

ADDENDUM NO. 1

April 3, 2026

CUBA MUNICIPAL AIRPORT
Construct T-Hangar Building
MoDOT Project No. 25-059A-1

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[Handwritten Signature]
04/03/2026

DATE: April 3, 2026

ADDENDUM NO. 1

**CUBA MUNICIPAL AIRPORT
Construct T-Hangar Building
MoDOT Project No. 25-059A-1**

TO: All Plan Holders of Record

The following addendum items modify, change, delete from or add to, the requirements of the contract documents for this project. The articles contained in the addendum take precedence over the requirements of the previously published contract documents. Where any article of the contract specifications or any detail of the contract drawings is modified or any paragraph, subparagraph or clause thereof is modified or deleted by the articles contained in this addendum, the unaltered provisions of that article, paragraph, subparagraph or clause shall remain in effect.

Item No. 1 – Project Manual

General Provisions – Section 60-05 – Engineer/Resident Project Representative (RPR) Field Office

Delete: Section 60-05 is deleted.

Technical Provisions – Section 133419 – Metal Building Systems

Delete: Beginning on page 133419-14, Section 2.12, Overhead Door for T-Hangar Unit, is deleted.

Item No. 2 – Plan Sheets

Plan Sheet No. 4 – Construction Safety and Phasing Plan

Delete: Plan Sheet 4, issued March 17, 2026.

Insert: Plan Sheet 4, revised April 3, 2026 (attached).

Revise: Object heights on several critical points have been updated.

Plan Sheet No. 15 – Proposed Hangar Layout

Delete: Plan Sheet 15, issued March 17, 2026.

Insert: Plan Sheet 15, revised April 3, 2026 (attached).

Clarification: Support columns have been added in units 16 and 17.

Plan Sheet No. 16 – Proposed Hangar Elevations

Delete: Plan Sheet 16, issued March 17, 2026.

Insert: Plan Sheet 16, revised April 3, 2026 (attached).

Revise: Bi-fold door overall heights are revised to **14'-6"**.
Open bi-fold door clear heights are revised to **12'-0" Min.**

Revise: All references to "MIN. 24 GA." metal panels are revised to "**MIN. 26 GA.**"

Plan Sheet No. 17 – Hangar Notes

Delete: Plan Sheet 17, issued March 17, 2026.

Insert: Plan Sheet 17, revised April 3, 2026 (attached).

Revise: Note 23 is revised to reflect Interior Partition Panels as "**Aluminum-Zinc Alloy / Galvalume Coating.**"

Item No. 3 – Meeting Minutes and Sign-In Sheet

The minutes and sign-in sheet to the Pre-Bid Meeting, held on March 31, 2026, are attached.

