

BIDDING AND CONTRACT DOCUMENTS
ADDENDUM NUMBER ONE
STATE PROJECT NO. 23-017A-1

DATE: MAY 29, 2024

HIGGINSVILLE INDUSTRIAL MUNICIPAL AIRPORT

CRAWFORD, MURPHY, TILLY, INC.
1627 MAIN STREET, SUITE 600
KANSAS CITY, MISSOURI 64108



TO: ALL PLANHOLDERS AND POTENTIAL BIDDERS

SUBJECT: ADDENDUM NUMBER ONE TO THE BIDDING DOCUMENTS FOR:
RUNWAY 16-34 FULL-DEPTH RECLAMATION;
REPLACE RUNWAY 16-34 EDGE LIGHTS

This addendum forms a part of the bidding and contract documents, and modifies the original bidding documents dated May 7, 2024. This addendum must be signed on the last page and included with the submitted Bid Package. An acknowledgement sheet is also attached. This must be signed and returned to Crawford, Murphy, & Tilly, Inc via e-mail by June 3, 2024. FAILURE TO NOT RECOGNIZE THE ADDENDUM ON THE BID FORM MAY SUBJECT THE BIDDER TO DISQUALIFICATION.

The Contract Documents are revised as follows:

CONTRACT PROPOSAL

Page iv

REVISE in the paragraph titled Contract Time the words “sixty (60) calendar days” to “seventy-seven (77) calendar days”.

Page 38

REPLACE the table in Section 80-08 with the following:

Schedule Item	Allowed Construction Time	Liquidated Damages Cost
Entire Project Duration	77 consecutive calendar days	\$1,000 per Day
Phase 2	21 consecutive calendar days	\$1,000 per Day

Page 166

REPLACE the first sentence in P-207-2.2.b. with the following:

Cement shall meet the requirements of ASTM C150, Types I, II or V; ASTM C595, Types IS, IP, IL, or IT.

Page 252

ADD to the first sentence of 125-2.6 after the word “L-853” the words “, 24” height.”.

Page 290

REVISE in Item g. the words “sixty (60) Calendar days” to “seventy-seven (77) Calendar days”.

REPLACE the table in Item h. with the following:

Schedule	Liquidated Damages Cost	Allowed Construction Time
Entire Project Duration	\$1,000 per calendar day	77 Calendar Days
Phase 2	\$1,000 per calendar day	21 Calendar Days

Page 309

REVISE in Article 5 the words “sixty (60) calendar days” to “seventy-seven (77) calendar days”.

REPLACE the table in Article 6 with the following:

Schedule	Liquidated Damages Cost	Allowed Construction Time
Entire Project Duration	\$1,000 per calendar day	77 Calendar Days
Phase 2	\$1,000 per calendar day	21 Calendar Days

PLANS

Sheet 2 of 53

REPLACE Sheet 2 with Sheet 2, Revision 1.

Sheet 4 of 53

REPLACE Sheet 4 with Sheet 4, Revision 1.

Sheet 21 of 53

REPLACE Sheet 21 with Sheet 21, Revision 1.

Sheet 22 of 53

REPLACE Sheet 22 with Sheet 22, Revision 1.

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REPLACE Sheet 23 with Sheet 23, Revision 1.

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REPLACE Sheet 24 with Sheet 24, Revision 1.

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REPLACE Sheet 25 with Sheet 25, Revision 1.

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REPLACE Sheet 26 with Sheet 26, Revision 1.

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REPLACE Sheet 27 with Sheet 27, Revision 1.

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REPLACE Sheet 28 with Sheet 28, Revision 1.

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REPLACE Sheet 29 with Sheet 29, Revision 1.

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REPLACE Sheet 30 with Sheet 30, Revision 1.

Sheet 31 of 53

REPLACE Sheet 31 with Sheet 31, Revision 1.

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REPLACE Sheet 34 with Sheet 34, Revision 1.

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REPLACE Sheet 35 with Sheet 35, Revision 1.

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REPLACE Sheet 48 with Sheet 48, Revision 1.

CLARIFICATION

1. The phasing is being revised to allow for Fall of 2024 paving with the airfield lighting being performed in the Spring of 2025. This is strictly optional to benefit the Contractor with estimating asphalt pricing. If a grant is received and the project is awarded, the Notice of Award will be issued no later than October 1, 2024. If the federal grant is issued earlier, the Notice of Award will be issued shortly after receipt of the grant to maximize the opportunity to complete paving in the Fall of 2024. If the Contractor chooses to start paving in the Fall of 2024, calendar days will be charged continuously until the runway and taxiway paving is completed, the initial coat of pavement markings has been applied, and the pavement is reopen to aircraft traffic. Weather limitations shall still apply. The bid prices shall not be changed throughout the project regardless of when paving occurs. Total contract time has been increased from 60 calendar days to 77 calendar days. The 77 calendar days shall apply regardless of when paving occurs.
2. The description for pay item P-207-5.2 is Cement – Type I but all types listed in the revised P-207-2.2.b. will be allowed.
3. The bituminous surface course shall be 3” as shown on the typical sections and may be paved in one or two lifts. The 2” milling depth may be adjusted by the Contractor to minimize the need to haul off excess FDR material or bring in additional material for FDR to meet proposed top of base grades.
4. The Engineer reserves the right to issue additional revisions to the staking plan (minor adjustments only) for the taxiway pavement between the award of the project and the start of construction.
5. The L-853 retroreflective markers are to be 24” tall to match the proposed lights.

CRAWFORD, MURPHY & TILLY, INC.

This Addendum consists of 3 pages, an acknowledgment transmittal sheet and 16 revised plan sheets.

Signed
(Contractor)

Date

Contractor to sign and date this Addendum #1 to acknowledge receipt. This signed Addendum must be included with the submitted Bid Package.

TRANSMITTAL

To: Crawford, Murphy & Tilly, Inc

Attention: **Brian Garkie**

Re: Addendum #1

E-mail: bgarkie@cmtengr.com

From:

(name)

(company)

Date:

To verify that all contractors are in receipt of this addendum, Contractors are asked to sign and date this acknowledgement sheet. The Contractor should e-mail to Brian Garkie of Crawford, Murphy, & Tilly, Inc. at the e-mail address listed below by **June 3, 2024**.

Phone: (314) 436-5500

E-mail : bgarkie@cmtengr.com

BY: CRAWFORD, MURPHY, & TILLY, INC.

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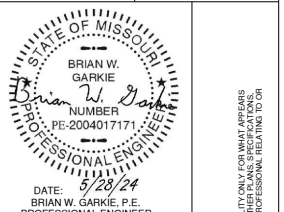
SUMMARY OF QUANTITIES

BASE BID					
ITEM	DESCRIPTION	UNIT	RUNWAY QUANTITY	TAXIWAYS QUANTITY	TOTAL QUANTITY
C-100-14.1	CONTRACTOR QUALITY CONTROL PROGRAM (CQCP)	LS	1	0	1
C-105-6.1	MOBILIZATION (10% LIMIT)	LS	1	0	1
P-101-5.1	MILLING - 2"	SY	37,500	9,500	47,000
P-207-5.1	IN-PLACE FULL-DEPTH RECLAMATION - 12"	SY	37,500	9,500	47,000
P-207-5.2	CEMENT - TYPE I	TON	1,600	400	2,000
P-401-8.1	ASPHALT SURFACE COURSE	TON	6,600	1,700	8,300
P-603-5.1	EMULSIFIED ASPHALT TACK COAT	GAL	4,500	1,200	5,700
P-620-5.1	WHITE MARKINGS, WITH REFLECTIVE MEDIA	SF	25,850	150	26,000
P-620-5.2	YELLOW MARKINGS, WITH REFLECTIVE MEDIA	SF	600	3,700	4,300
P-620-5.3	BLACK MARKINGS, WITHOUT REFLECTIVE MEDIA	SF	8,400	6,200	14,600
P-620-5.4	MARKING REMOVAL	SF	0	6,000	6,000
T-901-5.1	SEEDING	AC	0.7	0.3	1.0
T-908-5.1	HYDRAULICALLY APPLIED MULCHING	AC	0.7	0.3	1.0
L-107-5.1	L-807(L) WIND CONE, INTERNALLY LIGHTED, 12-FEET (IN-PLACE)	EA	1	0	1
L-107-5.2	REMOVE PRIMARY WIND CONE	EA	1	0	1
L-108-5.1	1/C #8 L-824, TYPE C, 5KV CABLE	LF	10,500	0	10,500
L-108-5.2	2 - 1/C #8, L-824, TYPE C, 5KV CABLE	LF	700	0	700
L-108-5.3	1/C #6 BARE COPPER COUNTERPOISE CABLE	LF	12,500	0	12,500
L-108-5.4	GROUND ROD - 3/4" X 10' COUNTERPOISE SYSTEM	EA	44	0	44
L-108-5.5	WIND CONE CIRCUIT, 2 #8 TYPE USE, 1 #8 GND., IN 1" UNIT DUCT	LF	80	0	80
L-108-5.6	BEACON CIRCUIT, 2 #10 TYPE USE, 1 #10 GND., IN 1" UNIT DUCT	LF	60	0	60
L-109-7.1	11'-2" X 9'-2" PREFABRICATED ELECTRICAL VAULT SHELTER	LS	1	0	1
L-109-7.2	VAULT FOUNDATION	LS	1	0	1
L-109-7.3	L-854 RADIO CONTROLLER	LS	1	0	1
L-109-7.4	L-828, 4 KW FERRORESONANT REGULATOR	EA	1	0	1
L-109-7.5	REMOVAL OF EXISTING ELECTRICAL VAULT EQUIPMENT	LS	1	0	1
L-110-5.1	3" HDPE CONDUIT DIRECTIONAL BORE	LF	530	0	530
L-110-5.2	2" PVC CONDUIT DIRECT BURIED	LF	11,000	0	11,000
L-110-5.3	CONDUIT DRAIN END SECTION	EA	5	0	5
L-110-5.4	CONDUIT DRAIN DIRECT CONNECTION	EA	2	0	2
L-125-5.1	MIRL LED BASE MOUNTED	EA	42	0	42
L-125-5.2	MITL LED BASE MOUNTED	EA	28	0	28
L-125-5.3	THRESHOLD LIGHT LED BASE MOUNTED	EA	16	0	16
L-125-5.4	L-853 RETROFLECTIVE MARKER	EA	0	109	109
L-125-5.5	RUNWAY HOLD SIGN, UNLIGHTED - ON EXISTING PCC BASE	EA	5	0	5
L-125-5.6	RUNWAY HOLD SIGN, UNLIGHTED - ON NEW PCC BASE	EA	2	0	2
L-125-5.7	REMOVE EDGE LIGHT	EA	84	0	84
L-125-5.8	SPARE MIRL LED	EA	4	0	4
L-125-5.9	SPARE MITL LED	EA	2	0	2
L-125-5.10	SPARE THRESHOLD LIGHT LED	EA	2	0	2
L-125-5.11	SPARE L-853 RETROFLECTIVE MARKER	EA	0	10	10
L-125-5.12	L-849(L) REIL(S) REPLACEMENT	PAIR	2	0	2
SP-1-5.1	TRAFFIC CONTROL	LS	1	0	1

ADDITIVE ALTERNATE NO 1					
ITEM	DESCRIPTION	UNIT	RUNWAY QUANTITY	TAXIWAYS QUANTITY	TOTAL QUANTITY
L-107-5.3	L-806(L) WIND CONE, INTERNALLY LIGHTED, 8-FEET (IN-PLACE)	EA	2	0	2
L-107-5.4	REMOVE SUPPLEMENTAL WIND CONE AND FOUNDATION	EA	2	0	2
L-108-5.2	2 - 1/C #8, L-824, TYPE C, 5KV CABLE	LF	600	0	600
L-108-5.7	PAPI 16 CIRCUIT, 2#8 TYPE USE, 1 #10 GND., 1-1/4" IN UNIT DUCT	LF	2,400	0	2,400
L-108-5.8	PAPI 34 CIRCUIT, 2#6 TYPE USE, 1 #10 GND., 1-1/4" IN UNIT DUCT	LF	2,500	0	2,500
L-110-5.2	2" PVC CONDUIT DIRECT BURIED	LF	600	0	600
L-125-5.13	LED RUNWAY PAPI, 4 BOX SYSTEM REPLACEMENT	EA	2	0	2

REVISIONS		
NUMBER	BY	DATE
1	DK	5/28/24

HIGGINSVILLE MUNICIPAL AIRPORT
HIGGINSVILLE, MISSOURI
LAFAYETTE COUNTY
RUNWAY 16-34 FULL-DEPTH RECLAMATION; REPLACE RUNWAY 16-34 EDGE LIGHTS



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CRAWFORD MURPHY & TILLY, INC.
 ENGINEERS & CONSULTANTS
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 PROFESSIONAL ENGINEERING - 00651
 MoDOT PROJECT NO. 23-017A-1

FILE: 02 - SUMMARY OF QUANTITIES.DWG
 DESIGN BY: DK
 DRAWN BY: DK
 CHECKED BY: JTZ
 APPROVED BY: BWG
 DATE: 5/7/2024
 JOB No: 22004664-00
SUMMARY OF QUANTITIES & INDEX OF SHEETS
 SHEET 2 OF 53 SHEETS

THE PROFESSIONAL WHOSE SIGNATURE AND PERSONAL SEAL APPEAR HEREON, ASSUMES RESPONSIBILITY ONLY FOR WHAT APPEARS ON THIS PAGE AND DOES NOT WARRANT TO OTHERS THAT THE INFORMATION ON THIS PAGE IS ACCURATE OR COMPLETE. THIS PROFESSIONAL ENGINEERING SEAL IS NOT TO BE USED FOR ANY PART OR PARTS OF THE PROJECT TO WHICH THIS PAGE REFERS.

