



SAFETY

IMPROVEMENTS PROJECT
A LIFESAVING PARTNERSHIP

CLARKSON ROAD IMPROVEMENTS

Project Summary

MoDOT and St. Louis County have formed a lifesaving partnership to make \$52 million worth of vehicle and pedestrian safety improvements at more than 230 locations throughout the City of St. Louis and St. Louis and Jefferson Counties. These improvements, installed from early 2024 to summer 2026, are estimated to result in a **savings of \$1.2 billion** and a **reduction of 170 fatal and serious crashes over 10 years.**

5,321

CRASHES WHERE AT LEAST ONE PERSON WAS KILLED OR SERIOUSLY INJURED IN THE CITY OF ST. LOUIS, ST. LOUIS COUNTY AND JEFFERSON COUNTY FROM 2016 – 2020

\$52 MILLION

WORTH OF VEHICLE AND PEDESTRIAN SAFETY IMPROVEMENTS BY 2026

230 LOCATIONS

THROUGHOUT THE CITY OF ST. LOUIS AND ST. LOUIS AND JEFFERSON COUNTIES

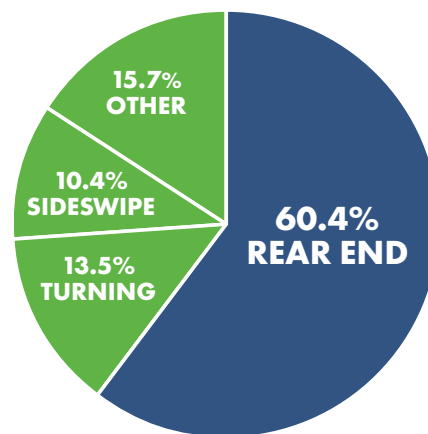
Clarkson Road Overview

USING INDUSTRY PRACTICES, WE ANALYZED CRASH HISTORIES AND HERE IS WHAT WE LEARNED ABOUT CLARKSON ROAD:

- During the last five years, there have been **606 crashes** between Clayton Road and Chesterfield Parkway.
- **60% were rear end crashes**, caused by traveling too fast for the conditions, not leaving enough room for the vehicle in front and/or lack of driver attention.

CRASH HISTORY

606
CRASHES IN FIVE YEARS



For more detailed information on the improvements on Clarkson Road and other locations please visit: modot.org/SLSafetyProject



Data collected from reports submitted to the Missouri State Highway Patrol by emergency management partners including law enforcement, fire and EMS.

Making Clarkson Road Safer

MoDOT plans to help reduce all crashes, especially those that result in serious or fatal injuries along Clarkson Road from south of Clayton Road to Chesterfield Parkway, by installing several different types of safety improvements. Such improvements include retroreflective backplates on signal heads, curve warning signs, improved right turns as well as median and curb bump out islands.

These improvements combined are estimated to reduce crashes on this segment of Clarkson Road by **97 crashes over five years**.



Curve Warning Sign

Retroreflective Backplate



Why Curb Bump Out Islands?

Traffic calming improves a driver's perception of speed and encourages better driver decision-making. In St. Louis and similar cities such as Milwaukee and Nashville, previous installations of traffic calming measures show that **drivers will drive more cautiously, which reduces the frequency and severity of crashes**.

Based on the *Highway Safety Manual's* methodology, previous projects show that traffic calming can **reduce driver, pedestrian and cyclist crashes by 32%**. Curb bump out islands, a type of traffic calming, not only provide a level of protection for motorists but also for bicyclists and pedestrians using the shoulder or sidewalk.

Without curb bump out islands, the estimated reduction of 97 crashes drops to 45 crashes reduced over five years by the remaining safety improvements.

Frequently Asked Questions (FAQs)

Will drivers be able to use the shoulder during an emergency?

The curb bump out islands are far enough apart that, assuming 25 feet per vehicle, at least 8 cars can fit in between them. Specifically, the islands are 30-feet long and more than 200 feet apart. This leaves almost 90% of the shoulder for drivers to use during an emergency.

How will MoDOT make these islands visible?

The end of each island nearest to approaching drivers is coated with reflective yellow paint. 36-inch-tall posts with white reflective strips are installed along the length of the island.

How will MoDOT plow the shoulder?

MoDOT generally focuses on clearing the lanes first, moving snow onto the shoulder. The 36-inch-tall delineator posts are expected to be visible during snow events.

Why didn't MoDOT hold a public meeting?

Improvements along Clarkson Road are not designed to modify traffic lanes or cause traffic delays; therefore, MoDOT would not usually hold a public meeting. However, MoDOT is re-evaluating how different types of improvements might affect the public in different ways.

What is MoDOT doing with the public's feedback?

We have heard the concerns from you and many other citizens and officials regarding the one safety measure of the curb bump out islands. In response to these concerns, MoDOT has stopped the contractor from construction of any additional curb bump out islands. MoDOT will further evaluate this contract, work with local officials, and follow up with more information soon.

Is there anything else MoDOT could have done to reduce Clarkson Road traffic crashes?

MoDOT could widen the roadway, build an interchange, or restrict access points and vehicle turning movements. However, the majority of the funding for this project came from the federal government and was designated specifically for safety improvements around the St. Louis area.

Won't these make Clarkson Road less safe for bicyclists and pedestrians?

1 in 5 crashes in St. Louis that involve a bicyclist result in a serious or fatal injury. That's 1 in 3 for pedestrians. Curb bump out islands improve safety for all users by providing physical separation and encouraging more cautious drivers.

