
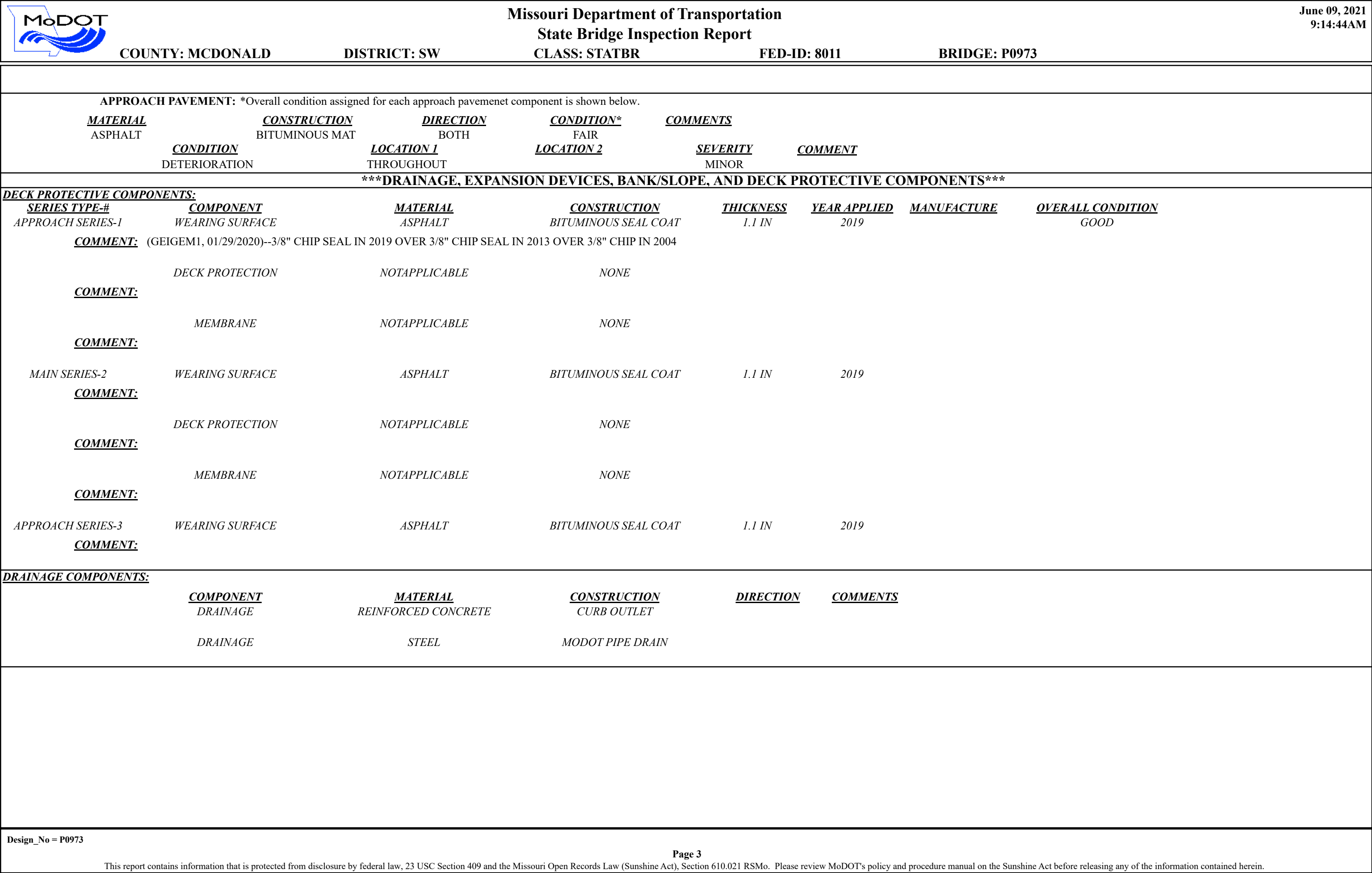

		<div>Missouri Department of Transportation</div> <div>State Bridge Inspection Report</div>				<div>June 09, 2021</div> <div>9:14:44AM</div>			
COUNTY: MCDONALD		DISTRICT: SW		CLASS: STATBR		FED-ID: 8011		BRIDGE: P0973	
***GENERAL STRUCTURE INFORMATION***							***BRIDGE INSPECTION INFORMATION***		
<div>ROUTE: MO90E</div> <div>FEATURE: LIT SUGAR CR</div> <div>STATUS: P-POSTLOAD</div> <div>LOG MILE: 21.872</div> <div>DETOUR: 18.00 MILES</div> <div>NHS: NO</div> <div>BUILT: 1957</div> <div>REHAB:</div> <div>LOCATION: S 10 T 21 R 31 W</div> <div>LATITUDE: 36 33 7.38 (DMS)</div> <div>LONGITUDE: 94 16 58.98 (DMS)</div>		<div># SPANS: 7</div> <div>LANES ON: 1</div> <div>LANES UNDER: 0</div> <div>COMPASS DIRECTION: WEST to EAST</div> <div>DIRECTION OF TRAFFIC: 1-LN/2-WAY</div> <div>FUNCTIONAL CLASS: RL-MAJOR COLLECTOR</div> <div>NBI OWNER: MODOT</div> <div>NBI MAINTAINED: MODOT</div> <div>MAINTENANCE DISTRICT: SW</div> <div>MAINTENANCE COUNTY: MCDONALD</div> <div>SUB AREA: 7G01</div>		<div>PLACE CODE: 79540 WHITE ROCK</div> <div>LENGTH: 391 FT 0 IN</div> <div>MAXIMUM SPAN: 85 FT 0 IN</div> <div>APPROACH ROADWAY: 21 FT 0 IN</div> <div>CURB TO CURB: 20 FT 0 IN</div> <div>OUT TO OUT: 22 FT 4 IN</div> <div>AADT: 786</div> <div>AADT YEAR: 2020</div> <div>AADT TRUCK: 11.3%</div> <div>FUTURE AADT: 1376</div> <div>FUTURE AADT YEAR: 2040</div>		<div>DATE: 10/05/2020</div> <div>RESPONSIBILITY: DISTRICT</div> <div>FREQUENCY: 24</div> <div>CALCULATED INTERVAL**: 24</div> <div>TEAM LEADER: MATTHEW GEIGER</div> <div>ELEMENT: NO</div> <div>INSPECTOR 2:</div> <div>INSPECTOR 4:</div> <div>INSPECTOR 3:</div> <div>** When calculated interval exceeds the frequency, a justification comment per BIRM is required.</div>			
						<div>GENERAL INSPECTION COMMENTS</div>			