GENERAL

- 1. THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL FOLLOW THE REQUIREMENTS OF THE AIRPORT'S APPROVED CONSTRUCTION SAFETY AND PHASING PLAN (CSPP), FAA AC 150/5370-2G, AND ALL AIRPORT SAFETY AND SECURITY REQUIREMENTS. THE CSPP SHALL CONSIST OF THE SITE PLAN, CSPP NOTES AND CONSTRUCTION ACTIVITY PLANS.
2. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SUBMIT TO THE CITY FOR APPROVAL A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA AC 150/5370-2G. NO CONSTRUCTION ACTIVITY SHALL BEGIN UNTIL THE CITY HAS APPROVED THE SPCD.
3. CHANGES TO THE CSPP PROPOSED BY THE CONTRACTOR SHALL BE SUBMITTED TO THE CITY FOR REVIEW AND APPROVAL. UNTIL ANY CHANGES ARE APPROVED, THE CONTRACTOR SHALL FOLLOW THE CURRENT APPROVED CSPP.
4. THE CSPP COVERS OPERATIONAL SAFETY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INDIVIDUAL SAFETY OF HIS/HER PERSONNEL AND MEETING OSHA REQUIREMENTS.
5. A MINIMUM OF 10 DAYS PRIOR TO THE PRECONSTRUCTION MEETING THE CONTRACTOR SHALL PROVIDE A LIST OF SUBCONTRACTORS AND MATERIAL SUPPLIERS.
6. ALL CONTRACTOR COSTS ASSOCIATED WITH THE REQUIREMENTS LISTED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT UNLESS A SPECIFIC PAY ITEM IS PROVIDED.
7. THE EXISTING FEATURES SHOWN ON THESE PLANS ARE THOSE NOTED IN THE FIELD AND THOSE TAKEN FROM RECORD DRAWINGS. THIS DOES NOT GUARANTEE THAT ALL FEATURES ARE SHOWN ON THE PLANS. THERE WILL BE NO ADDITIONAL PAYMENT TO THE CONTRACTOR DUE TO VARIATIONS IN SIZE, QUANTITY, OR LOCATION OF EXISTING FEATURES.
8. CRAWLER TYPE EQUIPMENT WILL NOT BE ALLOWED ON ANY PAVED SURFACE ON THE AIRPORT. ONLY RUBBER-TIRED VEHICLES, WHICH WILL NOT CAUSE DAMAGE TO THE PAVEMENTS, WILL BE ALLOWED WITHOUT PROVIDING SOME TYPE OF PROTECTION.
9. THE CONTRACTOR SHALL HAVE PROPER IDENTIFICATION ON ALL EQUIPMENT AND VEHICLES ON THE AIRPORT.
10. NO EDGE DROP GREATER THAN 3" WILL BE ALLOWED AT ANY ACTIVE RUNWAY OR TAXIWAY PAVEMENT EDGE OR SAFETY AREA. IF NECESSARY, THE CONTRACTOR WILL PLACE TEMPORARY MATERIAL TO ELIMINATE VERTICAL DROPS GREATER THAN 3" OR SLOPES GREATER THAN 5% IN THESE AREAS. THIS WORK SHALL BE SUBSIDIARY TO OTHER ITEMS IN THE PROJECT.
11. ALL DEWATERING REQUIRED TO COMPLETE THE WORK SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

1. COORDINATION

- 1. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL ATTEND A PRECONSTRUCTION CONFERENCE WITH THE AIRPORT, ENGINEER, AND THE MoDOT AVIATION SECTION. THE COST OF PREPARING FOR AND ATTENDING THE PRECONSTRUCTION CONFERENCE SHALL BE INCIDENTAL TO THE CONTRACT.
2. ON OR BEFORE THE PRECONSTRUCTION CONFERENCE, THE CONTRACTOR SHALL SUBMIT A PROPOSED SCHEDULE FOR THE PROJECT. THE SCHEDULE SHALL INCLUDE A START AND COMPLETION DATE FOR EACH ITEM OF WORK. THE SCHEDULE SHALL BE UPDATED ON A WEEKLY BASIS. ALL COSTS ASSOCIATED WITH THE SCHEDULE SHALL BE INCIDENTAL TO THE CONTRACT.
3. DURING CONSTRUCTION THE CONTRACTOR SHALL ATTEND A WEEKLY COORDINATION MEETING WITH THE ENGINEER. ATTENDANCE BY OTHER STAKEHOLDERS IS OPTIONAL. ALL COSTS ASSOCIATED WITH ATTENDING THE WEEKLY MEETING SHALL BE INCIDENTAL TO THE CONTRACT.
4. THIS PROJECT WILL NOT REQUIRE COORDINATION WITH FAA ATO. IF COORDINATION IS REQUIRED, THE CITY WILL HANDLE THE COORDINATION.

2. PHASING

- 1. TOTAL CONTRACT TIME SHALL BE 77 CALENDAR DAYS. THIS INCLUDES TIME FOR A FINAL INSPECTION, PUNCH LIST COMPLETION, FINAL PROJECT CLEANUP AND TIME FOR GENERAL CONSTRUCTION ACTIVITIES OUTSIDE OF THE SPECIFIC PHASES.
2. THE CONTRACTOR SHALL PLACE ALL BARRICADES SPECIFIED FOR USE DURING CONSTRUCTION AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN PRIOR TO STARTING WORK.
3. THE CONTRACTOR SHALL PROVIDE LIGHTED BARRICADES AT ALL CLOSURE LOCATIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING THE BARRICADES IN AN OPERABLE CONDITION FOR THE DURATION OF CONSTRUCTION.
4. AT THE END OF CONSTRUCTION, THE CONTRACTOR IS RESPONSIBLE FOR THE REMOVAL OF ALL CONSTRUCTION BARRICADES.
5. IF THE PROJECT IS AWARDED, THE NOTICE OF AWARD WILL BE ISSUED BY NO LATER THAN OCTOBER 1, 2024.

PHASE 1 NOTES

- 1. PHASE 1 SHALL CONSIST OF CONSTRUCTION OF THE ELECTRICAL VAULT BUILDING, REPLACEMENT OF THE PRIMARY WIND CONE, AND IMPROVEMENTS TO THE CONSTRUCTION ENTRANCE OF STATE HIGHWAY AA.
2. THE CONTRACTOR SHALL COORDINATE THE INSTALLATION OF THE NEW ELECTRICAL SERVICE ENTRANCE FOR THE ELECTRICAL VAULT BUILDING WITH THE CITY OF HIGGINSVILLE ELECTRICAL DEPARTMENT.
3. RUNWAY 16-34 AND ALL OTHER AIRFIELD PAVEMENTS SHALL REMAIN OPEN DURING PHASE 1.
4. PHASE 1 SHALL BE COMPLETED WITHIN 21 CONSECUTIVE CALENDAR DAYS OF THE START OF PHASE 1.
5. THE IMPROVEMENTS TO THE CONSTRUCTION ENTRANCE MAY OCCUR DURING PHASE 2, IF PHASE 2 IS TO OCCUR PRIOR TO PHASE 1.

PHASE 2 NOTES

- 1. PHASE 2 SHALL CONSIST OF 2" SURFACE MILLING OF THE PAVEMENTS TO BE RECLAIMED, CONSTRUCTION OF THE HAUL ROUTE, FULL DEPTH RECLAMATION OF THE REMAINING PAVEMENT AND BASE, PAVING OF THE BITUMINOUS SURFACE COURSE, AND PAVEMENT MARKINGS.
2. RUNWAY 16-34 AND ALL TAXIWAY PAVEMENTS SHALL BE CLOSED DURING PHASE 2 AND ALL APRON PAVEMENT SHALL BE CLOSED DURING THE PAVEMENT MARKING WORK.
3. PHASE 2 MAY OCCUR BEFORE PHASE 1 DURING THE FALL OF 2024, PROVIDED WEATHER LIMITATIONS CAN BE MET. IF PHASE 2 IS TO OCCUR IN THE SPRING OF 2025, THEN PHASE 2 SHALL BE STARTED AFTER THE COMPLETION OF PHASE 1. THERE SHALL BE NO ADJUSTMENT TO ANY CONTRACT PRICES REGARDLESS OF WHEN PHASE 2 OCCURS.
4. PHASE 2 SHALL BE COMPLETED WITHIN 21 CONSECUTIVE CALENDAR DAYS OF THE START OF PHASE 2.

PHASE 3 NOTES

- 1. PHASE 3 SHALL CONSIST OF THE AIRFIELD LIGHTING WORK, THE FINAL INSPECTION, PUNCH LIST COMPLETION AND FINAL CLEANUP AND REMOVAL/RESTORATION OF ANY REMAINING HAUL ROUTE.
2. PHASE 3 WORK SHALL BEGIN FOLLOWING THE COMPLETION OF PHASE 1 AND IF PHASE 2 IS STARTED IN THE SPRING OF 2025, PHASE 3 WORK MAY OCCUR DURING PHASE 2, BUT CALENDAR DAYS IN PHASE 3 WILL NOT BE CHARGED UNTIL AFTER THE FULL NUMBER OF DAYS ALLOWED IN PHASE 2 HAVE BEEN EXHAUSTED.
3. PHASE 3 SHALL BE COMPLETED WITHIN 35 CONSECUTIVE CALENDAR DAYS WHICH IS IN ADDITION TO THE DAYS ALLOWED IN PHASE 2.
4. FOR THE FIRST 21 CONSECUTIVE CALENDAR DAYS, RUNWAY 16-34 SHALL BE CLOSED. FOR THE FINAL 14 CONSECUTIVE CALENDAR DAYS, WORK IN PHASE 3 SHALL BE LIMITED TO MONDAY THROUGH FRIDAY FROM 7:30 AM TO 4:30 PM WITH RUNWAY 16-34 CLOSED DURING WORKING HOURS AND OPEN DURING NON-WORKING HOURS.

RUNWAY CLOSURES

- 1. TO CLOSE THE RUNWAY, THE CONTRACTOR SHALL NOTIFY THE AIRPORT A MINIMUM OF 72 HOURS IN ADVANCE. AT THE TIME OF THE CLOSURE, THE CONTRACTOR SHALL DEACTIVATE THE CIRCUIT FOR THE RUNWAY LIGHTS AND BOTH PAPI LOCATIONS, PLACE THE RUNWAY CLOSURE MARKERS ON THE RUNWAY AND PLACE THE BARRICADES AT THE LOCATIONS SHOWN ON THE PHASING SHEETS. TO REOPEN THE RUNWAY, THE CONTRACTOR SHALL CLEAN THE RUNWAY PAVEMENT, RESTORE POWER TO ANY CIRCUITS NOT STILL UNDER CONSTRUCTION, REMOVE THE RUNWAY CLOSURE MARKERS, AND NOTIFY THE ENGINEER THAT THE PAVEMENT IS READY TO BE INSPECTED.

3. AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY

- 1. ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED ON THE CONSTRUCTION ACTIVITY PLAN.
2. WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE CITY.
3. ALL CONSTRUCTION TRAFFIC SHALL IMMEDIATELY YIELD TO ONCOMING AIRCRAFT AT ALL TIMES.

4. PROTECTION OF NAVIGATION AIDS (NAVAIDS)

- 1. THE CONTRACTOR SHALL REMAIN CLEAR OF THE PAPI SYSTEMS, REIL SYSTEMS, WIND CONE, BEACON AND OTHER NAVAIDS FACILITIES AT ALL TIMES.
2. THE CONTRACTOR SHALL DESIGNATE AT LEAST ONE PERSON TO MONITOR THE AIRPORT UNICOM FREQUENCY OF 122.80. THE PERSON DESIGNATED SHALL HAVE THE ABILITY TO EASILY COMMUNICATE WITH OTHER CONTRACTOR PERSONNEL WORKING ON THE JOBSITE. THE CONTRACTOR SHALL PROVIDE HIS/HER OWN AIRPORT RADIO(S).
3. THE STORAGE AND STAGING AREA WILL BE AS SHOWN ON THE SITE PLAN.