Item No. 4 – Bidder Questions

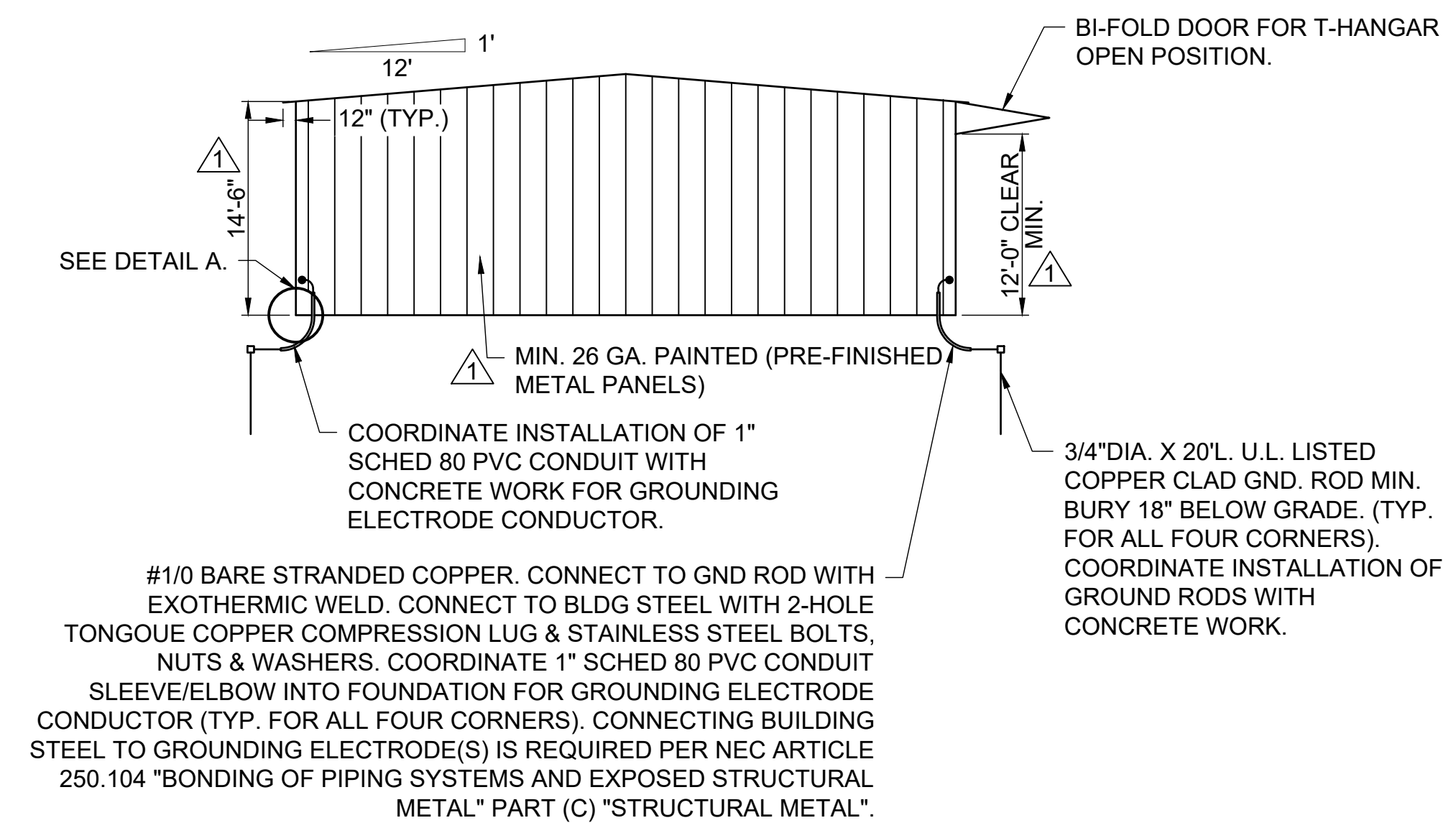
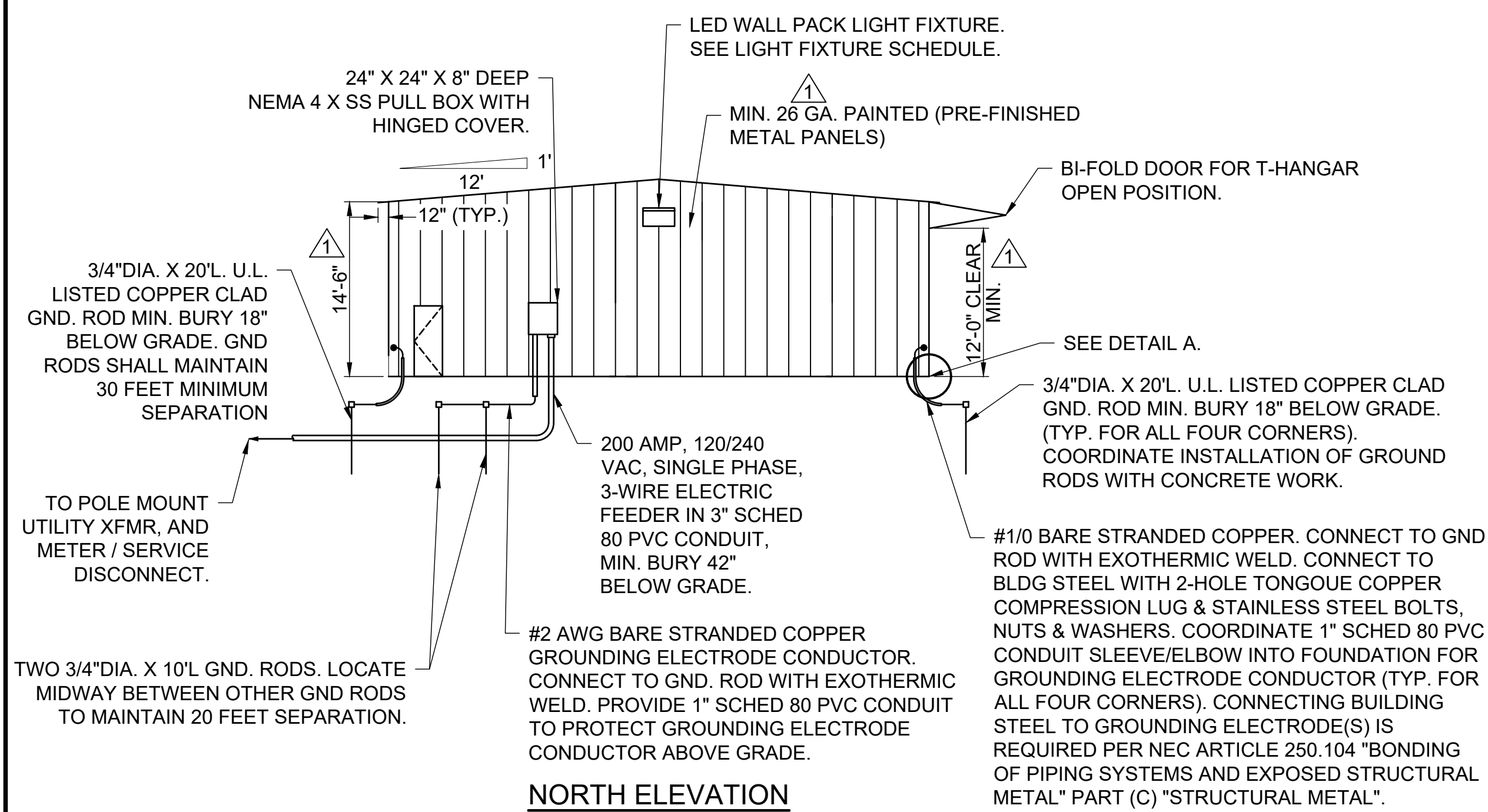
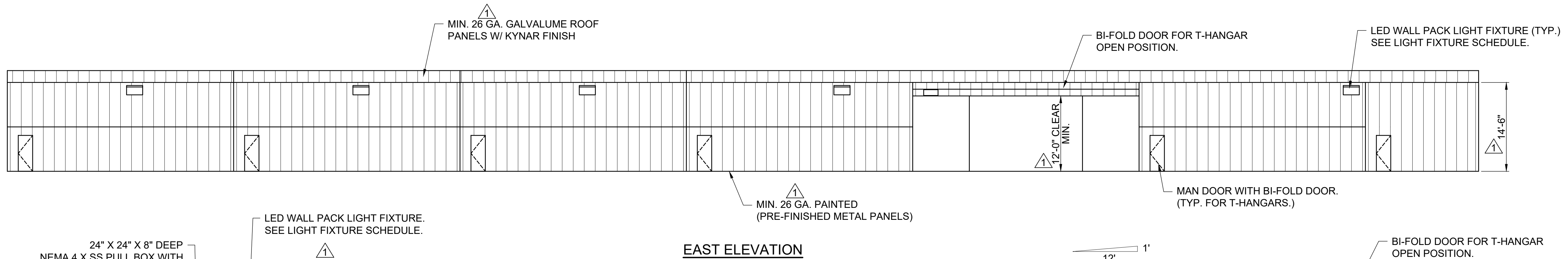
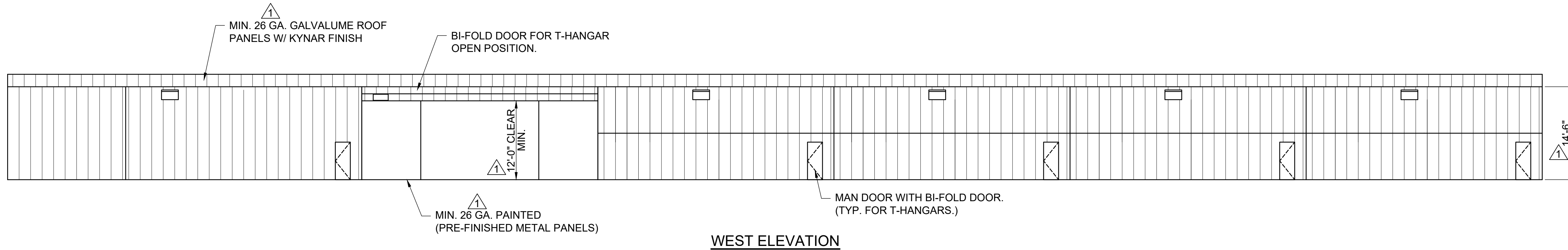
- Q1:** The specifications indicate 26 ga exterior wall and roof panels while the drawings indicate 24 ga. Please clarify the gauge of the exterior panels.
- A1:** Refer to Item 2 – Plan Sheets, Plan Sheet No. 16, of this Addendum.
- Q2:** The specification seems to indicate that the interior partition panels are to be painted white. Building panels are only painted on one side with the back side of the panel having an off-whitewash coat. Since both the front and the back of the partition panels can be seen in any one unit, partition panels are normally unpainted galvalume since the two white colors do not match perfectly. Please clarify if the interior partition panels can be unpainted galvalume.
- A2:** Refer to Item 2 – Plan Sheets, Plan Sheet No. 17, of this Addendum.
- Q3:** Our bifold doors have exposed galvanized framing when the door is closed, as do our structural roller columns. Please clarify if exposed galvanized framing is acceptable for the project.
- A3:** This is acceptable.
- Q4:** The specifications indicate some 10' x 12' overhead doors but none are shown in the building plans. Please clarify the location of these doors.
- A4:** Refer to Item 1 – Project Manual, of this Addendum.
- Q5:** The specifications indicate that the door operator is to be compatible with the on-site electric. Please note that we require a 220v to operate our bifold doors and that we utilize a 2-button constant pressure system – drawing attached. No components will be explosion proof.
- A5:** Please refer to Note 20 on Plan Sheet 17. The 2-button constant pressure system is acceptable. Regarding no components being explosion proof, please refer to the large-print note on Plan Sheet 19 and Note 2 on Plan Sheet 29. The door motor would need to

