will be at the Contractors expense.

501-5.4 Control charts. The Contractor shall maintain linear control charts for fine and coarse aggregate gradation, slump, and air content. The Contractor shall also maintain a control chart plotting the coarseness factor/workability factor from the combined gradations in accordance with paragraph 501-2.1d.

Control charts shall be posted in a location satisfactory to the RPR and shall be kept up to date at all times. As a minimum, the control charts shall identify the project number, the contract item number, the test number, each test parameter, the Action and suspension Limits, or Specification limits, applicable to each test parameter, and the Contractor’s test results. The Contractor shall use the control charts as part of a process control system for identifying potential problems and assignable causes before they occur. If the Contractor’s projected data during production indicates a potential problem and the Contractor is not taking satisfactory corrective action, the RPR may halt production or acceptance of the material.

a. Fine and coarse aggregate gradation. The Contractor shall record the running average of the last five gradation tests for each control sieve on linear control charts. Superimposed on the control charts shall be the action and suspension limits. Gradation tests shall be performed by the Contractor per ASTM C136. The Contractor shall take at least **two** samples per lot to check the final gradation. Sampling shall be per ASTM D75 from the flowing aggregate stream or conveyor belt.

b. Slump and air content. The Contractor shall maintain linear control charts both for individual measurements and range (that is, difference between highest and lowest measurements) for slump and air content in accordance with the following Action and Suspension Limits.

c. Combined gradation. The Contractor shall maintain a control chart plotting the coarseness factor and workability factor on a chart in accordance with paragraph 501-2.1d.

Control Chart Limits¹

Control Parameter	Individual Measurements	
	Action Limit	Suspension Limit
Gradation ²	*3	*3
Coarseness Factor (CF)	±3.5	±5
Workability Factor (WF)	±2	±3
Slump	+0.5 to -1 inch (+13 to -25 mm)	+1 to -1.5 inch (+25 to -38 mm)
Air Content	±1.5%	±2.0%

¹ Control charts shall developed and maintained for each control parameter indicated.

² Control charts shall be developed and maintained for each sieve size.

³ Action and suspension limits shall be determined by the Contractor.

501-5.5 Corrective action at Suspension Limit. The CQCP shall indicate that appropriate action shall be taken when the process is believed to be out of control. The CQCP shall detail what action will be taken to bring the process into control and shall contain sets of rules to gauge when a process is out of control. As a minimum, a process shall be deemed out of control and corrective action taken if any one of the following conditions exists.

- a. **Fine and coarse aggregate gradation.** When two consecutive averages of five tests are outside of the suspension limits, immediate steps, including a halt to production, shall be taken to correct the grading.
- b. **Coarseness and Workability factor.** When the CF or WF reaches the applicable suspension limits, the Contractor, immediate steps, including a halt to production, shall be taken to correct the CF and WF.
- c. **Fine and coarse aggregate moisture content.** Whenever the moisture content of the fine or coarse aggregate changes by more than 0.5%, the scale settings for the aggregate batcher and water batcher shall be adjusted.
- d. **Slump.** The Contractor shall halt production and make appropriate adjustments whenever:
 - (1) one point falls outside the Suspension Limit line for individual measurements

OR

 - (2) two points in a row fall outside the Action Limit line for individual measurements.
- d. **Air content.** The Contractor shall halt production and adjust the amount of air-entraining admixture whenever:
 - (1) one point falls outside the Suspension Limit line for individual measurements

OR

 - (2) two points in a row fall outside the Action Limit line for individual measurements.

MATERIAL ACCEPTANCE

501-6.1 Quality Assurance (QA) Acceptance sampling and testing. All acceptance sampling and testing necessary to determine conformance with the requirements specified in this section, with the exception of coring for thickness determination, will be performed by the RPR. The Contractor shall provide adequate facilities for the initial curing of beams. The Contractor shall bear the cost of providing initial curing facilities and coring and filling operations, per paragraph 501-6.5b(1).

The samples will be transported while in the molds. The curing, except for the initial cure period, will be accomplished using the immersion in saturated lime water method. During the 24 hours after molding, the temperature immediately adjacent to the specimens must be maintained in the range of 60° to 80°F (16° to 27°C), and loss of moisture from the specimens must be prevented. The specimens may be stored in tightly constructed wooden boxes, damp sand pits, temporary buildings at construction sites, under wet burlap in favorable weather, or in heavyweight closed plastic bags, or using other suitable methods, provided the temperature and moisture loss requirements are met.

501-6.2 Quality Assurance (QA) testing laboratory. Quality assurance testing organizations performing these acceptance tests will be accredited in accordance with ASTM C1077. The quality assurance laboratory accreditation must be current and listed on the accrediting authority's website. All test methods required for acceptance sampling and testing must be listed on the lab accreditation. A copy of the laboratory's current accreditation and accredited test methods will be submitted to the RPR prior to start of construction.

501-6.3 Lot size. Concrete will be accepted for strength and thickness on a lot basis. A lot will consist of a day's production not to exceed **1,000** cubic yards (**6,000 square yards**). Each lot will be divided into approximately equal sublots with individual sublots between **200** to **300** cubic yards. Where three sublots are produced, they will constitute a lot. Where one or two sublots are produced, they will be incorporated into the previous or next lot. Where more than one plant is simultaneously producing concrete for the job, the lot sizes will apply separately for each plant.

501-6.4 Partial lots. When operational conditions cause a lot to be terminated before the specified number of tests have been made for the lot or for overages or minor placements to be considered as partial lots, the following procedure will be used to adjust the lot size and the number of tests for the lot.