5. CONTRACTOR ACCESS

- 1. CONTRACTOR ACCESS SHALL BE AS NOTED BELOW AND AS SHOWN ON THE SITE PLAN AND CONSTRUCTION ACTIVITY PLAN SHEET.
2. THE CONTRACTOR SHALL DESIGNATE AT LEAST ONE PERSON TO MONITOR THE AIRPORT UNICOM FREQUENCY OF 122.80. THE PERSON DESIGNATED SHALL HAVE THE ABILITY TO EASILY COMMUNICATE WITH OTHER CONTRACTOR PERSONNEL WORKING ON THE JOBSITE. THE CONTRACTOR SHALL PROVIDE HIS/HER OWN AIRPORT RADIO(S).
3. THE STORAGE AND STAGING AREA WILL BE AS SHOWN ON THE SITE PLAN.
4. THE CONTRACTOR SHALL KEEP A RECORD OF THE NAMES OF ALL EMPLOYEES ENTERING THE JOB SITE ON A DAILY BASIS. A RECORD OF EACH SUBCONTRACTOR ENTERING THE JOB SITE SHALL ALSO BE KEPT BY THE CONTRACTOR.
5. WHEN THE CONTRACTOR IS NOT WORKING, EQUIPMENT SHALL BE STORED AT THE STAGING AREA OR WITHIN THE WORK AREA LIMITS (SEE NOTE 6).
6. THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS ONLY AT THE LOCATIONS SHOWN. PARKED EQUIPMENT AND MATERIAL STOCKPILES SHALL NOT PENETRATE SURFACES DEFINED BY F.A.R. TITLE 14 PART 77 - OBJECTS AFFECTING NAVIGABLE AIRSPACE.
7. ALL CONSTRUCTION TRAFFIC OPERATING ON, OR CROSSING RUNWAYS, TAXIWAYS AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL BY A FLAGMAN OR ESCORT WHO IS MONITORING THE AIRPORT UNICOM FREQUENCY. THE CONTRACTOR SHALL PROVIDE HIS/HER OWN FLAGMEN.
8. THE CONTRACTOR SHALL THOROUGHLY CLEAN ALL CONSTRUCTION AREAS AND HAUL ROUTES WHICH WILL BE OPENED TO AIR TRAFFIC TO THE SATISFACTION OF THE ENGINEER. A POWER BROOM AND OPERATOR SHALL BE ON SITE AT ALL TIMES WHEN ACTIVE PAVEMENTS ARE UTILIZED FOR CONSTRUCTION TRAFFIC.

5. CONTRACTOR ACCESS CONT.

- 9. ALL PAVEMENTS, DRIVEWAYS OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED TO THE SAME CONDITION OR BETTER THAN THEY WERE PRIOR TO BEGINNING CONSTRUCTION. NO ADDITIONAL COMPENSATION WILL BE MADE TO THE CONTRACTOR FOR THIS WORK.
10. ALL VEHICLE AND EQUIPMENT OPERATORS USED BY THE CONTRACTOR SHALL BE PROPERLY TRAINED BY THE CONTRACTOR.
11. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE RESTORATION, SEEDING, AND MULCHING OF THE CONTRACTOR'S STORAGE AREAS AND HAUL ROADS.

6. WILDLIFE MANAGEMENT

- 1. WITH THE EXCEPTION OF SMALL BIRDS, THE CONTRACTOR SHALL NOTIFY THE ENGINEER OR CITY IF ANY WILDLIFE IS SEEN ENTERING OR MOVING AROUND THE AIRPORT.
2. THE CONTRACTOR SHALL DISPOSE OF ALL TRASH INCLUDING FOOD SCRAPS IN APPROVED CONTRACTOR PROVIDED CONTAINERS.
3. THE CONTRACTOR SHALL AVOID CREATING ANY STANDING WATER AND SHALL GRADE TEMPORARY DISTURBED AREAS TO DRAIN.
4. THE CONTRACTOR SHALL MOW TALL GRASS WITHIN THE WORK AREA PRIOR TO THE HEIGHT REACHING 12" ABOVE THE GROUND.

7. FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT

- 1. THE CONTRACTOR SHALL PICK UP ANY FOREIGN OBJECT DEBRIS (FOD) SEEN ON THE AIRFIELD PAVEMENTS.
2. THE CONTRACTOR SHALL SECURE ALL LOOSE ITEMS FROM VEHICLES PRIOR TO DRIVING ON AIRFIELD PAVEMENTS.

8. HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT

- 1. THE CONTRACTOR SHALL DEVELOP A HAZMAT MANAGEMENT PLAN AND KEEP COPIES ON THE JOBSITE OF MATERIAL SAFETY DATA SHEETS (MSDS) FOR ALL MATERIALS HANDLED ON THE JOBSITE.

9. NOTIFICATION OF CONSTRUCTION ACTIVITIES

- 1. THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER.
2. THE CONTRACTOR SHALL GIVE A MINIMUM OF 72 HOURS NOTICE TO THE CITY PRIOR TO CLOSING ANY PAVEMENTS SO THAT PROPER NOTAMS MAY BE ISSUED BY THE CITY AND TO ALLOW FOR COORDINATION WITH THE AIRPORT TENANTS BY THE CITY.
3. FOR ANY EQUIPMENT USED BY THE CONTRACTOR WITH A HEIGHT GREATER THAN THE HEIGHTS SPECIFIED BELOW, THE CONTRACTOR SHALL SUBMIT FAA FORM 7460-1 TO THE FAA FOR AN AIRSPACE STUDY. NO EQUIPMENT WITH A HEIGHT GREATER THAN 25' SHALL BE USED UNTIL A DETERMINATION FROM FAA IS RECEIVED.
4. THE CONTRACTOR SHALL NOTIFY THE LOCAL FIRE DEPARTMENT IF CONSTRUCTION ACTIVITY WILL REQUIRE THE BLOCKAGE OF EMERGENCY ACCESS TO THE AIRPORT.
5. IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911.
6. CONTACTS FOR THIS PROJECT ARE AS LISTED BELOW.

Table with 3 columns: Role, Name, Contact Info. Includes City (Owner) Scott McCormick, Project Engineer Brian Garkie, Water/Wastewater Brandon Craig, Electric Ron Kumm, and Emergency contact 911.

10. INSPECTION REQUIREMENTS

- 1. THE CONTRACTOR SHALL INSPECT THE JOBSITE DAILY TO ENSURE COMPLIANCE WITH THE CSPP. THE CHECKLIST FOUND IN APPENDIX 3 OF FAA AC 150/5370-2G MAY BE USED TO AID IN THE INSPECTIONS.
2. THE CONTRACTOR SHALL ATTEND A FINAL INSPECTION WITH THE ENGINEER, MoDOT AVIATION AND CITY OF HIGGINSVILLE AND COMPLETE ANY PUNCH LIST ITEMS WITHIN THE SPECIFIED CONTRACT TIME.

11. UNDERGROUND UTILITIES

- 1. EXTREME CAUTION SHALL BE USED WHEN WORKING NEAR EXISTING LIGHT FIXTURES, UNDERGROUND ELECTRICAL CABLE OR DUCTS, AND ANY OTHER EXISTING AIRPORT FIXTURE. SHOULD DAMAGE OCCUR TO ANY OF THESE ITEMS, THEY WILL BE REPLACED AT THE CONTRACTOR'S EXPENSE TO THE SATISFACTION OF THE ENGINEER.
2. IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION.
3. BEFORE INITIATING ANY DIGGING, DRILLING OR EXCAVATING ON THE AIRPORT PROPERTY, THE CONTRACTOR SHALL CALL 1-800-DIG-RITE TO ARRANGE FOR UTILITY LOCATES.

12. PENALTIES

- 1. NONCOMPLIANCE BY THE CONTRACTOR WITH AIRPORT RULES AND REGULATIONS OR FAILURE TO COMPLY WITH THE AIRPORT'S APPROVED CSPP AND THE CONTRACTOR'S APPROVED SPCD MAY RESULT IN FINES AS ALLOWED BY LAW.

13. SPECIAL CONDITIONS

- 1. TENANT ACCESS FROM THE HANGAR AREA TO THE RUNWAY MAY IMPACT THE OPERATIONS OF THE CONTRACTOR. SEE THE COORDINATION NOTES FOR ADDITIONAL INFORMATION.

14. RUNWAY AND TAXIWAY VISUAL AIDS

- 1. BARRICADES SHALL BE USED AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEET.

15. MARKING AND SIGNS FOR ACCESS ROUTES

- 1. LATH LINES SHALL BE USED ALONG THE CONTRACTOR'S ACCESS ROUTE AS DETAILED ON THIS SHEET AND THE CONSTRUCTION ACTIVITY PLAN SHEET.

16. HAZARD MARKING AND LIGHTING

- 1. THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN MARKINGS AND ASSOCIATED LIGHTING OF OPEN TRENCHES, EXCAVATIONS, TEMPORARY STOCKPILES, AND HIS/HER CONSTRUCTION EQUIPMENT.
2. ALL CONSTRUCTION EQUIPMENT SHALL BE FLAGGED AND/OR LIGHTED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2G AND 150/5210-5D AT ALL TIMES WHILE OPERATING ON AIRPORT PROPERTY. THE MAXIMUM EQUIPMENT HEIGHT IS 25' EXCEPT FOR TALLER EQUIPMENT SUBMITTED BY THE CONTRACTOR AND APPROVED BY FAA.
3. BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEET OR AS DIRECTED BY THE ENGINEER.
4. THE CONTRACTOR SHALL INSPECT THE BARRICADES ONCE DURING EACH WORK DAY TO INSURE PROPER PLACEMENT AND PROPER OPERATION OF THE RED LIGHTS.

17. PROTECTION

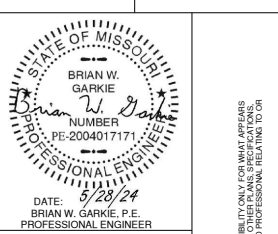
- 1. NO WORK SHALL BE PERFORMED/REQUIRED INSIDE OF THE ACTIVE RUNWAY 16-34 SAFETY AREA WHICH EXTENDS 75' FROM THE RUNWAY CENTERLINE.
2. NO WORK SHALL BE PERFORMED/REQUIRED INSIDE OF AN ACTIVE TAXIWAY SAFETY AREA WHICH EXTENDS 39.5' FROM THE TAXIWAY CENTERLINE AND 24.5' FROM THE T-HANGAR TAXILANE CENTERLINE.
3. ANY WORK INSIDE AN ACTIVE TAXILANE OBJECT FREE AREA SHALL BE PERFORMED SO THAT A 5' CLEARANCE IS MAINTAINED BETWEEN EQUIPMENT AND MATERIALS AND ANY PART OF AN AIRCRAFT INCLUDING WINGTIP OVERHANG. IF THE 5' CLEARANCE CANNOT BE MAINTAINED, THE CONTRACTOR SHALL MOVE ALL EQUIPMENT AND PERSONNEL FROM THE TAXILANE OBJECT FREE AREA FOR EACH PASSING AIRCRAFT. THE FULL OBJECT FREE AREA EXTENDS 39.5' FROM THE TAXILANE CENTERLINE.

18. OTHER LIMITATIONS ON CONSTRUCTION

- 1. IF, DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE CITY, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES, PERSONNEL AND EQUIPMENT.
2. BROKEN CONCRETE, BROKEN ASPHALT, AND OTHER MISCELLANEOUS DEBRIS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS OTHERWISE SPECIFIED.
3. THE CONTRACTOR SHALL PROMOTE POLICIES AND INITIATIVES FOR EMPLOYEES AND OTHER WORK PERSONNEL THAT DECREASE CRASHES BY DISTRACTED DRIVERS, INCLUDING POLICIES TO BAN TEXT MESSAGING WHILE DRIVING VEHICLES OR OPERATING EQUIPMENT. THE CONTRACTOR SHALL INCLUDE THESE POLICIES IN EACH THIRD PARTY SUBCONTRACT INVOLVED ON THIS PROJECT.
4. THE CONTRACTOR SHALL CONTROL DUST CREATED BY HIS/HER OPERATIONS AS DIRECTED BY THE ENGINEER. AT A MINIMUM, A WATER TRUCK SHALL BE AVAILABLE WITHIN 24 HOURS OF NOTIFICATION BY THE ENGINEER, CITY OR AIRPORT MANAGER. THERE SHALL BE NO ADDITIONAL COMPENSATION FOR WORK REQUIRED FOR DUST CONTROL.