be installed more than 18 inches above the hangar floor to remain outside of the hazardous location.

- Q6:** Drawing #15 shows the end units as having no interior support columns in the floor area. Can there be interior columns where the partition corners would normally be or do these units have to be clear span?
- A6:** Refer to Item 2 – Plan Sheets, Plan Sheet No. 15, of this Addendum.
- Q7:** Drawing #16 show the 42' bifold doors as having a 14'-0" clear height. Hangars with 42' wide door normally have a 12' clear height due to the class of aircraft being stored. What is the bifold door clear height required for the project?
- A7:** Refer to Item 2 – Plan Sheets, Plan Sheet No. 16, of this Addendum.
- Q8:** Note 30 on drawing #17 indicated R19 roof insulation. Insulation thicker than 4" (R13) is not recommended since the screws tend to pull through the panel when the insulation is that thick. Can the roof insulation be changed to 4" thick since it is likely only to prevent condensation?
- A8:** Minimum R19 roof insulation will be required.
- Q9:** Page 1-3 in the project manual states a goal of 11.4 and 6.9 for MBE/WBE companies, however the last paragraph on this page also states there has been a goal set of 0% for DBE companies. Can you please clarify what the MBE/WBE/DBE goals are on this project?
- A9:** The goals stated at the top of page 1-3 are referring to minority and female participation goals for the contractor's workforce, not for subcontractors. For DBE goals please refer to the DBE section at the bottom of page 1-3.
- Q10:** Does the GC need to be pre-qualified with state of MO highway division?
- A10:** Please refer to General Provision 20-02, Qualification of Bidders, in the Project Manual.
- Q11:** Section 80.01 states the GC must perform at least 25% of the contract cost. We would plan to self-perform as much work as we could, but we would sub out certain trades as well. We are asking for this requirement to be waived.
- A11:** Please follow the requirements as stated in General Provision 80-01 in the Project Manual.
- Q12:** What is the anticipated Notice to Proceed?
- A12:** Following the bids, the City will apply for a grant through MoDOT and the FAA. We anticipate it will be months before the grant is fully executed, at that time we would proceed with the project administration, shop drawings, etc. The Notice to Proceed for actual site work would be up to the contractor to determine, and at that time the calendar days would start being counted.
- Q13:** Can you confirm who is to provide and pay for builders risk insurance?
- A13:** Builder's risk insurance is not a current requirement in the project documents.

- Q14:** Can you confirm the third party inspection and testing is to be paid by the owner?
- A14:** Acceptance testing for subgrade compaction, concrete testing, asphalt testing will be performed by Hanson and paid by the owner. The contractor is still required to meet their own quality control testing requirements in the specifications, as applicable.
- Q15:** Is the owners intent to proof roll the site and deal with remediation if required by change order based on unit prices?
- A15:** Refer to Technical Provision P-152 for subgrade construction requirements. If there are scope items required that are not clearly included in the project plans and project manual, that may be discussed as extra work in accordance with General Provision 40-04.
- Q16:** Is the owners exiting utilities available onsite for us to use during construction?
- A16:** There is nearby electric that the Contractor can coordinate through the serving utility. Any use of nearby water would also need to be coordinated through the serving utility. Contractor is required to provide their own restroom facilities.
- Q17:** Are liquated damages \$1500/day or \$1000/day?
- A17:** Please refer to General Provision 80-08 in the Project Manual.
- Q18:** Will we need to install an AOA fence to establish are project site outside the limits?
- A18:** No.
- Q19:** Who is responsible for providing weld inspections for the building construction?
- A19:** The Contractor shall provide third-party inspections for building welds and include with their bid.

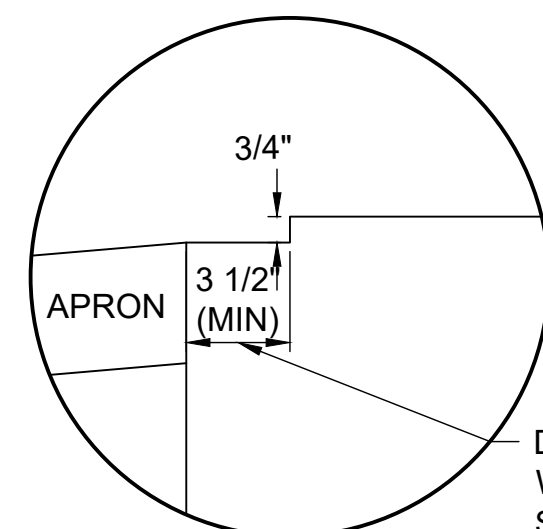
End of Addendum 1 Text.



