


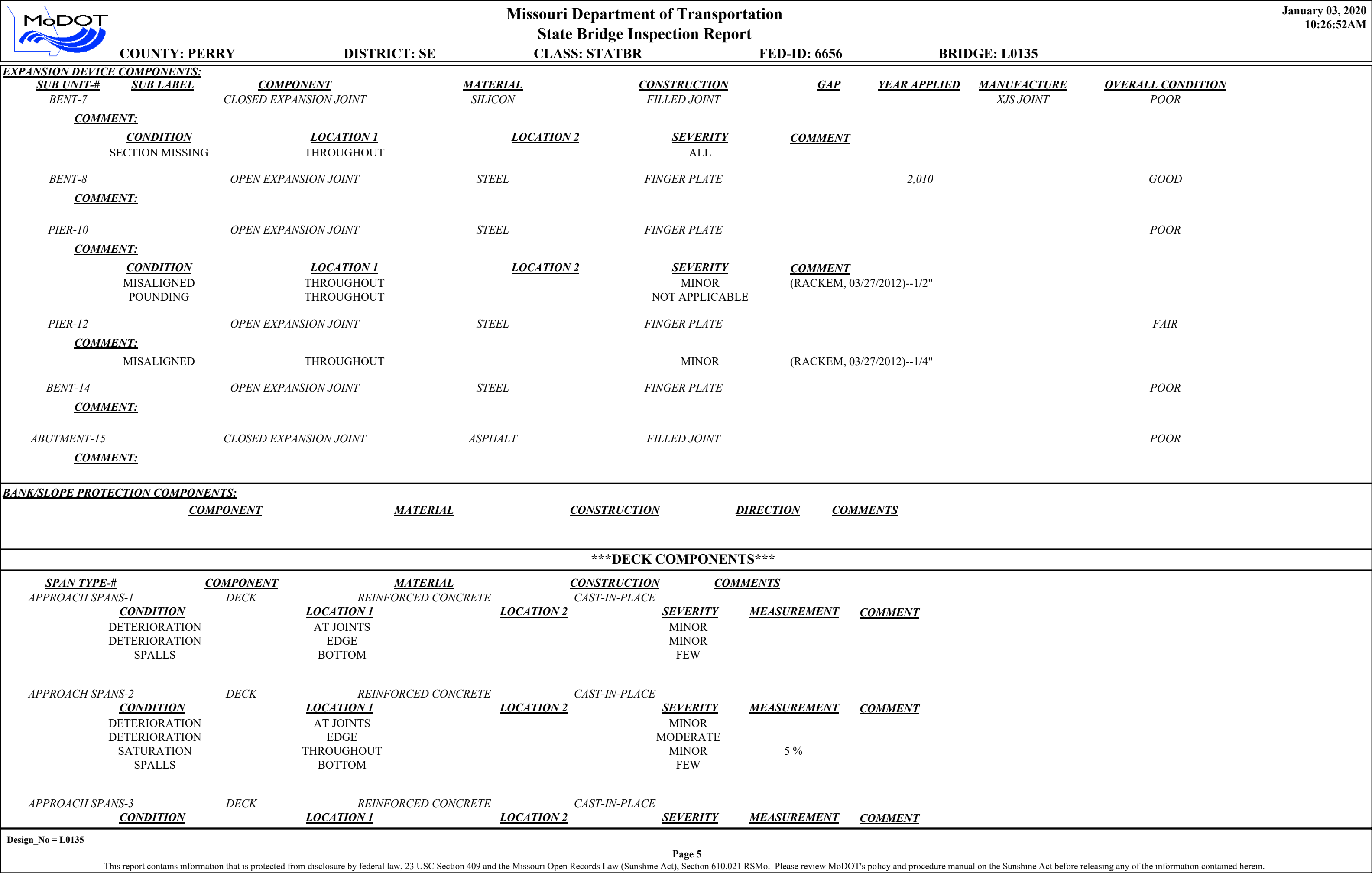
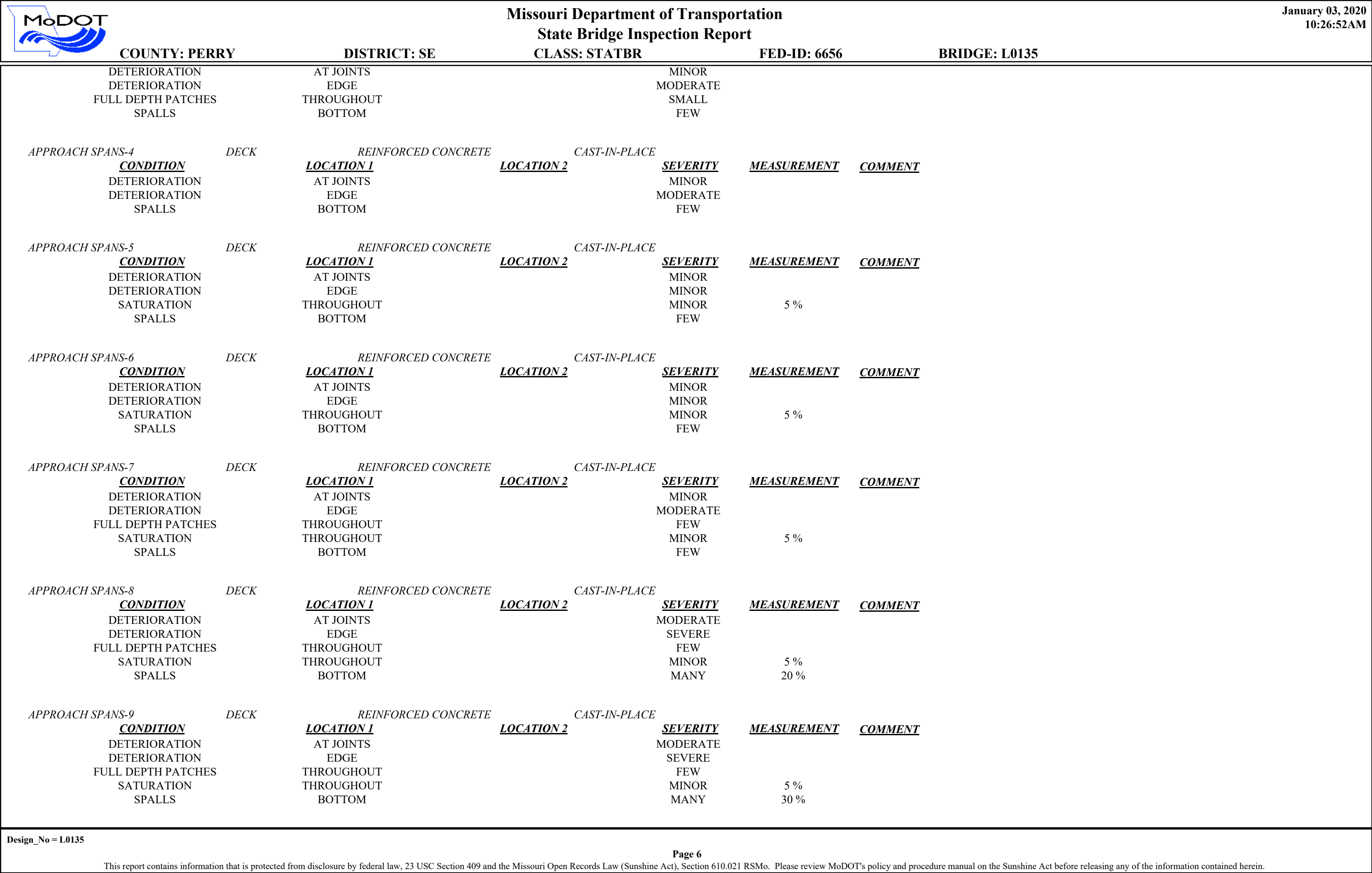
		<b>Missouri Department of Transportation</b>		<b>January 03, 2020</b>		
		<b>State Bridge Inspection Report</b>		<b>10:26:52AM</b>		
<b>COUNTY: PERRY</b>		<b>DISTRICT: SE</b>		<b>CLASS: STATBR</b>		
		<b>FED-ID: 6656</b>		<b>BRIDGE: L0135</b>		
<b>***GENERAL STRUCTURE INFORMATION***</b>				<b>***BRIDGE INSPECTION INFORMATION***</b>		
<b>ROUTE: MO51S</b> <b>FEATURE: MISSISSIPPI RVR, UP RR,</b> <b>STATUS: B-NOT CORR</b> <b>LOG MILE: 0.000</b> <b>DETOUR: 88.40 MILES</b> <b>NHS: NO</b> <b>BUILT: 1942</b> <b>REHAB:</b> <b>LOCATION: S 1007 T 37 R 11 E</b> <b>LATITUDE: 37 54 2.72 (DMS)</b> <b>LONGITUDE: 89 50 19.61 (DMS)</b>		<b># SPANS: 14</b> <b>LANES ON: 2</b> <b>LANES UNDER: 2</b> <b>COMPASS DIRECTION: SOUTH to NORTH</b> <b>DIRECTION OF TRAFFIC: 2-WAY TRAF</b> <b>FUNCTIONAL CLASS: RL-MINOR ARTERIAL</b> <b>NBI OWNER: MODOT</b> <b>NBI MAINTAINED: MODOT</b> <b>MAINTENANCE DISTRICT: SE</b> <b>MAINTENANCE COUNTY: PERRY</b> <b>SUB AREA: 7H36</b>		<b>PLACE CODE: 06886</b> BOIS BRULE <b>LENGTH: 2,827 FT 0 IN</b> <b>MAXIMUM SPAN: 670 FT 0 IN</b> <b>APPROACH ROADWAY: 22 FT 0 IN</b> <b>CURB TO CURB: 22 FT 0 IN</b> <b>OUT TO OUT: 22 FT 8 IN</b> <b>AADT: 8788</b> <b>AADT YEAR: 2019</b> <b>AADT TRUCK: 23.6%</b> <b>FUTURE AADT: 11864</b> <b>FUTURE AADT YEAR: 2039</b>		<b>DATE: 09/17/2018</b> <b>FREQUENCY: 24</b> <b>TEAM LEADER: JEFF MADSEN</b> <b>INSPECTOR 2: CURT RICKERSON</b> <b>INSPECTOR 3: KEVIN WEGENER</b> <b>RESPONSIBILITY: BRIDGEDIV</b> <b>CALCULATED INTERVAL**: 12</b> <b>ELEMENT: NO</b> <b>INSPECTOR 4:</b> <b>** When calculated interval exceeds the frequency, a justification comment per BIRM is required.</b>
				<b>GENERAL INSPECTION COMMENTS</b>		
<b>***FRACTURE CRITICAL INSPECTION INFORMATION***</b>			<b>***INDEPTH INSPECTION INFORMATION***</b>			
<b>DATE: 09/26/2019</b> <b>FREQUENCY: 12</b> <b>TEAM LEADER: JEFF MADSEN</b> <b>INSPECTOR 2: STEVEN EALEY</b> <b>RESPONSIBILITY: BRIDGEDIV</b> <b>CALCULATED INTERVAL**: 12</b> <b>INSPECTOR 3: STEVE HULBERT</b> <b>INSPECTOR 4:</b> <b>CATEGORY: THRU TRUSS</b> <b>NBI: YES</b> <b>METHOD: SNOOPER</b> <b>** When calculated interval exceeds the frequency, a justification comment per BIRM is required.</b>			<b>DATE:</b> <b>FREQUENCY:</b> <b>TEAM LEADER:</b> <b>INSPECTOR 2:</b> <b>RESPONSIBILITY:</b> <b>CALCULATED INTERVAL**:</b> <b>INSPECTOR 3:</b> <b>INSPECTOR 4:</b> <b>CATEGORY:</b> <b>NBI:</b> <b>METHOD:</b> <b>** When calculated interval exceeds the frequency, a justification comment per BIRM is required.</b>			
<b>FRACTURE CRITICAL INSPECTION COMMENTS</b>			<b>INDEPTH INSPECTION COMMENTS</b>			
<b>***SPECIAL INSPECTION INFORMATION***</b>			<b>***UNDERWATER INSPECTION INFORMATION***</b>			
<b>DATE: 05/11/2019</b> <b>FREQUENCY: 999</b> <b>TEAM LEADER: ED HESS</b> <b>INSPECTOR 2:</b> <b>RESPONSIBILITY: DISTRICT</b> <b>CALCULATED INTERVAL**: 85</b> <b>INSPECTOR 3:</b> <b>INSPECTOR 4:</b> <b>CATEGORY: DAMAGE POST INCIDE</b> <b>NBI: NO</b> <b>METHOD:</b> <b>** When calculated interval exceeds the frequency, a justification comment per BIRM is required.</b>			<b>DATE: 08/26/2016</b> <b>FREQUENCY: 60</b> <b>TEAM LEADER: STANTEC INC</b> <b>INSPECTOR 2:</b> <b>RESPONSIBILITY: CONSULTANT</b> <b>CALCULATED INTERVAL**:</b> <b>INSPECTOR 3:</b> <b>INSPECTOR 4:</b> <b>CATEGORY: DIVE</b> <b>NBI: YES</b> <b>METHOD: OTHER, BOAT, SCUBA</b> <b>** When calculated interval exceeds the frequency, a justification comment per BIRM is required.</b>			
<b>SPECIAL INSPECTION COMMENTS</b>			<b>UNDERWATER INSPECTION COMMENTS</b>			
(HESSE, 05/13/2019)--BARGE HIT AND LODGED AGAINST PIER 10. BARGE REMOVED AND HAD VISUAL INSPECTION BRIDGE OPENED TO TRAFFIC AROUND 11:30 AM. MINOR SPALL ON UPSTREAM SIDE OF PIER 10.						
<b>OTHER SPECIAL INSPECTIONS</b>			<b>OTHER UNDERWATER INSPECTIONS</b>			