***FRACTURE CRITICAL INSPECTION INFORMATION***					***INDEPTH INSPECTION INFORMATION***				
<div>DATE:</div> <div>FREQUENCY:</div> <div>TEAM LEADER:</div> <div>INSPECTOR 2:</div> <div>** When calculated interval exceeds the frequency, a justification comment per BIRM is required.</div>					<div>RESPONSIBILITY:</div> <div>CALCULATED INTERVAL**:</div> <div>INSPECTOR 3:</div> <div>INSPECTOR 4:</div> <div>CATEGORY:</div> <div>NBI:</div> <div>METHOD:</div> <div>** When calculated interval exceeds the frequency, a justification comment per BIRM is required.</div>				
<div>FRACTURE CRITICAL INSPECTION COMMENTS</div>					<div>INDEPTH INSPECTION COMMENTS</div>				
***SPECIAL INSPECTION INFORMATION***					***UNDERWATER INSPECTION INFORMATION***				
<div>DATE: 01/18/2019</div> <div>FREQUENCY: 120</div> <div>TEAM LEADER:</div> <div>INSPECTOR 2: OTHER</div> <div>** When calculated interval exceeds the frequency, a justification comment per BIRM is required.</div>					<div>RESPONSIBILITY: DISTRICT</div> <div>CALCULATED INTERVAL**: 71</div> <div>INSPECTOR 3:</div> <div>INSPECTOR 4:</div> <div>CATEGORY: CHANNEL CROSS SEC</div> <div>NBI: NO</div> <div>METHOD: EMD</div>				
<div>SPECIAL INSPECTION COMMENTS</div>					<div>UNDERWATER INSPECTION COMMENTS</div>				
OTHER SPECIAL INSPECTIONS					OTHER UNDERWATER INSPECTIONS				
<div>DATE</div> <div>FREQUENCY</div> <div>CATEGORY</div> <div>NBI</div> <div>CALCULATED INTERVAL</div> <div>RESPONSIBILITY</div> <div>METHOD</div>					<div>DATE</div> <div>FREQUENCY</div> <div>CATEGORY</div> <div>NBI</div> <div>CALCULATED INTERVAL</div> <div>RESPONSIBILITY</div> <div>METHOD</div>				
<div>Design_No = P0973</div>									
<div>Page 1</div> <div>This report contains information that is protected from disclosure by federal law, 23 USC Section 409 and the Missouri Open Records Law (Sunshine Act), Section 610.021 RSMo. Please review MoDOT's policy and procedure manual on the Sunshine Act before releasing any of the information contained herein.</div>									