Where three sublots have been produced, they will constitute a lot. Where one or two sublots have been produced, they will be incorporated into the next lot or the previous lot and the total number of sublots will be used in the acceptance criteria calculation, that is, $n=5$ or $n=6$.

501-6.5 Acceptance Sampling and Testing.

a. Strength.

- **(1) Sampling.** One sample will be taken for each subplot from the concrete delivered to the job site. Sampling locations will be determined by the RPR in accordance with random sampling procedures contained in ASTM D3665. The concrete will be sampled in accordance with ASTM C172.
- **(2) Test Specimens.** The RPR will be responsible for the casting, initial curing, transportation, and curing of specimens in accordance with ASTM C31. Two (2) specimens will be made from each sample and slump, air content, unit weight, and temperature tests will be conducted for each set of strength specimens. Within 24 to 48 hours, the samples will be transported from the field to the laboratory while in the molds. Samples will be cured in saturated lime water.
 - The strength of each specimen will be determined in accordance with **ASTM C78**. The strength for each subplot will be computed by averaging the results of the two test specimens representing that subplot.
- **(3) Acceptance.** Acceptance of pavement for strength will be determined by the RPR in accordance with paragraph 501-6.6b(1). All individual strength tests within a lot will be checked for outliers in accordance with ASTM E178, at a significance level of 5%. Outliers will be discarded and the remaining test values will be used to determine acceptance in accordance with paragraph 501-6.5b.

b. Pavement thickness.

(1) Sampling. One core will be taken by the Contractor for each subplot in the presence of the RPR. Sampling locations will be determined by the RPR in accordance with random sampling procedures contained in ASTM D3665. Areas, such as thickened edges, with planned variable thickness, will be excluded from sample locations.

Cores shall be a minimum 4 inch (100 mm) in diameter neatly cut with a core drill. The Contractor will furnish all tools, labor, and materials for cutting samples and filling the cored hole. Core holes will be filled by the Contractor with a non-shrink grout approved by the RPR within one day after sampling.

(2) Testing. The thickness of the cores will be determined by the RPR by the average caliper measurement in accordance with ASTM C174. Each core shall be photographed and the photograph included with the test report.

(3) Acceptance. Acceptance of pavement for thickness will be determined by the RPR in accordance with paragraph 501-6.6.

501-6.6 Acceptance criteria.

a. General. Acceptance will be based on the following characteristics of the completed pavement discussed in paragraph 501-6.5b:

- (1) Strength
- (2) Thickness
- (3) Grade
- (4) Profilograph smoothness **Not used.**
- (5) Adjustments for repairs

Acceptance for strength, thickness, and grade, will be based on the criteria contained in accordance with paragraph 501-6.6b(1), 501-6.6b(2), and 501-6.6b(3), respectively.

Production quality must achieve 90 PWL or higher to receive full payment.

Strength and thickness will be evaluated for acceptance on a lot basis using the method of estimating PWL. Production quality must achieve 90 PWL or higher to receive full pavement. The PWL will be determined in accordance with procedures specified in Item C-110.

The lower specification tolerance limit (L) for strength and thickness will be:

Lower Specification Tolerance Limit (L)

Strength	0.93 × strength specified in paragraph 501-3.3
Thickness	Lot Plan Thickness in inches, - 0.50 in

b. Acceptance criteria.

(1) Strength. If the PWL of the lot equals or exceeds 90%, the lot will be acceptable. Acceptance and payment for the lot will be determined in accordance with paragraph 501-8.1.

(2) Thickness. If the PWL of the lot equals or exceeds 90%, the lot will be acceptable. Acceptance and payment for the lot will be determined in accordance with paragraph 501-8.1.

(3) Grade. The final finished surface of the pavement of the completed project will not vary from the gradeline elevations and cross-sections shown on the plans by more than 1/2 inch (12 mm) vertically or 0.1 feet laterally. The documentation, stamped and signed by a licensed surveyor shall be in accordance with paragraph 501-5.3h. Payment for sublots that do not meet grade for over 25% of the subplot shall reduced by 5% and not be more than 95%.

(4) Profilograph roughness for QA Acceptance. Not used.

(5) Adjustments for repair. Sublots with spall repairs, crack repairs, or partial panel replacement, will be limited to no more than 95% payment.

(6) Adjustment for grinding. For sublots with grinding over 25% of a subplot, payment will be reduced 5%.

METHOD OF MEASUREMENT

501-7.1 Concrete pavement shall be measured by the number of square yards of plain and reinforced pavement as specified in-place, completed and accepted.

BASIS OF PAYMENT

501-8.1 Payment. Payment for concrete pavement meeting all acceptance criteria as specified in paragraph 501-6.6. Acceptance Criteria shall be based on results of strength and thickness tests. Payment for acceptable lots of concrete pavement shall be adjusted in accordance with paragraph 501-8.1a for strength and thickness; 501-8.1b for repairs; 501-8.1c for grinding; and 501-8.1d for smoothness, subject to the limitation that:

The total project payment for concrete pavement shall not exceed **100** percent of the product of the contract unit price and the total number of **square yards** of concrete pavement used in the accepted work (See Note 1 under the Price Adjustment Schedule table below).

Payment shall be full compensation for all labor, materials, tools, equipment, and incidentals required to complete the work as specified herein and on the drawings.

a. Basis of adjusted payment. The pay factor for each individual lot shall be calculated in accordance with the Price Adjustment Schedule table below. A pay factor shall be calculated for both strength and thickness. The lot pay factor shall be the higher of the two values when calculations for both strength and thickness are 100% or higher. The lot pay factor shall be the product of the two values when only one of the calculations for either strength or thickness is 100% or higher. The lot pay factor shall be the lower of the two values when calculations for both strength and thickness are less than 100%.