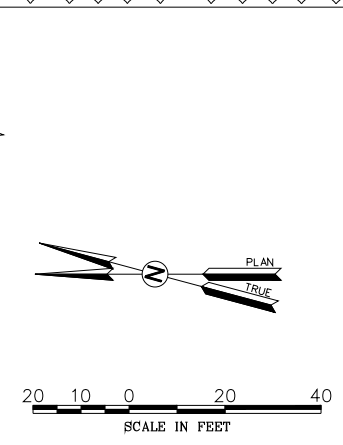
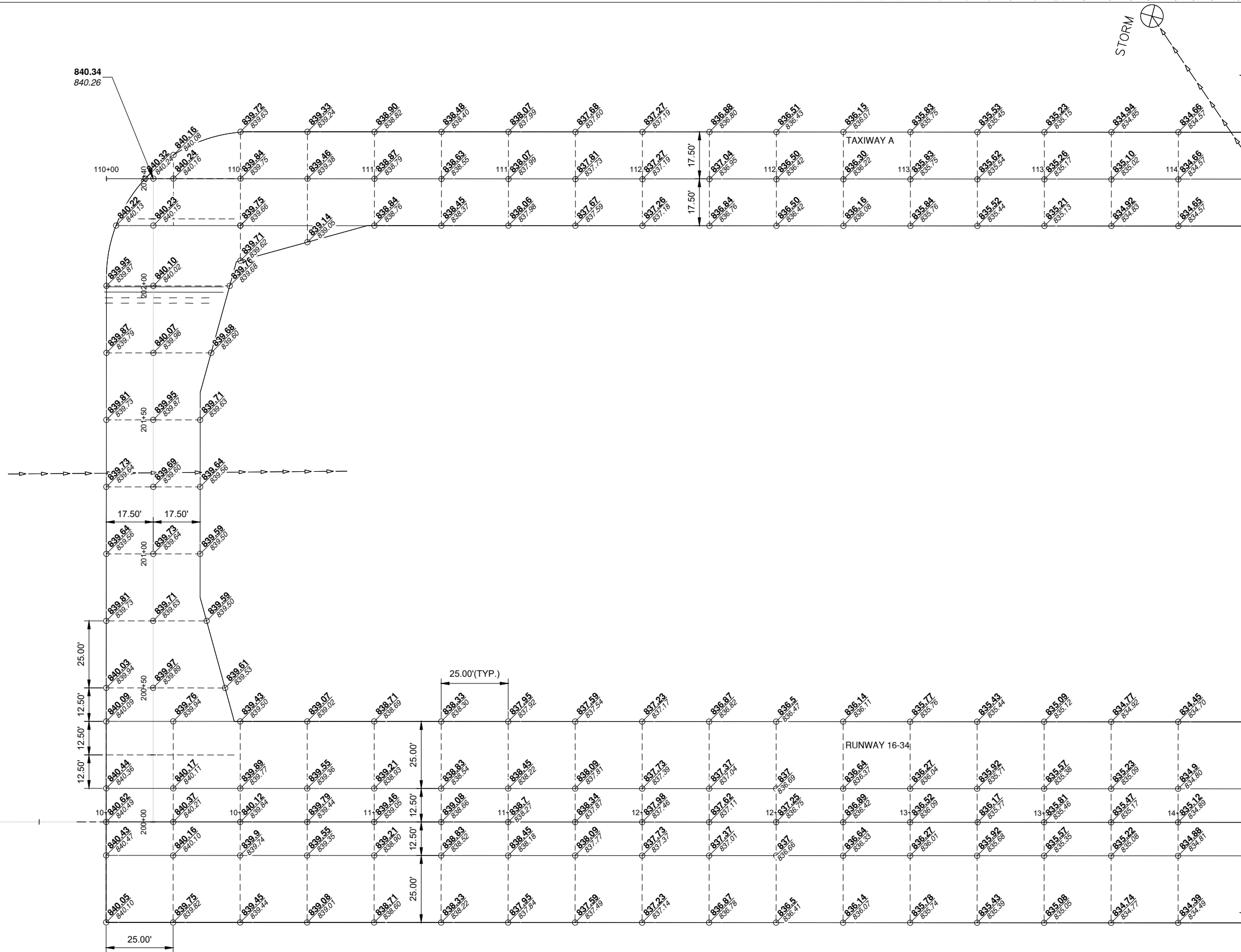
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HIGGINSVILLE, MISSOURI
LAFAYETTE COUNTY
RUNWAY 16-34 FULL-DEPTH RECLAMATION; REPLACE RUNWAY 16-34 EDGE LIGHTS



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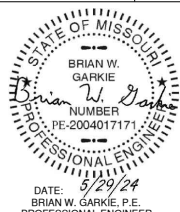
NOTES

1. THE TURF ALONG THE EDGE OF THE PAVEMENT SHALL MAINTAIN 1.5' DROP AND 5% SLOPE FOR 10'. FOR MORE INFORMATION, SEE TYPICAL SECTION DETAIL SHEET.

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DATE: 5/29/24
BRIAN W. GARKIE, P.E.
PROFESSIONAL ENGINEER

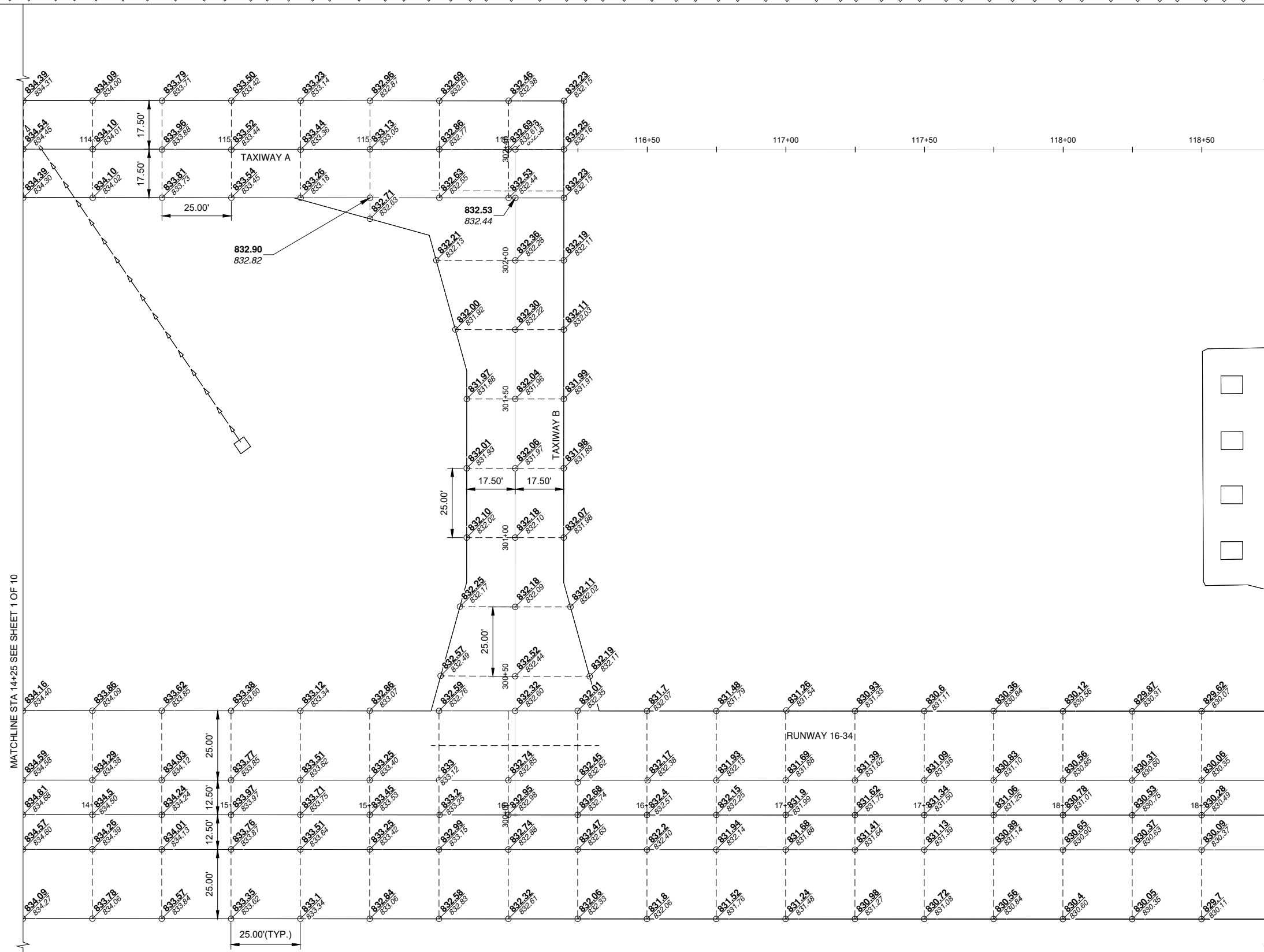
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JOB No: 22004664-00

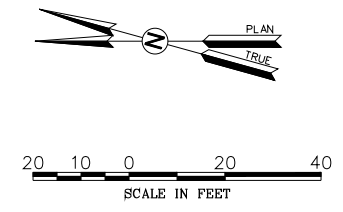
STAKING PLAN SHEET 1 OF 10

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MATCHLINE STA 18+75 SEE SHEET 3 OF 10



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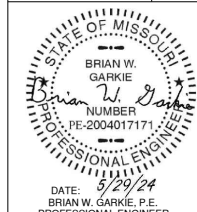
NOTES

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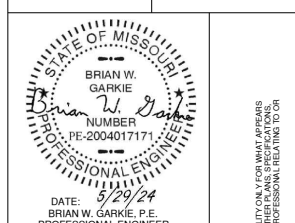
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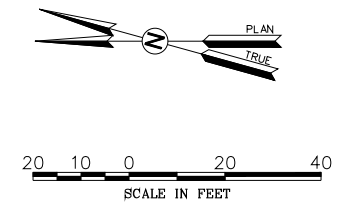
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SHEET 23 OF 53 SHEETS

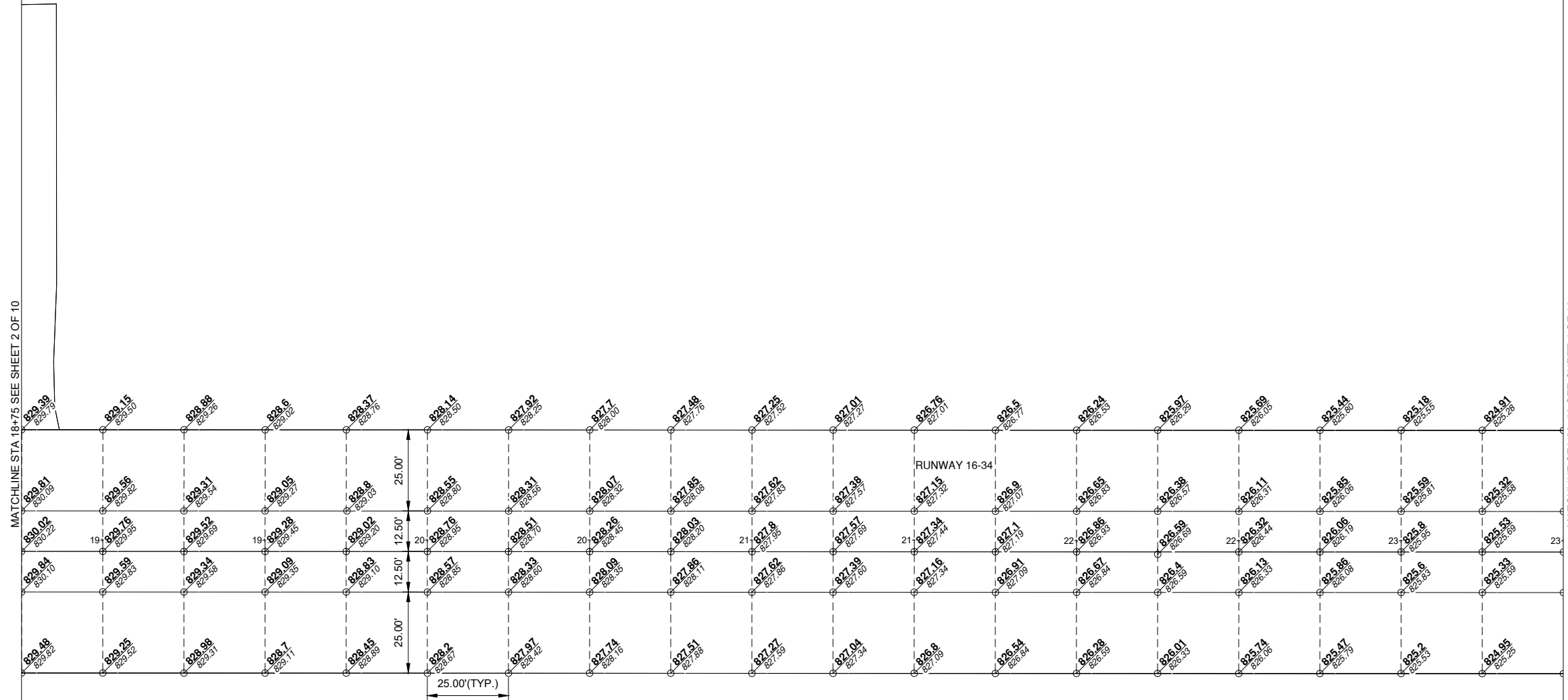


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EXTENDED TAXIWAY A
 CENTRELINE ALIGNMENT



MATCHLINE STA 18+75 SEE SHEET 2 OF 10

MATCHLINE STA 23+50 SEE SHEET 4 OF 10

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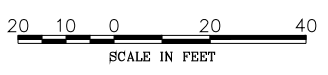
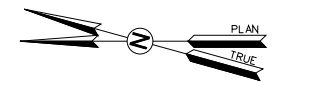
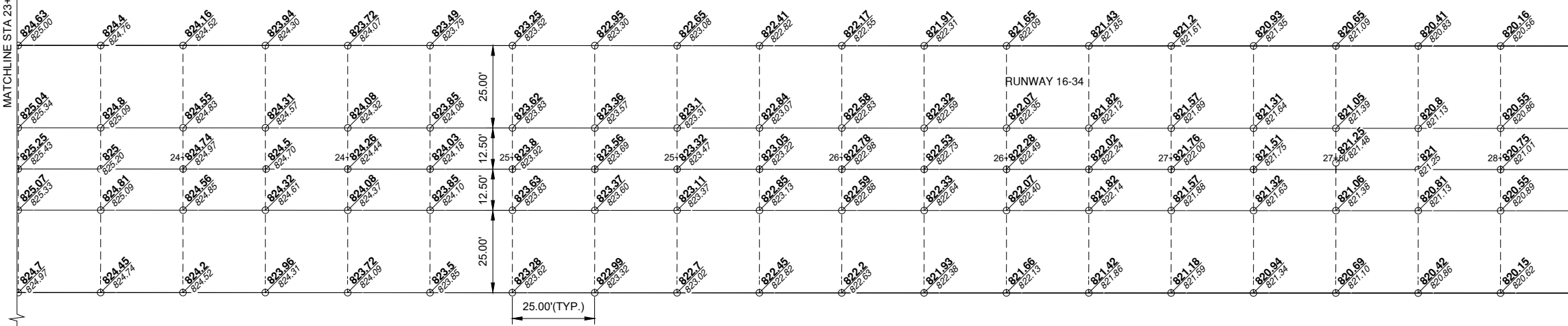
1. THE TURF ALONG THE EDGE OF THE PAVEMENT SHALL MAINTAIN 1.5' DROP AND 5% SLOPE FOR 10'. FOR MORE INFORMATION, SEE TYPICAL SECTION DETAIL SHEET.

