

ADDENDUM NO. 4

COVER SHEET

09/23/2025

CITY OF ROLLA
TAXIWAY A RECONSTRUCTION AND TAXIWAY CONNECTOR REHABILITATION
at
ROLLA NATIONAL AIRPORT (VIH)
VICHY, MO

MoDOT Project # 23-056A-1

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DATE: 09/23/2025

ADDENDUM NO. 4

**CITY OF ROLLA
TAXIWAY A RECONSTRUCTION AND TAXIWAY CONNECTOR REHABILITATION
at
ROLLA NATIONAL AIRPORT (VIH)
VICHY, MO**

MoDOT Project # 23-056A-1

TO: All Plan-holders of Record

The following addendum items supplement, clarify, modify, change, replace, delete from or add to, the requirements of the contract documents for this project. The articles contained in the addendum take precedence over the requirements of the previously published contract documents. Where any article of the contract specifications or any detail of the contract drawings is modified or any paragraph, subparagraph or clause thereof is modified or deleted by the articles contained in this addendum, the unaltered provisions of that article, paragraph, subparagraph or clause shall remain in effect.

PREPARED BY: Woolpert, Inc.
931 Wildwood Drive, Suite 101
Jefferson City, MO 65109

CERTIFIED BY:



Laura Koonce, Project Manager

ADDENDUM BEGINS

PROJECT MANUAL UPDATES

1. Page 10 of 396 – Notice to Bidders

Revision: Updated bid opening date.

Justification: The bid opening date was updated to September 30, 2025.

2. Page 27 of 396 – Bid Proposal Form

Revision: Updated bid opening date.

Justification: The bid opening date was updated to September 30, 2025.

Revision: Adjusted calendar day count.

Justification: The calendar day count was revised to reflect the updated phasing plan.

3. Page 85 of 396 – Article 2: Notice to Proceed

Revision: Adjusted calendar day count.

Justification: The calendar day count was revised to reflect the updated phasing plan, which now includes simultaneous execution of Schedules I and III.

4. Page 139 of 396 – Subsection 80-08, Failure to complete on time

Revision: Updated maximum construction time allowed for all schedules and updated schedule breakdown by phases.

Justification: The revision reflects the updated number of calendar days and includes a revised schedule breakdown along with clarifying notes.

5. Page 140 of 396 – Subsection 80-08, Failure to complete on time (Continued)

Revision: Updated maximum construction time allowed for all schedules and added Schedule III.

Justification: The revision reflects the new calendar day count and includes Schedule III, which was previously omitted.

6. Page 229 of 396 – Phase Elements for Schedule I, Phases 1 and 2

Revision: Updated Schedule I Phase 1 – North Taxiway A Construction title from North to South and increased calendar day count.

Justification: The revision reflects the new calendar day count and includes an update to the title referencing the portion of the taxiway that is being constructed. Additional references were also updated within the phasing breakdown.

Revision: Updated Schedule I Phase 2 – South Taxiway A Construction title from South to North and increased calendar day count.

Justification: The revision reflects the new calendar day count and includes an update to the title referencing the portion of the taxiway that is being constructed. Additional references were also updated within the phasing breakdown.

7. Page 231 of 396 – Phase Elements for Schedule III, Phase 1

Revision: Updated Schedule III Phase 1 – North Taxiway A Underdrains title from North to South and changed calendar day count.

Justification: The revision reflects the new calendar day count and includes an update to the title referencing the portion of the taxiway that is being constructed.

8. Page 232 of 396 – Phase Elements for Schedule III, Phase 2

Revision: Updated Schedule III Phase 2 – South Taxiway A Underdrains title from South to North and changed calendar day count.

Justification: The revision reflects the new calendar day count and includes an update to the title referencing the portion of the taxiway that is being constructed.

9. Page 243 of 396 – Construction Safety Overall Phasing Plan

Revision: Updated sheet to reflect the revised calendar day count.

Justification: The update aligns with changes made to plan sheet G050, which reflects the revised construction phasing and schedule.

10. Pages 245 through 251 of 396 – Construction Safety Individual Phasing Plans

Revision: Updated sheets to show gate access, revised calendar day count, and additional notes.

Justification: These updates were made based on revisions to plan sheets G052–G058.

CONSTRUCTION DRAWING UPDATES

1. Sheet G050 – Construction Safety Overall Phasing Plan (Sheet 10 of 54)

Revision: Updated project schedule table.

Justification: The project schedule changed to 56 calendar days. The schedules and phases were updated accordingly.

Revision: Added gate location along haul route.

Justification: A gate located near the apron along one of the haul routes was not previously shown on certain construction phasing sheets. This addition ensures accurate representation of site access.

Revision: Updated note 5 under detail 3 R/W Closure Marker Detail (Lighted/Unlighted).

Justification: Clarified that payment for this work is incidental to the project and not a separate pay item.

2. Sheet G052 – Construction Safety Individual Phasing Plan – Schedule I Phase 1 (Sheet 12 of 54)

Revision: Removed taxiway closure markers.

Justification: These markers are not needed for Schedule I Phase 1.

Revision: Added gate location along haul route.

Justification: A gate located near the apron along one of the haul routes was not previously shown on certain construction phasing sheets. This addition ensures accurate representation of site access.

Revision: Updated construction phasing notes.

Justification: The notes were revised to include the specific runway reference relevant to this project, clarify the removal or covering of closed taxiway lead-in lines, and update the schedule phasing information.

3. Sheet G053 – Construction Safety Individual Phasing Plan – Schedule I Phase 2 (Sheet 13 of 54)

Revision: Added gate location along haul route.

Justification: A gate located near the apron along one of the haul routes was not previously shown on certain construction phasing sheets. This addition ensures accurate representation of site access.

Revision: Updated construction phasing notes.

Justification: The notes were revised to include the specific runway reference relevant to this project, clarify the removal or covering of closed taxiway lead-in lines, and update the schedule phasing information.

4. Sheet G054 – Construction Safety Individual Phasing Plan – Schedule I Phase 3 (Sheet 14 of 54)

Revision: Added gate location along haul route.

Justification: A gate located near the apron along one of the haul routes was not previously shown on certain construction phasing sheets. This addition ensures accurate representation of site access.

Revision: Updated construction phasing notes.

Justification: The notes were revised to include the specific runway reference relevant to this project and clarify the removal or covering of closed taxiway lead-in lines.

5. Sheet G055 – Construction Safety Individual Phasing Plan – Schedule II Phase 1 (Sheet 15 of 54)

Revision: Updated construction phasing notes.

Justification: The notes were revised to include the specific runway reference relevant to this project and clarify the removal or covering of closed taxiway lead-in lines.

6. Sheet G056 – Construction Safety Individual Phasing Plan – Schedule II Phase 2 (Sheet 16 of 54)

Revision: Updated construction phasing notes.

Justification: The notes were revised to include the specific runway reference relevant to this project and clarify the removal or covering of closed taxiway lead-in lines.

7. Sheet G057 – Construction Safety Individual Phasing Plan – Schedule III Phase 1 (Sheet 17 of 54)

Revision: Added gate location along haul route.

Justification: A gate located near the apron along one of the haul routes was not previously shown on certain construction phasing sheets. This addition ensures accurate representation of site access.

Revision: Updated construction phasing notes.

Justification: The notes were revised to include the specific runway reference relevant to this project, clarify the removal or covering of closed taxiway lead-in lines, and update the schedule phasing information.

8. Sheet G058 – Construction Safety Individual Phasing Plan – Schedule III Phase 2 (Sheet 18 of 54)

Revision: Added gate location along haul route.

Justification: A gate located near the apron along one of the haul routes was not previously shown on certain construction phasing sheets. This addition ensures accurate representation of site access.

Revision: Updated construction phasing notes.

Justification: The notes were revised to include the specific runway reference relevant to this project, clarify the removal or covering of closed taxiway lead-in lines, and update the schedule phasing information.

CLARIFICATIONS / RFI RESPONSES

1. Please note that Addendum No. 3 contained a typo in one of the RFI responses. An additional 22 calendar days have been added to the project schedule.
2. The bid opening date will be pushed to Tuesday, September 30, 2025 at 9:00 A.M. (CST) at Rolla City Hall.

ADDENDUM ENDS



NOTICE TO BIDDERS

City of Rolla
Rolla, Missouri
Taxiway A Reconstruction and Taxiway Connector Rehabilitation
AIP Project No. 23-056A-1

Project Bids, subject to the conditions contained herein, for improvements to the Rolla National Airport, Vichy, Missouri, AIP Project No. 23-056A-1 will be received by the City of Rolla, until **Tuesday, September 30, 2025 at 9:00 a.m.** Bids to be mailed shall be addressed to the attention of the Darin Pryor, City of Rolla, 901 N Elm St, Rolla, Missouri 65401, and will be clearly marked "Sealed Bids: Taxiway A Reconstruction and Taxiway Connector Rehabilitation. DO NOT OPEN". At the time listed above, the received sealed bids will be publicly opened and read aloud immediately in the aforementioned location that they were received.

The bidding documents are available at <https://woolpert.com/bid> and/or www.questcdn.com - Reference Quest Number 9801915. To be considered a plan holder for bids, register with QuestCDN.com for a free Regular membership and download the bidding documents in digital form at a cost of \$22.00. Downloading the documents and becoming a plan holder is recommended as plan holder's receive automatic notice of addenda, other bid updates. Contact QuestCDN Customer Support at 952-233-1632 or Support@QuestCDN.com for assistance in membership registration or downloading digital bidding documents. Interested parties may view the contract documents at no cost prior to deciding to become a plan holder.

In order to submit a responsive bid as a Prime Contractor and to receive all necessary addendum(s) for this project, you must be on the Planholder's List. It is the planholder's responsibility to review the site for addendums and changes before submitting their proposal. This includes review for environmental changes. Environmental changes during construction could take up to three months for approval. For additional information, please contact us via email at bid.info@woolpert.com.

The work involved will include the following:

Bid Package No. 1:
Schedule I - Taxiway A Reconstruction
Schedule II - Taxiway B Rehabilitation
Schedule III - Taxiway A Underdrains

The approximate quantities of major bid items involved in the proposed work are:



BID PROPOSAL FORM

TO: City of Rolla
Rolla, Missouri

1. The undersigned hereby certifies that they have examined the form of contract, plans and specifications and other associated Contract Documents for the improvement of Rolla National Airport, Project No. 23-056A-1. . The undersigned further certifies that he/she has examined the site of the work, has determined for himself/herself the conditions affecting the work and subject to acceptance of the proposal, agrees to provide at his or her expense, all labor , insurance, superintendence, machinery, plant, equipment, tools, apparatus, appliances, and means of construction, and all materials and supplies complete the entire work, including work incidental thereto, in conformance with the plans, specifications, and associated Contract Documents.
2. The undersigned acknowledges that the Contract Documents consist of the Notice to Bidders, Instruction to Bidders, all issued Addenda, Proposal, Statement of Qualifications, Anticipated Sub-Contracts, Form of Proposal Guaranty, Notice of Award, Construction Agreement, Performance & Payment Bonds, Notice to Proceed, Notice of Contractor's Settlement, Wage Rates, General Provisions, Special Provisions, Plans, Project Manual including Technical Specifications, Federal Contract Provisions, and Drug-free Workplace, and all other appendices and reference items included in the Project Manual and plans.
3. The undersigned, in compliance with your Notice to Bidders dated **September 30, 2025**, hereby proposes to do the work called for in said contract and specifications and shown on said plans and to furnish all materials, tools, labor, and all appliances and appurtenances necessary for the said work at the following unit rates and prices:

Bid Spreadsheet begins on page 77.

4. The undersigned understands that the above quantities of work to be done are approximate only and are intended principally to serve as a guide in evaluating the bids. Final project payments will be made on actual quantities and unit prices.
5. It is understood that the schedule of minimum wage rates, as established by the Secretary of Labor and included in the Specifications, are to govern on this project, and the undersigned certifies that he/she has examined this schedule of wage rates and that the prices bid are based on such established wage rates.
6. The undersigned prime contractor, if not a certified DBE, hereby assures that they will make sufficient and reasonable efforts to meet the DBE goals, that they will subcontract 3% of the dollar value of the prime contract to DBE firms, and that they will include the DBE clauses required by the sponsor's DBE Program in all subcontracts which offer subcontracting opportunities. The undersigned will complete and submit with the bid the attached DBE Participation Form. If unable to meet the project goal, the undersigned shall submit a demonstration of good faith effort in accordance with Part 5 Special Provisions, DBE Award Documentation and Procedure.
7. The undersigned agree upon written notice of the acceptance of this bid, that within thirty (30) days after the award, that he/she will execute the contract in accordance with the bid as accepted and give contract (Performance and Payment) bonds on attached forms.
8. The undersigned further agrees that if awarded the contract, he/she will commence the work within ten (10) calendar days after the receipt of a Notice to Proceed and that he/she will complete the work within **79 calendar day(s)**. An extension of time may be allowed when extra or additional work is ordered by the engineer.
9. As evidence of good faith in submitting this proposal, the undersigned encloses a certified check or Bid Bond in the amount of:



CONSTRUCTION AGREEMENT

**Rolla National Airport
Vichy, Missouri
Taxiway A Reconstruction and Taxiway Connector Rehabilitation
Project No. 23-056A-1**

THIS AGREEMENT, made and entered into this _____ day of _____, 20____, by and between City of Rolla, Party of the First Part, hereinafter referred to as the "Sponsor", and _____, Party of the Second Part, hereinafter referred to as the "Contractor," for the construction of airport improvements including Taxiway A Reconstruction and Taxiway Connector Rehabilitation, AIP No. 23-056A-1 at the Rolla National Airport.

WITNESSETH THAT the Contractor and Sponsor for the consideration stated herein agree as follows:

ARTICLE 1: SCOPE OF WORK. It is hereby mutually agreed that for and in consideration of the payments as provided for herein to the Contractor by the Sponsor, the Contractor shall furnish all labor, utilities, transportation services, tools, equipment, and material and shall perform all work necessary including all incidental and appurtenant work to complete the improvements in a good and substantial manner, ready for use in strict accordance with this Contract, a copy of which is filed pursuant to law in the office of the legal representative of the Sponsor.

ARTICLE 2: NOTICE TO PROCEED. The Contractor agrees to commence work within ten (10) calendar days after the date indicated within the Notice to Proceed. Contract times commence to run as provided in paragraph 80-07 of General Provisions and will continue to be counted until the project is accepted and complete, including punch list and administrative closeout submittals. Contractor further agrees to complete said work within **79 calendar day(s)**. Extensions of the Contract time may only be permitted execution of a formal modification to Contract Agreement as approved by the Sponsor.

ARTICLE 3: COMPENSATION. In consideration of the completion of the work described herein and in fulfillment of all stipulations of this Contract to the satisfaction and acceptance of the Engineer and the Sponsor, the Sponsor shall pay and the said Contractor further agrees to receive and accept payment based on the contract price bid per unit as full compensation for furnishing all the equipment, labor, incidentals, and materials, and for the costs of all premiums on insurance and bonds and for doing all work contemplated and specified in this Contract; also for all loss or damage arising out of the nature of the work aforesaid, or from any unforeseen obstructions or difficulties which may be encountered in the prosecution of the same; and for all risks of every description connected with the work; and for well and faithfully completing the work and the whole thereof, in full compliance with the Contract Documents and the requirements of the Engineer under them.

Payments are to be made to the Contractor in accordance with and subject to the provisions embodied in the Contract documents hereto attached.



precaution to prevent damage or deterioration of the work performed and provide for normal drainage of the work. The Contractor shall erect temporary structures where necessary to provide for traffic on, to, or from the airport.

80-07 Determination and extension of contract time. The number of calendar days shall be stated in the proposal and contract and shall be known as the Contract Time.

If the contract time requires extension for reasons beyond the Contractor’s control, it shall be adjusted as follows:

80-07.1 Contract time based on calendar days. Contract Time based on calendar days shall consist of the number of calendar days stated in the contract counting from the effective date of the Notice to Proceed and including all Saturdays, Sundays, holidays, and non-work days. All calendar days elapsing between the effective dates of the Sponsor’s orders to suspend and resume all work, due to causes not the fault of the Contractor, shall be excluded.

At the time of final payment, the contract time shall be increased in the same proportion as the cost of the actually completed quantities bears to the cost of the originally estimated quantities in the proposal. Such increase in the contract time shall not consider either cost of work or the extension of contract time that has been covered by a change order or supplemental agreement. Charges against the contract time will cease as of the date of final acceptance.

If the Contractor finds it impossible for reasons beyond their own control to complete the work within the contract time as specified, or as extended in accordance with the provisions of this paragraph, the Contractor may, at any time prior to the expiration of the contract time as extended, make a written request to the Sponsor for an extension of time setting forth the reasons which the Contractor believes will justify the granting of their own request. Requests for extension of time, caused by inclement weather, shall be supported with National Weather Bureau data showing the actual amount of inclement weather exceeded what could normally be expected during the contract period. The Contractor’s plea that insufficient time was specified is not a valid reason for extension of time. If the supporting documentation justify the work was delayed because of conditions beyond the control and without the fault of the Contractor, the Sponsor may extend the time for completion by a change order that adjusts the contract time or completion date. The extended time for completion shall then be in full force and effect, the same as though it were the original time for completion.