<b>Design_No = L0135</b>						
<b>Page 1</b>						
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<u>DATE</u>	<u>FREQUENCY</u>	<u>CATEGORY</u>	<u>NBI</u>	<u>CALCULATED INTERVAL</u>	<u>RESPONSIBILITY</u>	<u>METHOD</u>	<u>DATE</u>	<u>FREQUENCY</u>	<u>CATEGORY</u>	<u>NBI</u>	<u>CALCULATED INTERVAL</u>	<u>RESPONSIBILITY</u>	<u>METHOD</u>
11/28/2018	24	CHANNEL CROSS SECTIONS	NO	27	BRIDGEDIV	OTHER							
09/17/2018	24	SUPERSTRUCTURE	NO	24	BRIDGEDIV								
09/12/2018	24	JOINT RIVER	NO	24	BRIDGEDIV								
04/25/2011	999	SCOUR ACTION PLAN	NO		BRIDGEDIV								
08/06/2009	999	GUSSET PLATES	NO		BRIDGEDIV	SNOOPERUB							
***STRUCTURE POSTING***													
<div><div>APPROVED CATEGORY: S-3</div><div>WEIGHT LIMIT 40 TONS.</div><div>Ton 1: 40</div><div>Ton 2:</div><div>Ton 3:</div><div>COMMENTS:</div></div>													
<div><div>FIELD CATEGORY: S-1</div><div>NO POSTING REQUIRED</div><div>Ton 1:</div><div>Ton 2:</div><div>Ton 3:</div><div>PROBLEM: SIGNMISSNG</div><div>PROBLEM DIRECTION: BOTH</div><div>COMMENTS:</div></div>													
***GENERAL COMMENTS/MAJOR RATED ITEMS***													
GENERAL COMMENTS: (KOENID, 06/19/2012)--7 @ (60') SMP WF - (251'-251') CONT DECK TRUSS - (670'-670') CONT THRU TRUSS - (252'-250') CONT DECK TRUSS - (60') SMP WF SPANS													
<div><div>[ITEM 58] DECK: 4-POOR CONDITION</div><div>RATING : 08/16/2013</div><div>COMMENTS: (MADSEJ, 01/04/2017)--APPROXIMATELY 50% SATURATION, DELAMINATIONS, PATCHES, AND SPALLS THROUGHOUT SPANS 10 AND 11.</div></div>													
<div><div>[ITEM 59] SUPER: 4-POOR CONDITION</div><div>RATING : 11/20/2013</div><div>COMMENTS: (MADSEJ, 01/04/2017)--ADVANCED SECTION LOSS IN THE BEARING AREAS OF MANY FLOORBEAMS THROUGHOUT SPANS 8 THRU 13 WITH HOLES RUSTED THROUGH THE WEBS IN THE BEARING AREAS.</div></div>													
<div><div>[ITEM 60] SUB: 4-POOR CONDITION</div><div>RATING : 01/04/2017</div><div>COMMENTS: (MADSEJ, 01/04/2017)--ADVANCED SECTION LOSS ON THE BENT 8 STEEL BEAMCAP WEB AND BOTTOM FLANGE WITH HOLES RUSTED THROUGH THE WEBS IN BOTH BEARING AREAS.</div></div>													
<div><div>[ITEM 61] BANK/CHANNEL: 7-MINOR DAMAGE</div><div>RATING : 11/16/2016</div><div>COMMENTS: (KOENID, 11/16/2016)--PER 2016 UW INSPECTION MINOR SLOUGHING AND A LACK OF VEGETATION NEAR THE EDGE OF WATER RESULTING IN RATING OF 7.</div></div>													
<div><div>[ITEM 113] SCOUR: 3-SCOUR CRITICAL-UNSTABLE</div><div>RATING : 01/19/2004</div><div>EVALUATION TYPE :</div><div>COMMENTS: (FOSTEK, 01/17/2006)--ITEM 113 RATING = 3 DUE TO USGS EVALUATION, SCOUR CATEGORY = A, SEVERE PILE EXPOSURE, LOCAL SCOUR. BENT 8 AND BENT 2 ONLY BENTS SHOWN AT RISK IN SCOUR ANALYSIS. ROADWAY WILL BE FLOODED DURING MAJOR FLOOD EVENTS. 100-YR FLOOD ELEVATION ABOUT 11.7 FEET BELOW DECK ELEVATION AT BENT 2. (KOENID, 11/16/2016)--2016 UW INSPECTION RATED SCOUR AT A 5 BECAUSE OF FOOTING EXPOSURE. USGS CALCULATED IT AT A 3, SO WE WILL NOT UPDATE THE RATING.</div></div>													
<div><div>[ITEM 71] WATERWAY ADEQUACY: DECK/APPRCH OVERTOP SLIGT</div><div>RATING : 05/18/2001</div><div>COMMENTS:</div></div>													
<div><div>[ITEM 72] APPRRDWY ALIGNMENT: 6-SATISFACTORY</div><div>RATING : 05/18/2001</div><div>COMMENTS:</div></div>													
***RAILING AND APPROACH PAVEMENT COMPONENTS AND RATINGS***													
<div><div>[ITEM 36A] BRIDGE RAILING RATING: DOESNT MEET CURRNT STND-0</div><div>RATING : 02/23/2004</div><div>COMMENTS:</div></div>													
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<div>Page 2</div> <div>This report contains information that is protected from disclosure by federal law, 23 USC Section 409 and the Missouri Open Records Law (Sunshine Act), Section 610.021 RSMo. Please review MoDOT's policy and procedure manual on the Sunshine Act before releasing any of the information contained herein.</div>													