Price Adjustment Schedule¹

Percentage of Materials Within Specification Limits (PWL)	Lot Pay Factor (Percent of Contract Unit Price)
96 – 100	106
90 – 95	PWL + 10
75 – 90	0.5 PWL + 55
55 – 74	1.4 PWL – 12
Below 55	Reject ²

¹ Although it is theoretically possible to achieve a pay factor of 106% for each lot, actual payment in excess of 100% shall be subject to the total project payment limitation specified in paragraph 501-8.1.

² The lot shall be removed and replaced unless, after receipt of FAA concurrence, the Owner and Contractor agree in writing that the lot will remain; the lot paid at 50% of the contract unit price; and the total project payment limitation reduced by the amount withheld for that lot.

For each lot accepted, the adjusted contract unit price shall be the product of the lot pay factor for the lot and the contract unit price. Payment shall be subject to the total project payment limitation specified in paragraph 501-8.1. Payment in excess of 100% for accepted lots of concrete pavement shall be used to offset payment for accepted lots of concrete pavement that achieve a lot pay factor less than 100%; except for rejected lots which remain in place and/or sublots with adjustments for repairs.

b. Adjusted payment for repairs. The PWL lot pay factor shall be reduced by 5% and be no higher than 95% for sublots which contain repairs in accordance with paragraph 501-4.19 on more than 20% of the slabs within the subplot. Payment factors greater than 100 percent for the strength and thickness cannot be used to offset adjustments for repairs.

c. Adjusted payment for grinding. The PWL lot pay factor shall be reduced by 5% and be no higher than 95% for sublots with grinding over 25% of a subplot.

d. Profilograph Roughness. Not used.

e. Payment. Payment shall be made under:

P.C.C. Pavement (6")	--Per Square Yard
Full and Partial Panel Material (Concrete)	--Per Cubic Yard
Full and Partial Panel Placement (Concrete)	--Per Square Yard

REFERENCES

The publications listed below form a part of this specification to the extent referenced. The publications are referred to within the text by the basic designation only.

ASTM International (ASTM)

ASTM A184	Standard Specification for Welded Deformed Steel Bar Mats for Concrete Reinforcement
ASTM A615	Standard Specification for Deformed and Plain Carbon-Steel Bars for Concrete Reinforcement
ASTM A704	Standard Specification for Welded Steel Plain Bar or Rod Mats for Concrete Reinforcement
ASTM A706	Standard Specification for Low-Alloy Steel Deformed and Plain Bars for Concrete Reinforcement
ASTM A775	Standard Specification for Epoxy-Coated Steel Reinforcing Bars
ASTM A884	Standard Specification for Epoxy-Coated Steel Wire and Welded Wire Reinforcement
ASTM A934	Standard Specification for Epoxy-Coated Prefabricated Steel Reinforcing Bars
ASTM A996	Standard Specification for Rail-Steel and Axle-Steel Deformed Bars for Concrete Reinforcement
ASTM A1035	Standard Specification for Deformed and Plain, Low-Carbon, Chromium, Steel Bars for Concrete Reinforcement
ASTM A1064	Standard Specification for Carbon-Steel Wire and Welded Wire Reinforcement, Plain and Deformed, for Concrete
ASTM A1078	Standard Specification for Epoxy-Coated Steel Dowels for Concrete Pavement
ASTM C29	Standard Test Method for Bulk Density ("Unit Weight") and Voids in Aggregate
ASTM C31	Standard Practice for Making and Curing Concrete Test Specimens in the Field
ASTM C33	Standard Specification for Concrete Aggregates
ASTM C39	Standard Test Method for Compressive Strength of Cylindrical Concrete Specimens
ASTM C70	Standard Test Method for Surface Moisture in Fine Aggregate

ASTM C78	Standard Test Method for Flexural Strength of Concrete (Using Simple Beam with Third-Point Loading)
ASTM C88	Standard Test Method for Soundness of Aggregates by Use of Sodium Sulfate or Magnesium Sulfate
ASTM C94	Standard Specification for Ready-Mixed Concrete
ASTM C114	Standard Test Methods for Chemical Analysis of Hydraulic Cement
ASTM C117	Standard Test Method for Materials Finer than 75- μm (No. 200) Sieve in Mineral Aggregates by Washing
ASTM C123	Standard Test Method for Lightweight Particles in Aggregate
ASTM C136	Standard Test Method for Sieve Analysis of Fine and Coarse Aggregates
ASTM C131	Standard Test Method for Resistance to Degradation of Small-Size Coarse Aggregate by Abrasion and Impact in the Los Angeles Machine
ASTM C136	Standard Test Method for Sieve or Screen Analysis of Fine and Coarse Aggregates
ASTM C138	Standard Test Method for Density (Unit Weight), Yield, and Air Content (Gravimetric) of Concrete
ASTM C142	Standard Test Method for Clay Lumps and Friable Particles in Aggregates
ASTM C143	Standard Test Method for Slump of Hydraulic-Cement Concrete
ASTM C150	Standard Specification for Portland Cement
ASTM C171	Standard Specification for Sheet Materials for Curing Concrete
ASTM C172	Standard Practice for Sampling Freshly Mixed Concrete
ASTM C173	Standard Test Method for Air Content of Freshly Mixed Concrete by the Volumetric Method
ASTM C174	Standard Test Method for Measuring Thickness of Concrete Elements Using Drilled Concrete Cores
ASTM C227	Standard Test Method for Potential Alkali Reactivity of Cement-Aggregate Combinations (Mortar-Bar Method)
ASTM C231	Standard Test Method for Air Content of Freshly Mixed Concrete by the Pressure Method
ASTM C260	Standard Specification for Air-Entraining Admixtures for Concrete
ASTM C295	Standard Guide for Petrographic Examination of Aggregates for Concrete
ASTM C309	Standard Specification for Liquid Membrane-Forming Compounds for Curing Concrete
ASTM C311	Standard Test Methods for Sampling and Testing Fly Ash or Natural Pozzolans for Use in Portland Cement Concrete
ASTM C494	Standard Specification for Chemical Admixtures for Concrete

