



## ADDENDUM NUMBER 4

Project Number 89020267 TAP-3324 (411)

Project Title (Reissue) Rte. 152 Trail – Seg. 10 Pedestrian Bridge over

US-169

ISSUE DATE: 06/30/2026

### Questions & Answers

1. I submitted our draft TMP to KCMO and MODOT for review on June 1st and had some back and forth with KCMO regarding the detour we will have when we close US-169 to erect the bridge. We originally assumed it would be acceptable to divert traffic onto local roads seeing as it will only be in place for one night. Jeff Bryan expressed some concerns about this and proposed a detour using I-29 and 152. I included an alternate for that detour in the revised TMP, but I'm unclear on who will make the call on what detour we need to have. I'm not sure how to account for the mobilization in the bid if we are unclear of what detour will be accepted.

*It is our understanding that both the city and MoDOT will allow the short-term closure detour to be routed on local roads. This should be confirmed directly with KCMO Traffic and MoDOT staff.*

2. Can you elaborate on what exactly will constitute as an approved draft TMP we need to bid the job?

*An aerial with locations of the signs and barricades, keyed to the types of signs/barricades is sufficient, along with a brief narrative how this would be handled.*

3. Jeff Bryan made this effort seem more preliminary and that a full review will not happen until the job is awarded. He also mentioned that the selected company needs to put together signed and sealed traffic control plans. That leads me to believe that a designer will need to put that together?

*That is a safe assumption if Jeff implied it. Please reference 02305 -Traffic Control for "Deliverables" and "Schedule."*

### NOTE:

**Bidders must acknowledge receipt of this Addendum by listing the number and date, where provided, on the Bid Form - Document 00410.**

4. There were a lot of clarifications in the original bid via addendums - can you also confirm that all of those questions and responses are still valid with this reissued bid?

*Only the clarifications and changes in the Project Manual and Bid Documents posted and identified in the re-issued advertisement are valid at this time. Other previous addendum items from the initial advertisement need to be resubmitted.*

5. Will we need another bid bond for the reissue of the project?

*Yes, a new bid bond will be needed.*

6. I submitted our draft TMP to KCMO and MODOT for review on June 1st and had some back and forth with KCMO regarding the detour we will have when we close US-169 to erect the bridge. We originally assumed it would be acceptable to divert traffic onto local roads seeing as it will only be in place for one night. Jeff Bryan expressed some concerns about this and proposed a detour using I-29 and 152. I included an alternate for that detour in the revised TMP, but I'm unclear on who will make the call on what detour we need to have. I'm not sure how to account for the mobilization in the bid if we are unclear of what detour will be accepted.

*It is our understanding that both the city and MoDOT will allow the short-term closure detour to be routed on local roads. This should be confirmed directly with KCMO Traffic and MoDOT staff.*

7. Can you elaborate on what exactly will constitute as an approved draft TMP we need to bid the job?

*An aerial with locations of the signs and barricades, keyed to the types of signs/barricades is sufficient, along with a brief narrative how this would be handled.*

8. Jeff made this effort seem more preliminary and that a full review will not happen until the job is awarded. He also mentioned that the selected company needs to put together signed and sealed traffic control plans. That leads me to believe that a designer will need to put that together?

*That is a safe assumption if Jeff implied it. Please reference 02305 -Traffic Control for "Deliverables" and "Schedule."*

9. Does Realm need to resubmit the TMP plan?

*Draft Traffic Management Plans need to be re-submitted to MoDOT and KCMO Traffic as part of the re-issued advertisement.*

**NOTE:**

**Bidders must acknowledge receipt of this Addendum by listing the number and date, where provided, on the Bid Form - Document 00410.**

10. What finish is required for the MSE wall panels?

*MSE panels to be similar to what has been constructed along the US-169 Corridor at NW 96th St. and NW 108th St (NW Shoal Creek Pkwy.).*

11. Do the embankments, bridge abutments, and MSE walls are to be constructed in 2026 to allow settlement over the winter shutdown period?