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EXTENDED TAXIWAY A
CENTRELINE ALIGNMENT

MATCHLINE STA 23+50 SEE SHEET 3 OF 10

MATCHLINE STA 28+25 SEE SHEET 5 OF 10



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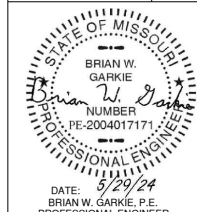
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DATE: 5/29/24
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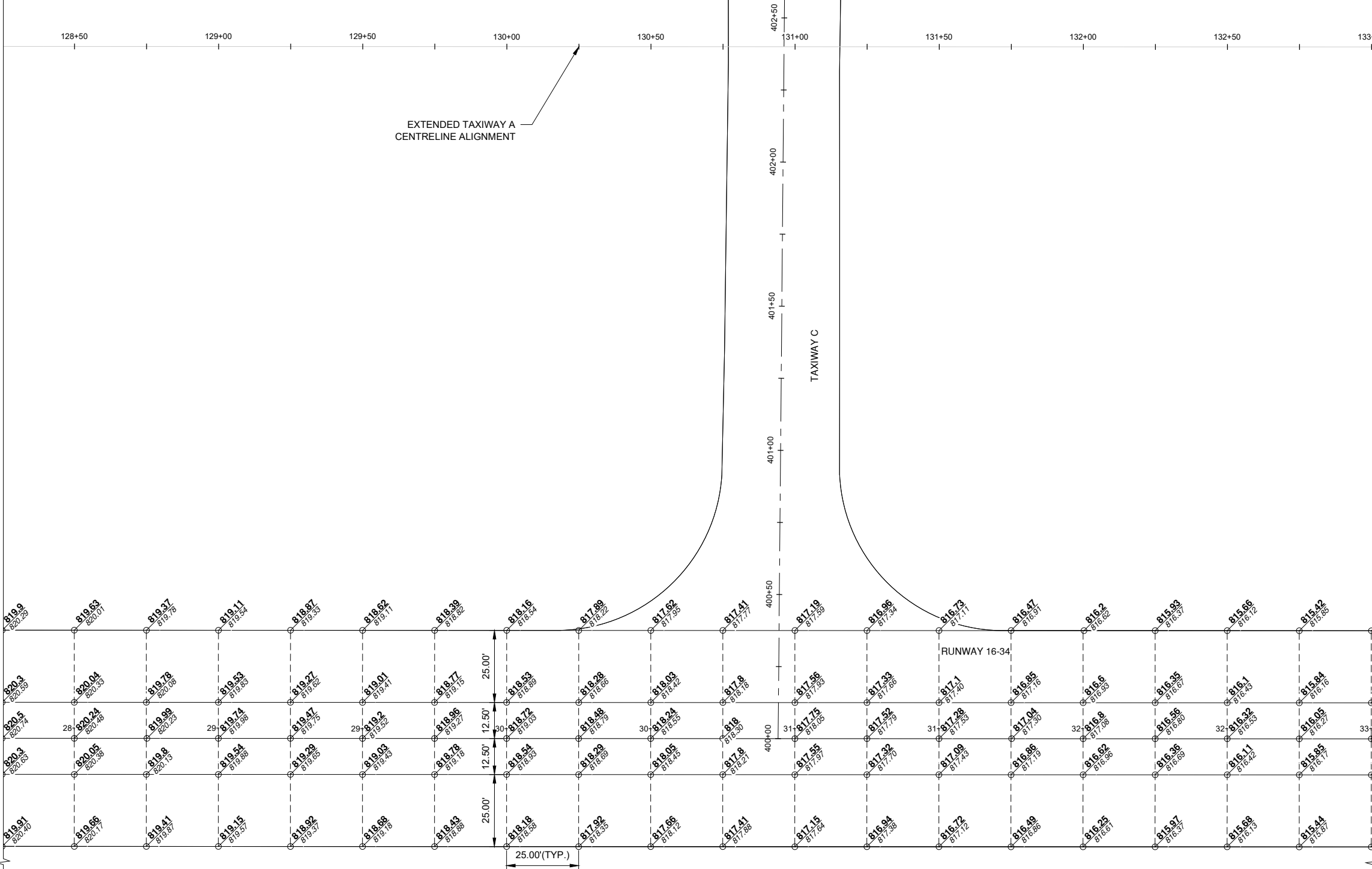
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STAKING PLAN SHEET 4 OF 10

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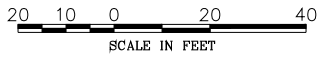
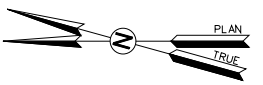
MATCHLINE STA 33+00 SEE SHEET 6 OF 10



EXTENDED TAXIWAY A
CENTRELINE ALIGNMENT

TAXIWAY C

RUNWAY 16-34



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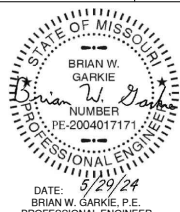
NOTES

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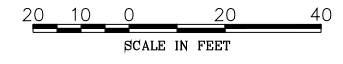
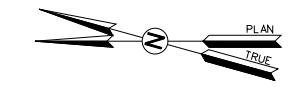
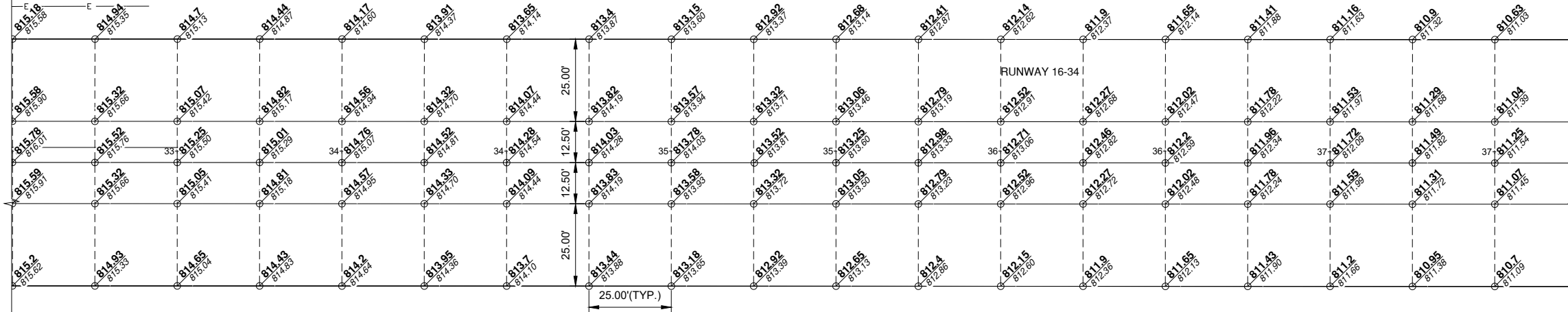
STAKING PLAN SHEET 5 OF 10

MATCHLINE STA 33+00 SEE SHEET 5 OF 10

MATCHLINE STA 37+75 SEE SHEET 7 OF 10

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EXTENDED TAXIWAY A
CENTRELINE ALIGNMENT



LEGEND

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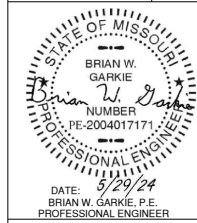
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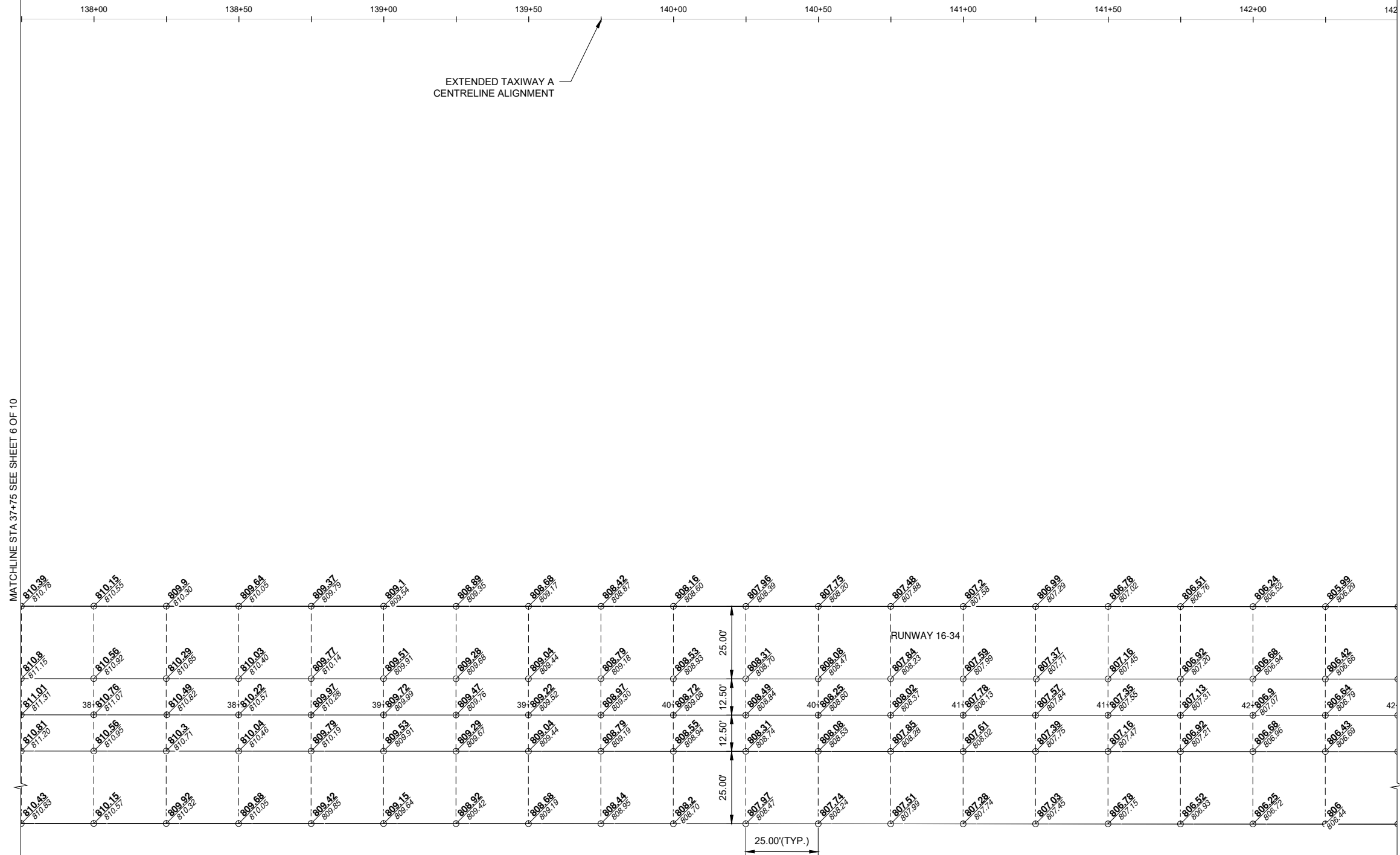
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OF 10

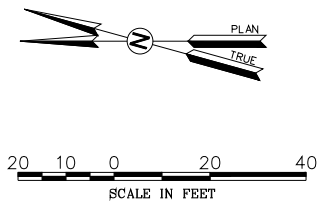
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MATCHLINE STA 42+50 SHEET 8 OF 10