ASTM C566	Standard Test Method for Total Evaporable Moisture Content of Aggregates by Drying
ASTM C595	Standard Specification for Blended Hydraulic Cements
ASTM C618	Standard Specification for Coal Fly Ash and Raw or Calcined Natural Pozzolan for Use in Concrete
ASTM C642	Standard Test Method for Density, Absorption, and Voids in Hardened Concrete
ASTM C666	Standard Test Method for Resistance of Concrete to Rapid Freezing and Thawing
ASTM C685	Standard Specification for Concrete Made by Volumetric Batching and Continuous Mixing
ASTM C881	Standard Specification for Epoxy-Resin-Base Bonding Systems for Concrete
ASTM C989	Standard Specification for Slag Cement for Use in Concrete and Mortars
ASTM C1017	Standard Specification for Chemical Admixtures for Use in Producing Flowing Concrete
ASTM C1064	Test Method for Temperature of Freshly Mixed Hydraulic-Cement Concrete
ASTM C1077	Standard Practice for Agencies Testing Concrete and Concrete Aggregates for Use in Construction and Criteria for Testing Agency Evaluation
ASTM C1157	Standard Performance Specification for Hydraulic Cement
ASTM C1260	Standard Test Method for Potential Alkali Reactivity of Aggregates (Mortar-Bar Method)
ASTM C1365	Standard Test Method for Determination of the Proportion of Phases in Portland Cement and Portland-Cement Clinker Using X-Ray Powder Diffraction Analysis
ASTM C1567	Standard Test Method for Determining the Potential Alkali-Silica Reactivity of Combinations of Cementitious Materials and Aggregate (Accelerated Mortar-Bar Method)
ASTM C1602	Standard Specification for Mixing Water Used in the Production of Hydraulic Cement Concrete
ASTM D75	Standard Practice for Sampling Aggregates
ASTM D1751	Standard Specification for Preformed Expansion Joint Filler for Concrete Paving and Structural Construction (Nonextruding and Resilient Bituminous Types)
ASTM D1752	Standard Specification for Preformed Sponge Rubber and Cork and Recycled PVC Expansion Joint Fillers for Concrete Paving and Structural Construction
ASTM D2419	Standard Test Method for Sand Equivalent Value of Soils and Fine Aggregate

ASTM D3665	Standard Practice for Random Sampling of Construction Materials
ASTM D4791	Standard Test Method for Flat Particles, Elongated Particles, or Flat and Elongated Particles in Coarse Aggregate
ASTM E178	Standard Practice for Dealing with Outlying Observations
ASTM E1274	Standard Test Method for Measuring Pavement Roughness Using a Profilograph
ASTM E2133	Standard Test Method for Using a Rolling Inclinator to Measure Longitudinal and Transverse Profiles of a Traveled Surface
American Concrete Institute (ACI)	
ACI 305R	Guide to Hot Weather Concreting
ACI 306R	Guide to Cold Weather Concreting
ACI 309R	Guide for Consolidation of Concrete
Advisory Circulars (AC)	
AC 150/5320-6	Airport Pavement Design and Evaluation
Federal Highway Administration (FHWA)	
HIPERPAV 3, version 3.2	
Portland Concrete Association (PCA)	
PCA	Design and Control of Concrete Mixtures, 16 th Edition
U.S. Army Corps of Engineers (USACE) Concrete Research Division (CRD)	
CRD C662	Determining the Potential Alkali-Silica Reactivity of Combinations of Cementitious Materials, Lithium Nitrate Admixture and Aggregate (Accelerated Mortar-Bar Method)
United States Air Force Engineering Technical Letter (ETL)	
ETL 97-5	Proportioning Concrete Mixtures with Graded Aggregates for Rigid Airfield Pavements

END ITEM P-501

PROPOSAL FORM
CITY OF BOONVILLE, MISSOURI
JESSE VIERTEL MEMORIAL AIRPORT (VER)
 State Block Grant Project No. 23-039A-1

TO: City of Boonville

The undersigned, in compliance with the request for bids for construction of the following Project:

Base Bid

Reconstruct North T-Hangar Taxilanes

Add Alternate No. 1

Rehabilitate Shade Hangar Taxilane

Add Alternate No. 2

Rehabilitate South T-Hangar Taxilanes and South Connecting Taxiway

hereby proposes to furnish all labor, permits, material, machinery, tools, supplies and equipment to faithfully perform all work required for construction of the Project in accordance with the project manual, project drawings and issued Addenda within the specified time of performance for the following prices:

BID ITEM	FAA or MoDOT SPEC.	ITEM DESCRIPTION	APPROX. QTY. AND UNITS	UNIT PRICE		EXTENSION	
				DOLLARS	CTS	DOLLARS	CTS
BASE BID							
RECONSTRUCT NORTH T-HANGAR TAXILANES							
1	C-100	Contractor Quality Control Program (CQCP)	1 L.S.				
2	C-102	Erosion Control Barrier (Silt Fence) <i>(Revised per Addendum No. 1)</i>	760 L.F.				
3	C-102	Erosion Control Barrier (Straw Wattle)	15 L.F.				
4	C-102	Stabilized Construction Exit	1 L.S.				
5	C-105	Mobilization (NTE 10% of Total Bid Amount)	1 L.S.				
6	TEMP	Temporary Marking, Lighting, and Barricades	1 L.S.				
7	P-101	Remove Pavement and Aggregate Base Course	6,439 S.Y.				
8	P-101	Saw Cut (Full Depth)	1,802 L.F.				
9	P-101	Remove Underdrain System	1 L.S.				
10	P-152	Unclassified Excavation	1,499 C.Y.				
11	P-152	Unsuitable Subgrade Removal and Replacement	400 C.Y.				
12	P-154	Aggregate Subbase Course (10")	6,524 S.Y.				
13	P-208, P-209, or P-219	Aggregate Base Course (4")	6,524 S.Y.				
14	P-501	P.C.C. Pavement (6")	6,458 S.Y.				
15	P-620	Surface Preparation, Pavement Marking Removal	845 S.F.				
16	P-620	Permanent ReflectORIZED Pavement Marking (Yellow)	1,098 S.F.				
17	P-620	Permanent Non-ReflectORIZED Pavement Marking (Black)	2,196 S.F.				

***** OFFICIAL BID FORM *****
 (REVISED PER ADDENDUM NO. 1)

BID ITEM	FAA or MoDOT SPEC.	ITEM DESCRIPTION	APPROX. QTY. AND UNITS	UNIT PRICE		EXTENSION	
				DOLLARS	CTS	DOLLARS	CTS
18	P-620	Temporary Non-Reflectorized Pavement Marking (Yellow)	1,098 S.F.				
19	D-705	Perforated Underdrain (Schedule 40) (4")	458 L.F.				
20	D-705	Perforated Underdrain (Schedule 80) (4")	560 L.F.				
21	D-705	Non-Perforated Outlet Pipe (Schedule 40) (4")	19 L.F.				
22	D-705	Non-Perforated Outlet Pipe (Schedule 80) (4")	313 L.F.				
23	D-705	Underdrain Cleanout Riser	7 Each				
24	D-705	Splash Pad	2 Each				
25	TREC	Erosion Control Blanket, Type 2C <i>(Revised per Addendum No. 1)</i>	3,974 S.Y.				
26	SPEC.	Temporary Seeding <i>(Revised per Addendum No. 1)</i>	3,974 S.Y.				
27	T-901	Permanent Seeding <i>(Revised per Addendum No. 1)</i>	3,974 S.Y.				
BASE BID TOTAL							

BID ITEM	FAA or MoDOT SPEC.	ITEM DESCRIPTION	APPROX. QTY. AND UNITS	UNIT PRICE		EXTENSION	
				DOLLARS	CTS	DOLLARS	CTS
ADD ALTERNATE NO. 1							
REHABILITATE SHADE HANGAR TAXILANE							
1	P-101	Clean and Seal Joints and Cracks in Asphalt Pavement	500 L.F.				
2	P-101	Route, Clean, and Seal Cracks in Concrete Pavement	150 L.F.				
3	P-101	Temporary Non-Reflectorized Pavement Marking (Yellow)	134 S.F.				
4	P-620	Permanent Reflectorized Pavement Marking (Yellow)	134 S.F.				
5	P-620	Permanent Non-Reflectorized Pavement Marking (Black)	268 S.F.				
6	P-631	Refined Coal Tar Emulsion with Additives for Slurry Seal	578 S.Y.				
ADD ALTERNATE NO. 1 TOTAL							

PRE-BID MEETING AGENDA

JESSE VIERTEL MEMORIAL AIRPORT (VER) BOONVILLE, MISSOURI

BASE BID

Reconstruct North T-Hangar Taxilanes

ADD ALTERNATE NO. 1

Rehabilitate Shade Hangar Taxilane

ADD ALTERNATE NO. 2

Rehabilitate South T-Hangar Taxilanes and South Connecting Taxiway

MoDOT PROJECT NO.: 23-039A-1

Wednesday, March 20, 2024 – 2:00 P.M. (CDT)

I. Introduction of Attendees

- A. See attached list of attendees.

II. Site Visit Opportunity

- A. Following the prebid conference, prospective bidders will have the opportunity to tour the project site. This is the only time prospective bidders will have to tour the project site.

III. Bid Opening

- A. Proposals will be received until **10:00 A.M. (CDT) on Tuesday, April 2, 2024 at Boonville City Hall, 401 Main Street, Boonville, MO 65233**. Bids received after this time will not be read.

Bids will then be immediately publicly opened and read at **Boonville Council Chambers, 525 E. Spring**.

- B. Envelopes must be sealed and addressed as shown on Page 2-1 of the Project Manual. Bids shall be addressed to the attention of the Boonville City Clerk.
- C. Bids may be held by the City of Boonville for a period not to exceed ninety (90) calendar days from the date of the bid opening. **Award of contract is contingent upon the City of Boonville receiving federal funding assistance from the Federal Aviation Administration (FAA).**

IV. Official Bid Form and Bidding Process

- A. Proposals must be submitted on the “Official Bid Form” which is separate from the Project Manual. The Official Bid Form consists of 12 pages and is provided by Quest CDN along with the Construction Plans and Project Manual.
- B. When completing the Official Bid Form, the unit price needs to be written in numerical form in the column under the header “Unit Price” and the extension (quantity x unit price) needs to be written in numerical form under the header “Extension”.
- C. All bidders submitting proposals must acknowledge receipt of all addendums issued in the space provided on Page 5 of the Official Bid Form.