*Reissued Project Completion Date is August 27, 2027, to account for prefab bridge lead time. Embankments, bridge abutments, and MSE walls to be constructed in 2026 to allow for settlement over winter shutdown months.*

12. What is the Modified Type A Gutter behind the MSE wall?

*Information on the Modified Type A Gutter for behind the MSE wall is in the Specifications 02209 (see below). It notes to conform to the MoDOT Standard Plan for Chain-Link Fence for Retaining Walls (607.11) since the fence must be attached to the curb. All the curb dimensions are on that sheet. Unrelated, we noticed that the Type A Integral Curb (to be used on the bridge deck and the approaches to the bridge) has a typo. It should read MoDOT Standard Plan 609.00 not 6609.00.*

#### 02209 CURBING

##### A. GENERAL

*All the requirements of the Kansas City Metropolitan Chapter of the American Public Works Association (APWA), Standard Specifications and Design Criteria apply as amended and supplemented by the Department of Public Works of the City of Kansas City, Missouri (KCMO). Sections of said Specifications will be hereinafter referred to as "APWA-KCMO."*

*The City's current standards can be found online under the Kansas City Public Works Department website under "Public Works Design & Construction Standards:" <https://www.kcmo.gov/city-hall/departments/public-works/public-works-designconstructionstandards>*

*Work shall also comply with the Missouri Standard Specifications for Construction (2025) where noted.*

*Modified Type A Gutter shall conform to MoDOT Standard Specification Section 609 and MoDOT Standard Plan 607.11. Type A Integral Curb shall conform to MoDOT Standard Specification Section 609 and MoDOT Standard Plan 609.00. Curbing within Kansas City right of way shall conform to APWA-KCMO Section 2209 with the following modifications, deletions, or additions. All provisions that are not so amended, removed, or supplemented remain in full force and effect.*

13. Some plan sheet scales appear incorrect. Can this be verified? Example, profile on Sheet appears to be 1"=20' not the 1"=40' as noted.

*If you could be more specific as to where this occurs (Sheet No., etc.), our design professional could review it. In any case unless an error is found, the contractor should follow the dimensions, coordinates, stationing, and elevations and not rely on the scale.*

14. Should contraction joints in the trail be sealed?

*The Joint Detail is incorrect and should not show the joints being sealed with joint filler. Trail contraction joints shall be sawn (<1/4" wide) and remain "unsealed."*

#### NOTE:

**Bidders must acknowledge receipt of this Addendum by listing the number and date, where provided, on the Bid Form - Document 00410.**

15. Does the ADA crossing require a full-depth pavement patch? If so, what bid item should this be under.

*Yes. Full depth is correct. Not sure what detail could be shown that would provide additional information. The depth of the pavement is unknown. The contractor should investigate the site and make his own determination or assume a conservative depth. All work in the ADA detail on sheet 10 is included in the Bid Item "ADA Ramp Street Tie-in" (See Division Section 2301.B. Measurement and Payment).*

16. Who is responsible for compaction testing and concrete testing?

*KCMO Public Works staff will be responsible for onsite compaction and onsite concrete testing.*

17. Who is responsible for preparing the project SWPPP?

*The contractor shall develop the job specific SWPPP. The design professional (DP) has not completed a SWPPP. General Conditions Article 6.09.B.3 implies the contractor is to develop and implement the SWPPP. The DP will do what they can to supply information performed under our scope that the Contractor and City will require.*

18. What is the project warranty period?

*LPA projects do not have a warranty.*

19. The project has a 0% DBE goal. Are workforce goals still required?

*City workforce goals of 10%/2% are in effect on the project.*

20. Can stationing and elevations for the MSE wall leveling pad and finished grade line be provided? Please see revised plan sheets W1 and W2 depicting stationing and bottom elevations for the proposed finished grade at the wall face or top of leveling pad.