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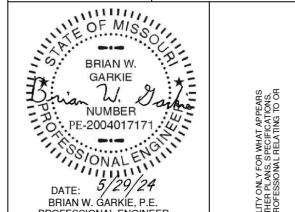
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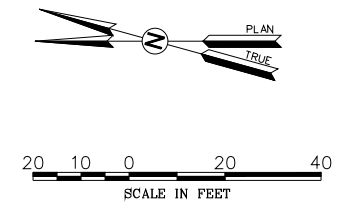
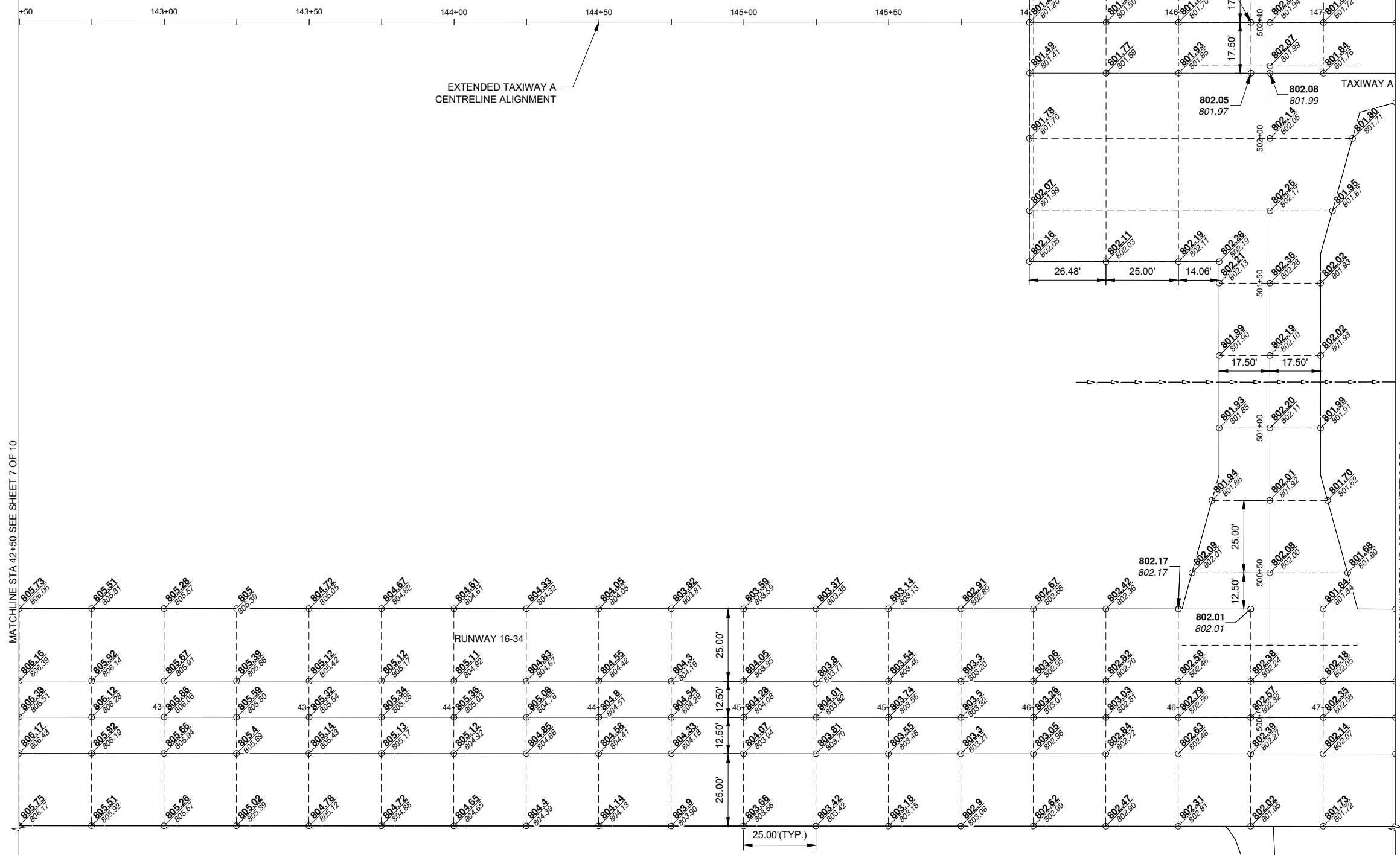
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STAKING PLAN SHEET 7 OF 10

SHEET 27 OF 53 SHEETS

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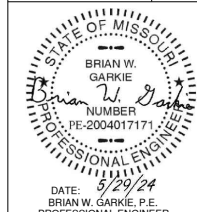


NOTES

1. THE TURF ALONG THE EDGE OF THE PAVEMENT SHALL MAINTAIN 1.5" DROP AND 5% SLOPE FOR 10'. FOR MORE INFORMATION, SEE TYPICAL SECTION DETAIL SHEET.

REVISIONS		
NUMBER	BY	DATE
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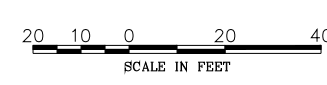
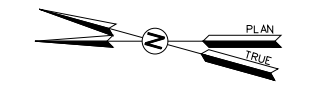
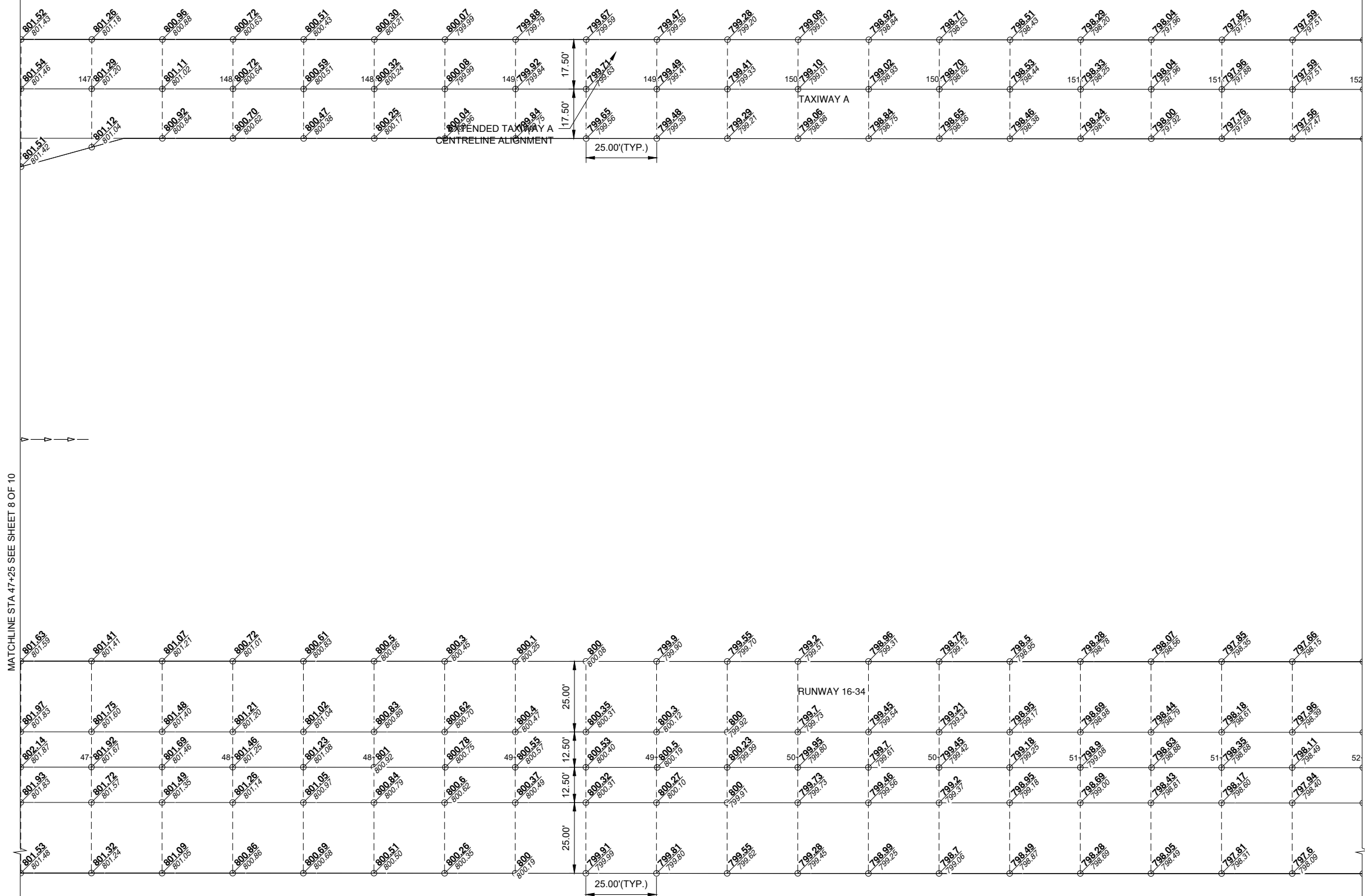


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FILE: 21 - 30 STAKING PLANS.DWG
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 CHECKED BY: JTZ
 APPROVED BY: BWG
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STAKING PLAN SHEET 8
OF 10
 SHEET 28 OF 53 SHEETS

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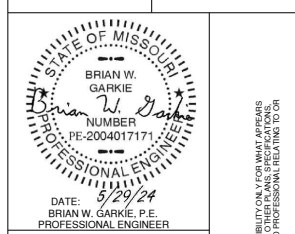
LEGEND
 ○ PROPOSED ELEV.
 ⊙ EXISTING ELEV.

NOTES

1. THE TURF ALONG THE EDGE OF THE PAVEMENT SHALL MAINTAIN 1.5' DROP AND 5% SLOPE FOR 10'. FOR MORE INFORMATION, SEE TYPICAL SECTION DETAIL SHEET.

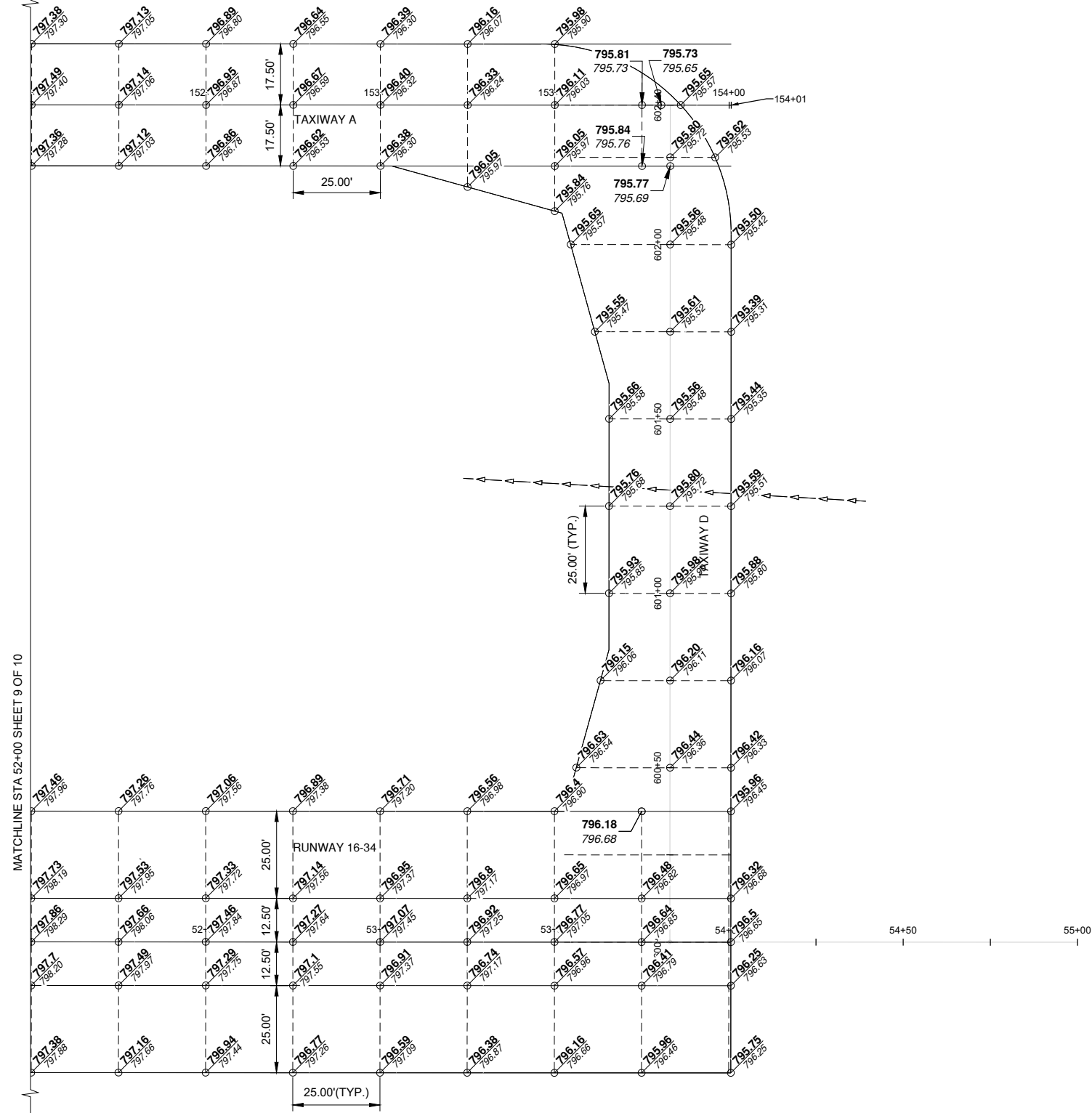
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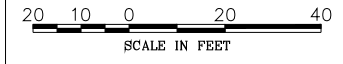
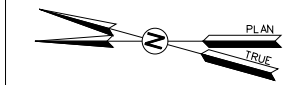


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 APPROVED BY: BWG
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STAKING PLAN SHEET 9 OF 10
 SHEET 29 OF 53 SHEETS



MATCHLINE STA 52+00 SHEET 9 OF 10



LEGEND

- PROPOSED ELEV.
- EXISTING ELEV.

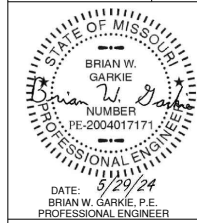
NOTES

1. THE TURF ALONG THE EDGE OF THE PAVEMENT SHALL MAINTAIN 1.5" DROP AND 5% SLOPE FOR 10'. FOR MORE INFORMATION, SEE TYPICAL SECTION DETAIL SHEET.