		Missouri Department of Transportation		June 09, 2021	
		State Bridge Inspection Report		9:14:44AM	
COUNTY: MCDONALD		DISTRICT: SW		CLASS: STATBR	
		FED-ID: 8011		BRIDGE: P0973	
***STRUCTURE POSTING***					
APPROVED CATEGORY: S-5		CENTERLINE OF BRIDGE AND TRUCKS OVER 14 TONS 15 MPH ON BRIDGE.			
Ton 1: 14		Ton 2:		Ton 3:	
COMMENTS:					
FIELD CATEGORY: S-5		CENTERLINE OF BRIDGE AND TRUCKS OVER 14 TONS 15 MPH ON BRIDGE.			
Ton 1: 14		Ton 2:		Ton 3:	
COMMENTS:		PROBLEM:		PROBLEM DIRECTION:	
***GENERAL COMMENTS/MAJOR RATED ITEMS***					
GENERAL COMMENTS: (FODGEC1, 12/05/2018)--(46'-46') SMP WF - (70'-85'-70') CONT WF - (35'-36') SMP WF GDR SPANS					
[ITEM 58] DECK: 3-SERIOUS CONDITION		COMMENTS: (FODGEC1, 12/05/2018)--60% SATURATION SP. 1			
RATING : 12/05/2018					
[ITEM 59] SUPER: 6-SATISFACTORY CONDITION		COMMENTS: (FODGEC1, 12/05/2018)--MODERATE TOP FLANGE PACK RUST WITH MINOR SECTION LOSS			
RATING : 12/05/2018					
[ITEM 60] SUB: 6-SATISFACTORY CONDITION		COMMENTS: (FODGEC1, 12/05/2018)--SATURATION AND MINOR DETERIORATION OF BENT CAPS			
RATING : 12/05/2018					
[ITEM 61] BANK/CHANNEL: 6-WIDESPREAD MINOR DAMAGE		COMMENTS: (BOWDEJ1, 02/25/2003)--FTGS EXPOSED ON ROCK - SCOURING E. BANK			
RATING : 12/04/2020		(WEHMES, 11/30/2012)--MINOR DAMAGE TO SLOPE PROTECTION @ W BANK			
		(NUNNT, 12/04/2020)--MEDIUM TO LARGE TREE DRIFT @ BENTS 3,4,5			
[ITEM 113] SCOUR: 8-STABLE FOR CALCULATED		COMMENTS: (BRITTT1, 11/14/2014)--FOOTINGS EXPOSED ON ROCK - NO UNDERMINING			
RATING : 05/18/2001					
EVALUATION TYPE :					
[ITEM 71] WATERWAY ADEQUACY: DECK/APPRCH OVERTOP SLIGT		COMMENTS:			
RATING : 05/18/2001					
[ITEM 72] APPRRDWY ALIGNMENT: 8-VERYGOOD		COMMENTS:			
RATING : 05/18/2001					
***RAILING AND APPROACH PAVEMENT COMPONENTS AND RATINGS***					
[ITEM 36A] BRIDGE RAILING RATING: DOESNT MEET CURRNT STND-0		RATING : 12/21/2004		COMMENTS:	
<u>MATERIAL</u>		<u>CONSTRUCTION</u>		<u>DIRECTION</u>	
REINFORCED CONCRETE		CURB		BOTH	
STEEL		ANGLE-DOUBLE		BOTH	
[ITEM 36B] TRANSITION RAILING RATING: NOT PROVIDED-0		RATING : 05/18/2001		COMMENTS:	
[ITEM 36C] APPROACH RAILING RATING: NOT PROVIDED-0		RATING : 05/18/2001		COMMENTS:	
[ITEM 36D] RAIL END TREATMENT RATING: NOT PROVIDED-0		RATING : 05/18/2001		COMMENTS:	
Design_No = P0973					
Page 2					
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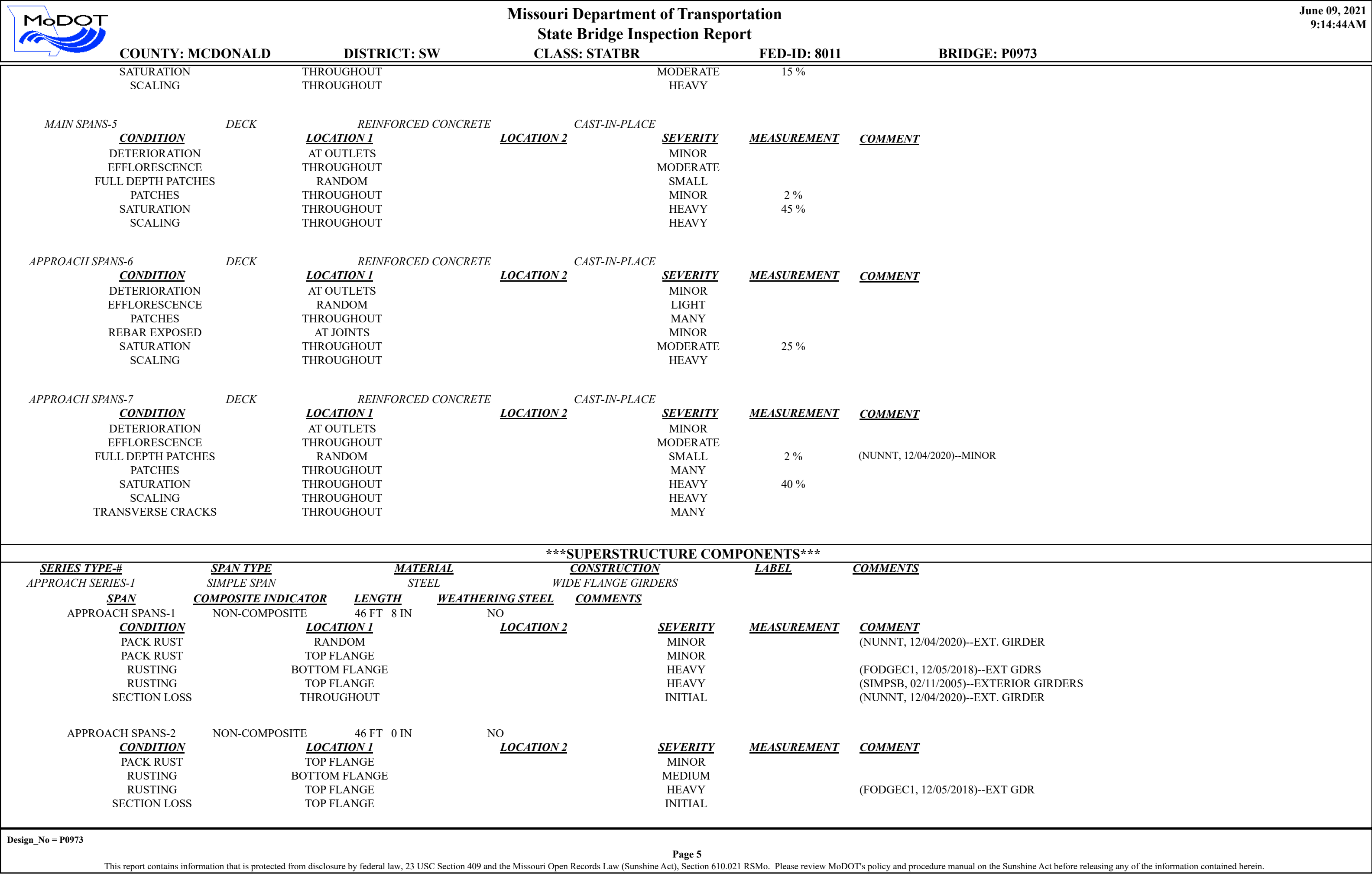