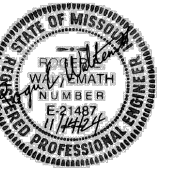
*Leveling pad elevations are set by the manufacturer based on an economic arrangement of the wall pieces. Contractors are to place the depth based on the parameters set on the plans. Also, the "proposed" groundline is the "existing" groundline and the contractor should verify it since it has been years since it was surveyed. (See Page 5 and Page 6 for W1 East Wall and W1 West Wall)*

**NOTE:**

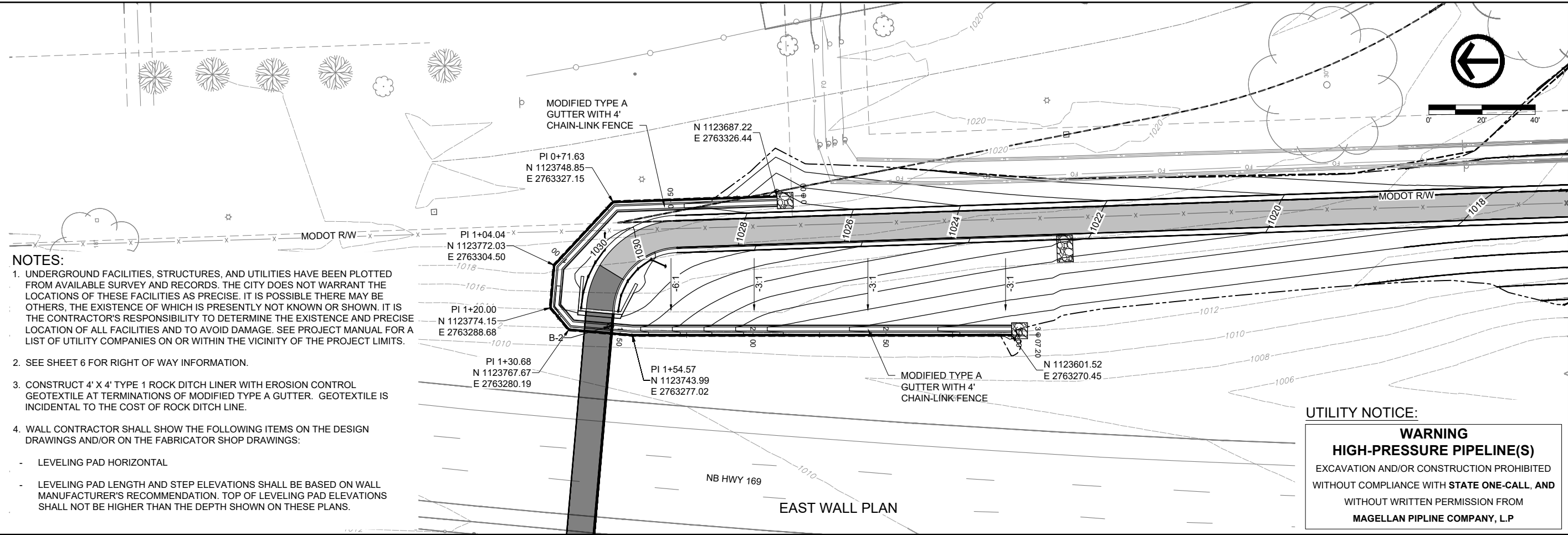
**Bidders must acknowledge receipt of this Addendum by listing the number and date, where provided, on the Bid Form - Document 00410.**

Monday, June 8, 2026 1:45:23 PM

V:\KANSASCITY-Z\DEPROJ\KANSASCITY\2015 FED TA TRAILS\89020267 - PED\_RDWY RTE 169\SHEETS\89020267 WALL PLAN & PROFILE.DWG



TAP 3324 (411)