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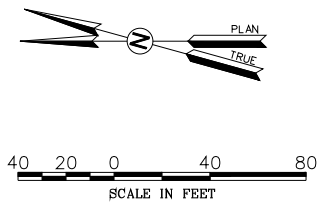
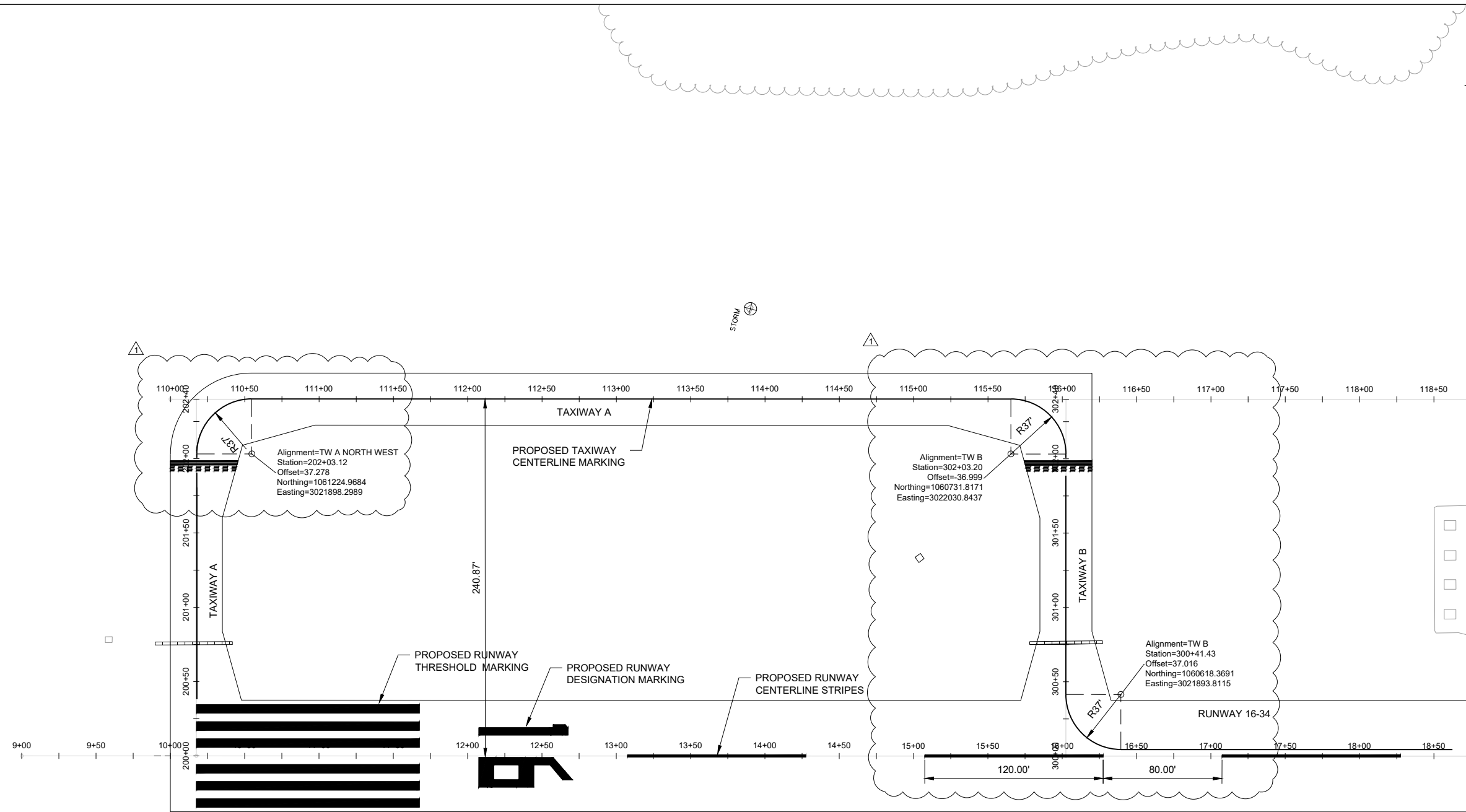
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APPROVED BY: BWG
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JOB No: 22004664-00

STAKING PLAN SHEET 10 OF 10

SHEET 30 OF 53 SHEETS



LEGEND

	PROPOSED TAXY CENTERLINE MARKING
	PROPOSED HOLD POSITION MARKING
	PROPOSED RWY DASHED CENTERLINE MARKING

MATCHLINE AT STA 18+75 SEE SHEET 2 OF 5

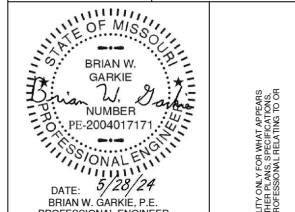
NOTES

1. ALL PROPOSED MARKINGS SHALL HAVE A 6" BLACK BORDERING.
2. ALL PROPOSED MARKINGS EXCEPT BLACK SHALL HAVE TYPE III GLASS BEADS.

REVISIONS		
NUMBER	BY	DATE
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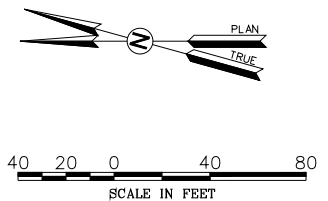
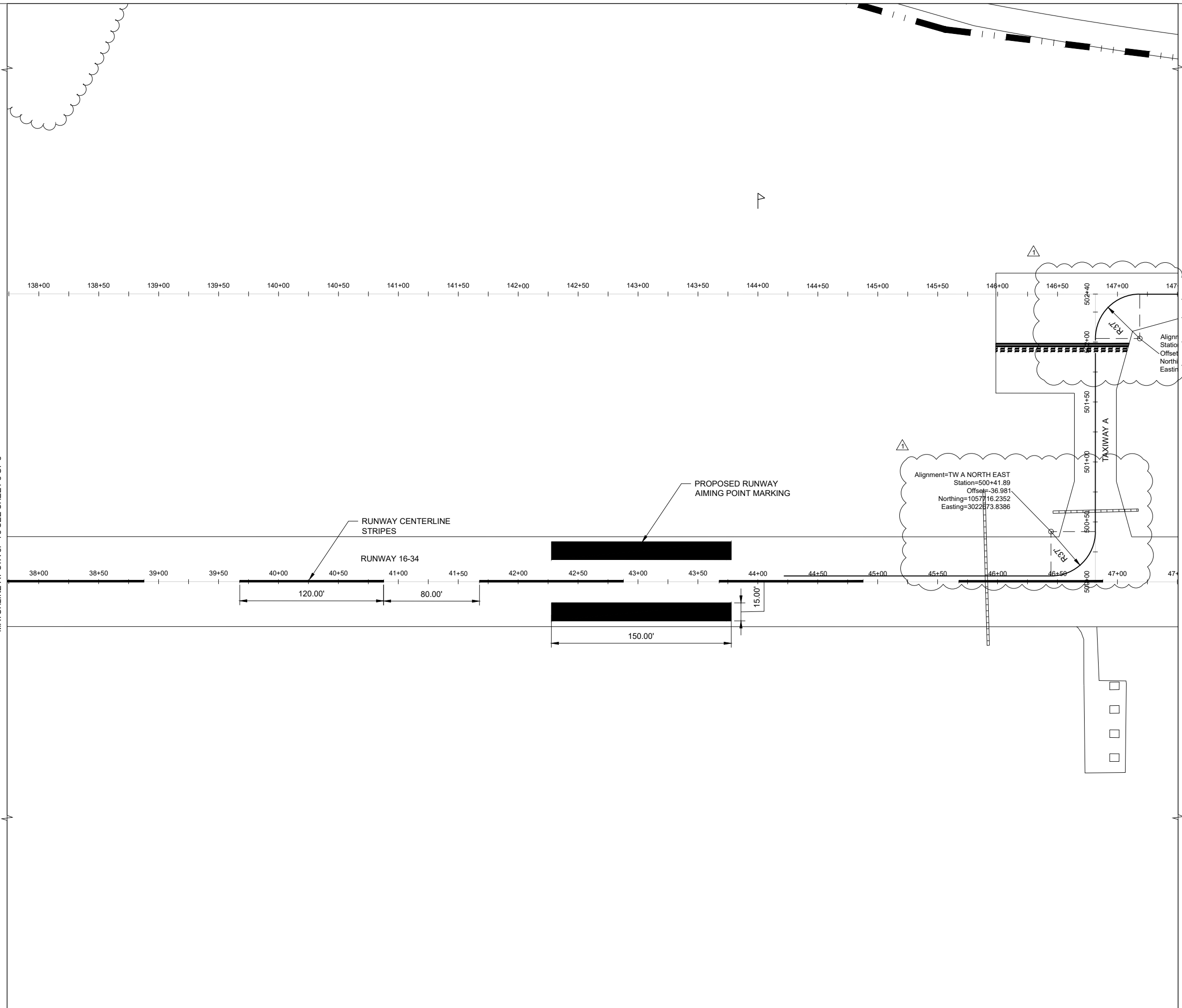
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FILE: 31 - 36 MARKING PLANS.DWG
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 JOB No: 22004664-00

MARKING PLAN SHEET 1
OF 6

MATCHLINE AT STA 37+75 SEE SHEET 3 OF 5

MATCHLINE AT STA 47+50 SEE SHEET 5 OF 5



LEGEND

	PROPOSED RWY DASHED CENTERLINE MARKING
	PROPOSED HOLD POSITION MARKING
	PROPOSED TXY CENTERLINE MARKING

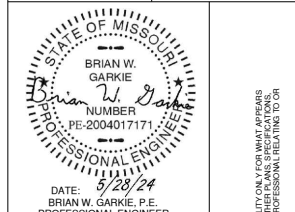
- NOTES**
- ALL PROPOSED MARKINGS SHALL HAVE A 6" BLACK BORDERING.
 - ALL PROPOSED MARKINGS EXCEPT BLACK SHALL HAVE TYPE III GLASS BEADS.

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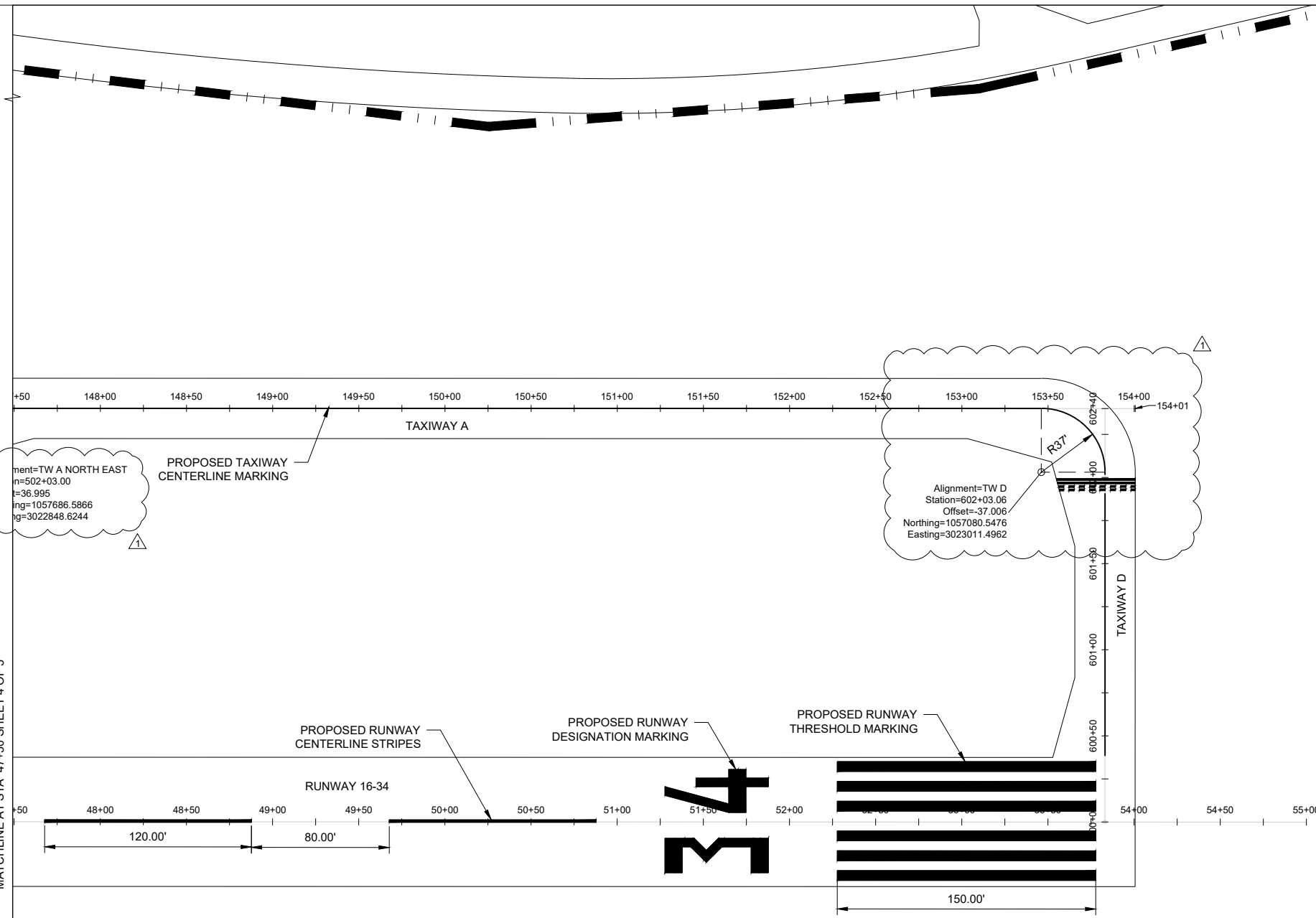
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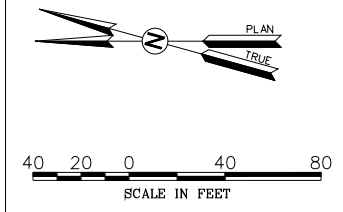
MARKING PLAN SHEET 4 OF 6

SHEET 34 OF 53 SHEETS

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MATCHLINE AT STA 47+50 SHEET 4 OF 5



LEGEND

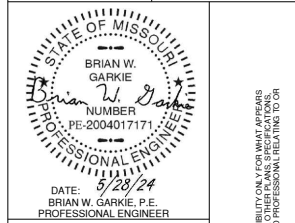
	PROPOSED TXY CENTERLINE MARKING
	PROPOSED HOLD POSITION MARKING
	PROPOSED RWY DASHED CENTERLINE MARKING

- NOTES**
- ALL PROPOSED MARKINGS SHALL HAVE A 6" BLACK BORDERING.
 - ALL PROPOSED MARKINGS EXCEPT BLACK SHALL HAVE TYPE III GLASS BEADS.