80-08 Failure to complete on time. For each calendar day or working day, as specified in the contract, that any work remains uncompleted after the contract time (including all extensions and adjustments as provided in paragraph 80-07, *Determination and extension of contract time*) the sum specified in the contract and proposal as liquidated damages (LD) will be deducted from any money due or to become due the Contractor or their own surety. Such deducted sums shall not be deducted as a penalty but shall be considered as liquidation of a reasonable portion of damages including but not limited to additional engineering services that will be incurred by the Sponsor should the Contractor fail to complete the work in the time provided in their contract.

| Schedule | Liquidated Damages Cost | Allowed Construction Time |
|--------------------|----------------------------|---------------------------|
| Sch I & III – Ph 1 | \$750.00 / calendar day(s) | 32 calendar day(s) |
| Sch I & III – Ph 2 | \$750.00 / calendar day(s) | 45 calendar day(s) |
| Sch I – Ph 3 | \$750.00 / calendar day(s) | 1 calendar day(s) |
| Sch II – Ph 1 | \$750.00 / calendar day(s) | 3 calendar day(s) |
| Sch II – Ph 2 | \$750.00 / calendar day(s) | 1 calendar day(s) |

1. Schedule II Phase 1 shall be concurrent with Schedule I Phase 1.
2. Pavement must have 30-day cure period before application of permanent marking.



The maximum construction time allowed for Schedules **I, II and III** will be the sum of the time allowed for individual schedules **not including concurrent phases**, but not more than **79 calendar day(s)**. Permitting the Contractor to continue and finish the work or any part of it after the time fixed for its completion, or after the date to which the time for completion may have been extended, will in no way operate as a waiver on the part of the Sponsor of any of its rights under the contract.

80-09 Default and termination of contract. The Contractor shall be considered in default of their contract and such default will be considered as cause for the Sponsor to terminate the contract for any of the following reasons, if the Contractor:

- a. Fails to begin the work under the contract within the time specified in the Notice to Proceed, or
- b. Fails to perform the work or fails to provide sufficient workers, equipment and/or materials to assure completion of work in accordance with the terms of the contract, or
- c. Performs the work unsuitably or neglects or refuses to remove materials or to perform anew such work as may be rejected as unacceptable and unsuitable, or
- d. Discontinues the execution of the work, or
- e. Fails to resume work which has been discontinued within a reasonable time after notice to do so, or
- f. Becomes insolvent or is declared bankrupt, or commits any act of bankruptcy or insolvency, or
- g. Allows any final judgment to stand against the Contractor unsatisfied for a period of 10 days, or
- h. Makes an assignment for the benefit of creditors, or
- i. For any other cause whatsoever, fails to carry on the work in an acceptable manner.

Should the Sponsor consider the Contractor in default of the contract for any reason above, the Sponsor shall immediately give written notice to the Contractor and the Contractor's surety as to the reasons for considering the Contractor in default and the Sponsor's intentions to terminate the contract.

If the Contractor or surety, within a period of 10 days after such notice, does not proceed in accordance therewith, then the Sponsor will, upon written notification from the RPR of the facts of such delay, neglect, or default and the Contractor's failure to comply with such notice, have full power and authority without violating the contract, to take the execution of the work out of the hands of the Contractor. The Sponsor may appropriate or use any or all materials and equipment that have been mobilized for use in the work and are acceptable and may enter into an agreement for the completion of said contract according to the terms and provisions thereof, or use such other methods as in the opinion of the RPR will be required for the completion of said contract in an acceptable manner.

All costs and charges incurred by the Sponsor, together with the cost of completing the work under contract, will be deducted from any monies due or which may become due the Contractor. If such expense exceeds the sum which would have been payable under the contract, then the Contractor and the surety shall be liable and shall pay to the Sponsor the amount of such excess.

80-10 Termination for national emergencies. The Sponsor shall terminate the contract or portion thereof by written notice when the Contractor is prevented from proceeding with the construction contract as a direct result of an Executive Order of the President with respect to the execution of war or in the interest of national defense. When the contract, or any portion thereof, is terminated before completion of all items of work in the contract, payment will be made for the actual number of units or items of work completed at the contract price or as mutually agreed for items of work partially completed or not started. No claims or loss of anticipated profits shall be considered.

2. PHASING

To minimize disruptions to airport operations during construction, construction will be broken up by areas to limit the number of aircraft operational areas affected at any given time. Maintaining continual access to the terminal building and air carrier apron is mandatory during all phases of construction to allow the aircraft to operate. The phasing plan proposed was developed with help from the Airport, and is considered to be the most effective way of allowing aircraft access while imposing the least amount of impact on construction operations while maintaining safety. The phasing for this project is presented below and is visually depicted in the Construction Safety Drawings (Sheets G050 through G058) attached at the back of this document.

This project will be completed in three schedules, with a total of seven phases. Each of the phases is discussed in further detail in the Construction Safety Drawing plan sheets included at the end of this document.

A. PHASE ELEMENTS

I. Schedule I, Phase 1 – **South Taxiway A Reconstruction** (Construction Safety Drawing G052, Bid Pkg No. 1)

During this phase, the Contractor will have **32** calendar days to complete all work. Work items for this phase include removing the existing 50' wide pavement section up to the adjoining taxilane on the **northwest** side of Taxiway A, and removal of electrical items. Following the pavement removal, they will narrow the area to a 35' width, cement treat the subgrade in place, and construct a new 12-inch asphalt pavement section. Temporary markings will be applied to the newly constructed pavement. A 30-day cure period will be required prior to the application of permanent pavement markings. Lighting installation shall include trenching and backfilling, installation of light cans, bases, conduits, and wire. Hauling activities will take place along the gravel road northwest of this area and should not interfere with aircraft operations. Prior to beginning work on this phase, the Contractor shall have barricades and taxiway closure markings in place in accordance with the plans.

During Phase 1, a temporary access route for emergency service vehicles will be required through the construction area northwest of the construction limits.

Access for aircraft traffic to the terminal apron during this phase will be via Taxiway A on the southwest side of Runway 13-31 **as well as Taxiway B when it is not being worked on concurrent to this phase.**

Emergency service vehicle access to the runways and apron will be through the temporary access route as described above and depicted on the Construction Safety Drawings.

Schedule I, Phase 1 and 2 will not be completed concurrently. **Schedule II Phase 1 shall be constructed concurrently with Schedule I Phase 1.** Schedule III Phase 1 shall be constructed concurrent with Schedule I Phase 1. The temporary haul route must be maintained through the construction area for emergency service vehicle traffic at all times.

II. Schedule I, Phase 2 – **North Taxiway A Reconstruction** (Construction Safety Drawing G053, Bid Pkg No. 1)

During this phase, the Contractor will have **45** calendar days to complete all work. Work items for this phase include removing the remaining existing 50' pavement section from the adjoining

pavement. A 30-day cure period will be required prior to the application of permanent pavement markings in Schedule II, Phase 2. Hauling activities will be required across the northeast corner of the active apron and down Airport Drive. Prior to beginning work on this phase, the Contractor shall have barricades and taxiway closure markings in place in accordance with the plans.

During Phase 1, a temporary access route for emergency service vehicles will be required through the construction area northwest of the construction limits.

Access for aircraft traffic to the terminal apron during this phase will be via Taxiway A.

Emergency service vehicle access to the runways and apron will be through the temporary access route as described above and depicted on the Construction Safety Drawings.

Schedule II Phase 1 may be concurrent with Schedule I Phase 2. The temporary haul route must be maintained through the construction area for emergency service vehicle traffic at all times.

**V. Schedule II, Phase 2 – Taxiway B Pavement Marking
(Construction Safety Drawing G056, Bid Pkg No. 1)**

During this schedule, the Contractor will have 1 calendar day to complete all work. Work items for this phase include application of permanent pavement markings. Prior to beginning work on this phase, the Contractor shall have barricades and taxiway closure markings in place in accordance with the plans.

During Phase 2, a temporary access route for emergency service vehicles will be required through the construction area northwest of the construction limits.

Access for aircraft traffic to the terminal apron during this phase will be via Taxiway A.

Emergency service vehicle access to the runways and apron will be through the temporary access route as described above and depicted on the Construction Safety Drawings.

Schedule II Phase 2 shall not be completed until after a 30 day cure period for Phase 1. The temporary haul route must be maintained through the construction area for emergency service vehicle traffic at all times.

**VI. Schedule III, Phase 1 – South Taxiway A Underdrains
(Construction Safety Drawing G057, Bid Pkg No. 1)**

During this schedule, the Contractor will be working concurrently on Schedule I Phase 1. Work items for this schedule include trenching and backfilling, installation of inspection pits and cleanout structures, placement of 6-inch perforated and non-perforated polyethylene pipe, and outfall headwall install. Hauling activities will take place along the gravel road northwest of this area and should not interfere with aircraft operations. Prior to beginning work on this phase, the Contractor shall have barricades and taxiway closure markings in place in accordance with the plans.

During Phase 1, a temporary access route for emergency service vehicles will be required through the construction area northwest of the construction limits.

Access for aircraft traffic to the terminal apron during this phase will be via Taxiway A on the southwest side of Runway 13-31 and Taxiway B.

Emergency service vehicle access to the runways and apron will be through the temporary access route as described above and depicted on the Construction Safety Drawings.

Schedule III Phase 1 shall be concurrent with Schedule I Phase 1. The temporary haul route must be maintained through the construction area for emergency service vehicle traffic at all times.

**VII. Schedule III Phase 2 – North Taxiway A Underdrains
(Construction Safety Drawing G058, Bid Pkg No. 1)**

During this schedule, the Contractor will be working concurrently on Schedule I Phase 2. Work items for this schedule include trenching and backfilling, installation of inspection pits and cleanout structures, placement of 6-inch perforated and non-perforated polyethylene pipe, and outfall headwall install. Hauling activities will take place along the asphalt unnamed road northwest of this area, connecting to the adjacent hangar area, and should not interfere with aircraft operations. Prior to beginning work on this phase, the Contractor shall have barricades and taxiway closure markings in place in accordance with the plans.

During Phase 2, a temporary access route for emergency service vehicles will be required through the construction area northwest of the construction limits.

Access for aircraft traffic to the terminal apron during this phase will be via Taxiway A on the southwest side of Runway 13-31 and Taxiway B.

Emergency service vehicle access to the runways and apron will be through the temporary access route as described above and depicted on the Construction Safety Drawings.

Schedule III Phase 2 shall be concurrent with Schedule I Phase 2. The temporary haul route must be maintained through the construction area for emergency service vehicle traffic at all times.

B. CONSTRUCTION SAFETY DRAWINGS

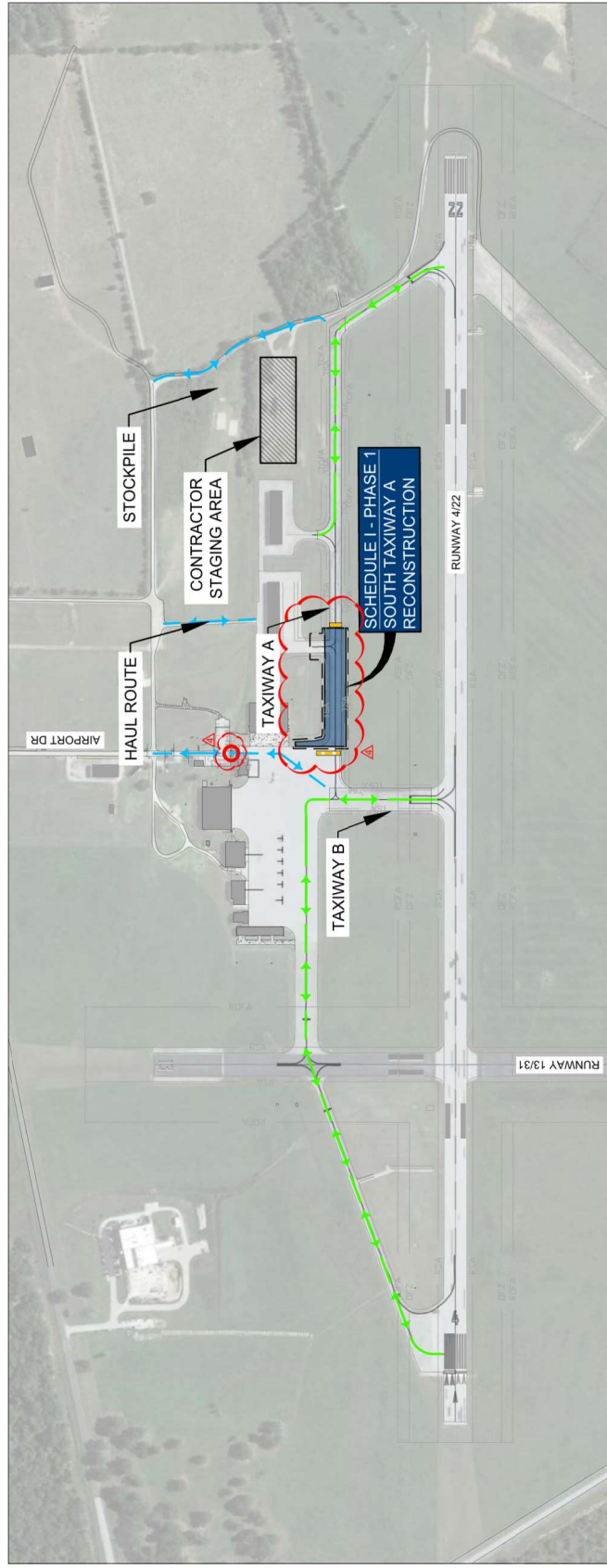
The Construction Safety Drawings for Bid Package No. 1 (Sheets G050 through G058) are attached at the back of this document to show the phasing requirements for this project. Along with the phasing information, those attached drawings also show aircraft access routes, emergency service vehicle access routes, pedestrian routes, and contractor operation limits to help assist with airport operations and maintaining safety during this project. The Construction Layout Plan for Bid Package No. 1 (Sheet G050) and Safety Plan (Sheet G052 and G058) are additional plan sheets containing safety requirements during construction and are also included in the back of this report.

3. AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY

All work within the Airport Operations Area shall be accomplished in conformance to Advisory Circular 150/5370-2G, Operational Safety on Airports During Construction. The contract drawings include information regarding requirements for operational safety on the airport during construction.

PHASING LEGEND

| | |
|--|---------------------------------------|
| | AIRCRAFT TAXI ROUTE |
| | CONTRACTOR TAXI ROUTE (2 WAY TRAFFIC) |
| | RSA |
| | RUNWAY OBJECT FREE AREA |
| | ISDA |
| | TSA |
| | TAXIWAY OBJECT FREE AREA |
| | CONSTRUCTION WORK AREA |
| | FLASHER BARRICADE |
| | RUNWAY CLOSURE MARKER |
| | FLAGMAN / GATE GUARD |
| | CONTRACTOR GATE ACCESS |



ISSUED FOR BID

THESE DRAWINGS ARE FOR PURPOSES ONLY. THEY WERE PREPARED BY OR UNDER THE SUPERVISION OF:

LAURA K. KOONCE
NAME
REG. NO. 2022012014
DATE 08/05/2025
FOR AND ON BEHALF OF WOOLPERT, INC.

| | |
|--|-------------------------|
| SHEET NAME | G052 |
| SHEET NO. | 12 of 54 |
| CONSTRUCTION SAFETY INDIVIDUAL PHASING PLAN - SCHEDULE I PHASE 1 | |
| PROJECT PROJ. NO. 25000001 | WSP PROJ. NO. 001001010 |

- NOTES**
10. ALL STOCKPILES OR EQUIPMENT ADJACENT TO OBJECT FREE AREAS SHALL BE MARKED AND LIGHTED PER AC 1505370-2G.
 11. THE CONTRACTOR SHALL PROVIDE A TRAFFIC CONTROL PLAN AND A WORK AREA CONTROL PLAN FOR THIS SCHEDULE OF WORK.
 12. THE CONTRACTOR SHALL HAVE A SHEEPER ON SITE AT ALL TIMES TO CLEAN DEBRIS FROM HAUL ROUTES, CONSTRUCTION ACCESS POINTS, OR AREAS ADJACENT TO CONSTRUCTION.
 13. THE CONTRACTOR SHALL KEEP ALL CONSTRUCTION TRAFFIC LIMITED TO THE APPROVED HAUL ROUTES AS SHOWN ON THE PLANS OR AS INDICATED BY THE CONTRACTOR. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE CAUSED ALONG THE HAUL ROUTES OR CONSTRUCTION ACCESS ROUTES TO THE PROJECT SITE. HAUL ROADS WILL BE REPAIRED TO ORIGINAL CONDITION AT THE CONTRACTOR'S EXPENSE TO THE SPONSOR.
 14. PRIOR TO ACCEPTANCE AND OPENING OF AN AREA TO AIRCRAFT, ALL FINAL GRADING MUST BE COMPLETED. THIS INCLUDES ROCK REMOVAL, FINE GRADING, AND SEED PLACEMENT.
 15. ACCESS TO WORK AREAS WILL BE BY HAUL ROUTES ONLY.
 16. UNICOM - MONITOR 123.0 MHz AT ALL TIMES.
 17. ALL COMPONENTS OF THE AIRFIELD LIGHTING SYSTEM OUTSIDE OF THE PROJECT AREA SHALL BE OPERATIONAL AT THE END OF EACH WORK SHIFT AND FOR EVERY PERIOD OF LOW VISIBILITY. THE CONTRACTOR SHALL MAINTAIN OPERATIONAL LIGHTING SYSTEMS THROUGHOUT DURING CONSTRUCTION. TEMPORARY JUMPERS SHALL BE CONSIDERED NECESSARY TO MAINTAIN AN OPERATIONAL LIGHTING SYSTEM THROUGHOUT DURING CONSTRUCTION. ALL TEMPORARY JUMPERS SHALL BE INSTALLED IN CONDUIT.
 18. PRIOR TO BEGINNING ELECTRICAL DETAILING THE CONTRACTOR SHALL PROVIDE A PLAN TO THE ENGINEER DETAILING THE INSTALLATION OF ALL REQUIRED TEMPORARY JUMPERS. THE PLAN SHALL SHOW THE LOCATION AND INSTALLATION TECHNIQUES OF ALL JUMPERS AND