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COUNTY: PERRY		DISTRICT: SE		CLASS: STATBR		FED-ID: 6656	
				BRIDGE: L0135			
<u>MATERIAL</u> STEEL		<u>CONSTRUCTION</u> DOUBLE RECTANGULAR TUBE		<u>DIRECTION</u> BOTH		<u>COMMENTS</u>	
<u>CONDITION</u> COLLISION DAMAGE		<u>LOCATION 1</u> THROUGHOUT		<u>LOCATION 2</u>		<u>SEVERITY</u> MINOR	
REINFORCED CONCRETE		CURB		BOTH			
<u>CONDITION</u> FAILING		<u>LOCATION 1</u> THROUGHOUT		<u>LOCATION 2</u>		<u>SEVERITY</u> BOTH	
<i>[ITEM 36B] TRANSITION RAILING RATING: NOT PROVIDED-0</i>				<i>RATING : 05/18/2001</i>		<i>COMMENTS:</i>	
<i>[ITEM 36C] APPROACH RAILING RATING: NOT PROVIDED-0</i>				<i>RATING : 05/18/2001</i>		<i>COMMENTS:</i>	
<i>[ITEM 36D] RAIL END TREATMENT RATING: MEETS CURRENT STANDARDS-1</i>				<i>RATING : 05/18/2001</i>		<i>COMMENTS:</i>	
<u>MATERIAL</u> GALVANIZED STEEL		<u>CONSTRUCTION</u> BREKAWAY SYSTEM		<u>DIRECTION</u> ALL		<u>COMMENTS</u>	
APPROACH PAVEMENT: *Overall condition assigned for each approach pavemenet component is shown below.							
<u>MATERIAL</u> ASPHALT		<u>CONSTRUCTION</u> BITUMINOUS MAT		<u>DIRECTION</u> BOTH		<u>CONDITION*</u>	
						<u>COMMENTS</u>	
***DRAINAGE, EXPANSION DEVICES, BANK/SLOPE, AND DECK PROTECTIVE COMPONENTS***							
<u>DECK PROTECTIVE COMPONENTS:</u>							
<u>SERIES TYPE-#</u> APPROACH SERIES-1		<u>COMPONENT</u> WEARING SURFACE		<u>MATERIAL</u> ASPHALT		<u>CONSTRUCTION</u> BITUMINOUS MAT	
				<u>THICKNESS</u> .37 IN		<u>YEAR APPLIED</u> 2018	
				<u>MANUFACTURE</u>		<u>OVERALL CONDITION</u> FAIR	
<u>COMMENT:</u>							
		DECK PROTECTION		NOTAPPLICABLE		NONE	
<u>COMMENT:</u>							
		MEMBRANE		NOTAPPLICABLE		NONE	
<u>COMMENT:</u>							
APPROACH SERIES-2		WEARING SURFACE		ASPHALT		BITUMINOUS MAT	
				<u>THICKNESS</u> .37 IN		<u>YEAR APPLIED</u> 2018	
						FAIR	
<u>COMMENT:</u>							
		DECK PROTECTION		NOTAPPLICABLE		NONE	
<u>COMMENT:</u>							
		MEMBRANE		NOTAPPLICABLE		NONE	
<u>COMMENT:</u>							
MAIN SERIES-3		WEARING SURFACE		ASPHALT		BITUMINOUS MAT	
				<u>THICKNESS</u> .37 IN		<u>YEAR APPLIED</u> 2018	
						FAIR	
<u>COMMENT:</u>							
		DECK PROTECTION		NOTAPPLICABLE		NONE	
<u>COMMENT:</u>							
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<div><div><div>MEMBRANE</div><div>NOTAPPLICABLE</div><div>NONE</div></div><div><div>COMMENT:</div><div>APPROACH SERIES-4</div><div>WEARING SURFACE</div><div>ASPHALT</div><div>BITUMINOUS MAT</div><div>.37 IN</div><div>2018</div><div>FAIR</div></div><div><div>COMMENT:</div><div>DECK PROTECTION</div><div>NOTAPPLICABLE</div><div>NONE</div></div><div><div>COMMENT:</div><div>MEMBRANE</div><div>NOTAPPLICABLE</div><div>NONE</div></div><div><div>COMMENT:</div><div>APPROACH SERIES-5</div><div>WEARING SURFACE</div><div>ASPHALT</div><div>BITUMINOUS MAT</div><div>.37 IN</div><div>2018</div><div>FAIR</div></div><div><div>COMMENT:</div><div>DECK PROTECTION</div><div>NOTAPPLICABLE</div><div>NONE</div></div><div><div>COMMENT:</div><div>MEMBRANE</div><div>NOTAPPLICABLE</div><div>NONE</div></div></div>																			
<div><div>DRAINAGE COMPONENTS:</div><table><tr><td>COMPONENT</td><td>MATERIAL</td><td>CONSTRUCTION</td><td>DIRECTION</td><td>COMMENTS</td></tr><tr><td colspan="5"></td></tr></table></div>										COMPONENT	MATERIAL	CONSTRUCTION	DIRECTION	COMMENTS					
COMPONENT	MATERIAL	CONSTRUCTION	DIRECTION	COMMENTS															
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# Missouri Department of Transportation