REVISIONS		
NUMBER	BY	DATE
1	DK	5/28/24

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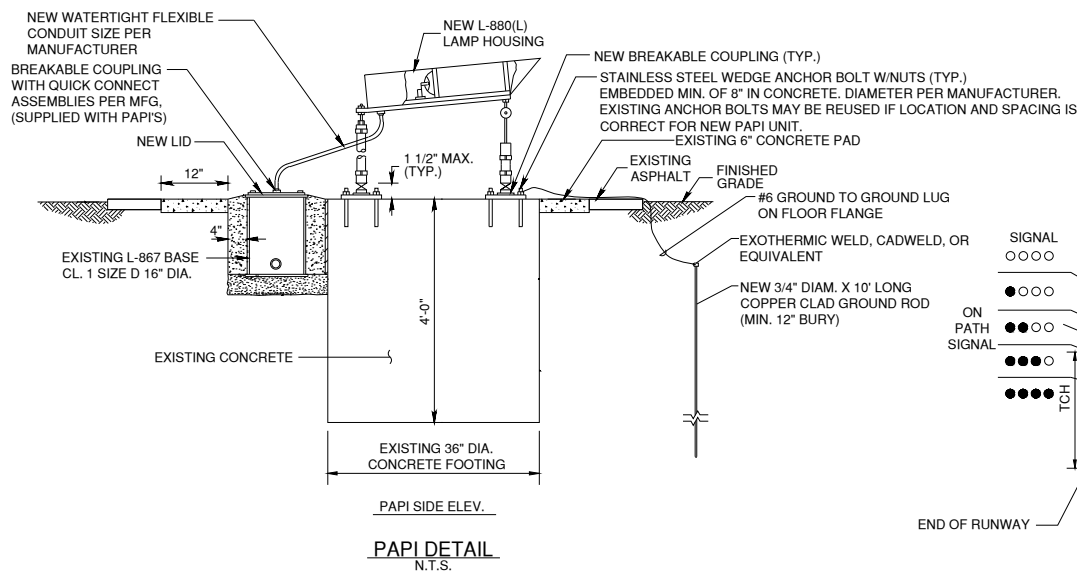
JOB No: 22004664-00

MARKING PLAN SHEET 5 OF 6

SHEET 35 OF 53 SHEETS

REVISIONS

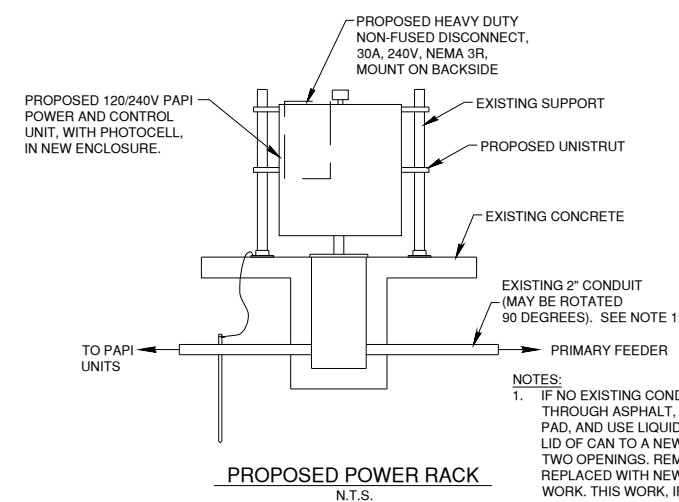
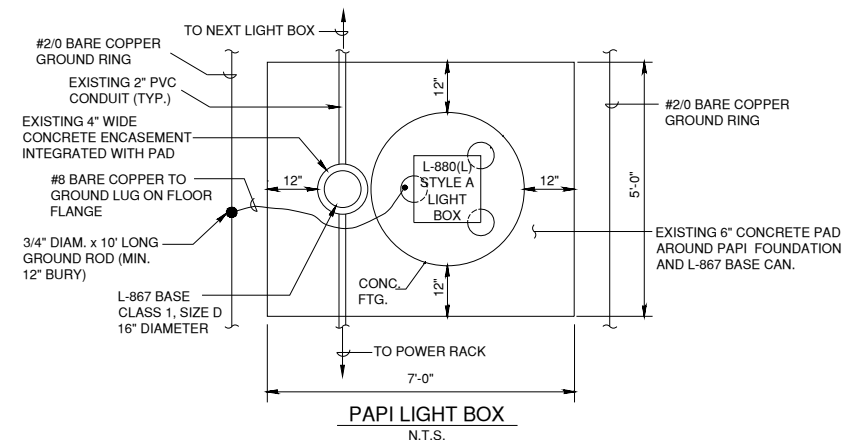
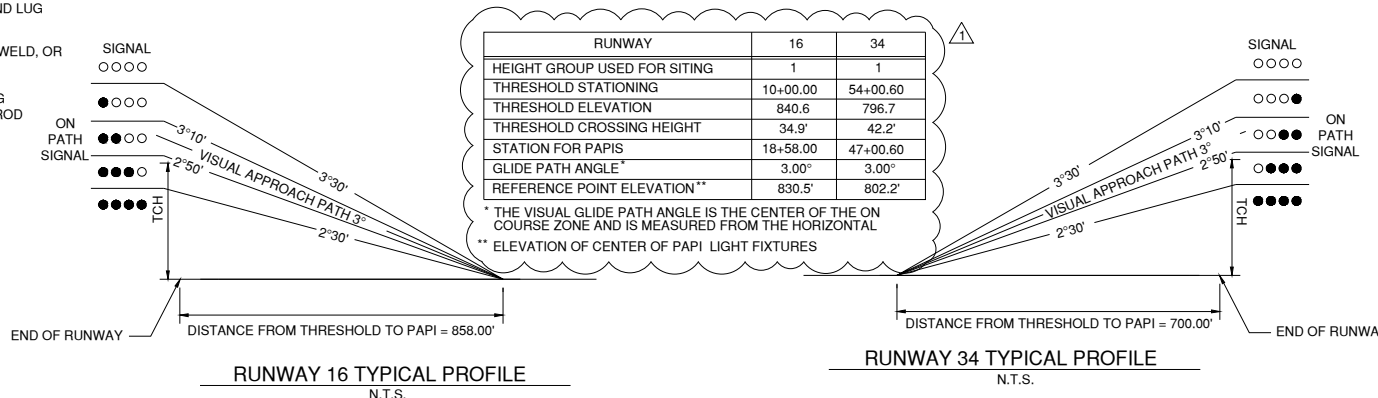
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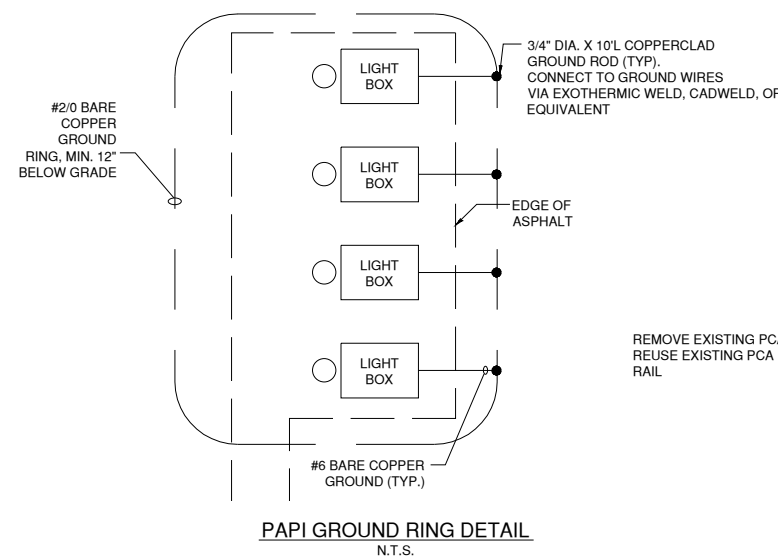
AIMING OF TYPE L-880(L) (4-BOX) PAPI RELATIVE TO PRESPECTED GLIDE PATH (3°00')

LIGHT UNIT	AIMING ANGLE (IN MINUTES OF ARC)
	STANDARD INSTALLATION
UNIT NEAREST RUNWAY	30' ABOVE GLIDE PATH
NEXT ADJACENT UNIT	10' ABOVE GLIDE PATH
NEXT ADJACENT UNIT	10' BELOW GLIDE PATH
NEXT ADJACENT UNIT	30' BELOW GLIDE PATH

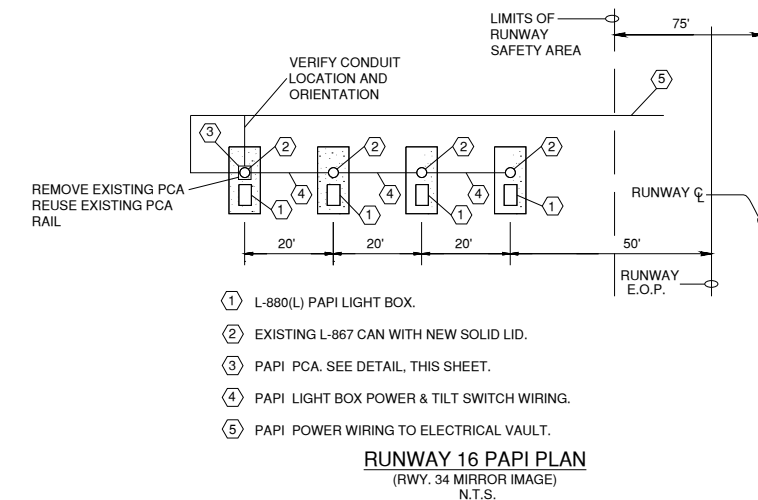
- PAPI NOTES:
EACH PAPI INSTALLATION SHALL ALSO INCLUDE, BUT NOT BE LIMITED TO, THE FOLLOWING ITEMS, WHICH SHALL BE CONSIDERED INCIDENTAL TO THE PAPI INSTALLATION:
- HEAVY DUTY FUSED (20A FUSE) DISCONNECT, 240VAC, 1 PHASE, 30A, NEMA 3R ENCLOSURE (PAPI 16)
 - HEAVY DUTY FUSED (20A FUSE) DISCONNECT, 240VAC, 1 PHASE, 30A, NEMA 3R ENCLOSURE (PAPI 34)
 - #12 XLP-USE WIRING IN EXISTING AND NEW CONDUITS AS DETAILED ON THE PLANS.
 - PAPI POWER AND TILT SWITCH WIRING IN EXISTING CONDUIT.
 - L-867 BASE PLATE WITH FRANGIBLE COUPLING AND LIQUIDTIGHT CONDUIT, GROUND RODS AND GROUND RING.



- FOUNDATIONS:**
ALL LIGHT BOXES SHALL BE FRANGIBLY MOUNTED TO THE FOUNDATION.
- AZIMUTHAL AIMING:**
EACH LIGHT UNIT SHALL BE AIMED OUTWARD INTO THE APPROACH ZONE ON A LINE PARALLEL TO THE RUNWAY CENTERLINE WITHIN TOLERANCE OF ±1/2 DEGREE.
- MOUNTING HEIGHT TOLERANCES:**
THE BEAM CENTERS OF ALL LIGHT UNITS SHALL BE WITHIN ±1 INCH OF A HORIZONTAL PLANE AT THE REFERENCE ELEVATION GIVEN IN THE TABLE.
- TOLERANCE ALONG LINE PERPENDICULAR TO RUNWAY:**
THE FRONT FACE OF EACH LIGHT UNIT IN A BAR SHALL BE LOCATED ON A LINE PERPENDICULAR TO THE RUNWAY CENTERLINE WITHIN ±6 INCHES.
- LATERAL SPACING**
THE DIFFERENCE IN LATERAL SPACING BETWEEN LIGHT UNITS SHALL NOT EXCEED 1'-0".
- BOLT TREATMENT**
ALL BOLTS SHALL BE TREATED WITH "NEVER-SEEZ" OR EQUAL.



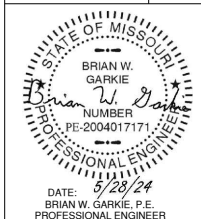
- NOTES:
1. INSTALL GROUNDING RING AROUND ASPHALT PAVEMENT. AT DRIVEWAY BETWEEN ASPHALT AND PAPI UNITS, TRENCH THROUGH ASPHALT AND REPLACE WITH NEW ASPHALT FROM RUNWAY PAVING. INSTALL #6 BARE COPPER GROUND OVER TOP OF ASPHALT TO LIGHT BOX LEG.



- L-880(L) PAPI LIGHT BOX.
- EXISTING L-867 CAN WITH NEW SOLID LID.
- PAPI PCA. SEE DETAIL, THIS SHEET.
- PAPI LIGHT BOX POWER & TILT SWITCH WIRING.
- PAPI POWER WIRING TO ELECTRICAL VAULT.

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FILE: 43 - 52 ELECTRICAL DETAILS SHEET.DWG

DESIGN BY: DK

DRAWN BY: DK

CHECKED BY: JTZ

APPROVED BY: BWG

DATE: 5/7/2024

JOB No: 22004664-00

ELECTRICAL DETAILS
SHEET 2 OF 7