- CONSTRUCTION PHASING NOTES**
- IMPACTS ON OPERATIONS**
1. THE CONTRACTOR SHALL GIVE RIGHT OF WAY TO ALL AIRCRAFT AND EMERGENCY VEHICLES AT ALL TIMES.
 2. THE CONTRACTOR SHALL MAINTAIN EMERGENCY VEHICLE ACCESS TO RUNWAY AT ALL TIMES.
 3. ALL NECESSARY TAXIWAY LIGHTS AND SIGNS SHALL BE COVERED FOR TEMPORARY TAXIWAY CLOSURES. CONTRACTOR METHOD FOR COVERING TAXIWAY LIGHTS SHALL BE SECURELY FASTENED TO PREVENT FREING OF COVER FROM WIND. DURING PAVEMENT MARKING APPLICATIONS AND CURE TIMES, CONSTRUCTION BARRICADES SHALL BE INSTALLED AS DIRECTED BY THE CONTRACTOR TO BE SECURELY FASTENED TO TAXIWAY.
 5. PERSONNEL ARE ALLOWED IN THE SAFETY AREAS FOR AN ACTIVE RUNWAY WHEN THE AIRPORT IS OPEN.
 6. ALL SAFETY AREAS AND OBJECT FREE AREAS FOR ACTIVE AIRPORT OPERATIONS SHALL BE MAINTAINED THROUGHOUT CONSTRUCTION WITHOUT APPROVAL FROM AIRPORT OPERATIONS.
 7. PRIOR TO OPENING A RUNWAY OR TAXIWAY, ALL OBSTRUCTIONS AND TRENCHESS IN THE SAFETY AREAS SHALL BE BACKFILLED AND COMPACTED TO P-152 SPECIFICATIONS. GRADING WITHIN THE SAFETY AREAS SHALL BE COMPLETED PRIOR TO THE START OF CONSTRUCTION.
 8. CONTRACTOR TO PROVIDE RUNWAY CLOSURE MARKERS AND BARRICADES TO BE INSTALLED ON RUNWAY 4/22 DURING CLOSURES. CONTRACTOR IS RESPONSIBLE FOR MAINTENANCE OF RUNWAY 4/22.
 9. BARRICADES SHALL BE INSTALLED PER PHASING PLANS AND AS REQUIRED BY AC 1505370-2G OR AS DIRECTED BY THE ENGINEER.

- SCHEDULE I / PHASE I**
- DURATION**
- PHASE I: 30 CALENDAR DAYS
 - FOR SCHEDULE I TO BE CONCURRENT WITH PHASE I OR 3 PHASE I TO BE COMPLETED CONCURRENTLY WITH SCHEDULE II
 - PHASE I TO BE COMPLETED CONCURRENTLY WITH SCHEDULE II
 - PHASE I TO BE COMPLETED CONCURRENTLY WITH SCHEDULE III
- CONTRACTOR ACCESS TIMES**
- 24 HOUR ACCESS TO APPROVED WORK AREAS
 - ALL AIRPORT OPERATIONS AREAS SHALL REMAIN OPEN AND ACCESSIBLE DURING THIS SCHEDULE WITH THE FOLLOWING EXCEPTIONS:
 - TAXIWAY A WILL BE PARTIALLY CLOSED THROUGHOUT THE DURATION OF SCHEDULE I PHASE I TO PROVIDE ACCESS TO THE ADDING HANGAR

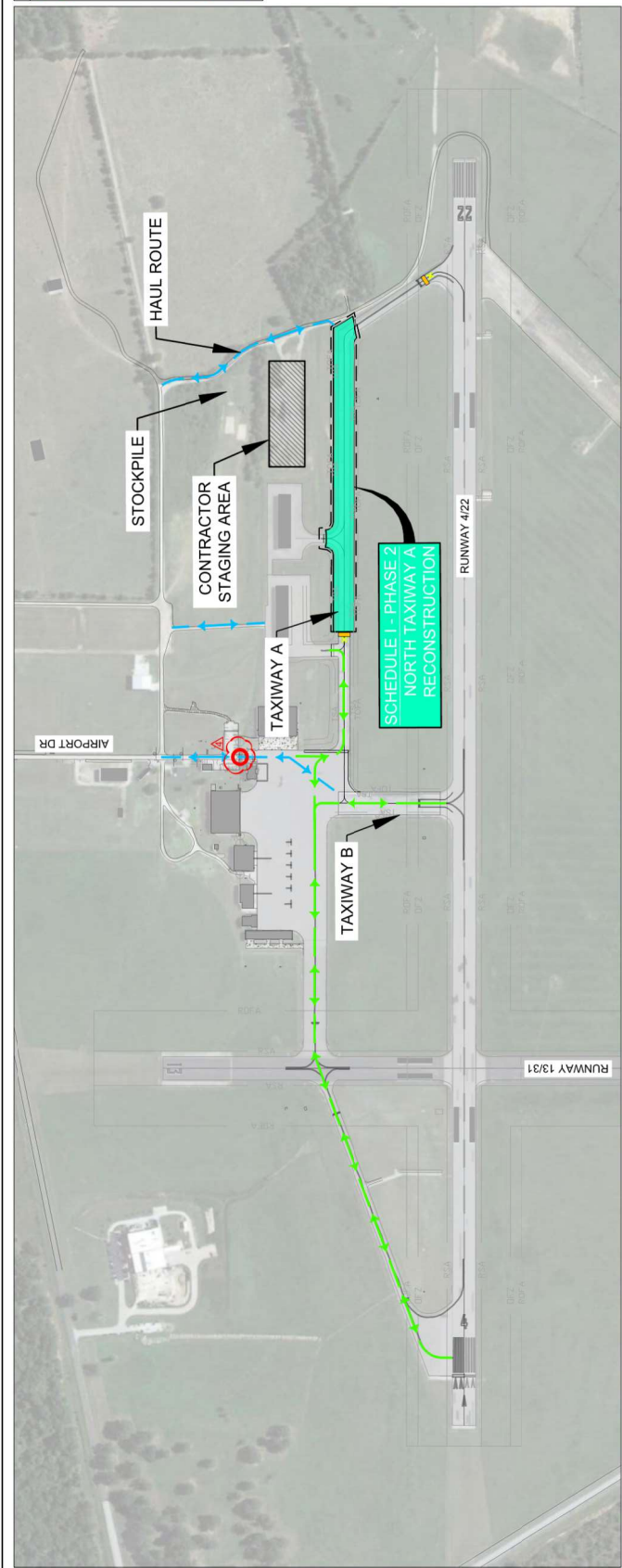
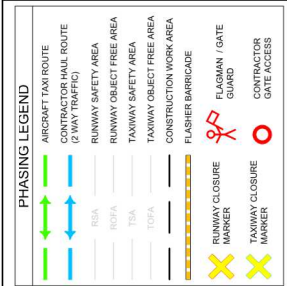
- MAJOR WORK TO BE COMPLETED**
- SITE PREPARATION**
1. EROSION CONTROL MEASURES
 2. EXCAVATION AND DEBRIS REMOVAL
 3. ELECTRICAL REMOVAL
- EXCAVATION**
1. UNCLASSIFIED EXCAVATION
 1. CEMENT TREATED SUBGRADE, 8" DEPTH
 2. AGGREGATE BASE COURSE, 8" DEPTH
 3. ASPHALT PAVEMENT, 4" DEPTH
- UTILITY**
1. TALL TAXIWAY EDGE LIGHT FIXTURES AND ELECTRICAL CABLES
- PAVEMENT MARKINGS**
1. TEMPORARY PAVEMENT MARKING
- SITE RECLAMATION**
1. TAXIWAY SAFETY AREA GRADING
 2. EROSION CONTROL MEASURES

| DES'N.B.B. | NO. | BY | DATE | ISSUE RECORD |
|-------------|-----|--------|------------|----------------|
| DR. V.S.B. | 1 | L.K.K. | 08/05/2025 | ISSUED FOR BID |
| CH: C.L.G. | 2 | L.K.K. | 08/05/2025 | REVISION #10.4 |
| APP: L.K.K. | | | | |

TAXIWAY A RECONSTRUCTION AND TAXIWAY CONNECTOR REHABILITATION

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| CONSTRUCTION SAFETY INDIVIDUAL PHASING PLAN - SCHEDULE I PHASE 1 |
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|-------------------------------------|-----------------|
| ISSUED FOR BID | |
| NAME | LAURA K. KOONCE |
| REG. NO. | 2022012014 |
| DATE | 08/05/2025 |
| FOR AND ON BEHALF OF WOOLPERT, INC. | |

| | |
|--------------------|--|
| SHEET NAME | CONSTRUCTION SAFETY INDIVIDUAL PHASING PLAN - SCHEDULE I PHASE 2 |
| SHEET NO. | G053 |
| PROJECT PROJ. NO. | 10019891.00 |
| WOOLPERT PROJ. NO. | 24258641 |
| 13 of 54 | |

| CONSTRUCTION PHASING NOTES | |
|--|--|
| <p>DURATION</p> <p>PHASE 2: 45 CALENDAR DAYS</p> <p>FOR SCHEDULE I NOT CONCURRENT WITH PHASE 1 OR 3, BUT SHOULD BE CONCURRENT WITH SCHEDULE II PHASE 2</p> <p>CONTRACTOR ACCESS TIMES</p> <p>24 HOUR ACCESS TO APPROVED WORK AREAS</p> <p>ALL AIRPORT OPERATIONS AREAS SHALL REMAIN OPEN AND UNIMPACTED DURING THIS SCHEDULE WITH THE FOLLOWING EXCEPTIONS:</p> <ul style="list-style-type: none"> • CONTRACTOR ACCESS TO TAXIWAY A DURING THE DURATION OF SCHEDULE I PHASE 2 TO PROVIDE ACCESS TO THE ADJOINING HANGAR | <p>MAJOR WORK TO BE COMPLETED</p> <p>SITE PREPARATION</p> <ol style="list-style-type: none"> 1. EROSION CONTROL MEASURES 2. EXISTING PAVEMENT REMOVAL 3. ELECTRICAL REMOVAL 4. DEMOLITION 5. UNCLASSIFIED EXCAVATION <p>PAVEMENT SECTION</p> <ol style="list-style-type: none"> 1. CEMENT TREATED SUBGRADE, 18" DEPTH 2. AGGREGATE BASE COURSE, 8" DEPTH 3. ASPHALT PAVEMENT, 4" DEPTH <p>UTILITY</p> <ol style="list-style-type: none"> 1. REPAIR ALL TAXIWAY EDGE LIGHT FIXTURES AND ELECTRICAL CABLES <p>PAVEMENT MARKINGS</p> <ol style="list-style-type: none"> 1. TEMPORARY PAVEMENT MARKING <p>SITE RECLAMATION</p> <ol style="list-style-type: none"> 1. TAXIWAY SAFETY AREA GRADING 2. EROSION CONTROL MEASURES |
| <p>IMPACTS ON OPERATIONS</p> <ol style="list-style-type: none"> 1. THE CONTRACTOR SHALL GIVE RIGHT OF WAY TO ALL AIRCRAFT AND EMERGENCY VEHICLES AT ALL TIMES. 2. THE CONTRACTOR TO MAINTAIN EMERGENCY VEHICLE ACCESS TO RUNWAY AT ALL TIMES. 3. ALL NECESSARY TAXIWAY LIGHTS AND SIGNS SHALL BE COVERED FOR TEMPORARY TAXIWAY CLOSURES. CONTRACTOR METHOD FOR COVERING TAXIWAY LIGHTS SHALL BE SECURELY FASTENED TO PREVENT FREING OF COVER FROM WIND. DURING PAVEMENT MARKING APPLICATIONS AND CURE TIMES, CONSTRUCTION BARRICADES SHALL BE INSTALLED AS DIRECTED BY WOOLPERT. CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTENANCE OF BARRICADES TO TAXIWAYS. PERSONNEL ARE ALLOWED IN THE SAFETY AREAS OR OBJECT FREE AREAS FOR ANY ACTIVE TAXIWAY OR SAFETY AREAS FOR AN ACTIVE RUNWAY WHEN THE AIRPORT IS OPEN. 6. ALL SAFETY AREAS AND OBJECT FREE AREAS FOR ACTIVE AIRPORT OPERATIONS SHALL BE MAINTAINED THROUGHOUT THE DURATION OF CONSTRUCTION WITHOUT APPROVAL FROM AIRPORT OPERATIONS. 7. TRENCHES IN THE SAFETY AREAS SHALL BE BACKFILLED AND COMPACTED TO P-152 SPECIFICATIONS. GRADING WITHIN THE SAFETY AREAS SHALL BE TO MATCH EXISTING GRADING. 8. CONTRACTOR TO PROVIDE RUNWAY CLOSURE MARKERS AND BARRICADES TO MAINTAIN OPERATIONAL SAFETY. CONTRACTOR IS RESPONSIBLE FOR MAINTENANCE OF RUNWAY X'S. 9. BARRICADES SHALL BE INSTALLED PER PHASING PLANS AND AS REQUIRED BY AC 150/5370-50 OR AS DIRECTED BY THE ENGINEER. | <p>NOTES</p> <ol style="list-style-type: none"> 10. ALL STOCKPILES OR EQUIPMENT ADJACENT TO OBJECT FREE AREAS SHALL BE MARKED AND LIGHTED PER AC 150/5370-50. 11. THE CONTRACTOR SHALL PROVIDE A TRAFFIC CONTROL PLAN AND A TRAFFIC CONTROL PLAN FOR THE PROJECT, JURISDICTION, AND A WORK PLAN FOR THIS SCHEDULE OF WORK. 12. THE CONTRACTOR SHALL HAVE A SHEEPER ON SITE AT ALL TIMES TO CLEAN DEBRIS FROM HAUL ROUTES, CONSTRUCTION ACCESS POINTS, OR AREAS ADJACENT TO CONSTRUCTION. 13. THE CONTRACTOR SHALL KEEP ALL CONSTRUCTION TRAFFIC LIMITED TO THE APPROVED HAUL ROUTES AS SHOWN ON THE PLANS OR AS INDICATED BY THE ENGINEER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE CAUSED ALONG THE HAUL ROUTES OR CONSTRUCTION ACCESS ROUTES TO THE PROJECT SITE. HAUL ROUTES WILL BE MAINTAINED THROUGHOUT THE DURATION OF CONSTRUCTION BY THE CONTRACTOR AT NO EXPENSE TO THE SPONSOR. 14. PRIOR TO ACCEPTANCE AND OPENING OF AN AREA TO AIRCRAFT, ALL FINAL GRADING MUST BE COMPLETED. THIS INCLUDES ROCK REMOVAL, FINE GRADING, AND SEED PLACEMENT. 15. ACCESS TO WORK AREAS WILL BE BY HAUL ROUTES ONLY. 16. UNICOM - MONITOR 123.0 MHz AT ALL TIMES. 17. ALL COMPONENTS OF THE AIRFIELD LIGHTING SYSTEM OUTSIDE OF THE PROJECT AREA SHALL BE OPERATIONAL AT THE END OF EACH WORK SHIFT AND FOR EVERY PERIOD OF LOW VISIBILITY. THE CONTRACTOR SHALL MAINTAIN OPERATIONAL SAFETY THROUGHOUT THE DURATION OF CONSTRUCTION. TEMPORARY JUMPERS SHALL BE CONSIDERED DURING CONSTRUCTION. TEMPORARY JUMPERS SHALL BE CONSIDERED NECESSARY TO MAINTAIN AN OPERATIONAL SYSTEM THROUGHOUT THE DURATION OF CONSTRUCTION. ALL TEMPORARY JUMPERS SHALL BE INSTALLED IN CONDUIT. 18. PRIOR TO BEGINNING ELECTRICAL DEMOLITION THE CONTRACTOR SHALL PROVIDE A PLAN TO THE ENGINEER DETAILING THE INSTALLATION OF ALL REQUIRED TEMPORARY JUMPERS. THE PLAN SHALL SHOW THE LOCATION AND INSULATION TECHNIQUES OF ALL JUMPERS AND |