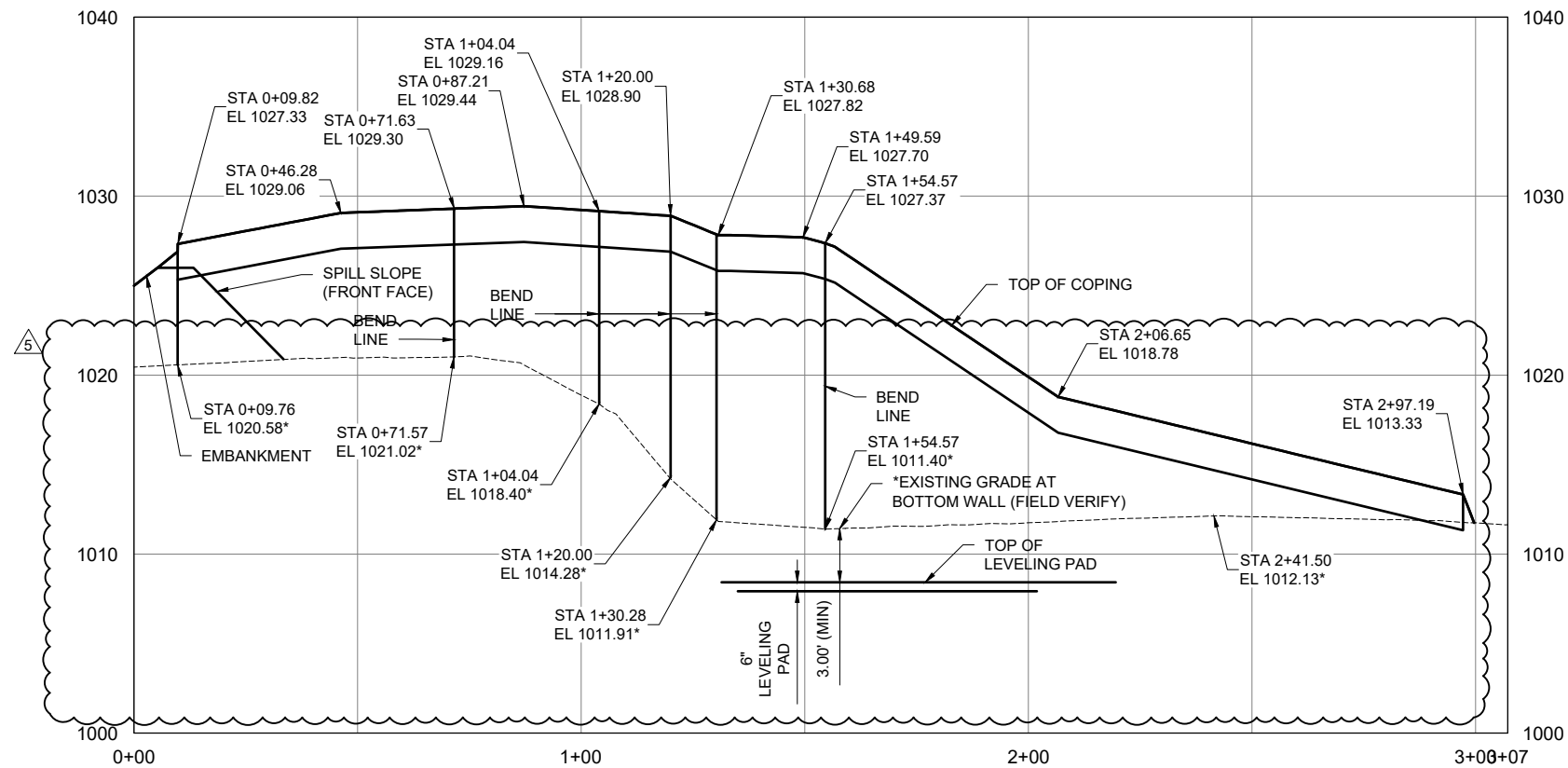


**NOTES:**

1. UNDERGROUND FACILITIES, STRUCTURES, AND UTILITIES HAVE BEEN PLOTTED FROM AVAILABLE SURVEY AND RECORDS. THE CITY DOES NOT WARRANT THE LOCATIONS OF THESE FACILITIES AS PRECISE. IT IS POSSIBLE THERE MAY BE OTHERS, THE EXISTENCE OF WHICH IS PRESENTLY NOT KNOWN OR SHOWN. IT IS THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE EXISTENCE AND PRECISE LOCATION OF ALL FACILITIES AND TO AVOID DAMAGE. SEE PROJECT MANUAL FOR A LIST OF UTILITY COMPANIES ON OR WITHIN THE VICINITY OF THE PROJECT LIMITS.
2. SEE SHEET 6 FOR RIGHT OF WAY INFORMATION.
3. CONSTRUCT 4' X 4' TYPE 1 ROCK DITCH LINER WITH EROSION CONTROL GEOTEXTILE AT TERMINATIONS OF MODIFIED TYPE A GUTTER. GEOTEXTILE IS INCIDENTAL TO THE COST OF ROCK DITCH LINE.
4. WALL CONTRACTOR SHALL SHOW THE FOLLOWING ITEMS ON THE DESIGN DRAWINGS AND/OR ON THE FABRICATOR SHOP DRAWINGS:
  - LEVELING PAD HORIZONTAL
  - LEVELING PAD LENGTH AND STEP ELEVATIONS SHALL BE BASED ON WALL MANUFACTURER'S RECOMMENDATION. TOP OF LEVELING PAD ELEVATIONS SHALL NOT BE HIGHER THAN THE DEPTH SHOWN ON THESE PLANS.

**UTILITY NOTICE:**

**WARNING**  
**HIGH-PRESSURE PIPELINE(S)**  
 EXCAVATION AND/OR CONSTRUCTION PROHIBITED  
 WITHOUT COMPLIANCE WITH STATE ONE-CALL, AND  
 WITHOUT WRITTEN PERMISSION FROM  
**MAGELLAN PIPELINE COMPANY, L.P**



DATE	DESCRIPTION
06-08-2026	ADDENDUM 5 - EX. GRADE CALL-OFFS ADDED

9788 N ASH AVENUE  
 KANSAS CITY, MISSOURI 64157  
 816-781-6182  
 865-241-8011 (WATS)  
 816-781-0643 (FAX)  
 COA NO. 2002066347



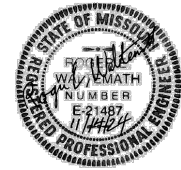
CITY OF KANSAS CITY  
 PUBLIC WORKS  
 DEPARTMENT  
 CAPITAL PLANNING  
 414 E. 12TH STREET  
 18TH FLOOR  
 KANSAS CITY, MO 64151  
 V: (816) 513-2600  
 F: (816) 513-2572  
 www.kcmo.org