		Missouri Department of Transportation				June 09, 2021	
		State Bridge Inspection Report				9:14:44AM	
COUNTY: MCDONALD		DISTRICT: SW		CLASS: STATBR		FED-ID: 8011	
				BRIDGE: P0973			
<b>EXPANSION DEVICE COMPONENTS:</b>							
<u>SUB UNIT-#</u>	<u>SUB LABEL</u>	<u>COMPONENT</u>	<u>MATERIAL</u>	<u>CONSTRUCTION</u>	<u>GAP</u>	<u>YEAR APPLIED</u>	<u>MANUFACTURE</u>
BENT-2		CLOSED EXPANSION JOINT	FELT	FILLED JOINT			
<u>COMMENT:</u>							
PIER-3		CLOSED EXPANSION JOINT	STEEL	FLAT PLATE			FAIR
<u>COMMENT:</u>							
	<u>CONDITION</u>	<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>COMMENT</u>		
	LEAKING	THROUGHOUT		MODERATE			
BENT-7		CLOSED EXPANSION JOINT	FELT	FILLED JOINT			
<u>COMMENT:</u>							
<b>BANK/SLOPE PROTECTION COMPONENTS:</b>							
	<u>COMPONENT</u>	<u>MATERIAL</u>	<u>CONSTRUCTION</u>	<u>DIRECTION</u>	<u>COMMENTS</u>		
	BANK PROTECTION	ROCK	GROUTED	BOTH			
***DECK COMPONENTS***							
<u>SPAN TYPE-#</u>	<u>COMPONENT</u>	<u>MATERIAL</u>	<u>CONSTRUCTION</u>	<u>COMMENTS</u>			
APPROACH SPANS-1	DECK	REINFORCED CONCRETE	CAST-IN-PLACE	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>	
	<u>CONDITION</u>	<u>LOCATION 1</u>	<u>LOCATION 2</u>				
	DETERIORATION	AT OUTLETS		MODERATE			
	EFFLORESCENCE	THROUGHOUT		HEAVY			
	FULL DEPTH PATCHES	RANDOM		SMALL			
	PATCHES	THROUGHOUT		MANY	30 %		
	REBAR EXPOSED	AT JOINTS		MODERATE			
	SATURATION	THROUGHOUT		SEVERE	60 %		
	SCALING	THROUGHOUT		HEAVY			
APPROACH SPANS-2	DECK	REINFORCED CONCRETE	CAST-IN-PLACE	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>	
	<u>CONDITION</u>	<u>LOCATION 1</u>	<u>LOCATION 2</u>				
	DETERIORATION	AT OUTLETS		MINOR			
	EFFLORESCENCE	THROUGHOUT		MODERATE			
	PATCHES	THROUGHOUT		MANY			
	SATURATION	THROUGHOUT		HEAVY	55 %		
	SCALING	THROUGHOUT		HEAVY			
	TRANSVERSE CRACKS	THROUGHOUT		MANY			
MAIN SPANS-3	DECK	REINFORCED CONCRETE	CAST-IN-PLACE	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>	
	<u>CONDITION</u>	<u>LOCATION 1</u>	<u>LOCATION 2</u>				
	DETERIORATION	AT OUTLETS		MINOR			
	PATCHES	THROUGHOUT		MANY			
	SATURATION	THROUGHOUT		MODERATE	15 %		
	SCALING	THROUGHOUT		HEAVY			
MAIN SPANS-4	DECK	REINFORCED CONCRETE	CAST-IN-PLACE	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>	
	<u>CONDITION</u>	<u>LOCATION 1</u>	<u>LOCATION 2</u>				
	PATCHES	THROUGHOUT		MANY			