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**CLASS: STATBR**

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## BRIDGE: L0135

MAIN SPANS-10	DECK	REINFORCED CONCRETE	CAST-IN-PLACE			
<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
FULL DEPTH PATCHES		THROUGHOUT		MANY		
SATURATION		THROUGHOUT		MODERATE	50 %	
SPALLS		AT JOINTS		HEAVY		
MAIN SPANS-11	DECK	REINFORCED CONCRETE	CAST-IN-PLACE			
<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
SATURATION		THROUGHOUT		MODERATE	50 %	
SPALLS		AT JOINTS		HEAVY		
APPROACH SPANS-12	DECK	REINFORCED CONCRETE	CAST-IN-PLACE			
<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
SATURATION		THROUGHOUT		MINOR	40 %	
SPALLS		AT JOINTS		HEAVY		
APPROACH SPANS-13	DECK	REINFORCED CONCRETE	CAST-IN-PLACE			
<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
SATURATION		THROUGHOUT		MINOR	30 %	
SPALLS		AT JOINTS		HEAVY		
APPROACH SPANS-14	DECK	REINFORCED CONCRETE	CAST-IN-PLACE	(BLALOR1, 10/24/2013)--FINGER EXP POUNDING @ PIER 14 REPAIRED 8-8-13 (BLALOR1, 10/24/2013)--MIDDLE SECTION IN NB DRIVING LANE BROKE & POUNDING W/ LOADS-REPAIRED 8-8-13		
<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
SATURATION		THROUGHOUT		MINOR	40 %	
SPALLS		AT JOINTS		HEAVY		

## \*\*\*SUPERSTRUCTURE COMPONENTS\*\*\*

<u>SERIES TYPE-#</u>	<u>SPAN TYPE</u>	<u>MATERIAL</u>	<u>CONSTRUCTION</u>	<u>LABEL</u>	<u>COMMENTS</u>
APPROACH SERIES-1	SIMPLE SPAN	STEEL	WIDE FLANGE GIRDERS		
<u>SPAN</u>	<u>COMPOSITE INDICATOR</u>	<u>LENGTH</u>	<u>WEATHERING STEEL</u>	<u>COMMENTS</u>	
APPROACH SPANS-1	NON-COMPOSITE	60 FT 1 IN	NO		
<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>
PACK RUST		TOP FLANGE		LIGHT	<u>COMMENT</u>
APPROACH SPANS-2	NON-COMPOSITE	60 FT 0 IN	NO		
<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>
PACK RUST		TOP FLANGE		LIGHT	<u>COMMENT</u>
SECTION LOSS		AT BEARING		MINOR	
APPROACH SPANS-3	NON-COMPOSITE	60 FT 0 IN	NO		
<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>
PACK RUST		TOP FLANGE		LIGHT	<u>COMMENT</u>
SECTION LOSS		AT BEARING		MINOR	
APPROACH SPANS-4	NON-COMPOSITE	60 FT 0 IN	NO		
<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>