- D. Official Bid Form Pages 8 through 12 need to be completed and signed, as required and applicable.
- E. If for some reason any of the pages of the Official Bid Form are changed by addendum, utilize the new pages when preparing your bid. The cutoff date for issuing an addendum is March 27th, 2024.
- F. As part of your proposal, you are required to complete the Worker Eligibility Verification Affidavit on Page 10 of the Official Bid Form. Note also that you are required to submit with your proposal a completed copy of the first page and a valid copy of the signature page of your E-Verify Memorandum of Understanding.
- G. Bidders shall submit with their bid package all required DBE information, as contained on the DBE forms located on Page 11 of the Official Bid Form.
- H. The successful bidder will be required to submit evidence of when and by whom each DBE was last certified after the bid opening.
- I. A Bid Bond guarantee will be required with each bid as a certified check or a bid bond in the amount of five (5) percent of the total amount of the bid, made payable to the City of Boonville, Missouri. Please include the bid bond with the Official Bid Form in the sealed envelope.
- J. The successful bidder will be required to execute the Performance Bond, Payment Bond, and Contract Agreement forms located in the back of the Project Manual. These bonds will be in the amount of 100% of the contract price.
- K. Please note that per Section 5, Item C-105, Mobilization of the Project Manual, the unit price for Mobilization shall not exceed 10% of the total bid amount for the Base Bid.
- L. Intentions are to execute contracts after receiving concurrence in award from MoDOT Aviation, and then issue a Notice-To-Proceed (NTP) to begin construction as soon as practicable. The early NTP date for construction is July 2024 and the late NTP date is April 2025.

V. Project Award Scenarios

- A. Award of contract will be evaluated on the following scenarios:
 - Base Bid
(Reconstruct North T-Hangar Taxilanes)
 - Base Bid + Add Alternate No. 1
(Reconstruct North T-Hangar Taxilanes and Rehabilitate Shade Hangar Taxilane)
 - Base Bid + Add Alternate No. 2
(Reconstruct North T-Hangar Taxilanes and Rehabilitate South T-Hangar Taxilanes and South Connecting Taxiway)
 - Base Bid + Add Alternate No. 1 + Add Alternate No. 2
(Reconstruct North T-Hangar Taxilanes and Rehabilitate Shade Hangar and South T-Hangar Taxilanes and South Connecting Taxiway)
- B. The FAA, MoDOT, and/or the City of Boonville reserve the right to select any of the combinations of the base bid and alternate bid(s).

VI. Contract Provisions

- A. Mandatory contract provisions are identified in Sections 3 and 4 of the Project Manual.

- B. The DBE goal for this project, as described on Page 1-5 of the Project Manual, is to subcontract **4.00%** of the dollar value of the prime contract to DBEs. It is the responsibility of the Prime Contractor to subcontract **4.00%** of the dollar value of the Base Bid to Disadvantaged Business Enterprises (DBE) or make good faith efforts to meet this DBE contract goal. The DBE Forms are located on Page 11 within the Official Bid Form.
- C. **The DBE goal is applicable only to the Base Bid portion of the project.**
- D. If the DBE goal is not met, there needs to be substantial documentation of good faith efforts (as described on Page 1-5 of the Project Manual) in attempts to attain the DBE goal.
- E. If applicable, sixty percent (60%) of the cost of materials or supplies purchased from a DBE-certified distributor may be counted. If the materials or supplies are obtained from a DBE-certified manufacturer, 100% of the cost may be counted as DBE participation.
- F. This contract is subject to the “Buy American Preferences” as explained on Pages 7, 8, and/or 9 of the Official Bid Form. Prospective Bidders are required to certify that manufactured products have been produced in the United States per the certification included within the Official Bid Form.
- G. The Contractor is required to provide a 10-hour OSHA construction safety program for all employees who will be on-site during the project as provided on Page 4-20 of the Project Manual.
- H. The Contractor and all Subcontractors will be required to pay minimum wage rates as established by the United States Department of Labor. Minimum wage requirements are identified within Section 4 of the Project Manual. Minimum wage requirements for this project are subject to the “Highway Types”. Both Federal and State wage rates are included. If there is a discrepancy between the two published wage rates, the higher of the two will be used as the requirement.
- I. The Contractor shall pay the minimum wage rate, as established previously, that corresponds to the appropriate task. Lochner’s RPR will be conducting random wage rate interviews during construction and will be monitoring and ensuring that the Contractor’s personnel are compensated for the amount stated within the wage rate section for the task that they are completing.
- J. All bidders shall provide a statement of qualifications shall be included with proposals highlighting past similar work, a financial statement, and a list of equipment along with year and overall condition. In lieu of the financial statement, Contractors may provide evidence that they are pre-qualified with MoDOT for similar work and are on the current MoDOT bidders list.
- K. The Contractor and his/her Subcontractors will be required to provide certificates of insurance for at least the minimum amounts specified in Section 4.
- L. Prior to procurement and upon the request of the successful bidder, the City of Boonville will provide the successful bidder a state tax exempt certification, applicable to materials only. This tax-exempt certification is not applicable to labor costs.
- M. The Prime Contractor shall perform, with their organization, an amount of work equal to at least 25% of the total contract cost per Section 3, General Provisions, Section 80-01 Subletting of Contract within the Project Manual.
- N. The Prime Contractor shall provide copies of all subcontracts to the Engineer prior to the Subcontractor being utilized on the project. See Section 3, General Provisions, Section 80-01 Subletting of Contract within the Project Manual for more information. Additionally, the Prime Contractor, and all Subcontractors, shall provide certificates of insurance for at least the minimum amounts specified in the Special Provisions.