NORTH ELEVATION

SOUTH ELEVATION

**12-UNIT T-HANGAR (BASE BID +
ADD ALT 2) ELEVATIONS**
NOT TO SCALE



DETAIL A

DIMENSION TO BE COORDINATED WITH BUILDING MANUFACTURER. SEE ADDITIONAL INFORMATION ON TYPICAL SECTION SHEET.

NOTES:

- PLEASE NOTE THAT NO ARCHITECT'S OR STRUCTURAL ENGINEER'S SEAL IS PROVIDED WITH THESE CONSTRUCTION PLANS. THE DIMENSIONS ARE APPROXIMATE. THE CONTRACTOR SELECTED WILL BE REQUIRED TO PRODUCE AND SUBMIT THE ACTUAL PLANS FOR THE BUILDING ITSELF, INCLUDING A SEAL FROM AN ARCHITECT OR STRUCTURAL ENGINEER LICENSED IN THE STATE OF MISSOURI.
- THE PROJECT PLANS ARE DRAFTED TO REFLECT THE BASE BID + ADD ALT 2. IN THE EVENT THAT ONLY THE BASE BID IS AWARDED, A BULLETIN WILL BE ISSUED TO REFLECT A 10-UNIT NESTED T-HANGAR, WHERE UNITS #12 AND #19, AS LABELED ON SHEET 15, ARE ELIMINATED.
- HANGAR CLASSIFICATION: NFPA 409 GROUP III, TYPE II (000) CONSTRUCTION WITH A MAXIMUM SINGLE FIRE AREA OF 12,000 S.F.
- PROVIDE HANGAR IDENTIFICATION/NUMBER LABELING ON THE MAN DOOR TO EACH HANGAR.
- SEE "HANGAR NOTES" SHEET.
- COORDINATE ELECTRICAL WORK WITH BUILDING WORK, CONCRETE WORK, AND SITE WORK.
- BUILDING HEIGHT DIMENSION IS APPROXIMATE AND WILL ULTIMATELY BE DETERMINED BASED ON SELECTED HANGAR MANUFACTURER AND MODEL NUMBER.

FOR BID

**CONSTRUCT
T-HANGAR BUILDING**

Project 25-059A-1

Cuba Municipal Airport
Cuba, Crawford Co., MO

NO.	DATE	DESCRIPTION			
		DES	DWN	REV	
△	04/03/26	ADDENDUM 1	BSS	MRG	BSS

ISSUE: MARCH 17, 2026
PROJECT NO: 24A0111.00
CAD FILE: A-201-ELEV.DWG
DESIGN BY: KNL 02/18/2026
DRAWN BY: MRG 01/09/2026
REVIEWED BY: KNL 03/04/2026

SHEET TITLE

**PROPOSED HANGAR
ELEVATIONS**

PRE-BID CONFERENCE MEETING AGENDA / MINUTES

**CUBA MUNICIPAL AIRPORT, CUBA, MO
MARCH 31, 2026 – 10:30 AM LOCAL TIME**

1. Introductions
 - See attached sign-in sheet for list of attendees.

2. Project Information

**CONSTRUCT T-HANGAR BUILDING
STATE BLOCK GRANT PROJECT NO. 25-059A-1**

BIDS DUE: APRIL 14, 2026, 2:00 PM LOCAL TIME AT CUBA CITY HALL

Barry noted that the project information is posted through other online plan rooms, however the Drexel Technologies plan room is the official source of information, and the location to obtain electronic or hard copies of the project documents.

3. Project Plans & Specifications Review

- a. Scope of Work

- Base Bid: Construct 10-unit nested T-hangar building
- Additive Alternate 1: Demolish existing building/foundation
 - The demolition could potentially be performed by the City, which is why it is broken out separately from the Base Bid. In order to construct the Base Bid, the demolition will need to occur, either by the City self-performing or by awarding Additive Alternate 1.
- Additive Alternate 2: Construct an additional 2 units (for a total of 12)
- Additive Alternate 3: Construct a bituminous taxilane for the 10-unit building
- Additive Alternate 4: Construct a bituminous taxilane for the additional 2-units

- Bidders are to submit a complete bid consisting of completing all schedules in full. The low bid and award will be based on a combination of the pieces that are affordable within the available project funds. This combination is to be determined by the City of Cuba to best serve their interests on the project.