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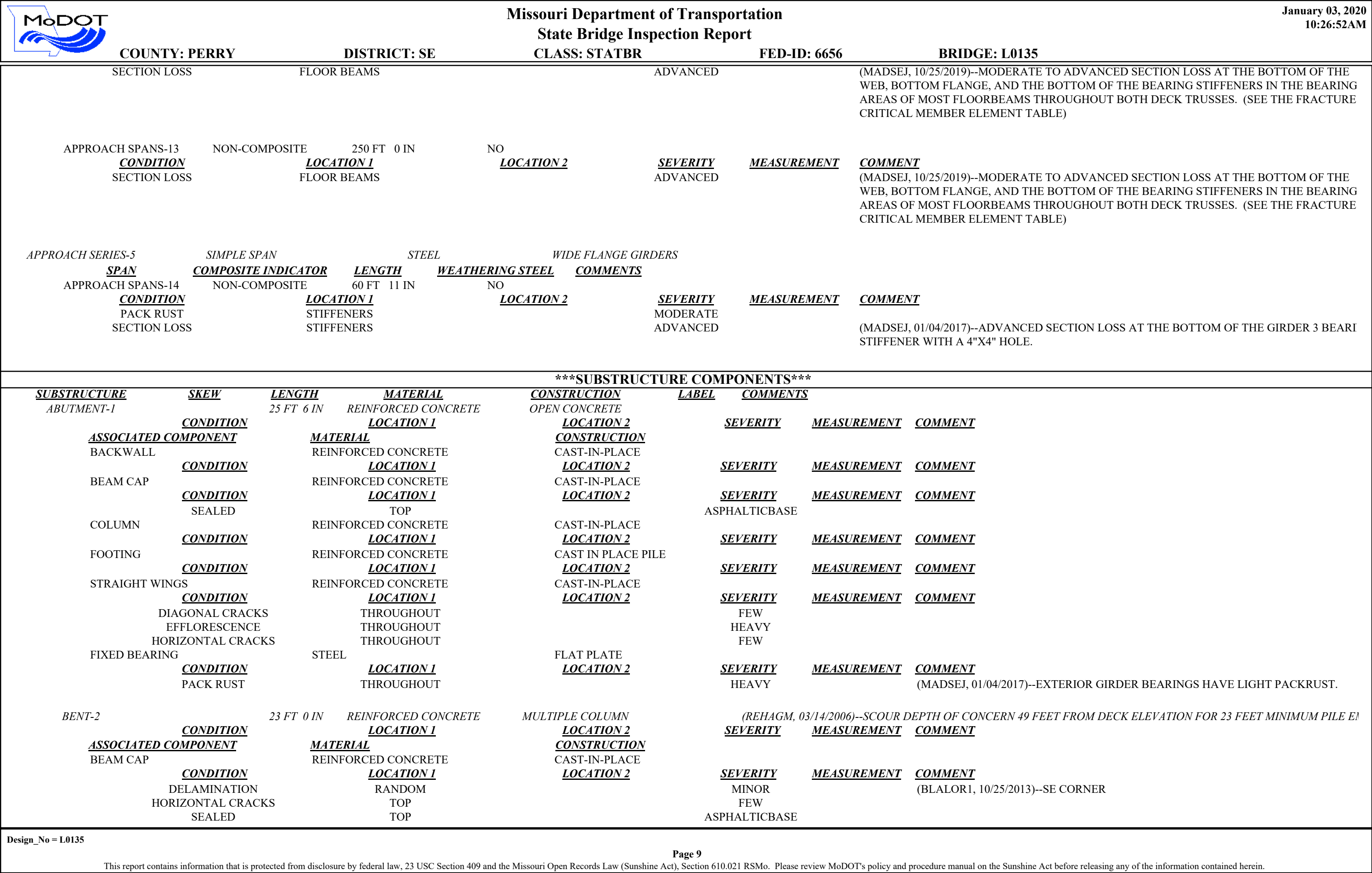
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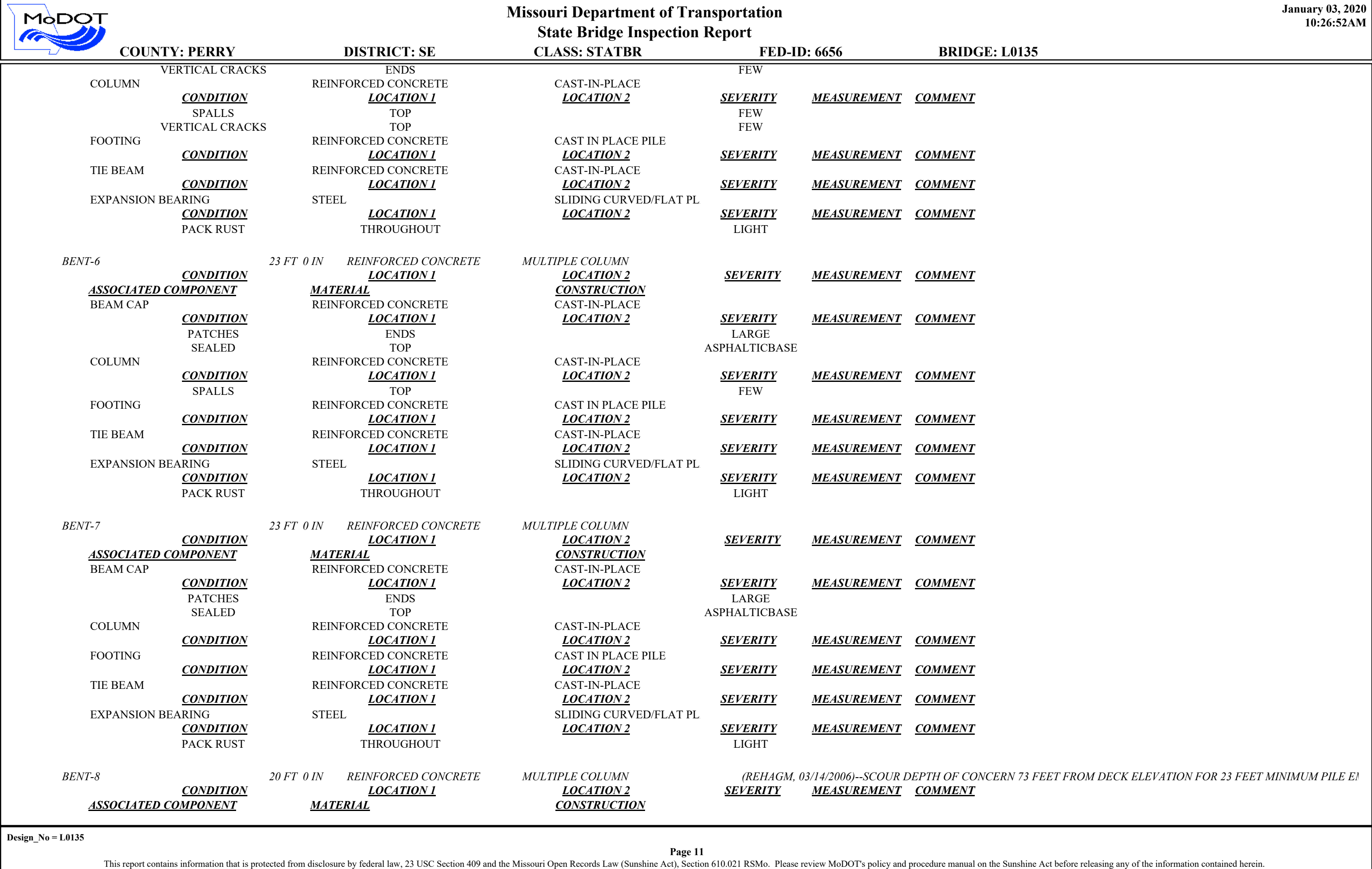
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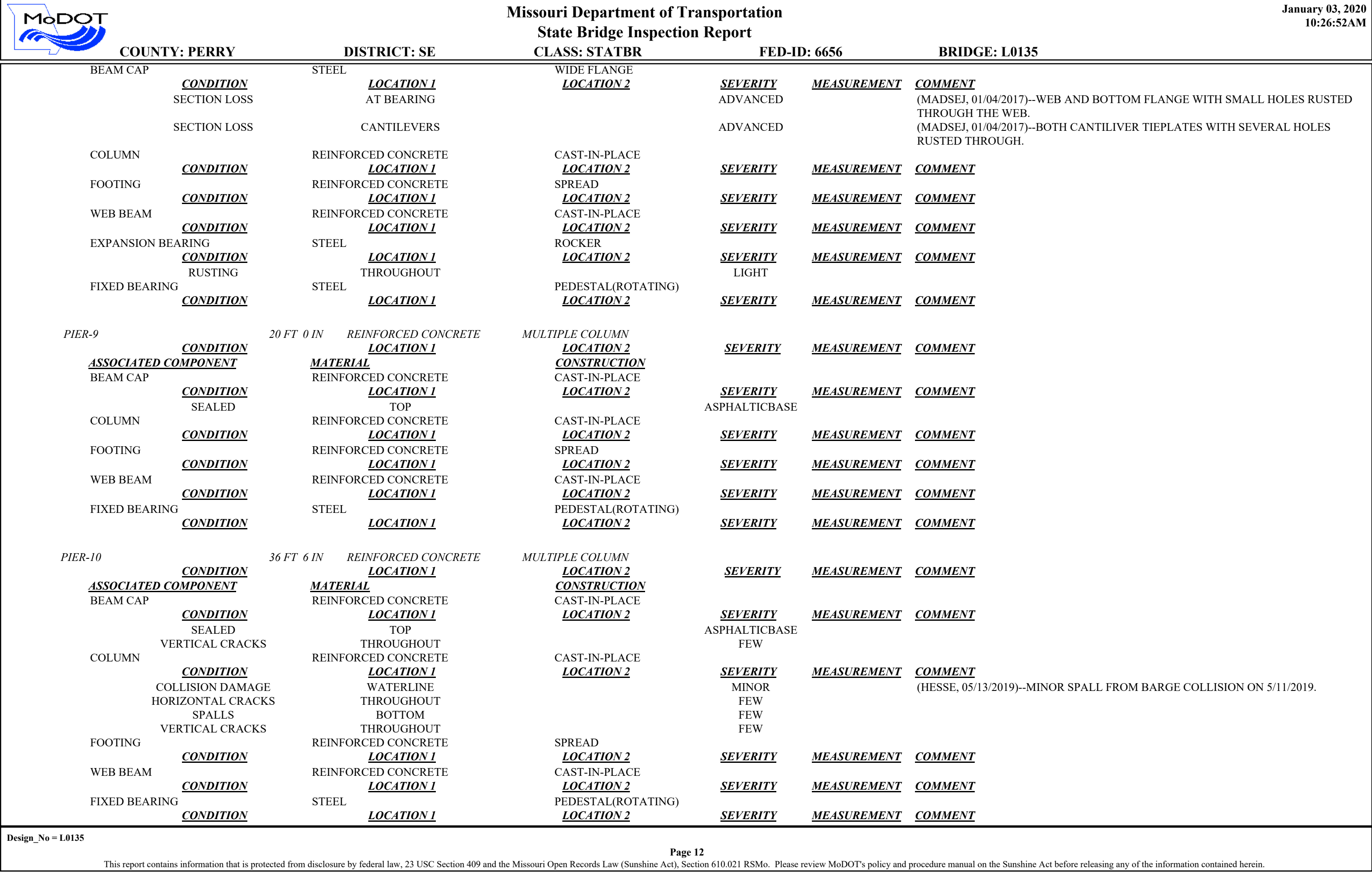
PACK RUST SECTION LOSS		TOP FLANGE AT BEARING		LIGHT MINOR			
APPROACH SPANS-5 <u>CONDITION</u>	NON-COMPOSITE	60 FT 0 IN	NO	<u>LOCATION 2</u>	<u>SEVERITY</u> LIGHT MODERATE	<u>MEASUREMENT</u>	<u>COMMENT</u>
PACK RUST SECTION LOSS		TOP FLANGE AT BEARING					
APPROACH SPANS-6 <u>CONDITION</u>	NON-COMPOSITE	60 FT 0 IN	NO	<u>LOCATION 2</u>	<u>SEVERITY</u> LIGHT MODERATE	<u>MEASUREMENT</u>	<u>COMMENT</u>
PACK RUST SECTION LOSS		TOP FLANGE AT BEARING					
APPROACH SPANS-7 <u>CONDITION</u>	NON-COMPOSITE	60 FT 9 IN	NO	<u>LOCATION 2</u>	<u>SEVERITY</u> LIGHT MODERATE	<u>MEASUREMENT</u>	<u>COMMENT</u>
PACK RUST SECTION LOSS		TOP FLANGE AT BEARING					(MADSEJ, 01/04/2017)--MINOR SECTION LOSS AT THE BOTTOM OF THE WEB AND BOTTOM FLANGE IN THE BEARING AREAS OF BENT 7 GIRDERS 1, 2, 3, AND 4. MODERATE SECTION L IN THE BEARING AREAS OF GIRDERS 5, 6, AND 7.
APPROACH SERIES-2 <u>SPAN</u>		CONTINUOUS SPAN <u>COMPOSITE INDICATOR</u>		STEEL <u>LENGTH</u>	DECK TRUSS <u>WEATHERING STEEL</u> <u>COMMENTS</u>		
APPROACH SPANS-8 <u>CONDITION</u>	NON-COMPOSITE	250 FT 10 IN	NO	<u>LOCATION 2</u>	<u>SEVERITY</u> ADVANCED	<u>MEASUREMENT</u>	<u>COMMENT</u>
SECTION LOSS		FLOOR BEAMS					(MADSEJ, 10/25/2019)--MODERATE TO ADVANCED SECTION LOSS AT THE BOTTOM OF THE WEB, BOTTOM FLANGE, AND THE BOTTOM OF THE BEARING STIFFENERS IN THE BEARING AREAS OF MOST FLOORBEAMS THROUGHOUT BOTH DECK TRUSSES. (SEE THE FRACTURE CRITICAL MEMBER ELEMENT TABLE)
APPROACH SPANS-9 <u>CONDITION</u>	NON-COMPOSITE	251 FT 0 IN	NO	<u>LOCATION 2</u>	<u>SEVERITY</u> ADVANCED	<u>MEASUREMENT</u>	<u>COMMENT</u>
SECTION LOSS		FLOOR BEAMS					(MADSEJ, 10/25/2019)--MODERATE TO ADVANCED SECTION LOSS AT THE BOTTOM OF THE WEB, BOTTOM FLANGE, AND THE BOTTOM OF THE BEARING STIFFENERS IN THE BEARING AREAS OF MOST FLOORBEAMS THROUGHOUT BOTH DECK TRUSSES. (SEE THE FRACTURE CRITICAL MEMBER ELEMENT TABLE)
MAIN SERIES-3 <u>SPAN</u>		CONTINUOUS SPAN <u>COMPOSITE INDICATOR</u>		STEEL <u>LENGTH</u>	THRU TRUSS <u>WEATHERING STEEL</u> <u>COMMENTS</u>		
MAIN SPANS-10 <u>CONDITION</u>	NON-COMPOSITE	669 FT 11 IN	NO	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
MAIN SPANS-11 <u>CONDITION</u>	NON-COMPOSITE	670 FT 0 IN	NO	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
APPROACH SERIES-4 <u>SPAN</u>		CONTINUOUS SPAN <u>COMPOSITE INDICATOR</u>		STEEL <u>LENGTH</u>	DECK TRUSS <u>WEATHERING STEEL</u> <u>COMMENTS</u>		
APPROACH SPANS-12 <u>CONDITION</u>	NON-COMPOSITE	251 FT 10 IN	NO	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>