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		Missouri Department of Transportation					June 09, 2021		
		State Bridge Inspection Report					9:14:44AM		
COUNTY: MCDONALD		DISTRICT: SW		CLASS: STATBR		FED-ID: 8011		BRIDGE: P0973	
MAIN SERIES-2		CONTINUOUS SPAN		STEEL		WIDE FLANGE GIRDERS			
<u>SPAN</u>		<u>COMPOSITE INDICATOR</u>		<u>LENGTH</u>		<u>WEATHERING STEEL</u>		<u>COMMENTS</u>	
MAIN SPANS-3		NON-COMPOSITE		70 FT 9 IN		NO			
<u>CONDITION</u>		<u>LOCATION 1</u>		<u>LOCATION 2</u>		<u>SEVERITY</u>		<u>MEASUREMENT</u>	
RUSTING		TOP FLANGE				HEAVY		(FODGEC1, 12/05/2018)--EXT GDR	
MAIN SPANS-4		NON-COMPOSITE		85 FT 0 IN		NO			
<u>CONDITION</u>		<u>LOCATION 1</u>		<u>LOCATION 2</u>		<u>SEVERITY</u>		<u>MEASUREMENT</u>	
RUSTING		TOP FLANGE				LIGHT			
MAIN SPANS-5		NON-COMPOSITE		70 FT 9 IN		NO			
<u>CONDITION</u>		<u>LOCATION 1</u>		<u>LOCATION 2</u>		<u>SEVERITY</u>		<u>COMMENTS</u>	
PACK RUST		TOP FLANGE				MINOR			
PAINT PEELING		RANDOM				LIGHT			
RUSTING		BOTTOM FLANGE				MEDIUM		(FODGEC1, 12/05/2018)--@ BT. 6	
RUSTING		TOP FLANGE				HEAVY			
SECTION LOSS		TOP FLANGE				INITIAL			
APPROACH SERIES-3		SIMPLE SPAN		STEEL		WIDE FLANGE GIRDERS			
<u>SPAN</u>		<u>COMPOSITE INDICATOR</u>		<u>LENGTH</u>		<u>WEATHERING STEEL</u>		<u>COMMENTS</u>	
APPROACH SPANS-6		NON-COMPOSITE		35 FT 7 IN		NO			
<u>CONDITION</u>		<u>LOCATION 1</u>		<u>LOCATION 2</u>		<u>SEVERITY</u>		<u>MEASUREMENT</u>	
PACK RUST		TOP FLANGE				MINOR		(FODGEC1, 12/05/2018)--EXT GDR	
RUSTING		BOTTOM FLANGE				MODERATE			
RUSTING		TOP FLANGE				HEAVY			
SECTION LOSS		TOP FLANGE				INITIAL			
APPROACH SPANS-7		NON-COMPOSITE		36 FT 8 IN		NO			
<u>CONDITION</u>		<u>LOCATION 1</u>		<u>LOCATION 2</u>		<u>SEVERITY</u>		<u>MEASUREMENT</u>	
PACK RUST		BOTTOM FLANGE				MODERATE		(FODGEC1, 12/05/2018)--@ BT. 7	
PACK RUST		TOP FLANGE				HEAVY		(FODGEC1, 12/05/2018)--EXT GDR	
RUSTING		BOTTOM FLANGE				HEAVY		(NUNNT, 12/04/2020)--AT BT. 7	
RUSTING		TOP FLANGE				HEAVY			
SECTION LOSS		BOTTOM FLANGE				MINOR		(FODGEC1, 12/05/2018)--@ BT. 7	
SECTION LOSS		TOP FLANGE				MODERATE		(FODGEC1, 12/05/2018)--EXT GDR INT. BT	
***SUBSTRUCTURE COMPONENTS***									
<u>SUBSTRUCTURE</u>		<u>SKEW</u>		<u>LENGTH</u>		<u>MATERIAL</u>		<u>CONSTRUCTION</u>	
ABUTMENT-1		RA-45 DEGREES		31 FT 7 IN		REINFORCED CONCRETE		OPEN CONCRETE	
<u>CONDITION</u>		<u>LOCATION 1</u>		<u>LOCATION 2</u>		<u>SEVERITY</u>		<u>MEASUREMENT</u>	
<u>ASSOCIATED COMPONENT</u>		<u>MATERIAL</u>		<u>CONSTRUCTION</u>		<u>COMMENT</u>			
BEAM CAP		REINFORCED CONCRETE		CAST-IN-PLACE					
<u>CONDITION</u>		<u>LOCATION 1</u>		<u>LOCATION 2</u>		<u>SEVERITY</u>		<u>MEASUREMENT</u>	
DELAMINATION		RANDOM				SMALL			
HORIZONTAL CRACKS		RANDOM				FINE			
COLUMN		REINFORCED CONCRETE		CAST-IN-PLACE					
<u>CONDITION</u>		<u>LOCATION 1</u>		<u>LOCATION 2</u>		<u>SEVERITY</u>		<u>MEASUREMENT</u>	
STRAIGHT WINGS		REINFORCED CONCRETE		CAST-IN-PLACE					
<u>CONDITION</u>		<u>LOCATION 1</u>		<u>LOCATION 2</u>		<u>SEVERITY</u>		<u>MEASUREMENT</u>	