ROUTE 152 TRAIL  
 SEGMENT 10  
 PROJECT NO.  
 89020267

**EAST WALL  
 PLAN & PROFILE**

Monday, June 8, 2026 1:45:23 PM

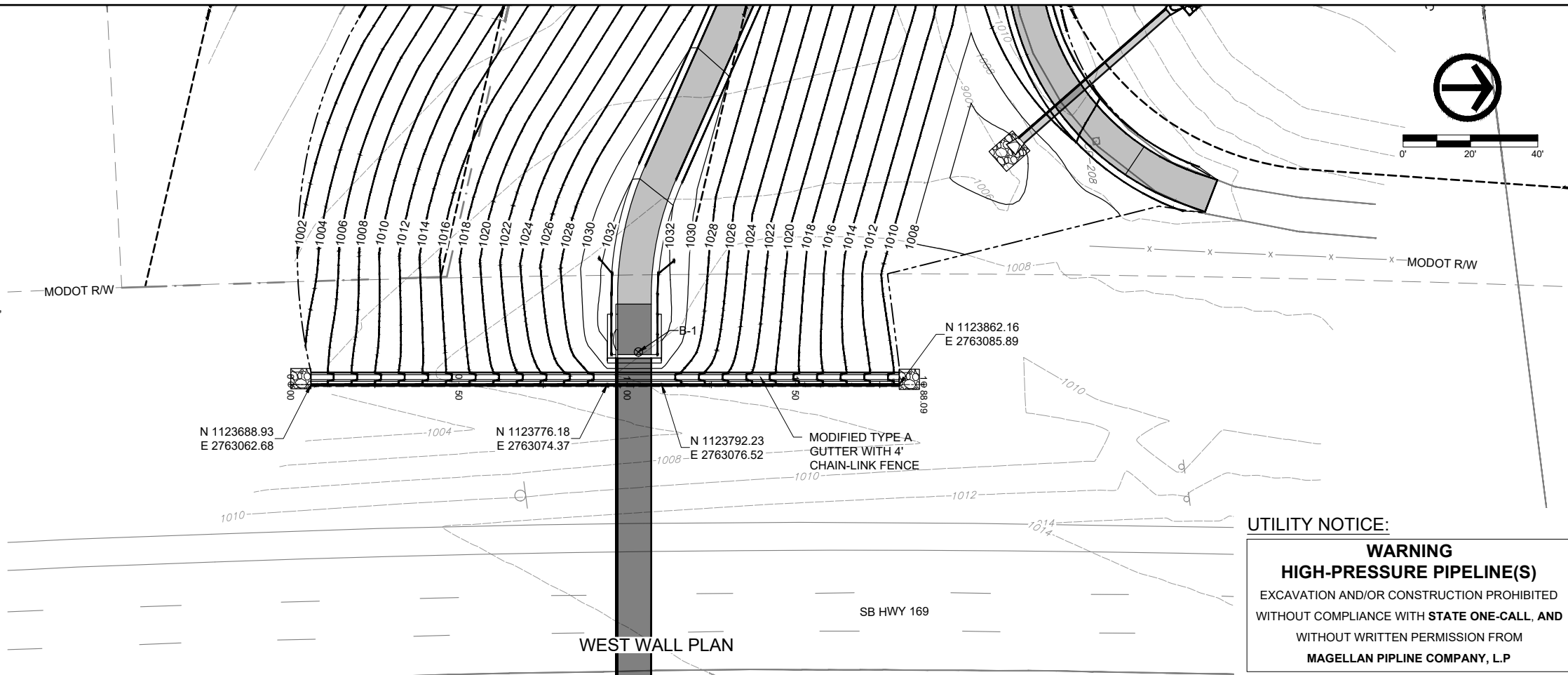
V:\KANSASCITY-Z\DEPROJ\KANSASCITY\2015 FED TA TRAILS\89020267 - PED\_RDWY RTE 169\SHEETS\89020267 WALL PLAN & PROFILE.DWG



TAP 3324 (411)