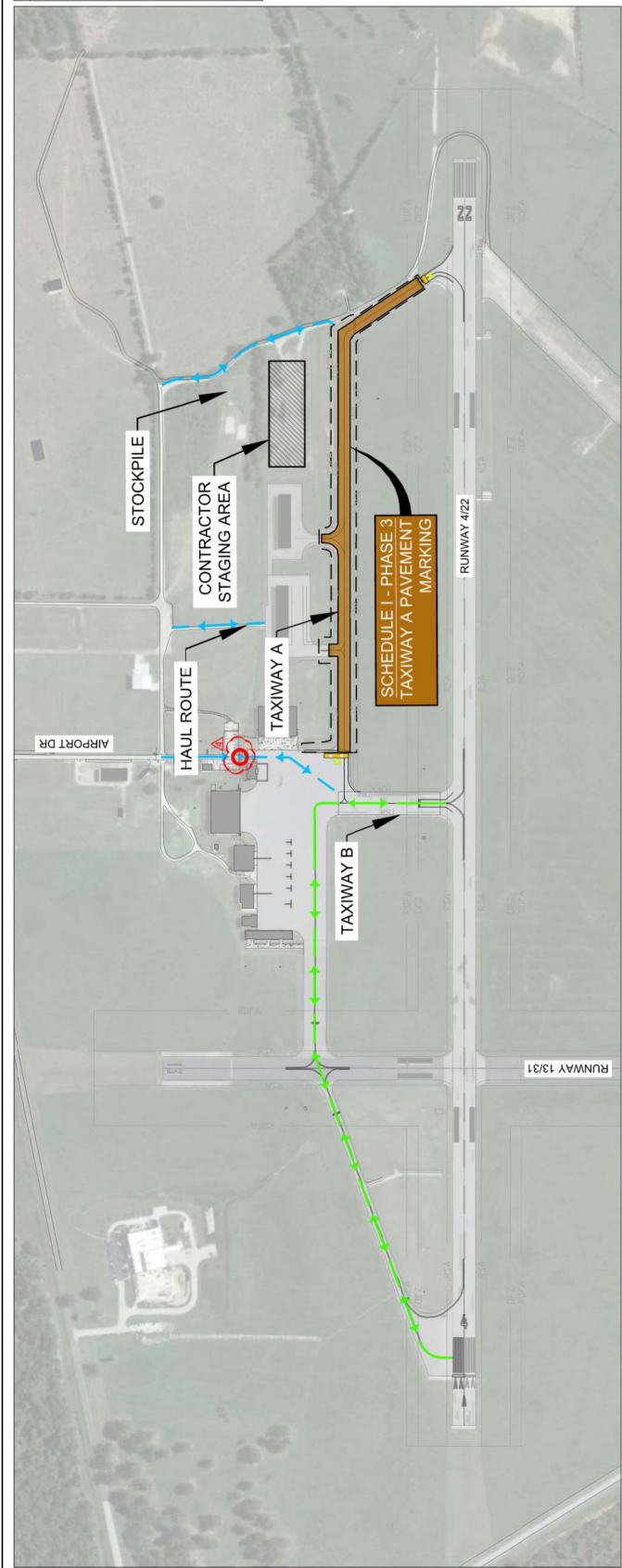
CITY OF ROLLA, MISSOURI

| DESIGN NO. | BY | DATE | ISSUE RECORD |
|-------------|----------|------------|----------------------|
| DR: V.S.B. | A.L.K.K. | 08/05/2025 | ISSUED FOR BID |
| CH: C.L.G. | A.L.K.K. | 08/05/2025 | PHASE 2 PHASING PLAN |
| APP: L.K.K. | | | |

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PHASING LEGEND

| | |
|--|---------------------------------------|
| | AIRCRAFT TAXI ROUTE |
| | CONTRACTOR TAXI ROUTE (2 WAY TRAFFIC) |
| | RSA |
| | TSR |
| | TOFA |
| | CONSTRUCTION WORK AREA |
| | FLASHER BARRICADE |
| | FLAGMAN / GATE GUARD |
| | CONTRACTOR GATE ACCESS |
| | CONTRACTOR GATE ACCESS |



ISSUED FOR BID

THESE DRAWINGS ARE FOR PURPOSES ONLY. THEY WERE PREPARED BY OR UNDER THE SUPERVISION OF:

LAURA K. KOONCE
 NAME
 2022012014
 REG. NO.
 08/05/2025
 DATE
 FOR AND ON BEHALF OF WOOLPERT, INC.

| | |
|--------------------|--|
| SHEET NAME | CONSTRUCTION SAFETY INDIVIDUAL PHASING PLAN - SCHEDULE I PHASE 3 |
| SHEET NO. | G054 |
| WOOLPERT PROJ. NO. | 1001880110 |
| 14 of 54 | |

- NOTES**
- ALL STOCKPILES OR EQUIPMENT ADJACENT TO OBJECT FREE AREAS SHALL BE MARKED AND LIGHTED PER AC 1503370-20.
 - THE CONTRACTOR SHALL PROVIDE A TRAFFIC CONTROL PLAN AND EMERGENCY VEHICLES AT ALL TIMES.
 - CONTRACTOR TO MAINTAIN EMERGENCY VEHICLE ACCESS TO RUNWAY AT ALL TIMES.
 - ALL NECESSARY TAXIWAY LIGHTS AND SIGNS SHALL BE COVERED FOR TEMPORARY TAXIWAY CLOSURES. CONTRACTOR METHOD FOR COVERING TAXIWAY LIGHTS SHALL BE APPROVED BY THE ENGINEER AND SHALL PREVENT DAMAGE TO THE EQUIPMENT AND SHALL BE SECURELY FASTENED TO PREVENT FREING OF COVER FROM WIND.
 - DURING PAVEMENT MARKING APPLICATIONS AND CURE TIMES, CONSTRUCTION BARRICADES SHALL BE INSTALLED AS DIRECTED BY THE ENGINEER TO PREVENT ACCESS TO TAXIWAY AREAS.
 - PERSONNEL ARE ALLOWED IN THE SAFETY AREAS OR OBJECT FREE AREAS FOR ANY ACTIVE TAXIWAY OR SAFETY AREAS FOR AN ACTIVE RUNWAY WHEN THE AIRPORT IS OPEN.
 - ALL SAFETY AREAS AND OBJECT FREE AREAS FOR ACTIVE AIRPORT OPERATIONS SHALL BE MAINTAINED BY THE CONTRACTOR WITHOUT PRIOR APPROVAL FROM AIRPORT OPERATIONS.
 - PRIOR TO OPENING A RUNWAY OR TAXIWAY, ALL OBSTRUCTIONS AND TRENCES IN THE SAFETY AREAS SHALL BE BACKFILLED AND COMPACTED TO P-152 SPECIFICATIONS. GRADING WITHIN THE SAFETY AREAS SHALL BE TO THE ORIGINAL FINISH GRADE.
 - CONTRACTOR TO PROVIDE RUNWAY CLOSURE MARKERS AND BARRICADES TO MAINTAIN THE SAFETY AREAS. CONTRACTOR IS RESPONSIBLE FOR MAINTENANCE OF RUNWAY X'S.
 - BARRICADES SHALL BE INSTALLED PER PHASING PLANS AND AS REQUIRED BY AC 1503370-20 OR AS DIRECTED BY THE ENGINEER.

- ALL STOCKPILES OR EQUIPMENT ADJACENT TO OBJECT FREE AREAS SHALL BE MARKED AND LIGHTED PER AC 1503370-20.
- THE CONTRACTOR SHALL PROVIDE A TRAFFIC CONTROL PLAN AND EMERGENCY VEHICLES AT ALL TIMES.
- CONTRACTOR TO MAINTAIN EMERGENCY VEHICLE ACCESS TO RUNWAY AT ALL TIMES.
- ALL NECESSARY TAXIWAY LIGHTS AND SIGNS SHALL BE COVERED FOR TEMPORARY TAXIWAY CLOSURES. CONTRACTOR METHOD FOR COVERING TAXIWAY LIGHTS SHALL BE APPROVED BY THE ENGINEER AND SHALL PREVENT DAMAGE TO THE EQUIPMENT AND SHALL BE SECURELY FASTENED TO PREVENT FREING OF COVER FROM WIND.
- DURING PAVEMENT MARKING APPLICATIONS AND CURE TIMES, CONSTRUCTION BARRICADES SHALL BE INSTALLED AS DIRECTED BY THE ENGINEER TO PREVENT ACCESS TO TAXIWAY AREAS.
- PERSONNEL ARE ALLOWED IN THE SAFETY AREAS OR OBJECT FREE AREAS FOR ANY ACTIVE TAXIWAY OR SAFETY AREAS FOR AN ACTIVE RUNWAY WHEN THE AIRPORT IS OPEN.
- ALL SAFETY AREAS AND OBJECT FREE AREAS FOR ACTIVE AIRPORT OPERATIONS SHALL BE MAINTAINED BY THE CONTRACTOR WITHOUT PRIOR APPROVAL FROM AIRPORT OPERATIONS.
- PRIOR TO OPENING A RUNWAY OR TAXIWAY, ALL OBSTRUCTIONS AND TRENCES IN THE SAFETY AREAS SHALL BE BACKFILLED AND COMPACTED TO P-152 SPECIFICATIONS. GRADING WITHIN THE SAFETY AREAS SHALL BE TO THE ORIGINAL FINISH GRADE.
- CONTRACTOR TO PROVIDE RUNWAY CLOSURE MARKERS AND BARRICADES TO MAINTAIN THE SAFETY AREAS. CONTRACTOR IS RESPONSIBLE FOR MAINTENANCE OF RUNWAY X'S.
- BARRICADES SHALL BE INSTALLED PER PHASING PLANS AND AS REQUIRED BY AC 1503370-20 OR AS DIRECTED BY THE ENGINEER.

- ALL STOCKPILES OR EQUIPMENT ADJACENT TO OBJECT FREE AREAS SHALL BE MARKED AND LIGHTED PER AC 1503370-20.
- THE CONTRACTOR SHALL PROVIDE A TRAFFIC CONTROL PLAN AND EMERGENCY VEHICLES AT ALL TIMES.
- CONTRACTOR TO MAINTAIN EMERGENCY VEHICLE ACCESS TO RUNWAY AT ALL TIMES.
- ALL NECESSARY TAXIWAY LIGHTS AND SIGNS SHALL BE COVERED FOR TEMPORARY TAXIWAY CLOSURES. CONTRACTOR METHOD FOR COVERING TAXIWAY LIGHTS SHALL BE APPROVED BY THE ENGINEER AND SHALL PREVENT DAMAGE TO THE EQUIPMENT AND SHALL BE SECURELY FASTENED TO PREVENT FREING OF COVER FROM WIND.
- DURING PAVEMENT MARKING APPLICATIONS AND CURE TIMES, CONSTRUCTION BARRICADES SHALL BE INSTALLED AS DIRECTED BY THE ENGINEER TO PREVENT ACCESS TO TAXIWAY AREAS.
- PERSONNEL ARE ALLOWED IN THE SAFETY AREAS OR OBJECT FREE AREAS FOR ANY ACTIVE TAXIWAY OR SAFETY AREAS FOR AN ACTIVE RUNWAY WHEN THE AIRPORT IS OPEN.
- ALL SAFETY AREAS AND OBJECT FREE AREAS FOR ACTIVE AIRPORT OPERATIONS SHALL BE MAINTAINED BY THE CONTRACTOR WITHOUT PRIOR APPROVAL FROM AIRPORT OPERATIONS.
- PRIOR TO OPENING A RUNWAY OR TAXIWAY, ALL OBSTRUCTIONS AND TRENCES IN THE SAFETY AREAS SHALL BE BACKFILLED AND COMPACTED TO P-152 SPECIFICATIONS. GRADING WITHIN THE SAFETY AREAS SHALL BE TO THE ORIGINAL FINISH GRADE.
- CONTRACTOR TO PROVIDE RUNWAY CLOSURE MARKERS AND BARRICADES TO MAINTAIN THE SAFETY AREAS. CONTRACTOR IS RESPONSIBLE FOR MAINTENANCE OF RUNWAY X'S.
- BARRICADES SHALL BE INSTALLED PER PHASING PLANS AND AS REQUIRED BY AC 1503370-20 OR AS DIRECTED BY THE ENGINEER.

- ALL STOCKPILES OR EQUIPMENT ADJACENT TO OBJECT FREE AREAS SHALL BE MARKED AND LIGHTED PER AC 1503370-20.
- THE CONTRACTOR SHALL PROVIDE A TRAFFIC CONTROL PLAN AND EMERGENCY VEHICLES AT ALL TIMES.
- CONTRACTOR TO MAINTAIN EMERGENCY VEHICLE ACCESS TO RUNWAY AT ALL TIMES.
- ALL NECESSARY TAXIWAY LIGHTS AND SIGNS SHALL BE COVERED FOR TEMPORARY TAXIWAY CLOSURES. CONTRACTOR METHOD FOR COVERING TAXIWAY LIGHTS SHALL BE APPROVED BY THE ENGINEER AND SHALL PREVENT DAMAGE TO THE EQUIPMENT AND SHALL BE SECURELY FASTENED TO PREVENT FREING OF COVER FROM WIND.
- DURING PAVEMENT MARKING APPLICATIONS AND CURE TIMES, CONSTRUCTION BARRICADES SHALL BE INSTALLED AS DIRECTED BY THE ENGINEER TO PREVENT ACCESS TO TAXIWAY AREAS.
- PERSONNEL ARE ALLOWED IN THE SAFETY AREAS OR OBJECT FREE AREAS FOR ANY ACTIVE TAXIWAY OR SAFETY AREAS FOR AN ACTIVE RUNWAY WHEN THE AIRPORT IS OPEN.
- ALL SAFETY AREAS AND OBJECT FREE AREAS FOR ACTIVE AIRPORT OPERATIONS SHALL BE MAINTAINED BY THE CONTRACTOR WITHOUT PRIOR APPROVAL FROM AIRPORT OPERATIONS.
- PRIOR TO OPENING A RUNWAY OR TAXIWAY, ALL OBSTRUCTIONS AND TRENCES IN THE SAFETY AREAS SHALL BE BACKFILLED AND COMPACTED TO P-152 SPECIFICATIONS. GRADING WITHIN THE SAFETY AREAS SHALL BE TO THE ORIGINAL FINISH GRADE.
- CONTRACTOR TO PROVIDE RUNWAY CLOSURE MARKERS AND BARRICADES TO MAINTAIN THE SAFETY AREAS. CONTRACTOR IS RESPONSIBLE FOR MAINTENANCE OF RUNWAY X'S.
- BARRICADES SHALL BE INSTALLED PER PHASING PLANS AND AS REQUIRED BY AC 1503370-20 OR AS DIRECTED BY THE ENGINEER.

CONSTRUCTION PHASING NOTES

| DESIGN NO. | BY | DATE | ISSUE RECORD |
|-------------|--------|------------|----------------|
| DR: V.S.B. | L.K.K. | 08/05/2025 | ISSUED FOR BID |
| CH: C.L.G. | L.K.K. | 05/26/2024 | PHASE 3/10/24 |
| APP: L.K.K. | | | |

MAJOR WORK TO BE COMPLETED

PAVEMENT MARKINGS

- PERMANENT PAVEMENT MARKING (AFTER 30 DAY CURE PERIOD) OF SCHEDULE I, PHASE 1 AND 2

SCHEDULE I / PHASE 3

DURATION FOR SCHEDULE I: 1 CALENDAR DAY

- PHASE 3 IS NOT CONCURRENT WITH PHASE 1 OR 2

CONTRACTOR ACCESS TIMES

24 HOUR ACCESS TO APPROVED WORK AREAS

ALL AIRPORT OPERATIONS AREAS SHALL REMAIN OPEN AND UNAFFECTED DURING THIS SCHEDULE WITH THE FOLLOWING EXCEPTIONS:

- TAXIWAY A WILL BE FULLY CLOSED THROUGHOUT THE DURATION OF PHASE 3 TO PROVIDE ACCESS TO THE ADJACENT HANGAR.

ISSUED FOR BID

THESE DRAWINGS ARE FOR PURPOSES ONLY. THEY WERE PREPARED BY OR UNDER THE SUPERVISION OF:

LAURA K. KOONCE
 NAME
 2022012014
 REG. NO.
 08/05/2025
 DATE
 FOR AND ON BEHALF OF WOOLPERT, INC.