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		Missouri Department of Transportation State Bridge Inspection Report					June 09, 2021 9:14:44AM	
COUNTY: MCDONALD		DISTRICT: SW		CLASS: STATBR		FED-ID: 8011		BRIDGE: P0973
FOOTING	<u>CONDITION</u>	REINFORCED CONCRETE	<u>LOCATION 1</u>	SPREAD	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
BACKWALL	<u>CONDITION</u>	REINFORCED CONCRETE	<u>LOCATION 1</u>	CAST-IN-PLACE	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
EXPANSION BEARING	<u>CONDITION</u>	STEEL	<u>LOCATION 1</u>	SLIDING FLAT PLATE	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
RUSTING		THROUGHOUT				MEDIUM		
BENT-2	RA-45 DEGREES	29 FT 2 IN	REINFORCED CONCRETE	MULTIPLE COLUMN				
	<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>	
<u>ASSOCIATED COMPONENT</u>		<u>MATERIAL</u>		<u>CONSTRUCTION</u>				
BEAM CAP	<u>CONDITION</u>	REINFORCED CONCRETE	<u>LOCATION 1</u>	CAST-IN-PLACE	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
	DELAMINATION	ENDS				MODERATE		
	REBAR EXPOSED	BOTTOM				FEW		
	SATURATION	THROUGHOUT				MODERATE		
	SPALLS	BOTTOM				MINOR		(NUNNT, 12/04/2020)--HIGH STEEL
	VERTICAL CRACKS	RANDOM				MINOR		
COLUMN	<u>CONDITION</u>	REINFORCED CONCRETE	<u>LOCATION 1</u>	CAST-IN-PLACE	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
TIE BEAM	<u>CONDITION</u>	REINFORCED CONCRETE	<u>LOCATION 1</u>	CAST-IN-PLACE	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
FOOTING	<u>CONDITION</u>	REINFORCED CONCRETE	<u>LOCATION 1</u>	SPREAD	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
EXPANSION BEARING	<u>CONDITION</u>	STEEL	<u>LOCATION 1</u>	SLIDING FLAT PLATE	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
RUSTING		THROUGHOUT				MEDIUM		
FIXED BEARING	<u>CONDITION</u>	STEEL	<u>LOCATION 1</u>	FLAT PLATE	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
RUSTING		THROUGHOUT				MEDIUM		
PIER-3	RA-45 DEGREES	30 FT 10 IN	REINFORCED CONCRETE	SOLID WALL		(STEGEC, 05/04/2005)--PROFILE GRADE ELEVATION = 933.2 (FLAT)		
	<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>	
<u>ASSOCIATED COMPONENT</u>		<u>MATERIAL</u>		<u>CONSTRUCTION</u>				
BEAM CAP	<u>CONDITION</u>	REINFORCED CONCRETE	<u>LOCATION 1</u>	CAST-IN-PLACE	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
	DELAMINATION	RANDOM				MINOR		
	SPALLS	RANDOM				MINOR		
	VERTICAL CRACKS	RANDOM				FINE		
COLUMN	<u>CONDITION</u>	REINFORCED CONCRETE	<u>LOCATION 1</u>	CAST-IN-PLACE	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
	DRIFT	WATERLINE				LARGE AMOUNT		
	SCALING	WATERLINE				MEDIUM		
FOOTING	<u>CONDITION</u>	REINFORCED CONCRETE	<u>LOCATION 1</u>	SPREAD	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
EXPOSED		TOP				PARTIAL		
WEB BEAM	<u>CONDITION</u>	REINFORCED CONCRETE	<u>LOCATION 1</u>	CAST-IN-PLACE	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
FIXED BEARING	<u>CONDITION</u>	STEEL	<u>LOCATION 1</u>	FLAT PLATE	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
EXPANSION BEARING	<u>CONDITION</u>	STEEL	<u>LOCATION 1</u>	ROCKER	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>