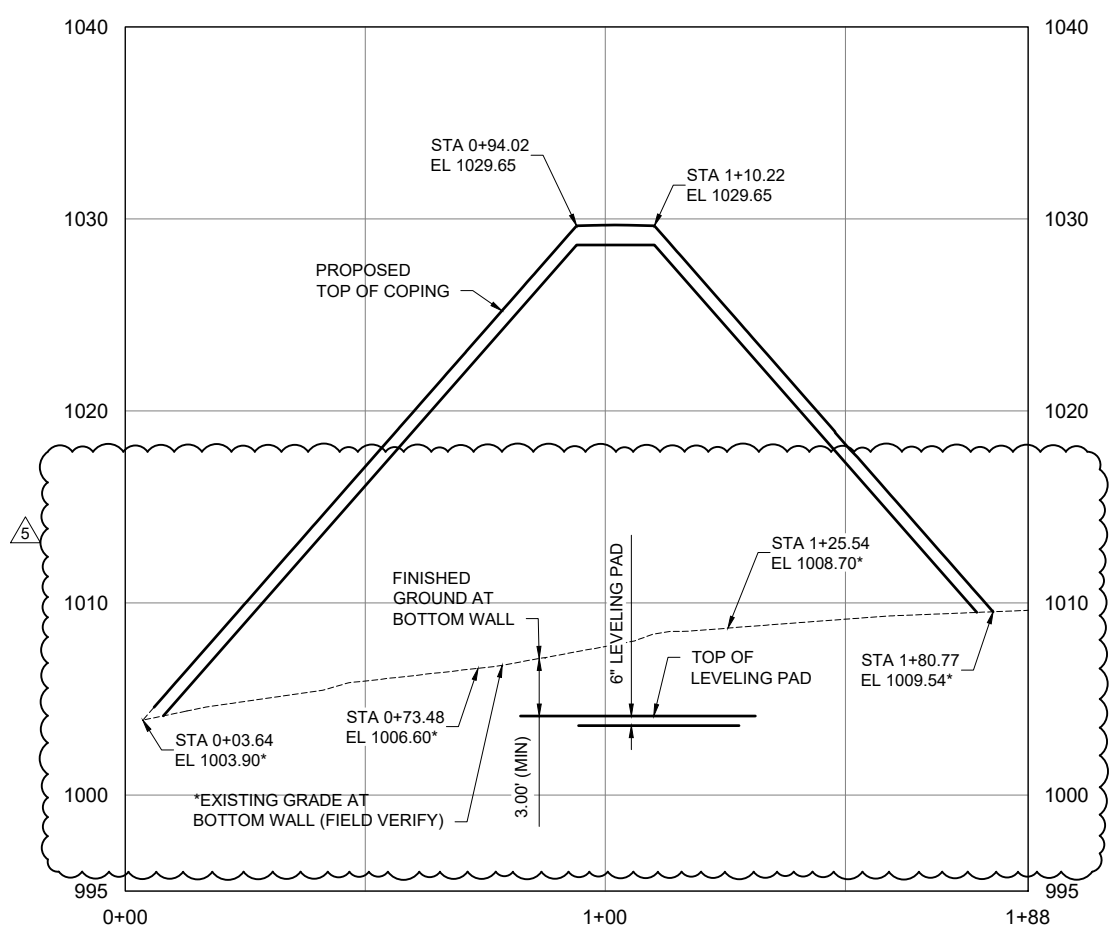
**NOTES:**

- UNDERGROUND FACILITIES, STRUCTURES, AND UTILITIES HAVE BEEN PLOTTED FROM AVAILABLE SURVEY AND RECORDS. THE CITY DOES NOT WARRANT THE LOCATIONS OF THESE FACILITIES AS PRECISE. IT IS POSSIBLE THERE MAY BE OTHERS, THE EXISTENCE OF WHICH IS PRESENTLY NOT KNOWN OR SHOWN. IT IS THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE EXISTENCE AND PRECISE LOCATION OF ALL FACILITIES AND TO AVOID DAMAGE. SEE PROJECT MANUAL FOR A LIST OF UTILITY COMPANIES ON OR WITHIN THE VICINITY OF THE PROJECT LIMITS.
- SEE SHEET 6 FOR RIGHT OF WAY INFORMATION.
- CONSTRUCT 4' X 4' TYPE 1 ROCK DITCH LINER WITH EROSION CONTROL GEOTEXTILE AT TERMINATIONS OF MODIFIED TYPE A GUTTER. GEOTEXTILE IS INCIDENTAL TO THE COST OF ROCK DITCH LINE.
- WALL CONTRACTOR SHALL SHOW THE FOLLOWING ITEMS ON THE DESIGN DRAWINGS AND/OR ON THE FABRICATOR SHOP DRAWINGS:
  - LEVELING PAD HORIZONTAL
  - LEVELING PAD LENGTH AND STEP ELEVATIONS SHALL BE BASED ON WALL MANUFACTURER'S RECOMMENDATION. TOP OF LEVELING PAD ELEVATIONS SHALL NOT BE HIGHER THAN THE DEPTH SHOWN ON THESE PLANS.