		Missouri Department of Transportation State Bridge Inspection Report				January 03, 2020 10:26:52AM	
COUNTY: PERRY		DISTRICT: SE		CLASS: STATBR		FED-ID: 6656	
						BRIDGE: L0135	
VERTICAL CRACKS		THROUGHOUT		FEW			
COLUMN	<u>CONDITION</u>	REINFORCED CONCRETE	<u>LOCATION 1</u>	CAST-IN-PLACE	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>
FOOTING	<u>CONDITION</u>	REINFORCED CONCRETE	<u>LOCATION 1</u>	CAST IN PLACE PILE	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>COMMENT</u>
TIE BEAM	<u>CONDITION</u>	REINFORCED CONCRETE	<u>LOCATION 1</u>	CAST-IN-PLACE	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>
EXPANSION BEARING	<u>CONDITION</u>	STEEL	<u>LOCATION 1</u>	SLIDING CURVED/FLAT PL	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>COMMENT</u>
PACK RUST		THROUGHOUT		MEDIUM			
BENT-3		23 FT 0 IN	REINFORCED CONCRETE	MULTIPLE COLUMN			
	<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
<u>ASSOCIATED COMPONENT</u>		<u>MATERIAL</u>		<u>CONSTRUCTION</u>			
BEAM CAP	<u>CONDITION</u>	REINFORCED CONCRETE	<u>LOCATION 1</u>	CAST-IN-PLACE	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>
SEALED		TOP		ASPHALTICBASE		<u>COMMENT</u>	
VERTICAL CRACKS		THROUGHOUT		FEW			
COLUMN	<u>CONDITION</u>	REINFORCED CONCRETE	<u>LOCATION 1</u>	CAST-IN-PLACE	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>
FOOTING	<u>CONDITION</u>	REINFORCED CONCRETE	<u>LOCATION 1</u>	CAST IN PLACE PILE	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>COMMENT</u>
TIE BEAM	<u>CONDITION</u>	REINFORCED CONCRETE	<u>LOCATION 1</u>	CAST-IN-PLACE	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>
EXPANSION BEARING	<u>CONDITION</u>	STEEL	<u>LOCATION 1</u>	SLIDING CURVED/FLAT PL	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>COMMENT</u>
PACK RUST		THROUGHOUT		MEDIUM			
BENT-4		23 FT 0 IN	REINFORCED CONCRETE	MULTIPLE COLUMN			
	<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
<u>ASSOCIATED COMPONENT</u>		<u>MATERIAL</u>		<u>CONSTRUCTION</u>			
BEAM CAP	<u>CONDITION</u>	REINFORCED CONCRETE	<u>LOCATION 1</u>	CAST-IN-PLACE	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>
SEALED		TOP		ASPHALTICBASE		<u>COMMENT</u>	
COLUMN	<u>CONDITION</u>	REINFORCED CONCRETE	<u>LOCATION 1</u>	CAST-IN-PLACE	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>
VERTICAL CRACKS		TOP		FEW		<u>COMMENT</u>	
FOOTING	<u>CONDITION</u>	REINFORCED CONCRETE	<u>LOCATION 1</u>	CAST IN PLACE PILE	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>
TIE BEAM	<u>CONDITION</u>	REINFORCED CONCRETE	<u>LOCATION 1</u>	CAST-IN-PLACE	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>COMMENT</u>
EXPANSION BEARING	<u>CONDITION</u>	STEEL	<u>LOCATION 1</u>	SLIDING CURVED/FLAT PL	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>
PACK RUST		THROUGHOUT		MEDIUM		<u>COMMENT</u>	
BENT-5		23 FT 0 IN	REINFORCED CONCRETE	MULTIPLE COLUMN			
	<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
<u>ASSOCIATED COMPONENT</u>		<u>MATERIAL</u>		<u>CONSTRUCTION</u>			
BEAM CAP	<u>CONDITION</u>	REINFORCED CONCRETE	<u>LOCATION 1</u>	CAST-IN-PLACE	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>
SEALED		TOP		ASPHALTICBASE		<u>COMMENT</u>	