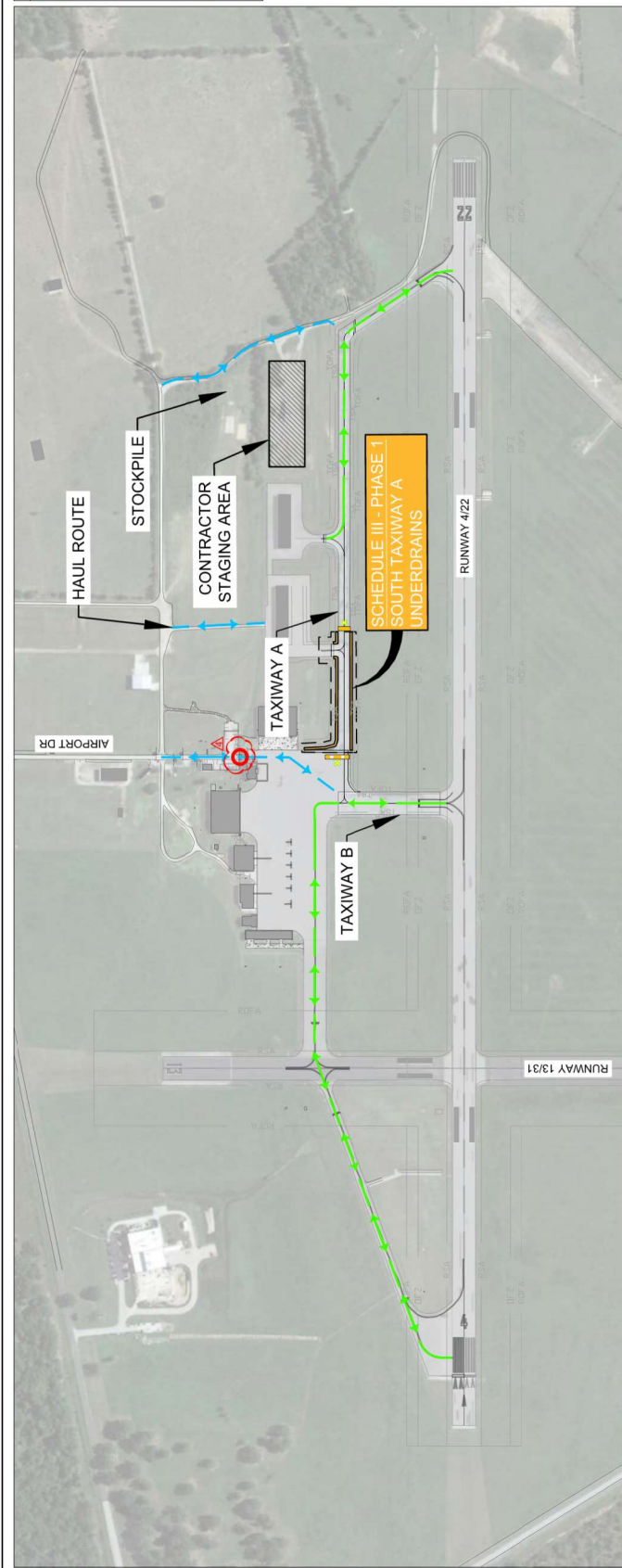
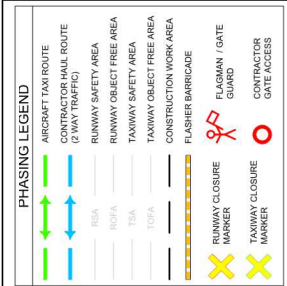
CONSTRUCTION SAFETY INDIVIDUAL PHASING PLAN - SCHEDULE I PHASE 3

G054

14 of 54

WOOLPERT

ROLLA NATIONAL AIRPORT
 CITY OF ROLLA, MISSOURI



ISSUED FOR BID

LAURA K. KOONCE 2022012014 08/05/2025
NAME REG. NO. DATE

FOR AND ON BEHALF OF WOOLPERT, INC.

CONSTRUCTION SAFETY INDIVIDUAL PHASING PLAN - SCHEDULE III PHASE 1

SHEET NO. 17 of 54

WOLPERT PROJ. NO. 20250801

WOOLPERT PROJ. NO. 20250801

| DESIGN NO. | BY | DATE | ISSUE RECORD |
|-------------|--------|------------|------------------|
| DR: V.S.B. | L.K.K. | 08/05/2025 | ISSUED FOR BID |
| CH: C.L.G. | L.K.K. | 08/05/2025 | PHASE III, NO. 4 |
| APP: L.K.K. | | | |

CONSTRUCTION PHASING NOTES

SCHEDULE III / PHASE 1

DURATION

PHASE 1: CONCURRENT WITH SCHEDULE I PHASE 1

FOR SCHEDULE III: PHASE 1 IS NOT CONCURRENT WITH PHASE 2 CONTRACTOR ACCESS TIMES

24 HOUR ACCESS TO APPROVED WORK AREAS

ALL AIRPORT OPERATIONS AREAS SHALL REMAIN OPEN AND UNAFFECTED DURING THIS SCHEDULE WITH THE FOLLOWING EXCEPTIONS:

- TAXIWAY A WILL BE PARTIALLY CLOSED THROUGHOUT THE DURATION OF THIS SCHEDULE AND 2 TO PROVIDE ACCESS TO THE ADJOINING HANGAR

MAJOR WORK TO BE COMPLETED

SITE PREPARATION

1. EROSION CONTROL MEASURES

EARTHWORK

1. UNCLASSIFIED EXCAVATION

2. TRENCHING AND BACKFILLING

UTILITY

1. UNDERDRAINS AND STRUCTURES

SITE RECLAMATION

2. EROSION CONTROL MEASURES

NOTES

10. ALL STOCKPILES OR EQUIPMENT ADJACENT TO OBJECT FREE AREAS SHALL BE MARKED AND LIGHTED PER AC 1503370-50.

11. THE CONTRACTOR SHALL PROVIDE A TRAFFIC CONTROL PLAN AND A WORK PLAN FOR THIS SCHEDULE OF WORK.

12. THE CONTRACTOR SHALL HAVE A SHEEPER ON SITE AT ALL TIMES TO CLEAN DEBRIS FROM HAUL ROUTES, CONSTRUCTION ACCESS POINTS, OR AREAS ADJACENT TO CONSTRUCTION.

13. THE CONTRACTOR SHALL KEEP ALL CONSTRUCTION TRAFFIC LIMITED TO THE APPROVED HAUL ROUTES AS SHOWN ON THE PLANS OR AS INDICATED BY THE CONTRACTOR. HAUL ROUTES OR CONSTRUCTION ACCESS ROUTES TO THE PROJECT SITE, HAUL ROADS WILL BE MAINTAINED AND OPEN TO TRAFFIC AT ALL TIMES UNLESS OTHERWISE INDICATED BY THE CONTRACTOR AT NO EXPENSE TO THE SPONSOR.

14. PRIOR TO ACCEPTANCE AND OPENING OF AN AREA TO AIRCRAFT, ALL FINAL GRADING MUST BE COMPLETED. THIS INCLUDES ROCK REMOVAL, FINE GRADING, AND SEED PLACEMENT.

15. ACCESS TO WORK AREAS WILL BE BY HAUL ROUTES ONLY.

16. UNICOM - MONITOR 123.0 MHz AT ALL TIMES.

17. ALL COMPONENTS OF THE AIRPORT LIGHTING SYSTEM OUTSIDE OF THE PROJECT AREA SHALL BE OPERATIONAL AT THE END OF EACH WORK SHIFT AND FOR EVERY PERIOD OF LOW VISIBILITY. THE CONTRACTOR SHALL MAINTAIN THE LIGHTING SYSTEM AND SHALL BE RESPONSIBLE FOR OTHER EQUIPMENT NECESSARY TO MAINTAIN AN OPERATIONAL SYSTEM DURING CONSTRUCTION. TEMPORARY JUMPERS SHALL BE CONSIDERED NECESSARY TO MAINTAIN AN OPERATIONAL SYSTEM. ALL TEMPORARY JUMPERS SHALL BE INSTALLED IN CONDUIT.

18. PRIOR TO BEGINNING ELECTRICAL DEMOLITION THE CONTRACTOR SHALL PROVIDE A PLAN TO THE ENGINEER DETAILING THE INSTALLATION OF ALL REQUIRED TEMPORARY JUMPERS. THE PLAN SHALL SHOW THE LOCATION AND INSULATION TECHNIQUES OF ALL JUMPERS AND

19. MATERIAL STORAGE WITHIN THE PROJECT LIMITS MUST BE APPROVED BY THE ENGINEER.

20. HAUL ROUTES TO AND FROM THE PROJECT SITE MUST BE APPROVED BY THE ENGINEER.

21. OPERATIONS WITHIN THE AIRPORT OPERATIONS AREA (AOA) MUST BE MEASURED AND THE RESULTS REPORTS TO THE ENGINEER BEFORE WORK BEGINS AND AFTER WORK IS COMPLETE.

22. SEE SAFETY PLAN FOR SPECIFIC SAFETY REQUIREMENTS.

23. ALL MATERIALS, DEBRIS, AND SOILS TO BE MOVED OFF SITE, MILLINGS ARE TO BE STOCKPILED IN THE CONTRACTOR STAGING AREA FOR AIRPORT USE.

24. ALL LEAD LINES TO CLOSED TAXIWAY AREAS MUST BE ORIGINATED OR COVERED PER AC 1503370-20 OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION. REMOVAL IS CONSIDERED INCIDENTAL TO ITEM P-200.

WOOLPERT

ROLLA NATIONAL AIRPORT
CITY OF ROLLA, MISSOURI

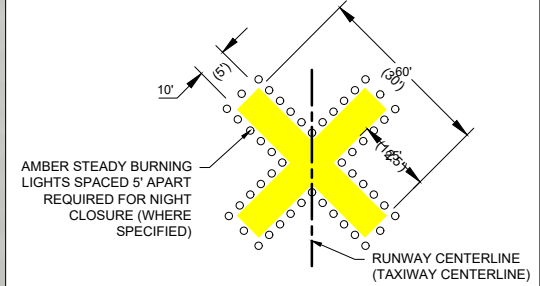
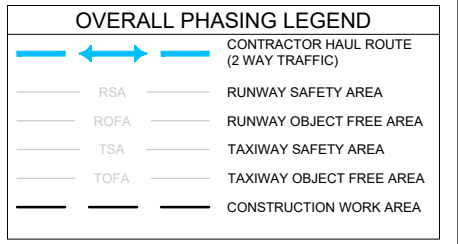
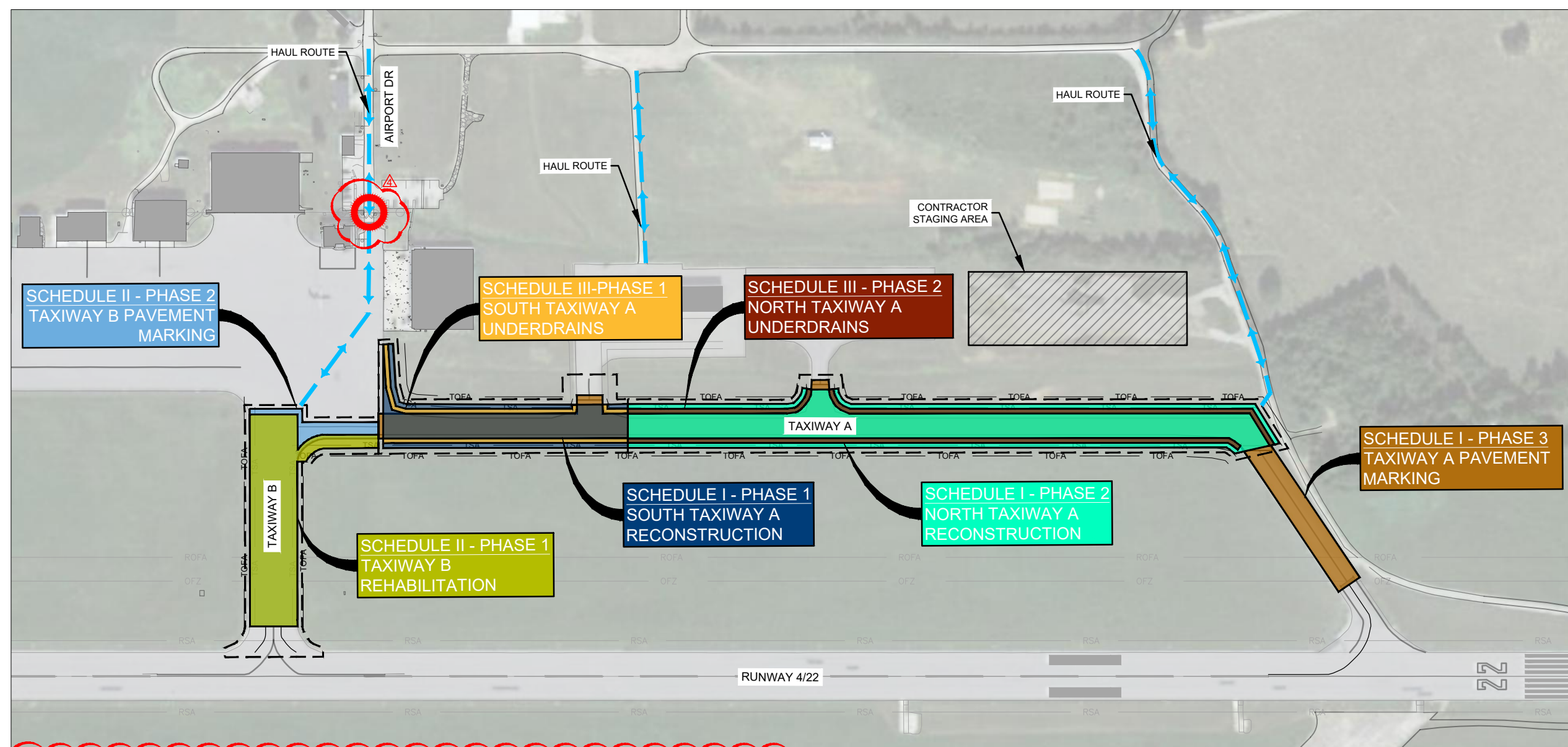
TAXIWAY A RECONSTRUCTION AND TAXIWAY CONNECTOR REHABILITATION

CONSTRUCTION SAFETY INDIVIDUAL PHASING PLAN - SCHEDULE III PHASE 1

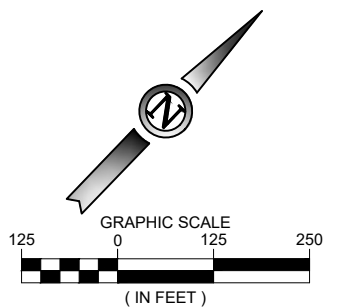
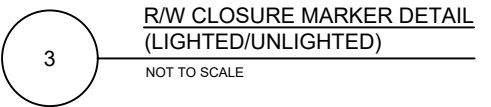
WOOLPERT PROJ. NO. 20250801

WOOLPERT PROJ. NO. 20250801

SHEET NO. 17 of 54



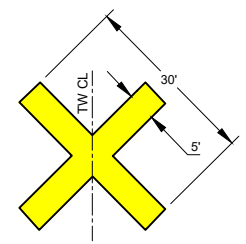
1. TEMPORARY CLOSURE CROSS MARKINGS SHALL BE "AVIATION YELLOW."
2. TEMPORARY CLOSURE CROSS MARKINGS SHALL BE CONSTRUCTED OF PLYWOOD, SNOW FENCE OR APPROVED FABRIC AND SHALL BE SECURED TO PAVEMENT BY SANDBAGS OR OTHER APPROVED METHOD.
3. RUNWAY CLOSURE CROSS MARKINGS SHALL BE LIGHTED DURING DARKNESS AND PERIODS OF REDUCED VISIBILITY.
4. ALL VALUES IN PARENTHESIS REPRESENT DIMENSIONS FOR THE TAXIWAY CLOSURE MARKER.
5. PAYMENT FOR THIS WORK TO BE CONSIDERED INCIDENTAL TO CONSTRUCTION.



| PROJECT SCHEDULE | |
|----------------------------|------------------|
| SCHEDULE / PHASE | DURATION |
| SCHEDULE I & III - PHASE 1 | 32 CALENDAR DAYS |
| SCHEDULE I & III - PHASE 2 | 45 CALENDAR DAYS |
| SCHEDULE I - PHASE 3 | 1 CALENDAR DAY |
| SCHEDULE II - PHASE 1 | 3 CALENDAR DAYS |
| SCHEDULE II - PHASE 2 | 1 CALENDAR DAY |

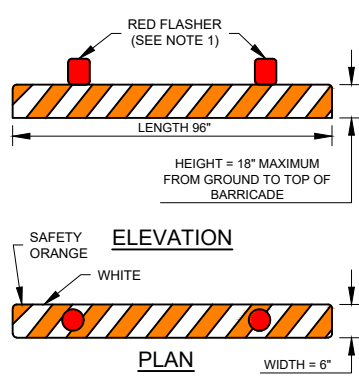
NOTE:
 1. SCHEDULE II PHASE 1 SHALL BE CONCURRENT WITH SCHEDULE I PHASE 1.
 * PAVEMENT MUST HAVE 30 DAY CURE PERIOD BEFORE APPLICATION OF PERMANENT MARKINGS.

| SAFETY & OBJECT FREE AREAS | |
|---------------------------------|--------------------------|
| RUNWAY 4/22 | |
| RUNWAY SAFETY AREA (RSA) | 75' FROM RW CENTERLINE |
| OBJECT FREE ZONE (OFZ) | 200' FROM RW CENTERLINE |
| RUNWAY OBJECT FREE AREA (ROFA) | 250' FROM RW CENTERLINE |
| TAXIWAYS | |
| TAXIWAY SAFETY AREA (TSA) | 39.5' FROM TW CENTERLINE |
| TAXIWAY OBJECT FREE AREA (TOFA) | 62' FROM TW CENTERLINE |



CONSTRUCT CLOSED TAXIWAY "X" WITH ANY OF THE FOLLOWING: DOUBLE-LAYERED PAINTED SNOW FENCE, COLORED PLASTIC, PAINTED SHEETS OF PLYWOOD OR SIMILAR MATERIALS. CONTRACTOR TO ENSURE THAT MATERIALS ARE PROPERLY CONFIGURED AND SECURED TO PREVENT MOVEMENT FROM JET BLAST OR WIND. CLOSED TAXIWAY "X" SHALL BE LESS THAN 3-INCHES THICK.