- O. During construction, the first pay estimate shall be processed when necessary. All subsequent pay estimates will only be processed once the Prime Contractor has submitted lien releases from their Subcontractors. This will be discussed in further detail during the preconstruction meeting.

VII. Project Overview and Phasing

- A. This project consists of:

BASE BID

Reconstruct North T-Hangar Taxilanes

ADD ALTERNATE NO. 1

Rehabilitate Shade Hangar Taxilane

ADD ALTERNATE NO. 2

Rehabilitate South T-Hangar Taxilanes and South Connecting Taxiway

- B. This project has been structured into four (4) phases and consists of a total contract period of seventy-four (74) total calendar days from the date of the NTP. Phase 1 shall be completed in thirty (30) calendar days or less, Phase 2 shall be completed in forty (40) calendar days or less, Phase 3 shall be completed in two (2) calendar days or less, and Phase 4 shall be completed in two (2) calendar days or less. This construction time schedule includes estimated delays due to weather for a typical season.
- C. Phase 1 consists of work associated with the Base Bid portion of the project and Phases 2 and 3 consist of work associated with the Base Bid, Add Alternate No. 1 and Add Alternate No. 2 portions of the project. Phase 4 is to complete the permanent pavement marking application.
- D. Liquidated damages are set at \$1,700.00 per calendar day. Delays due to weather and other factors out of the control of the Contractor that are above and beyond a typical season may be requested in writing as a reason for the contract period extension. The request should be made as soon as the Contractor is aware of an issue with the construction period. The Contractor shall also make every attempt to make up any lost days by working extended periods during the day and/or weekends.
- E. All bidders should carefully review the Construction Safety and Phasing (CSPP) Report, Plans, Notes, and Details located within the Project Manual and on Plan Sheets 1.0 through 1.4. The Contractor shall also familiarize themselves with FAA AC 150/5370-2G which is included in the Appendix section of the Project Manual – Book 2 of 2. Low Profile Barricades and Closed Taxiway and Runway Markings shall be placed as shown on the Plan Sheets prior to the start of any phase. **The Contractor shall submit a Safety Plan Compliance Document (SPCD) detailing how they will comply with the Construction Safety and Phasing Plan (CSPP) Report. This SPCD must be approved by the Engineer prior to the issuance of the notice to proceed.**
- F. The Contractor shall access the project via the airport Access Road with the staging area located on the west side of the road.
- G. The Contractor is required to construct a stabilized construction exit for construction traffic to utilize prior to traversing onto the airport Access Road. Material and labor required to complete, maintain, and remove the stabilized construction exit shall be paid for under Base Bid Item “Stabilized Construction Exit”. A detail for the stabilized construction exit is on Plan Sheet 1.4.
- H. The Contractor shall notify the Airport Manager and the Engineer at least forty-five (45) days prior to the start of construction so that the Airport Manager can issue all NOTAMs to Flight Service with regards to the construction at the airport as well as notify airfield tenants of the anticipated airfield closure.

- I. As shown on General Note 12 on Plan Sheet 0.1, the Contractor shall be responsible for obtaining water, as required for completion of the project. The cost of this work shall be considered subsidiary to other contract items.
- J. Lochner will provide Construction Observation and acceptance testing throughout the project. The Contractor shall provide Quality Control Measures as outlined within Section 3, General Provisions, Section 100 of the Project Manual.
- K. Extreme caution shall be used when working near underground electrical cable and other utilities located around the project site. The Contractor is responsible for locating any underground utilities prior to construction beginning. The Contractor shall Missouri One Call System 1-800-DIG-RITE (1-800-344-7483) at least 48 hours prior to any construction activities to allow sufficient time to locate and mark any existing field cables or utilities which might be affected by this project. Should damage occur to any of these items, they will be replaced immediately at the Contractor's expense to the satisfaction of the Engineer and the Owner.
- L. All personnel associated with the Contractor and Subcontractors that are intending to operate a vehicle on airport property for this project will be required to complete an initial safety briefing during orientation, including requirements for operating a motor vehicle within the Airport Operations Area (AOA).
- M. A Contractor furnished Engineer's field office is not required.

VIII. Demolition

- A. The existing pavement and base course removal shall be disposed of off of Airport Property. Boring Logs are included with the Construction Plans and appendix section of the Project Manual, Book 2 of 2 depicting existing pavement thickness.
- B. The Contractor shall be cautious of the existing water line, underground utility lines, and electrical duct to remain called out on the Demolition Plan Sheets.
- C. Removal of existing markings on pavement to remain shall be accomplished by rotary grinding, water blasting, or other methods approved by the Engineer minimizing damage to the pavement surface. Heavy weight equipment will not be allowed on the existing pavement.