- b. Project Schedule:

- Contract time is 180 consecutive calendar days from start of construction on site.
 - Refer to General Provisions 80-08, which also indicate liquidated damages.

- Project start dependent on timing of FAA grant process and approvals from MoDOT and the City of Cuba. Following the receipt of the grant funds and the City's award of the project, the timing of the start of construction will be up to the awarded contractor. Hanson and the City will not dictate the schedule, although sooner than later is always preferred.

- Bids will be able to be held for 120 days prior to award.

- To ensure the award process progresses smoothly, the low bidder needs to be sure their documentation is completed properly and fully. Even if the DBE Goal is zero and you intend to use a DBE subcontractor or supplier, please keep good documentation of the DBE efforts.
- Submittals for materials to be used on the project will need to be approved prior to beginning site work. Contractor is expected to have enough quantity in their possession to complete the project without unnecessary delays.

c. Construction Safety Plan

- Work Area
- Construction Access / Staging
 - Contractor trailer can be located in/near the staging area location.
 - Contractor is not required to provide an engineer's field office/trailer.
 - Due to some concern about heavy traffic on the asphalt taxiway shown on the Safety Plan, the addition of an alternate haul route across the concrete apron will be added via addendum.
- Maintenance of Traffic
- Stockpile Areas
 - Excess clean fill material can be placed/stockpiled on airport property at a location to be coordinated with the Airport, but generally located south of the terminal apron area and east of the runway.

d. Construction Plans and Notes

e. Project Manual

- Notice to Bidders
- Instructions to Bidders
 - Please note that bids are to be submitted and opened at City Hall and not at the Airport.
- General Provisions
 - Section 20-02 notes that a bidder may be deemed prequalified to bid the project if they are prequalified with MoDOT. Otherwise submit for prequalification for Hanson's evaluation, ideally prior to the bid opening.
- Supplementary Provisions
 - Part A – Federal and State Provisions
 - Part B – DBE Administration
DBE Goal: 0%
 - Part C – Local Provisions
Insurance Requirements
 - Part D – Wage Rates
Davis-Bacon and MO DOL
(In the event of a conflict, the higher of the two rates shall apply.)
- Technical Specifications
 - C-105 Mobilization

- Mobilization shall be limited to 10% of the total project cost.
 - Please review the payment schedule for mobilization prior to bidding.
 - P-403 Asphalt Mix Pavement Base/Surface Course
 - There is no profilograph requirement for this project.
 - Part H Hangar Construction Specifications
 - 133419 Metal Building Systems
 - Appendices
 - Appendix A – Construction Safety and Phasing Plan (CSPP)
 - Appendix B – Advisory Circular 150/5370-2G
 - Appendix C – Geotechnical Investigation Report
 - Proposal Forms
 - Bid Submittal Checklist
 - Please utilize the checklist to account for all of the forms required to be submitted with the bids.
 - Proposal Form (PF-1 – PF-16)
 - Federal Certifications
 - Certificate of Buy American Compliance (PF-8 – PF-9)
 FAA reference material available at http://www.faa.gov/airports/aip/buy_american/
 Please make sure to review the Buy American Compliance for all products to be installed on the project, otherwise the project may require a waiver from the FAA. If a product is already on the FAA Nationwide Waiver list it will not require a further waiver request. [Hanson reviewed the compliance with the FAA Buy American Preference. There is a certification form to be submitted with the bid, that the bidder will indicate compliance with the 100% Buy American Preference, or may qualify for a Type 3 waiver \(meeting more than 60% Buy American Preferences.\) There are waiver forms required to be completed by the low bidder and submitted to Hanson, who will submit to the FAA for review/approval. Hanson intends to assist the low bidder in this process as best as possible to allow the waiver process to be completed quickly so the grant can be issued by the FAA.](#)
 - Tax Delinquency (PF-14)
 - Debarment (PF-15)
 - Nonsegregated Facilities (PF-16)
 - Worker Eligibility Verification Affidavit (PF-11)
 - DBE Participation (PF-12)
 - Bid Security (5%)
4. Deadline for Questions – Tuesday, April 7, 2026, Close of Business
 - All requests for interpretations, questions and comments should be emailed to Barry Stolz at bstolz@hanson-inc.com.
 - Responses will be provided via addendum by Friday, April 10, 2026.
5. Addendum
 - Pre-bid Conference Meeting Minutes
 - Minor clarifications
 - Contractor Questions (if any)