1 TAXIWAY CLOSURE "X" DETAIL
NOT TO SCALE

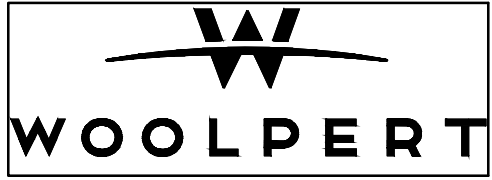


1. FLASHER BARRICADES WILL BE PROVIDED AND MAINTAINED BY THE CONTRACTOR AT ALL TIMES. CONTRACTOR SHALL ALSO PROVIDE SPARE BARRICADES, BATTERIES, AND LIGHT BULBS FOR MAINTENANCE DURING NIGHTTIME HOURS.
 2. LOW-PROFILE BARRICADES TO BE PLACED AT 10' INTERVALS ADJACENT TO CONSTRUCTION, AS DIRECTED BY THE ENGINEER.
 3. BARRICADES ARE TO BE PLACED IN LOCATIONS SHOWN ON THE PLANS AND AS DIRECTED BY THE ENGINEER THROUGHOUT ALL PHASES OF THE PROJECT. THE BARRICADE LOCATIONS PROVIDED ON THIS SHEET SHALL REMAIN THROUGHOUT CONSTRUCTION. ADDITIONAL BARRICADES WILL BE REQUIRED ACROSS PHASE SPECIFIC AREAS OF CLOSED PAVEMENT, AND ARE SHOWN ON PHASING SHEETS.
 4. FLASHER BARRICADES WILL BE REQUIRED ALONG THE EDGE OF ANY VERTICAL DROP OFF GREATER THAN 3". AIRPORT OPERATIONS WILL ISSUE NOTAM TO ADVISE AIRCRAFT OF THIS CONDITION
 5. FLASHER BARRICADES ARE TO BE ADEQUATELY WEIGHTED SO THEY WILL REMAIN IN PLACE DURING TIMES OF HIGH WINDS OR AS APPROVED BY THE ENGINEER.
- 2 FLASHER BARRICADE DETAIL
NOT TO SCALE

ISSUED FOR BID

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LAURA K. KOONCE 2022012014 08/05/2025
 NAME REG. NO. DATE
 FOR AND ON BEHALF OF WOOLPERT, INC.

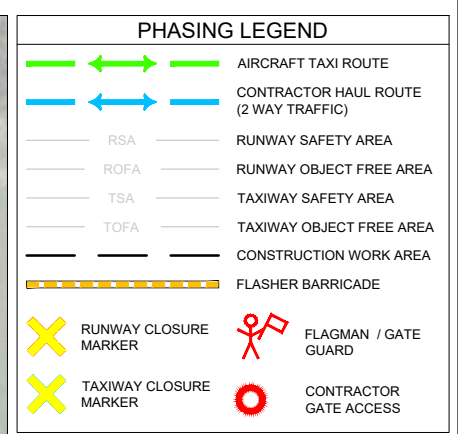
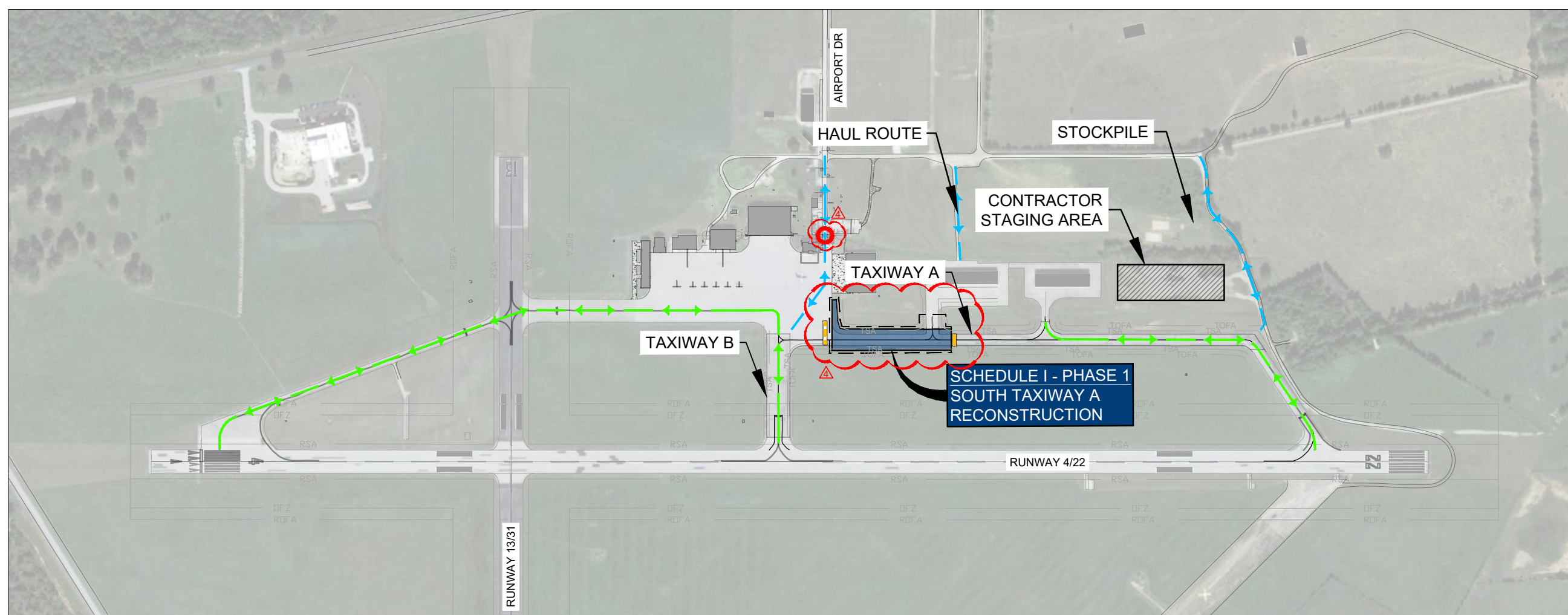


| DES: F.Z.D. | ISSUE RECORD | | | |
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| DR: N.B.B. | NO. | BY | DATE | DESCRIPTION |
| CH: C.L.G. | 1 | L.K.K. | 08/05/2025 | ISSUED FOR BID |
| APP: L.K.K. | | L.K.K. | 09/23/2025 | ADDENDUM NO. 4 |

TAXIWAY A RECONSTRUCTION AND TAXIWAY CONNECTOR REHABILITATION

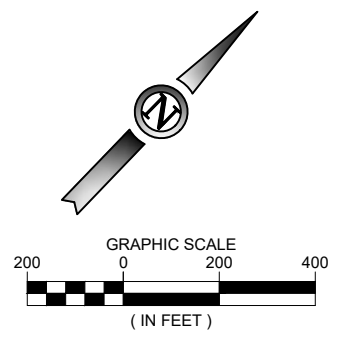
| CONSTRUCTION SAFETY OVERALL PHASING PLAN | | SHEET NAME |
|--|--------------------------------|--------------------|
| MoDOT PROJ. NO. 23-056A-1 | WOOLPERT PROJ. NO. 10016991.00 | G050 |
| | | SHEET NO. 10 of 54 |

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CONSTRUCTION PHASING NOTES

| SCHEDULE I / PHASE 1 | MAJOR WORK TO BE COMPLETED | NOTES |
|--|---|---|
| <p>DURATION</p> <p>PHASE 1: 32 CALENDAR DAYS</p> <p>FOR SCHEDULE I:</p> <ul style="list-style-type: none"> PHASE 1 IS NOT CONCURRENT WITH PHASE 2 OR 3 SHALL BE COMPLETED CONCURRENTLY WITH SCHEDULE II PHASE 1 SHALL BE COMPLETED CONCURRENT WITH SCHEDULE III PHASE 1 <p>CONTRACTOR ACCESS TIMES</p> <p>24 HOUR ACCESS TO APPROVED WORK AREAS</p> <p>ALL AIRPORT OPERATIONS AREAS SHALL REMAIN OPEN AND UNAFFECTED DURING THIS SCHEDULE WITH THE FOLLOWING EXCEPTIONS:</p> <ul style="list-style-type: none"> TAXIWAY A WILL BE PARTIALLY CLOSED THROUGHOUT THE DURATION OF SCHEDULE I PHASE 1 TO PROVIDE ACCESS TO THE ADJOINING HANGAR | <p>SITE PREPARATION</p> <ol style="list-style-type: none"> EROSION CONTROL MEASURES FULL DEPTH PAVEMENT REMOVAL ELECTRICAL REMOVALS <p>EARTHWORK</p> <ol style="list-style-type: none"> UNCLASSIFIED EXCAVATION <p>PAVEMENT SECTION</p> <ol style="list-style-type: none"> CEMENT TREATED SUBGRADE, 18" DEPTH AGGREGATE BASE COURSE, 8" DEPTH ASPHALT PAVEMENT, 4" DEPTH <p>UTILITY</p> <ol style="list-style-type: none"> INSTALL TAXIWAY EDGE LIGHT FIXTURES AND ELECTRICAL CABLES <p>PAVEMENT MARKINGS</p> <ol style="list-style-type: none"> TEMPORARY PAVEMENT MARKING <p>SITE RECLAMATION</p> <ol style="list-style-type: none"> TAXIWAY SAFETY AREA GRADING SEEDING EROSION CONTROL MEASURES | <p>IMPACTS ON OPERATIONS</p> <ol style="list-style-type: none"> THE CONTRACTOR SHALL GIVE RIGHT OF WAY TO ALL AIRCRAFT AND EMERGENCY VEHICLES AT ALL TIMES. CONTRACTOR TO MAINTAIN EMERGENCY VEHICLE ACCESS TO RUNWAY AT ALL TIMES. ALL NECESSARY TAXIWAY LIGHTS AND SIGNS SHALL BE COVERED FOR TEMPORARY TAXIWAY CLOSURES. CONTRACTOR METHOD FOR COVERING OF LIGHTS AND SIGNS SHALL BE APPROVED BY THE ENGINEER AND SHALL PREVENT DAMAGE TO THE EQUIPMENT AND SHALL BE SECURELY FASTENED TO PREVENT FREEING OF COVER FROM WIND. DURING PAVEMENT MARKING APPLICATIONS AND CURE TIMES, CONSTRUCTION BARRICADES SHALL BE INSTALLED AS DIRECTED BY ENGINEER FOR AREAS THAT ARE TEMPORARILY CLOSED TO TRAFFIC. NO WORK, OPEN EXCAVATIONS, EQUIPMENT, STOCKPILES, OR PERSONNEL ARE ALLOWED IN THE SAFETY AREAS OR OBJECT FREE AREAS FOR ANY ACTIVE TAXIWAY OR SAFETY AREAS FOR AN ACTIVE RUNWAY WHEN THE AIRPORT IS OPEN. ALL SAFETY AREAS AND OBJECT FREE AREAS FOR ACTIVE AIRPORT PAVEMENTS ARE OFF LIMITS TO THE CONTRACTOR WITHOUT PRIOR APPROVAL FROM AIRPORT OPERATIONS. PRIOR TO OPENING A RUNWAY OR TAXIWAY, ALL EXCAVATIONS AND TRENCHES IN THE SAFETY AREAS SHALL BE BACKFILLED AND COMPACTED TO P-152 SPECIFICATIONS. GRADING WITHIN THE SAFETY AREA SHALL CONFORM TO AC 150/5300-13B, CHAPTER 3. CONTRACTOR TO PROVIDE RUNWAY CLOSURE MARKERS AND INSTALLATION OF MARKERS. RUNWAY CLOSURE MARKERS SHALL BE INSTALLED ON RUNWAY 4/22 DURING CLOSURES. CONTRACTOR IS RESPONSIBLE FOR MAINTENANCE OF RUNWAY X'S. BARRICADES SHALL BE INSTALLED PER PHASING PLANS AND AS REQUIRED BY AC 150/5370-2G OR AS DIRECTED BY THE ENGINEER. <p>NOTES</p> <ol style="list-style-type: none"> ALL STOCKPILES OR EQUIPMENT ADJACENT TO OBJECT FREE AREAS SHALL BE MARKED AND LIGHTED PER AC 150/5370-2G. THE CONTRACTOR SHALL PROVIDE A TRAFFIC CONTROL PLAN APPROVED BY THE AIRPORT AND LOCAL JURISDICTION, AND A WORK PLAN FOR THIS SCHEDULE OF WORK. THE CONTRACTOR SHALL HAVE A SWEEPER ON SITE AT ALL TIMES TO CLEAN DEBRIS FROM HAUL ROUTES, CONSTRUCTION ACCESS POINTS, OR AREAS ADJACENT TO CONSTRUCTION. THE CONTRACTOR SHALL KEEP ALL CONSTRUCTION TRAFFIC LIMITED TO THE APPROVED HAUL ROUTES AS SHOWN ON THE PLANS OR AS APPROVED BY THE ENGINEER. THE CONTRACTOR IS RESPONSIBLE FOR ANY DAMAGE CAUSED ALONG THE HAUL ROUTES OR CONSTRUCTION ACCESS ROUTES TO THE PROJECT SITE. HAUL ROADS WILL BE MAINTAINED AND RESTORED TO THEIR ORIGINAL CONDITION BY THE CONTRACTOR AT NO EXPENSE TO THE SPONSOR. PRIOR TO ACCEPTANCE AND OPENING OF AN AREA TO AIRCRAFT, ALL FINAL GRADING MUST BE COMPLETED. THIS INCLUDES ROCK REMOVAL, FINE GRADING, AND SEED PLACEMENT. ACCESS TO WORK AREAS WILL BE BY HAUL ROUTES ONLY. UNICOM - MONITOR 123.0 MHZ AT ALL TIMES. ALL COMPONENTS OF THE AIRFIELD LIGHTING SYSTEM OUTSIDE OF THE PROJECT AREA SHALL BE OPERATIONAL AT THE END OF EACH WORK SHIFT AND FOR EVERY PERIOD OF LOW VISIBILITY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING ALL TEMPORARY JUMPERS AND OTHER EQUIPMENT NECESSARY TO MAINTAIN AN OPERATIONAL SYSTEM DURING CONSTRUCTION. TEMPORARY JUMPERS SHALL BE CONSIDERED INCIDENTAL TO THE VARIOUS BID ITEMS. ALL TEMPORARY JUMPERS SHALL BE INSTALLED IN CONDUIT. PRIOR TO BEGINNING ELECTRICAL DEMOLITION THE CONTRACTOR SHALL PROVIDE A PLAN TO THE ENGINEER DETAILING THE INSTALLATION OF ALL REQUIRED TEMPORARY JUMPERS. THE PLAN SHALL SHOW THE LOCATIONS AND INSTALLATION TECHNIQUES OF ALL JUMPERS AND <p>SHALL DEMONSTRATE TO THE ENGINEER THE CONTRACTOR'S PLAN TO MAINTAIN THE INTEGRITY OF THE AIRFIELD CIRCUITS FOR THE DURATION OF CONSTRUCTION. ALL CIRCUITS THAT WILL BE WORKED ON SHOULD BE MEGGERED AND THE RESULTS REPORTS TO THE ENGINEER BEFORE WORK BEGINS AND AFTER WORK IS COMPLETE.</p> <ol style="list-style-type: none"> MATERIAL STORAGE WITHIN THE PROJECT LIMITS MUST BE APPROVED BY THE ENGINEER. HAUL ROUTES TO AND FROM THE PROJECT SITE MUST BE APPROVED BY THE ENGINEER. OPERATIONS WITHIN THE AIRPORT OPERATIONS AREA (A.O.A.) MUST BE APPROVED BY THE ENGINEER. SUFFICIENT NOTICE OF THE OPERATIONS MUST BE GIVEN TO THE ENGINEER. 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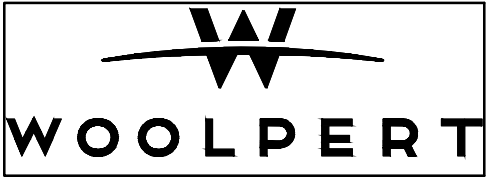


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|-----------------|------------|------------|
| LAURA K. KOONCE | 2022012014 | 08/05/2025 |
| NAME | REG. NO. | DATE |

FOR AND ON BEHALF OF WOOLPERT, INC.