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# Missouri Department of Transportation

## State Bridge Inspection Report

June 09, 2021  
9:14:44AM

**COUNTY: MCDONALD**

**DISTRICT: SW**

**CLASS: STATBR**

**FED-ID: 8011**

**BRIDGE: P0973**

BENT-7	RA-45 DEGREES	29 FT 2 IN	REINFORCED CONCRETE	MULTIPLE COLUMN			
	<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
	<u>ASSOCIATED COMPONENT</u>		<u>MATERIAL</u>	<u>CONSTRUCTION</u>			
	BEAM CAP		REINFORCED CONCRETE	CAST-IN-PLACE			
	<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
	DELAMINATION		THROUGHOUT		MINOR		
	HORIZONTAL CRACKS		RANDOM		FINE		
	SATURATION		THROUGHOUT		MODERATE		
	VERTICAL CRACKS		RANDOM		FINE		
	COLUMN		REINFORCED CONCRETE	CAST-IN-PLACE			
	<u>CONDITION</u>	<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>	
	FOOTING	REINFORCED CONCRETE	SPREAD				
	<u>CONDITION</u>	<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>	
	EXPANSION BEARING	STEEL	SLIDING FLAT PLATE				
	<u>CONDITION</u>	<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>	
	FIXED BEARING	STEEL	FLAT PLATE				
	<u>CONDITION</u>	<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>	
ABUTMENT-8	RA-45 DEGREES	31 FT 7 IN	REINFORCED CONCRETE	OPEN CONCRETE			
	<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
	<u>ASSOCIATED COMPONENT</u>		<u>MATERIAL</u>	<u>CONSTRUCTION</u>			
	BEAM CAP		REINFORCED CONCRETE	CAST-IN-PLACE			
	<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
	DELAMINATION		RANDOM		SMALL		
	HORIZONTAL CRACKS		TOP		LARGE		
	COLUMN		REINFORCED CONCRETE	CAST-IN-PLACE			
	<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
	STRAIGHT WINGS		REINFORCED CONCRETE	CAST-IN-PLACE			
	<u>CONDITION</u>	<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>	
	FOOTING	REINFORCED CONCRETE	SPREAD				
	<u>CONDITION</u>	<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>	
	BACKWALL	REINFORCED CONCRETE	CAST-IN-PLACE				
	<u>CONDITION</u>	<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>	
	EXPANSION BEARING	STEEL	SLIDING FLAT PLATE				
	<u>CONDITION</u>	<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>	
	PACK RUST	THROUGHOUT		MINOR			
	RUSTING	BOTTOM		HEAVY			