**UTILITY NOTICE:**

**WARNING**  
**HIGH-PRESSURE PIPELINE(S)**  
 EXCAVATION AND/OR CONSTRUCTION PROHIBITED  
 WITHOUT COMPLIANCE WITH **STATE ONE-CALL**, AND  
 WITHOUT WRITTEN PERMISSION FROM  
**MAGELLAN PIPELINE COMPANY, L.P**



**WEST WALL PROFILE**  
 CONCRETE LEVELING PAD NOT SHOWN FOR CLARITY.

DATE	DESCRIPTION
06-08-2026	ADDENDUM 5 - EX. GRADE CALL-OFFS ADDED

9788 N ASH AVENUE  
 KANSAS CITY, MISSOURI 64157  
 816-781-6182  
 865-241-8011 (WATS)  
 816-781-0643 (FAX)  
 COA NO. 2002066347



CITY OF KANSAS CITY  
 PUBLIC WORKS  
 DEPARTMENT  
 CAPITAL PLANNING  
 414 E. 12TH STREET  
 18TH FLOOR  
 KANSAS CITY, MO 64151  
 V: (816) 513-2600  
 F: (816) 513-2572  
 www.kcmo.org

ROUTE 152 TRAIL  
 SEGMENT 10  
 PROJECT NO.  
 89020267

**WEST WALL  
 PLAN & PROFILE**

21. Does it matter which bid bond you submit or as long as it has the correct and updated date it will be accepted? There are two different ones provided the one in the Bid Packet has the project number of 89020267 and the bid bond in the project manual has the project number as TAP 3324 (411). I have attached them both to show what I am talking about and showing the slight difference between the two.

*As long as the bid bond has the correct, updated date, either version will be accepted. One references the City of Kansas City, Missouri project number (89020267), while the other references the Federal project number (TAP-3324(411)); both numbers relate to the same project.*

22. There is a note to provide a conduit under the trail on the east side of US-169 for a future sign. Should we price this as a sleeve that has a dead end on both sides of the trail or does one end of the conduit need to land in some kind of electrical infrastructure. Can we cap the loose ends, or do we need to provide in ground pull boxes? This is all supposed to be coordinated with the property owner before construction. Please provide some contact information for Metro North Crossing LLC.

*The contact person for Metro North, LLC is Dan Horn, IAS Partners, O: 816-979-1829, M: 913-645-8243 (dan@iaspartners.com). If unsuccessful in getting in contact with Dan, assume the conduit runs near the bottom of the embankment until it daylights and cap both ends. Treat it like a sleeve and the sign installer can put their own conduit through it, so assume steel 2" diam. but please attempt to confirm with Dan Horn.*

23. We were previously told to match the finish of the MSE wall to the MSE walls North of this job on US-169. The walls we need to match are stained. Do we need to stain the walls to match, and if so, is that subsidiary to the MSE wall line item? Or do we just need to match the finish. Please advise on whether staining or any other type of product will need to be applied to the wall panels.

*As specified, the city would like the MSE wall surface finish to match the MSE wall surface finish of the bridges at NW 96th St. and 108th St. over Highway 169 just north of the project. The staining shall be subsidiary to the MSE wall line item. The Contractor shall contact the City for requirements of the finish and approval of the selected finish. MoDOT administered the interchange projects at both NW 96th St. and NW 108th St. and would have knowledge of the specific MSE wall surface finish.*

24. Assuming the bridge will fit in the abutment bearing seat as detailed on the plans. It is assumed that the abutments do not need to be redesigned by the bridge supplier. Please confirm.

*The minimum that the fabricator should do is a dead load analysis and design check if the total fabricated bridge and deck weight is over 230,000 lbs. An anchor bolt evaluation should be performed if bolt size and length, spacing and edge distance differ from the plans. If they have fixed vs. expansion bearings, a review should be performed.*