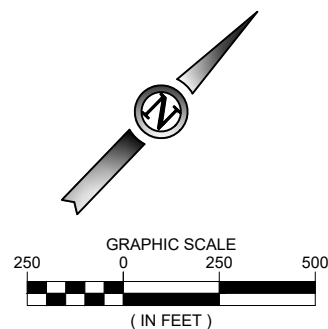
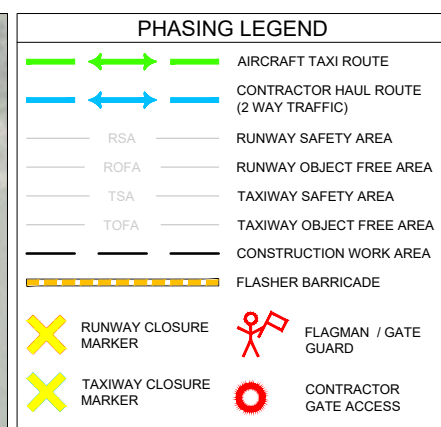
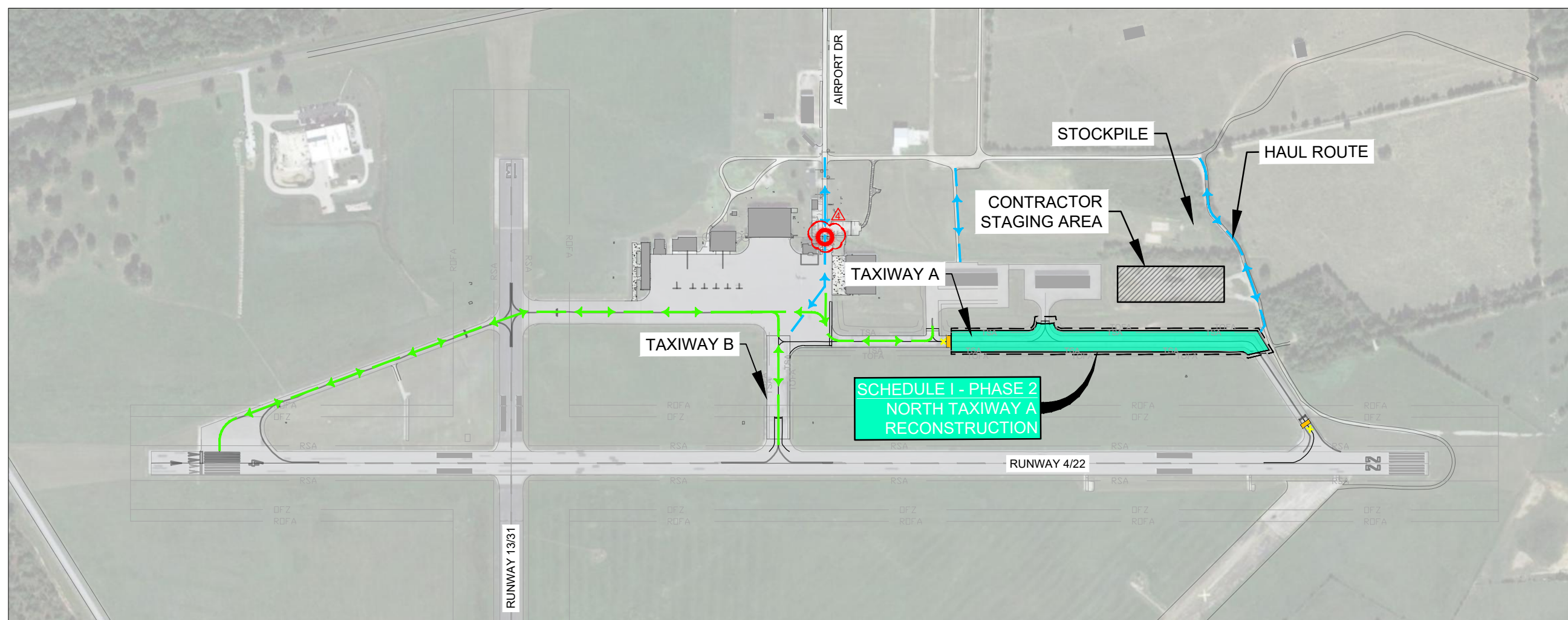


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| | NO. | BY | DATE | DESCRIPTION |
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| CH: C.L.G. | 2 | L.K.K. | 09/23/2025 | ADDENDUM NO. 4 |
| APP: L.K.K. | | | | |

TAXIWAY A RECONSTRUCTION AND TAXIWAY CONNECTOR REHABILITATION

| | | |
|--|-----------------------------------|-----------------------|
| CONSTRUCTION SAFETY INDIVIDUAL PHASING PLAN - SCHEDULE I PHASE 1 | | SHEET NAME G052 |
| MoDOT PROJ. NO. 23-056A-1 | WOOLPERT PROJ. NO. 10016991.00 | SHEET NO. 12 of 54 |

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CONSTRUCTION PHASING NOTES

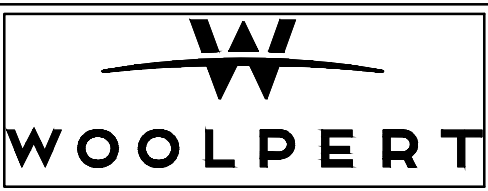
| SCHEDULE I / PHASE 2 | MAJOR WORK TO BE COMPLETED | NOTES |
|---|---|--|
| <p>DURATION</p> <p>PHASE 2: 45 CALENDAR DAYS</p> <p>FOR SCHEDULE I:</p> <ul style="list-style-type: none"> PHASE 2 IS NOT CONCURRENT WITH PHASE 1 OR 3, BUT SHOULD BE CONCURRENT WITH SCHEDULE III PHASE 2 <p>CONTRACTOR ACCESS TIMES</p> <p>24 HOUR ACCESS TO APPROVED WORK AREAS</p> <p>ALL AIRPORT OPERATIONS AREAS SHALL REMAIN OPEN AND UNAFFECTED DURING THIS SCHEDULE WITH THE FOLLOWING EXCEPTIONS:</p> <ul style="list-style-type: none"> TAXIWAY A WILL BE PARTIALLY CLOSED THROUGHOUT THE DURATION OF SCHEDULE I PHASE 2 TO PROVIDE ACCESS TO THE ADJOINING HANGAR | <p>SITE PREPARATION</p> <ol style="list-style-type: none"> EROSION CONTROL MEASURES FULL DEPTH PAVEMENT REMOVAL ELECTRICAL REMOVALS <p>EARTHWORK</p> <ol style="list-style-type: none"> UNCLASSIFIED EXCAVATION <p>PAVEMENT SECTION</p> <ol style="list-style-type: none"> CEMENT TREATED SUBGRADE, 18" DEPTH AGGREGATE BASE COURSE, 8" DEPTH ASPHALT PAVEMENT, 4" DEPTH <p>UTILITY</p> <ol style="list-style-type: none"> INSTALL TAXIWAY EDGE LIGHT FIXTURES AND ELECTRICAL CABLES <p>PAVEMENT MARKINGS</p> <ol style="list-style-type: none"> TEMPORARY PAVEMENT MARKING <p>SITE RECLAMATION</p> <ol style="list-style-type: none"> TAXIWAY SAFETY AREA GRADING SEEDING EROSION CONTROL MEASURES | <p>IMPACTS ON OPERATIONS</p> <ol style="list-style-type: none"> THE CONTRACTOR SHALL GIVE RIGHT OF WAY TO ALL AIRCRAFT AND EMERGENCY VEHICLES AT ALL TIMES. 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| LAURA K. KOONCE | 2022012014 | 08/05/2025 |
| NAME | REG. NO. | DATE |

FOR AND ON BEHALF OF WOOLPERT, INC.

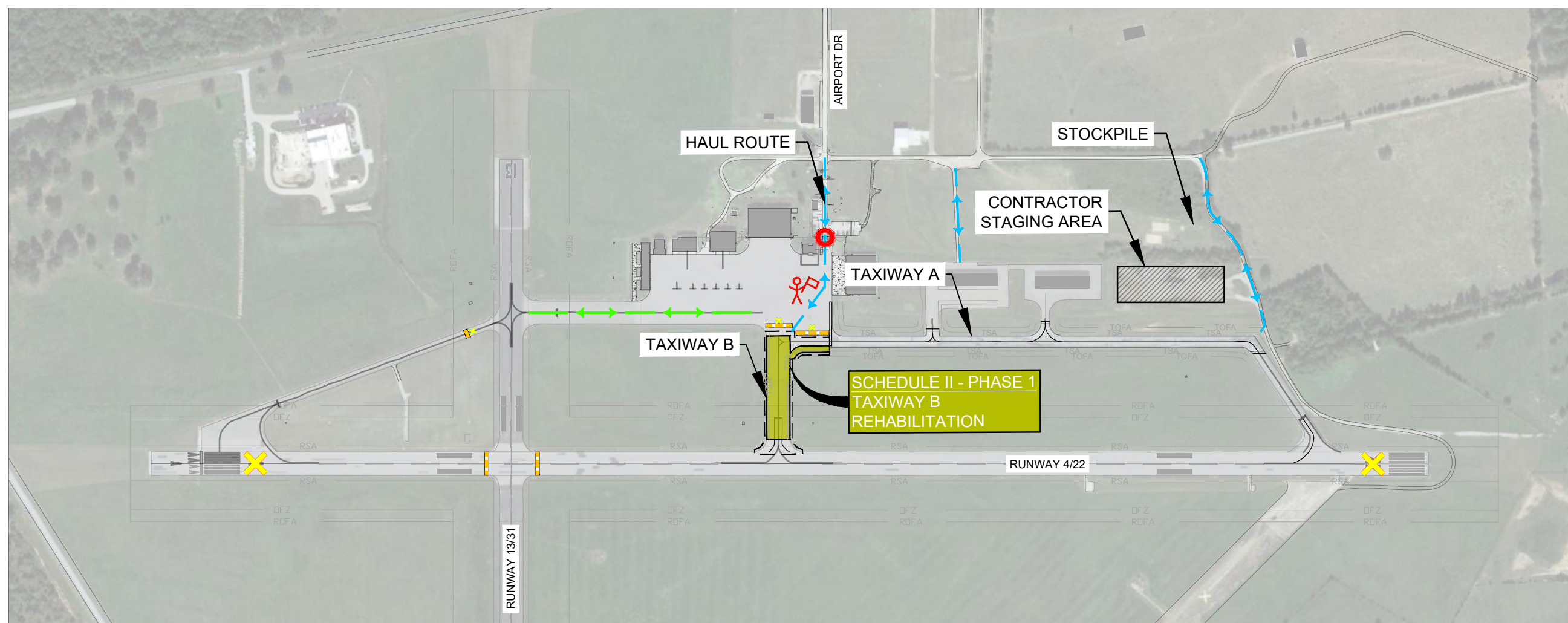


| DR: V.S.B. | ISSUE RECORD | | | |
|-------------|--------------|------------|----------------|----------------|
| | NO. | BY | DATE | DESCRIPTION |
| 1 | L.K.K. | 08/05/2025 | ISSUED FOR BID | |
| CH: C.L.G. | 1 | L.K.K. | 09/23/2025 | ADDENDUM NO. 4 |
| APP: L.K.K. | | | | |

TAXIWAY A RECONSTRUCTION AND TAXIWAY CONNECTOR REHABILITATION

| | | |
|---|-----------------------------------|-----------------------|
| CONSTRUCTION SAFETY INDIVIDUAL PHASING PLAN - SCHEDULE I PHASE 2 | | SHEET NAME G053 |
| | | SHEET NO. 13 of 54 |
| MoDOT PROJ. NO. 23-056A-1 | WOOLPERT PROJ. NO. 10016991.00 | |

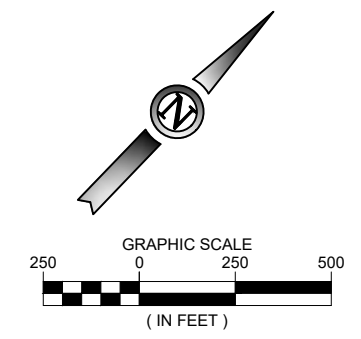
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| PHASING LEGEND | |
|----------------|---------------------------------------|
| | AIRCRAFT TAXI ROUTE |
| | CONTRACTOR HAUL ROUTE (2 WAY TRAFFIC) |
| | RSA RUNWAY SAFETY AREA |
| | ROFA RUNWAY OBJECT FREE AREA |
| | TSA TAXIWAY SAFETY AREA |
| | TOFA TAXIWAY OBJECT FREE AREA |
| | CONSTRUCTION WORK AREA |
| | FLASHER BARRICADE |
| | RUNWAY CLOSURE MARKER |
| | TAXIWAY CLOSURE MARKER |
| | FLAGMAN / GATE GUARD |
| | CONTRACTOR GATE ACCESS |

CONSTRUCTION PHASING NOTES

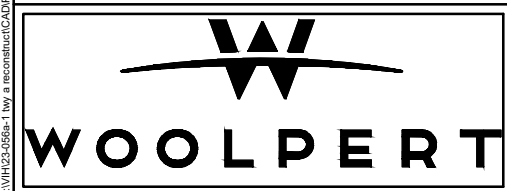
| SCHEDULE II / PHASE 1 | MAJOR WORK TO BE COMPLETED | NOTES |
|--|---|---|
| <p>DURATION</p> <p>PHASE 1: 3 CALENDAR DAYS</p> <p>FOR SCHEDULE II:</p> <ul style="list-style-type: none"> PHASE 1 IS NOT CONCURRENT WITH PHASE 2 SHALL BE CONCURRENT WITH SCHEDULE I, PHASE 1 <p>CONTRACTOR ACCESS TIMES</p> <p>24 HOUR ACCESS TO APPROVED WORK AREAS</p> <p>ALL AIRPORT OPERATIONS AREAS SHALL REMAIN OPEN AND UNAFFECTED DURING THIS SCHEDULE WITH THE FOLLOWING EXCEPTIONS:</p> <ul style="list-style-type: none"> THE SOUTHEAST SECTION OF THE GA APRON WILL BE PARTIALLY CLOSED THROUGHOUT THE DURATION OF SCHEDULE II THE EAST SIDE OF THE GA APRON WILL BE TEMPORARILY UNUSABLE WHEN A FLAGMAN IS PRESENT DURING HAULING ACTIVITIES RUNWAY 4/22 SHALL BE CLOSED FOR THE DURATION OF SCHEDULE II | <p>SITE PREPARATION</p> <ol style="list-style-type: none"> EROSION CONTROL MEASURES PARTIAL DEPTH PAVEMENT REMOVAL <p>PAVEMENT SECTION</p> <ol style="list-style-type: none"> ASPHALT PAVEMENT, 2" DEPTH <p>PAVEMENT MARKINGS</p> <ol style="list-style-type: none"> PAVEMENT MARKING OBLITERATION TEMPORARY PAVEMENT MARKING | <p>IMPACTS ON OPERATIONS</p> <ol style="list-style-type: none"> THE CONTRACTOR SHALL GIVE RIGHT OF WAY TO ALL AIRCRAFT AND EMERGENCY VEHICLES AT ALL TIMES. CONTRACTOR TO MAINTAIN EMERGENCY VEHICLE ACCESS TO RUNWAY AT ALL TIMES. ALL NECESSARY TAXIWAY LIGHTS AND SIGNS SHALL BE COVERED FOR TEMPORARY TAXIWAY CLOSURES. CONTRACTOR METHOD FOR COVERING OF LIGHTS AND SIGNS SHALL BE APPROVED BY THE ENGINEER AND SHALL PREVENT DAMAGE TO THE EQUIPMENT AND SHALL BE SECURELY FASTENED TO PREVENT FREEING OF COVER FROM WIND. DURING PAVEMENT MARKING APPLICATIONS AND CURE TIMES, CONSTRUCTION BARRICADES SHALL BE INSTALLED AS DIRECTED BY ENGINEER FOR AREAS THAT ARE TEMPORARILY CLOSED TO TRAFFIC. NO WORK, OPEN EXCAVATIONS, EQUIPMENT, STOCKPILES, OR PERSONNEL ARE ALLOWED IN THE SAFETY AREAS OR OBJECT FREE AREAS FOR ANY ACTIVE TAXIWAY OR SAFETY AREAS FOR AN ACTIVE RUNWAY WHEN THE AIRPORT IS OPEN. ALL SAFETY AREAS AND OBJECT FREE AREAS FOR ACTIVE AIRPORT PAVEMENTS ARE OFF LIMITS TO THE CONTRACTOR WITHOUT PRIOR APPROVAL FROM AIRPORT OPERATIONS. PRIOR TO OPENING A RUNWAY OR TAXIWAY, ALL EXCAVATIONS AND TRENCHES IN THE SAFETY AREAS SHALL BE BACKFILLED AND COMPACTED TO P-152 SPECIFICATIONS. GRADING WITHIN THE SAFETY AREA SHALL CONFORM TO AC 150/5300-13B, CHAPTER 3 CONTRACTOR TO PROVIDE RUNWAY CLOSURE MARKERS AND INSTALLATION OF MARKERS. RUNWAY CLOSURE MARKERS SHALL BE INSTALLED ON RUNWAY 4/22 DURING CLOSURES. CONTRACTOR IS RESPONSIBLE FOR MAINTENANCE OF RUNWAY X'S. BARRICADES SHALL BE INSTALLED PER PHASING PLANS AND AS REQUIRED BY AC 150/5370-2G OR AS DIRECTED BY THE ENGINEER. <p>NOTES</p> <ol style="list-style-type: none"> ALL STOCKPILES OR EQUIPMENT ADJACENT TO OBJECT FREE AREAS SHALL BE MARKED AND LIGHTED PER AC 150/5370-2G. THE CONTRACTOR SHALL PROVIDE A TRAFFIC CONTROL PLAN APPROVED BY THE AIRPORT AND LOCAL JURISDICTION, AND A WORK PLAN FOR THIS SCHEDULE OF WORK. THE CONTRACTOR SHALL HAVE A SWEEPER ON SITE AT ALL TIMES TO CLEAN DEBRIS FROM HAUL ROUTES, CONSTRUCTION ACCESS POINTS, OR AREAS ADJACENT TO CONSTRUCTION. THE CONTRACTOR SHALL KEEP ALL CONSTRUCTION TRAFFIC LIMITED TO THE APPROVED HAUL ROUTES AS SHOWN ON THE PLANS OR AS APPROVED BY THE ENGINEER. THE CONTRACTOR IS RESPONSIBLE FOR ANY DAMAGE CAUSED ALONG THE HAUL ROUTES OR CONSTRUCTION ACCESS ROUTES TO THE PROJECT SITE. HAUL ROADS WILL BE MAINTAINED AND RESTORED TO THEIR ORIGINAL CONDITION BY THE CONTRACTOR AT NO EXPENSE TO THE SPONSOR. PRIOR TO ACCEPTANCE AND OPENING OF AN AREA TO AIRCRAFT, ALL FINAL GRADING MUST BE COMPLETED. THIS INCLUDES ROCK REMOVAL, FINE GRADING, AND SEED PLACEMENT. ACCESS TO WORK AREAS WILL BE BY HAUL ROUTES ONLY. UNICOM - MONITOR 123.0 MHZ AT ALL TIMES. ALL COMPONENTS OF THE AIRFIELD LIGHTING SYSTEM OUTSIDE OF THE PROJECT AREA SHALL BE OPERATIONAL AT THE END OF EACH WORK SHIFT AND FOR EVERY PERIOD OF LOW VISIBILITY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING ALL TEMPORARY JUMPERS AND OTHER EQUIPMENT NECESSARY TO MAINTAIN AN OPERATIONAL SYSTEM DURING CONSTRUCTION. TEMPORARY JUMPERS SHALL BE CONSIDERED INCIDENTAL TO THE VARIOUS BID ITEMS. ALL TEMPORARY JUMPERS SHALL BE INSTALLED IN CONDUIT. PRIOR TO BEGINNING ELECTRICAL DEMOLITION THE CONTRACTOR SHALL PROVIDE A PLAN TO THE ENGINEER DETAILING THE INSTALLATION OF ALL REQUIRED TEMPORARY JUMPERS. THE PLAN SHALL SHOW THE LOCATIONS AND INSTALLATION TECHNIQUES OF ALL JUMPERS AND SHALL DEMONSTRATE TO THE ENGINEER THE CONTRACTOR'S PLAN TO MAINTAIN THE INTEGRITY OF THE AIRFIELD CIRCUITS FOR THE DURATION OF CONSTRUCTION. ALL CIRCUITS THAT WILL BE WORKED ON SHOULD BE MEGGERED AND THE RESULTS REPORTS TO THE ENGINEER BEFORE WORK BEGINS AND AFTER WORK IS COMPLETE. MATERIAL STORAGE WITHIN THE PROJECT LIMITS MUST BE APPROVED BY THE ENGINEER. HAUL ROUTES TO AND FROM THE PROJECT SITE MUST BE APPROVED BY THE ENGINEER. OPERATIONS WITHIN THE AIRPORT OPERATIONS AREA (A.O.A.) MUST BE APPROVED BY THE ENGINEER. SUFFICIENT NOTICE OF THE OPERATIONS MUST BE GIVEN TO THE ENGINEER. SEE SAFETY PLAN FOR SPECIFIC SAFETY REQUIREMENTS. ALL MATERIAL BESIDES MILLINGS ARE TO BE HAULED OFF SITE. MILLINGS ARE TO BE STOCKPILED IN THE CONTRACTOR STAGING AREA FOR AIRPORT USE. ALL LEAD-IN LINES TO CLOSED TAXIWAY AREAS MUST BE OBLITERATED OR COVERED PER AC 150/5370-2G OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION. REMOVAL IS CONSIDERED INCIDENTAL TO ITEM P-620. |