6. Questions / Comments

- All questions after the pre-bid meeting shall be submitted via email to Hanson via Barry Stolz (bstolz@hanson-inc.com).
- Hanson will have staff on site to inspect foundation rebar prior to pouring concrete.
- Acceptance testing for subgrade compaction, concrete testing, asphalt testing will be performed by Hanson and paid by the owner. The contractor is still required to meet their own quality control testing requirements in the specifications, as applicable.
- A question was asked about responsibility for weld inspections. Hanson will address via addendum.
- Any new damage to existing pavements is the responsibility of the contractor and is noted in the plans and project manual. It is recommended that existing conditions be documented with photos/video prior to construction beginning.
- A question was asked about having airport band radios while on site. Hanson indicated that the Airport and Hanson staff will have radios, however in the work area for this project the use of a radio will not be necessary, as that is typically to monitor use of the runway, not the apron/taxilane area.
- A question was asked regarding grounding for the entire building. Hanson indicated to follow the electrical plans/notes in the Plan Set.
- All permits/fees are the Contractor's responsibility. The City of Cuba is the authority having jurisdiction over this project. The Airport is on City property.
- The siren located on a pole to the north of the existing hangar is to remain in place and should not interfere with the proposed building limits. Power to the siren will need to be reestablished prior to demolition of the existing hangar, per Plan Sheet 20, Electrical Site Plan.

7. Site Visit

- A site visit was conducted with all interested attendees.



PRE-BID MEETING SIGN-IN SHEET

MEETING DATE: March 31, 2026 @ 10:30 A.M.

PROJECT NO.: 24A0111.00

PROJECT NAME: UBX Construct T-Hangar Building, Project 25-059A-1

PROJECT MEETING LOCATION: UBX Terminal Building

NAME	COMPANY	PHONE	E-MAIL
Barry Stolz	Hanson Professional Services	314-942-5288	bstolz@hanson-inc.com
Kim Masters	MoDOT Aviation	573-526-5201	Kimberly.Masters@modot.mo.gov
Travis Pritchett	Cuba Municipal Airport	573-578-5188	cubamoairport@gmail.com
Jake Sbabo	Sbabo Const. LLC	573-263-0626	Sbaboconstruction@hotmail.com
Mason Parker	BuildTec Const.	573-263-4494	mparker@buildteccon.com
Jason Petty	BuildTec Const	573-434-2482	jpetty@buildteccon.com
BRETT STICKLER	M-NEL ELECTRIC	577-261-0030	bretts@mcnelelectric.com
KIMLY TURNAGE	SBABO CONSTRUCTION	573-466-6584	SBABOCONSTRUCTIONLLC. @OUTLET.com
PJ Myers	PJ Myers Hauling & Exc	573-259-9422	Pjmyerscuba@yahoo.com
Josh Siebeneck	Brown and Root	573-301-8202	Josh.Siebeneck@BrownandRoot.com
Jim Rudolph	Wright construction	636-220-6850	Bids@wrightconstruc.com
Heagan Sanders	Courtney Construction	417-926-1479	estimating@courtneyconstructionllc.net
John Foster	Airport Board	573-259-1473	
Alex Anxier	Alexander construction	573-263-4369	alex.c.anxier@gmail.com