		Missouri Department of Transportation State Bridge Inspection Report				January 03, 2020 10:26:52AM	
COUNTY: PERRY		DISTRICT: SE		CLASS: STATBR		FED-ID: 6656	
						BRIDGE: L0135	
PIER-11		36 FT 6 IN	REINFORCED CONCRETE	MULTIPLE COLUMN			
	<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
	<u>ASSOCIATED COMPONENT</u>	<u>MATERIAL</u>	<u>CONSTRUCTION</u>				
	BEAM CAP	REINFORCED CONCRETE	CAST-IN-PLACE				
	<u>CONDITION</u>	<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>	
	EFFLORESCENCE	ENDS		LIGHT			
	HORIZONTAL CRACKS	ENDS		FEW			
	SCALING	LEFT SIDE		HEAVY			(RACKEM, 02/06/2006)--NE CORNER.
	SEALED	BEAM CAP		ASPHALTICBASE			
	VERTICAL CRACKS	ENDS		FEW			
	COLUMN	REINFORCED CONCRETE	CAST-IN-PLACE				
	<u>CONDITION</u>	<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>	
	HORIZONTAL CRACKS	THROUGHOUT		FEW			
	VERTICAL CRACKS	THROUGHOUT		FEW			
	FOOTING	REINFORCED CONCRETE	SPREAD				
	<u>CONDITION</u>	<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>	
	WEB BEAM	REINFORCED CONCRETE	CAST-IN-PLACE				
	<u>CONDITION</u>	<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>	
	EXPANSION BEARING	STEEL	ROCKER				
	<u>CONDITION</u>	<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>	
PIER-12		36 FT 6 IN	REINFORCED CONCRETE	MULTIPLE COLUMN			
	<u>CONDITION</u>	<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>	
	<u>ASSOCIATED COMPONENT</u>	<u>MATERIAL</u>	<u>CONSTRUCTION</u>				
	BEAM CAP	REINFORCED CONCRETE	CAST-IN-PLACE				
	<u>CONDITION</u>	<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>	
	REBAR EXPOSED	THROUGHOUT		MODERATE			
	SCALING	THROUGHOUT		MEDIUM			
	SEALED	BEAM CAP		ASPHALTICBASE			(RACKEM, 12/07/2007)--PEELING & THIN.
	SPALLS	THROUGHOUT		LARGE			
	COLUMN	REINFORCED CONCRETE	CAST-IN-PLACE				
	<u>CONDITION</u>	<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>	
	REBAR EXPOSED	THROUGHOUT		MODERATE			
	SCALING	THROUGHOUT		MEDIUM			
	SPALLS	THROUGHOUT		LARGE			
	FOOTING	REINFORCED CONCRETE	SPREAD				
	<u>CONDITION</u>	<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>	
	WEB BEAM	REINFORCED CONCRETE	CAST-IN-PLACE				
	<u>CONDITION</u>	<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>	
	EXPANSION BEARING	STEEL	ROCKER				
	<u>CONDITION</u>	<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>	
	TIPPED	ROCKER		MINOR			(BLALOR1, 10/25/2013)--TO THE NORTH
PIER-13		20 FT 0 IN	REINFORCED CONCRETE	MULTIPLE COLUMN			
	<u>CONDITION</u>	<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>	
	<u>ASSOCIATED COMPONENT</u>	<u>MATERIAL</u>	<u>CONSTRUCTION</u>				
	BEAM CAP	REINFORCED CONCRETE	CAST-IN-PLACE				
	<u>CONDITION</u>	<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>	
	SCALING	THROUGHOUT		MEDIUM			
	SEALED	BEAM CAP		ASPHALTICBASE			
	COLUMN	REINFORCED CONCRETE	CAST-IN-PLACE				





# Missouri Department of Transportation State Bridge Inspection Report

January 03, 2020  
10:26:52AM

**COUNTY: PERRY**

DISTRICT: SE

**CLASS: STATBR**

**FED-ID: 6656**

BRIDGE: L0135

FOOTING	<u>CONDITION</u>	SCALING	<u>LOCATION 1</u>	THROUGHOUT	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
				REINFORCED CONCRETE	SPREAD			
	<u>CONDITION</u>		<u>LOCATION 1</u>		<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
				REINFORCED CONCRETE	CAST-IN-PLACE			
WEB BEAM	<u>CONDITION</u>		<u>LOCATION 1</u>		<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
EXPANSION BEARING	<u>CONDITION</u>		<u>LOCATION 1</u>	STEEL	ROCKER	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
BENT-14	<u>ASSOCIATED COMPONENT</u>	20 FT 0 IN	REINFORCED CONCRETE	MULTIPLE COLUMN				
	BEAM CAP	<u>CONDITION</u>	<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>	
		SEALED	BEAM CAP	CAST-IN-PLACE				
	COLUMN	<u>CONDITION</u>	REINFORCED CONCRETE	CAST-IN-PLACE	ASPHALTICBASE			
	FOOTING	<u>CONDITION</u>	<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>	
	WEB BEAM	<u>CONDITION</u>	<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>	
	FIXED BEARING	<u>CONDITION</u>	<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>	
ABUTMENT-15	<u>ASSOCIATED COMPONENT</u>	29 FT 2 IN	REINFORCED CONCRETE	NON-INTEGRAL				
	BACKWALL	<u>CONDITION</u>	<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>	
	BEAM CAP	<u>CONDITION</u>	<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>	
		SEALED	BEAM CAP	CAST-IN-PLACE	ASPHALTICBASE			
	FOOTING	<u>CONDITION</u>	REINFORCED CONCRETE	SPREAD				
	FLARED WINGS	<u>CONDITION</u>	<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>	
	FIXED BEARING	<u>CONDITION</u>	<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>	


## \*\*\*OVER/UNDER ROUTES CLEARANCE INFORMATION\*\*\*


<b><u>CLEARANCES OVER DECK</u></b>				
<b><u>VERTICAL CLEARANCE TYPE**</u></b>	<b><u>VALUE</u></b>	<b><u>DIRECTION</u></b>	<b><u>DATE</u></b>	<b><u>COMMENT</u></b>
ACTUAL	19 FT 8 IN		05/25/2011	

**Design\_No = L0135**

This report contains information that is protected from disclosure by federal law, 23 USC Section 409 and the Missouri Open Records Law (Sunshine Act), Section 610.021 RSMo. Please review MoDOT's policy and procedure manual on the Sunshine Act before releasing any of the information contained herein.