IX. Grading and Paving Construction

- A. Excess excavation shall be wasted on-site in an area located to the south of the existing hangars. A plan sheet will be added by addendum identifying this area and grading requirements.
- B. Proposed typical pavement section for the taxilanes consists of:
 - 6" PCC Pavement (P-501)
 - *4,400 psi 28-day compressive strength – addressed in Addendum No. 1*
 - 4" Aggregate Base Course (P-208, P-209, or P-219)
 - 10" Aggregate Subbase (P-154)
 - Compacted Subgrade (P-152)
- C. A separation geotextile is required when utilizing subbase course specification P-154 and shall meet the requirements of Note 1 on Plan Sheet 2.2. Materials and labor required to procure and install the separation geotextile are considered subsidiary to Base Bid Item "Aggregate Subbase Course (10)". Separation geotextile is not required when utilizing specification P-208, P-209, or P-219.

- D. If GPS controlled equipment is used to construct the subgrade, the Contractor shall have the area surveyed and approved by the Engineer prior to placement of base course.
- E. The taxilanes pavement shall be constructed with the use of acceptable slip form or side form paving equipment. Odd shaped irregular sections of pavement shall be considered hand pours and constructed with the use of side forms.
- F. Concrete incentive pay is available for this project based on strength and thickness up to 106% of each lot. This may be used to offset lots requiring less than 100% pay for strength and thickness. However, the total product of the contract unit price and the total number of square yards of concrete shall not exceed **100% pay**.
- G. The 1/4" chamfer as shown on the Pavement Joint Details on Plan Sheet 6.1 shall be constructed in order to protect the pavement joint during snow removal operations.
- H. Silicone joint sealant meeting the requirements of ASTM D5893 shall be used.

X. Pavement Rehabilitation (Add Alternates No. 1 and No. 2)

- A. Existing concrete pavement cracks on shade hangar approaches (Add Alt. No. 1) are to be routed, cleaned, and sealed and existing concrete pavement joints on south T-Hangar taxilanes and south connecting taxiway (Add Alt. No. 2) are to be cleaned and resealed per Plan Sheets No. 7.0 through 7.3. Note that sealant removal by routing methods is not allowed. The sealant shall be removed by sawing operations. Per Specification Section 17, Item P-605, cold applied silicone sealant (ASTM D5893) shall be used on this project.
- B. Existing asphalt pavement joints and cracks on shade hangar taxilane (Add Alt. No. 1) are to be cleaned and sealed and a seal coat applied to the pavement in accordance with Specification Section 19, Item P-631 "Refined Coal Tar Emulsion with Additives, Slurry Seal Surface Treatment".
- C. If Add Alternate No. 2 is awarded, Bid Item No. 2 "Unsuitable Base Course Removal (4")" and Bid Item No. 6 "Aggregate Base Course (4")" are included with this project in the event that the base material beneath the concrete panels being removed are not suitable for the new pavement and must be replaced. The square yards shown are an estimated quantity and will be adjusted as necessary for the appropriate amount of material required. Multiple aggregate options are provided in the specification.
- D. For partial and full concrete panel replacement, the construction of the 1/4" x 1/4" chamfer is to be completed with the construction of the new joint.
- E. Concrete pop-out locations shall be identified during construction by the Contractor and the RPR. Payment for concrete pop-outs shall not be measured individually but measured per lump sum. As of July 2022, 156 Pop-Outs were identified within the pavement rehabilitation limits of the project with an average diameter of 2".
- F. Concrete pavement material for joint spall repair shall be epoxy resin (Type III, Grade I), elastomeric concrete, rapid set concrete, or approved equal.

XI. Drainage Improvements and Site Restoration

- A. The Contractor is responsible for implementation of the Stormwater Pollution Prevention Plan (SWPPP) contained in the Appendix of the Project Manual – Book 2 of 2. Plan Sheet No. 5.2 details the erosion control measures necessary for the Project; silt fence and straw wattles.
- B. Type 2C erosion control blanket shall be utilized in all areas requiring grading operations as shown on Grading Plan Sheet No. 5.1.

- C. A Base Bid Pay Item for “Temporary Seeding” is included in the proposal form in the event that permanent seeding cannot be done after grading operations are completed.
- D. It is the Contractor’s responsibility to assure that no construction materials or excessive dust impacts the aircraft stored in the respective hangars. The Contractor will be responsible for any hangar and/or aircraft cleaning operations and associated cost due to any negligent acts on their behalf.

XII. Pavement Marking Application

- A. The contractor shall apply markings to the reconstructed T-Hangar taxilanes and taxilane rehabilitation areas if the add alternates are awarded as shown within the Pavement Marking Plan on Plan Sheets No. 8.0 to 8.2. A bid item for temporary yellow non-reflectorized markings is included in the proposal form.

XIII. Proposed Addendum Items

- A. Addendum No. 1 will include today’s prebid meeting minutes and attendees list.
- B. P-501 change from 28-day flexural strength to compressive strength.
- C. Fence removal and replacement detail.
- D. Excess excavation waste area location and grading detail(s).
- E. Updated base bid quantities for inclusion of excess excavation waste area location related to erosion control, seeding, and erosion control blanket.

XIV. Open Discussion

**JESSE VIERTEL MEMORIAL AIRPORT (VER)
BOONVILLE, MISSOURI**

BASE BID

Reconstruct North T-Hangar Taxilanes

ADD ALTERNATE NO. 1

Rehabilitate Shade Hangar Taxilane

ADD ALTERNATE NO. 2

Rehabilitate South T-Hangar Taxilanes and South Connecting Taxiway

**MoDOT PROJECT NO.: 23-039A-1
LOCHNER JOB NO. TO0118627**

ATTENDEES LIST FOR PRE-BID CONFERENCE

Wednesday, March 20, 2024 – 2:00 P.M. (CDT)

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