**\*\*\*OVER/UNDER ROUTES CLEARANCE INFORMATION\*\*\***

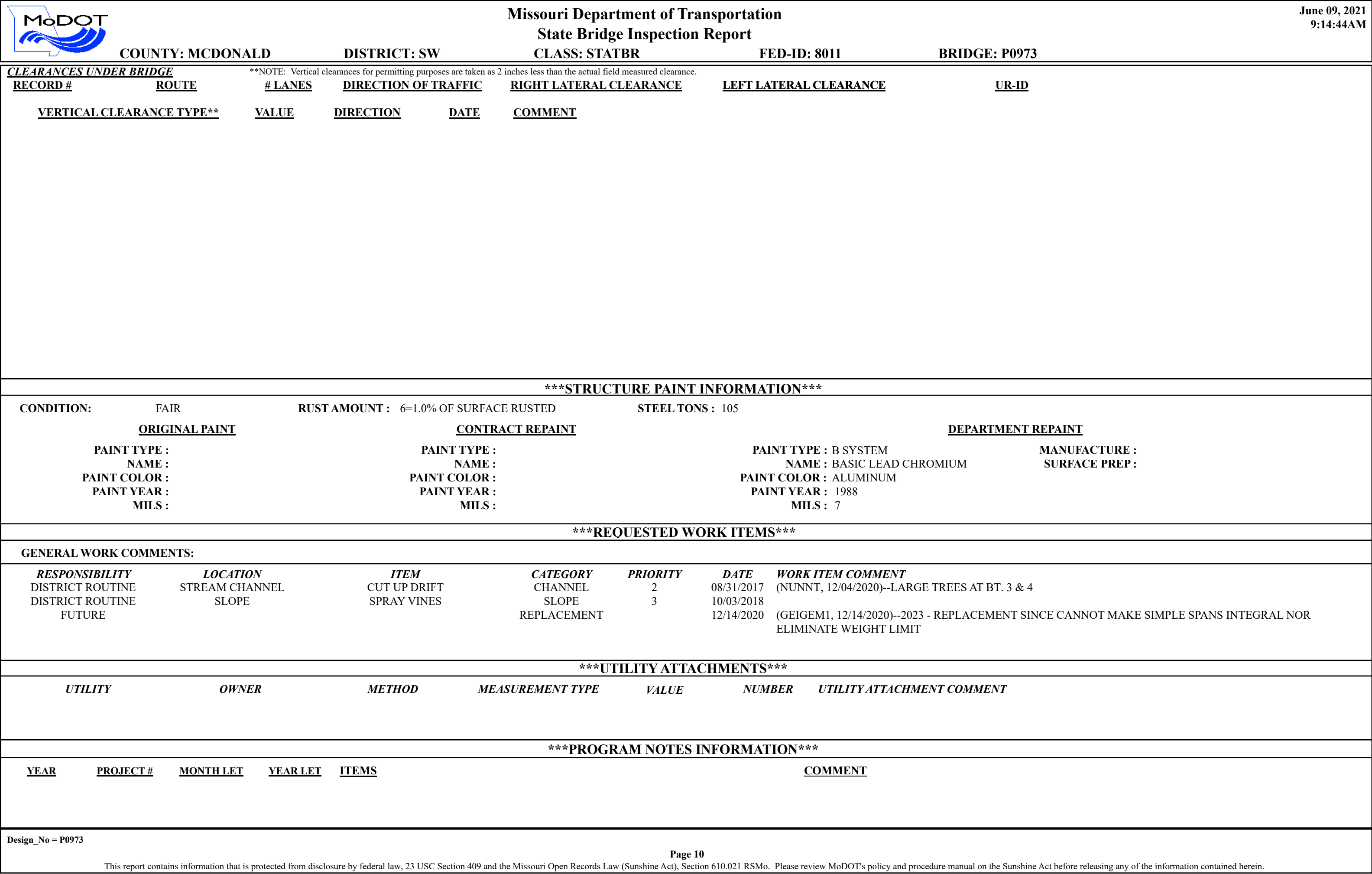
## CLEARANCES OVER DECK


**\*\*NOTE:** Vertical clearances for permitting purposes are taken as 2 inches less than the actual field measured clearance.

<u>VERTICAL CLEARANCE TYPE**</u>	<u>VALUE</u>	<u>DIRECTION</u>	<u>DATE</u>	<u>COMMENT</u>
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**Design\_No = P0973**

This report contains information that is protected from disclosure by federal law, 25 USC Section 409 and the Missouri Open Records Law (Sunshine Act), Section 610.021 RSMo. Please review MoDOT's policy and procedure manual on the Sunshine Act before releasing any of the information contained herein.



			<b>Missouri Department of Transportation</b>		<b>June 09, 2021</b>	
			<b>State Bridge Inspection Report</b>		<b>9:14:44AM</b>	
<b>COUNTY: MCDONALD</b>			<b>DISTRICT: SW</b>		<b>CLASS: STATBR</b>	
			<b>FED-ID: 8011</b>		<b>BRIDGE: P0973</b>	
<b>***COMPUTER GENERATED RATINGS AND DEFICIENCY ITEMS***</b>					<b>***ADVANCED SIGN INFORMATION***</b>	
NOTE: The items listed in this section are updated whenever computer edits are ran on a structure after the inspection updates have been entered in to TMS.					<b>SIGN #</b>	
<b><u>Rated Item</u></b>					<b><u>Rating</u></b>	
<b><u>Rating Date</u></b>					<b><u>Problem</u></b>	
<b><u>Problem Direction</u></b>						
[Item 67] Structure Evaluation Rating: 2-BASICALLY INTOLRBLE REQ 4/2/2003					1 DELINEATOR	
[Item 68] Deck Geometry Rating: 3-BASICALLY INTOL CORRECT 3/22/2002					2 B - ONE LANE BRIDGE	
[Item 69] Underclearance: N-NOT APPLICABLE 5/18/2001					3 YIELD TO ONCOMING TRAFFIC	
Sufficiency Rating: 47.5% 12/8/2020						
Deficiency: STRUCTURAL 4/2/2003						
Funding Eligibility: FULL ----						
Estimated New Structure Length: 420 FT. ----						
Estimated Structure Cost: \$1,461,348 ----						
Estimated Total Project Cost: \$2,192,022 ----						
Year of Cost Estimate: 2021 ----						
NOTE: The above structure length and cost estimates are computer generated using algorithms in the TMS system. These algorithms are generalized to use NBI items to come up with a new structure length and width to calculate a new area which is taken times a representative cost per square foot. The actual structure size and cost may vary significantly from these numbers once site specific engineering is done.						
					<b>***OUTFALL INSPECTION INFORMATION***</b>	
					<b># OUTFALLS:</b>	
					<b>INSPECTOR:</b>	
					<b>STATUS:</b>	
					<b>DATE:</b>	
					<b>NOTES:</b>	