		<b>Missouri Department of Transportation</b>				<b>January 03, 2020</b>	
		<b>State Bridge Inspection Report</b>				<b>10:26:52AM</b>	
<b>COUNTY: PERRY</b>		<b>DISTRICT: SE</b>		<b>CLASS: STATBR</b>		<b>FED-ID: 6656</b>	
				<b>BRIDGE: L0135</b>			
<b><u>CLEARANCES UNDER BRIDGE</u></b>		<small>**NOTE: Vertical clearances for permitting purposes are taken as 2 inches less than the actual field measured clearance.</small>					
<b><u>RECORD #</u></b>	<b><u>ROUTE</u></b>	<b><u># LANES</u></b>	<b><u>DIRECTION OF TRAFFIC</u></b>		<b><u>RIGHT LATERAL CLEARANCE</u></b>	<b><u>LEFT LATERAL CLEARANCE</u></b>	<b><u>UR-ID</u></b>
1	UP RR				65 FT 6 IN		14775
<b><u>VERTICAL CLEARANCE TYPE**</u></b>		<b><u>VALUE</u></b>	<b><u>DIRECTION</u></b>	<b><u>DATE</u></b>	<b><u>COMMENT</u></b>		
PLANNED		23 FT 0 IN					
<b><u>RECORD #</u></b>	<b><u>ROUTE</u></b>	<b><u># LANES</u></b>	<b><u>DIRECTION OF TRAFFIC</u></b>		<b><u>RIGHT LATERAL CLEARANCE</u></b>	<b><u>LEFT LATERAL CLEARANCE</u></b>	<b><u>UR-ID</u></b>
2	KASKASKIA ST	2	2-WAY TRAF				105027
<b><u>VERTICAL CLEARANCE TYPE**</u></b>		<b><u>VALUE</u></b>	<b><u>DIRECTION</u></b>	<b><u>DATE</u></b>	<b><u>COMMENT</u></b>		
<b>***STRUCTURE PAINT INFORMATION***</b>							
<b>CONDITION:</b>		POOR		<b>RUST AMOUNT :</b>		6=1.0% OF SURFACE RUSTED	
				<b>STEEL TONS :</b>		3,500	
<b><u>ORIGINAL PAINT</u></b>		<b><u>CONTRACT REPAINT</u></b>		<b><u>DEPARTMENT REPAINT</u></b>			
<b>PAINT TYPE :</b>		<b>PAINT TYPE :</b>		<b>PAINT TYPE :</b>		<b>MANUFACTURE :</b>	
<b>NAME :</b>		<b>NAME :</b>		<b>NAME :</b>		<b>SURFACE PREP :</b>	
<b>PAINT COLOR :</b>		<b>PAINT COLOR :</b>		<b>PAINT COLOR :</b>			
<b>PAINT YEAR :</b>		<b>PAINT YEAR :</b>		<b>PAINT YEAR :</b>			
<b>MILS :</b>		<b>MILS :</b>		<b>MILS :</b>			
<b>***REQUESTED WORK ITEMS***</b>							
<b>GENERAL WORK COMMENTS:</b>							
<b><i>RESPONSIBILITY</i></b>	<b><i>LOCATION</i></b>	<b><i>ITEM</i></b>	<b><i>CATEGORY</i></b>	<b><i>PRIORITY</i></b>	<b><i>DATE</i></b>	<b><i>WORK ITEM COMMENT</i></b>	
DISTRICT SPECIAL	LOWER CHORDS	CLEAN AND FLUSH	SUPERSTRUCTURE	3	01/04/2017		
DISTRICT SPECIAL	SEE COMMENT	CLEAN AND FLUSH	SUBSTRUCTURE	3	01/04/2017	(MUSSED, 08/16/2013)--CLEAN ABUTMENT CAPS	
DISTRICT SPECIAL	SEE COMMENT	MISCELLANEOUS	PAINT	3	01/04/2017	(MUSSED, 11/05/2014)--CLEAN AND PAINT AROUND HOLES IN TOP PLATE OF LOWER CHOED AT SPAN 10 TO PREVENT FUTURE CORROSION	
						(MUSSED, 11/05/2014)--CLEAN AND PAINT FLOORBEAMS IN DECK TRUSS	
DISTRICT SPECIAL	SUPER-GIRDERS	REPAIR GIRDER ENDS	SUPERSTRUCTURE	3	01/04/2017	(MUSSED, 11/05/2014)--REPAIR STRINGERS AT SPAN 11	
<b>***UTILITY ATTACHMENTS***</b>							
<b><i>UTILITY</i></b>	<b><i>OWNER</i></b>	<b><i>METHOD</i></b>	<b><i>MEASUREMENT TYPE</i></b>	<b><i>VALUE</i></b>	<b><i>NUMBER</i></b>	<b><i>UTILITY ATTACHMENT COMMENT</i></b>	
GAS		CONDUIT			1	(BOWDEJ1, 10/04/2004)--W. SIDE OF BRIDGE	
						(RACKEM, 12/07/2007)--SPAN 14 WEST SIDE PIPELINE BRACKET BROKE. 1 PIPELINE BRACKET BROEN NORTH SIDE @ BT 5, POOR WELD.	
<b>***PROGRAM NOTES INFORMATION***</b>							
<b>Design_No = L0135</b>							
<b>Page 15</b>							
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COUNTY: PERRY		DISTRICT: SE		CLASS: STATBR	FED-ID: 6656	BRIDGE: L0135																																			
<div>YEAR</div> <div>2020</div> <div>2018</div>	<div>PROJECT #</div> <div>J9P3585</div> <div>9P3104</div>	<div>MONTH LET</div> <div>10</div> <div>0</div>	<div>YEAR LET</div> <div>2019</div> <div>0</div>	<div>ITEMS</div> <div>DECK REPAIR, SEAL DECK, SUPERSTRUCTURE REPAIR</div> <div>DECK REPAIR, SUPERSTRUCTURE REPAIR, WEARING SURFACE</div>																																					
***COMPUTER GENERATED RATINGS AND DEFICIENCY ITEMS***					***ADVANCED SIGN INFORMATION***																																				
<div>NOTE: The items listed in this section are updated whenever computer edits are ran on a structure after the inspection updates have been entered in to TMS.</div> <table><tr><td><div>Rated Item</div></td><td><div>Rating</div></td><td><div>Rating Date</div></td></tr><tr><td>[Item 67] Structure Evaluation Rating:</td><td>4-MEETS MINIMUM TOLERABLE</td><td>3/5/2018</td></tr><tr><td>[Item 68] Deck Geometry Rating:</td><td>2-BASICALLY INTOLRBLE REQ</td><td>5/18/2001</td></tr><tr><td>[Item 69] Underclearance:</td><td>8-EQ TO PRESENT DESIRAB</td><td>2/5/2003</td></tr><tr><td>Sufficiency Rating:</td><td>29.7%</td><td>3/5/2018</td></tr><tr><td>Deficiency:</td><td>STRUCTURAL</td><td>8/19/2013</td></tr><tr><td>Funding Eligibility:</td><td>FULL</td><td>----</td></tr><tr><td>Estimated New Structure Length:</td><td>2,828 FT.</td><td>----</td></tr><tr><td>Estimated Structure Cost:</td><td>\$33,176,682</td><td>----</td></tr><tr><td>Estimated Total Project Cost:</td><td>\$49,765,023</td><td>----</td></tr><tr><td>Year of Cost Estimate:</td><td>2019</td><td>----</td></tr></table> <div>NOTE: The above structure length and cost estimates are computer generated using algorithms in the TMS system. These algorithms are generalized to use NB items to come up with a new structure length and width to calculate a new area which is taken times a representative cost per square foot. The actual structure siz and cost may vary significantly from these numbers once site specific engineering is done.</div>					<div>Rated Item</div>	<div>Rating</div>	<div>Rating Date</div>	[Item 67] Structure Evaluation Rating:	4-MEETS MINIMUM TOLERABLE	3/5/2018	[Item 68] Deck Geometry Rating:	2-BASICALLY INTOLRBLE REQ	5/18/2001	[Item 69] Underclearance:	8-EQ TO PRESENT DESIRAB	2/5/2003	Sufficiency Rating:	29.7%	3/5/2018	Deficiency:	STRUCTURAL	8/19/2013	Funding Eligibility:	FULL	----	Estimated New Structure Length:	2,828 FT.	----	Estimated Structure Cost:	\$33,176,682	----	Estimated Total Project Cost:	\$49,765,023	----	Year of Cost Estimate:	2019	----	<div>SIGN #</div> <div>1</div>	<div>SIGN TYPE</div>	<div>PROBLEM</div>	<div>PROBLEM DIRECTION</div>
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