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| NAME | REG. NO. | DATE |
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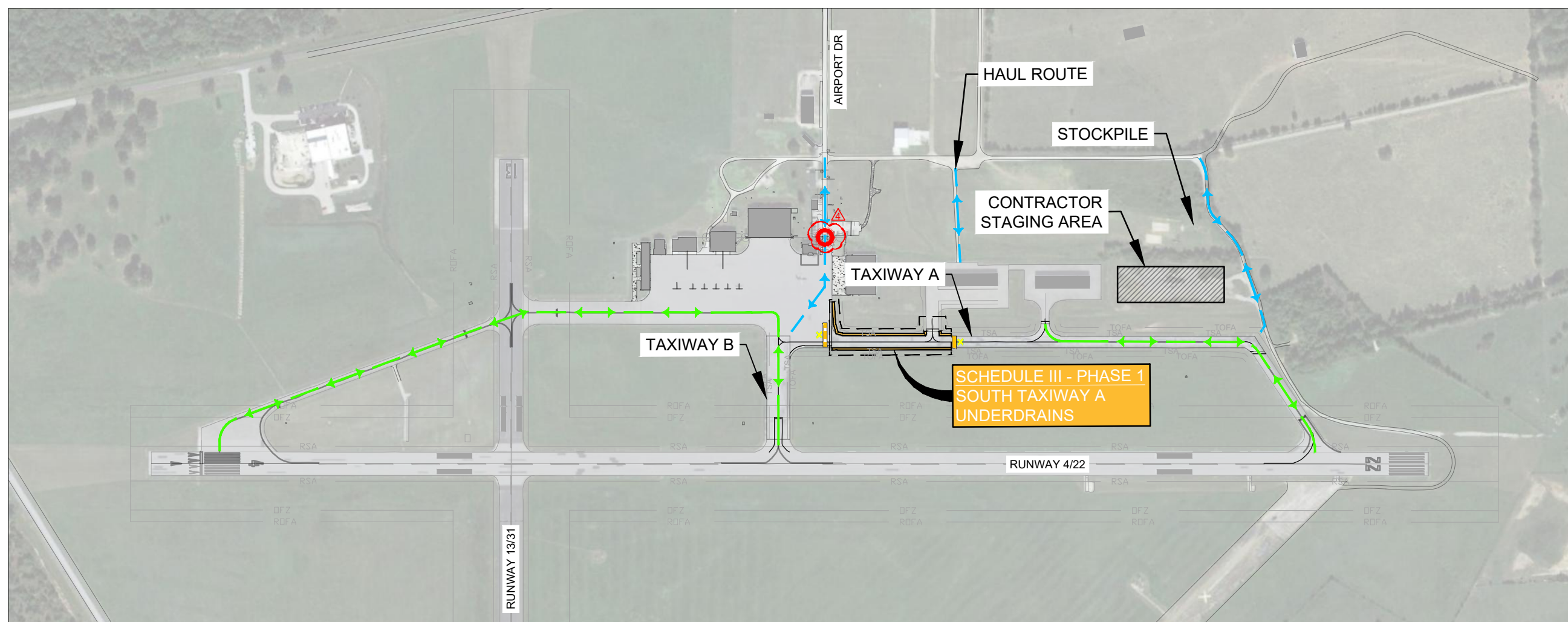
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| | NO. | BY | DATE | DESCRIPTION |
| DR: V.S.B. | 1 | L.K.K. | 08/05/2025 | ISSUED FOR BID |
| CH: C.L.G. | △ | L.K.K. | 09/23/2025 | ADDENDUM NO. 4 |
| APP: L.K.K. | | | | |

TAXIWAY A RECONSTRUCTION AND TAXIWAY CONNECTOR REHABILITATION

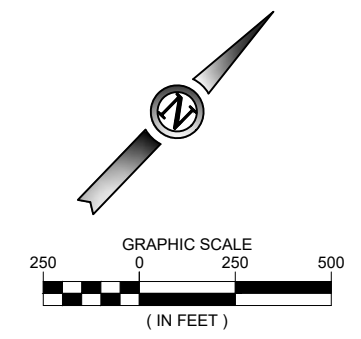
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| CONSTRUCTION SAFETY INDIVIDUAL PHASING PLAN - SCHEDULE II PHASE 1 | |
| MoDOT PROJ. NO. 23-056A-1 | WOOLPERT PROJ. NO. 10016991.00 |

SHEET NAME G055
SHEET NO. 15 of 54

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| PHASING LEGEND | |
|----------------|---------------------------------------|
| | AIRCRAFT TAXI ROUTE |
| | CONTRACTOR HAUL ROUTE (2 WAY TRAFFIC) |
| | RSA RUNWAY SAFETY AREA |
| | ROFA RUNWAY OBJECT FREE AREA |
| | TSA TAXIWAY SAFETY AREA |
| | TOFA TAXIWAY OBJECT FREE AREA |
| | CONSTRUCTION WORK AREA |
| | FLASHER BARRICADE |
| | RUNWAY CLOSURE MARKER |
| | TAXIWAY CLOSURE MARKER |
| | FLAGMAN / GATE GUARD |
| | CONTRACTOR GATE ACCESS |



CONSTRUCTION PHASING NOTES

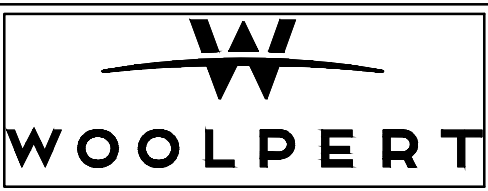
| SCHEDULE III / PHASE 1 | MAJOR WORK TO BE COMPLETED | IMPACTS ON OPERATIONS | NOTES |
|---|--|--|--|
| <p>DURATION</p> <p>PHASE 1: CONCURRENT WITH SCHEDULE I PHASE 1</p> <p>FOR SCHEDULE III:</p> <ul style="list-style-type: none"> PHASE 1 IS NOT CONCURRENT WITH PHASE 2 <p>CONTRACTOR ACCESS TIMES</p> <p>24 HOUR ACCESS TO APPROVED WORK AREAS</p> <p>ALL AIRPORT OPERATIONS AREAS SHALL REMAIN OPEN AND UNAFFECTED DURING THIS SCHEDULE WITH THE FOLLOWING EXCEPTIONS:</p> <ul style="list-style-type: none"> TAXIWAY A WILL BE PARTIALLY CLOSED THROUGHOUT THE DURATIONS OF SCHEDULE I, PHASES 1 AND 2 TO PROVIDE ACCESS TO THE ADJOINING HANGAR | <p>SITE PREPARATION</p> <p>1. EROSION CONTROL MEASURES</p> <p>EARTHWORK</p> <p>1. UNCLASSIFIED EXCAVATION</p> <p>2. TRENCHING AND BACKFILLING</p> <p>UTILITY</p> <p>1. UNDERDRAINS AND STRUCTURES</p> <p>SITE RECLAMATION</p> <p>1. SEEDING</p> <p>2. EROSION CONTROL MEASURES</p> | <p>1. THE CONTRACTOR SHALL GIVE RIGHT OF WAY TO ALL AIRCRAFT AND EMERGENCY VEHICLES AT ALL TIMES.</p> <p>2. CONTRACTOR TO MAINTAIN EMERGENCY VEHICLE ACCESS TO RUNWAY AT ALL TIMES.</p> <p>3. ALL NECESSARY TAXIWAY LIGHTS AND SIGNS SHALL BE COVERED FOR TEMPORARY TAXIWAY CLOSURES. CONTRACTOR METHOD FOR COVERING OF LIGHTS AND SIGNS SHALL BE APPROVED BY THE ENGINEER AND SHALL PREVENT DAMAGE TO THE EQUIPMENT AND SHALL BE SECURELY FASTENED TO PREVENT FREEING OF COVER FROM WIND.</p> <p>4. DURING PAVEMENT MARKING APPLICATIONS AND CURE TIMES, CONSTRUCTION BARRICADES SHALL BE INSTALLED AS DIRECTED BY ENGINEER FOR AREAS THAT ARE TEMPORARILY CLOSED TO TRAFFIC.</p> <p>5. NO WORK, OPEN EXCAVATIONS, EQUIPMENT, STOCKPILES, OR PERSONNEL ARE ALLOWED IN THE SAFETY AREAS OR OBJECT FREE AREAS FOR ANY ACTIVE TAXIWAY OR SAFETY AREAS FOR AN ACTIVE RUNWAY WHEN THE AIRPORT IS OPEN.</p> <p>6. ALL SAFETY AREAS AND OBJECT FREE AREAS FOR ACTIVE AIRPORT PAVEMENTS ARE OFF LIMITS TO THE CONTRACTOR WITHOUT PRIOR APPROVAL FROM AIRPORT OPERATIONS.</p> <p>7. PRIOR TO OPENING A RUNWAY OR TAXIWAY, ALL EXCAVATIONS AND TRENCHES IN THE SAFETY AREAS SHALL BE BACKFILLED AND COMPACTED TO P-152 SPECIFICATIONS. GRADING WITHIN THE SAFETY AREA SHALL CONFORM TO AC 150/5300-13B, CHAPTER 3.</p> <p>8. CONTRACTOR TO PROVIDE RUNWAY CLOSURE MARKERS AND INSTALLATION OF MARKERS. RUNWAY CLOSURE MARKERS SHALL BE INSTALLED ON RUNWAY 4/22 DURING CLOSURES. CONTRACTOR IS RESPONSIBLE FOR MAINTENANCE OF RUNWAY X'S.</p> <p>9. BARRICADES SHALL BE INSTALLED PER PHASING PLANS AND AS REQUIRED BY AC 150/5370-2G OR AS DIRECTED BY THE ENGINEER.</p> | <p>10. ALL STOCKPILES OR EQUIPMENT ADJACENT TO OBJECT FREE AREAS SHALL BE MARKED AND LIGHTED PER AC 150/5370-2G.</p> <p>11. THE CONTRACTOR SHALL PROVIDE A TRAFFIC CONTROL PLAN APPROVED BY THE AIRPORT AND LOCAL JURISDICTION, AND A WORK PLAN FOR THIS SCHEDULE OF WORK.</p> <p>12. 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ALL COMPONENTS OF THE AIRFIELD LIGHTING SYSTEM OUTSIDE OF THE PROJECT AREA SHALL BE OPERATIONAL AT THE END OF EACH WORK SHIFT AND FOR EVERY PERIOD OF LOW VISIBILITY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING ALL TEMPORARY JUMPERS AND OTHER EQUIPMENT NECESSARY TO MAINTAIN AN OPERATIONAL SYSTEM DURING CONSTRUCTION. TEMPORARY JUMPERS SHALL BE CONSIDERED INCIDENTAL TO THE VARIOUS BID ITEMS. ALL TEMPORARY JUMPERS SHALL BE INSTALLED IN CONDUIT.</p> <p>18. PRIOR TO BEGINNING ELECTRICAL DEMOLITION THE CONTRACTOR SHALL PROVIDE A PLAN TO THE ENGINEER DETAILING THE INSTALLATION OF ALL REQUIRED TEMPORARY JUMPERS. THE PLAN SHALL SHOW THE LOCATIONS AND INSTALLATION TECHNIQUES OF ALL JUMPERS AND</p> <p>19. MATERIAL STORAGE WITHIN THE PROJECT LIMITS MUST BE APPROVED BY THE ENGINEER.</p> <p>20. HAUL ROUTES TO AND FROM THE PROJECT SITE MUST BE APPROVED BY THE ENGINEER.</p> <p>21. OPERATIONS WITHIN THE AIRPORT OPERATIONS AREA (A.O.A.) MUST BE APPROVED BY THE ENGINEER. SUFFICIENT NOTICE OF THE OPERATIONS MUST BE GIVEN TO THE ENGINEER.</p> <p>22. SEE SAFETY PLAN FOR SPECIFIC SAFETY REQUIREMENTS.</p> <p>23. ALL MATERIAL BESIDES MILLINGS ARE TO BE HAULED OFF SITE. MILLINGS ARE TO BE STOCKPILED IN THE CONTRACTOR STAGING AREA FOR AIRPORT USE.</p> <p>24. ALL LEAD-IN LINES TO CLOSED TAXIWAY AREAS MUST BE OBLITERATED OR COVERED PER AC 150/5370-2G OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION. REMOVAL IS CONSIDERED INCIDENTAL TO ITEM P-620.</p> <p>SHALL DEMONSTRATE TO THE ENGINEER THE CONTRACTOR'S PLAN TO MAINTAIN THE INTEGRITY OF THE AIRFIELD CIRCUITS FOR THE DURATION OF CONSTRUCTION. ALL CIRCUITS THAT WILL BE WORKED ON SHOULD BE MEGGERED AND THE RESULTS REPORTS TO THE ENGINEER BEFORE WORK BEGINS AND AFTER WORK IS COMPLETE.</p> |

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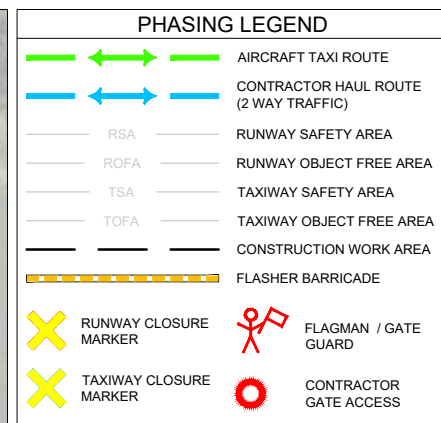
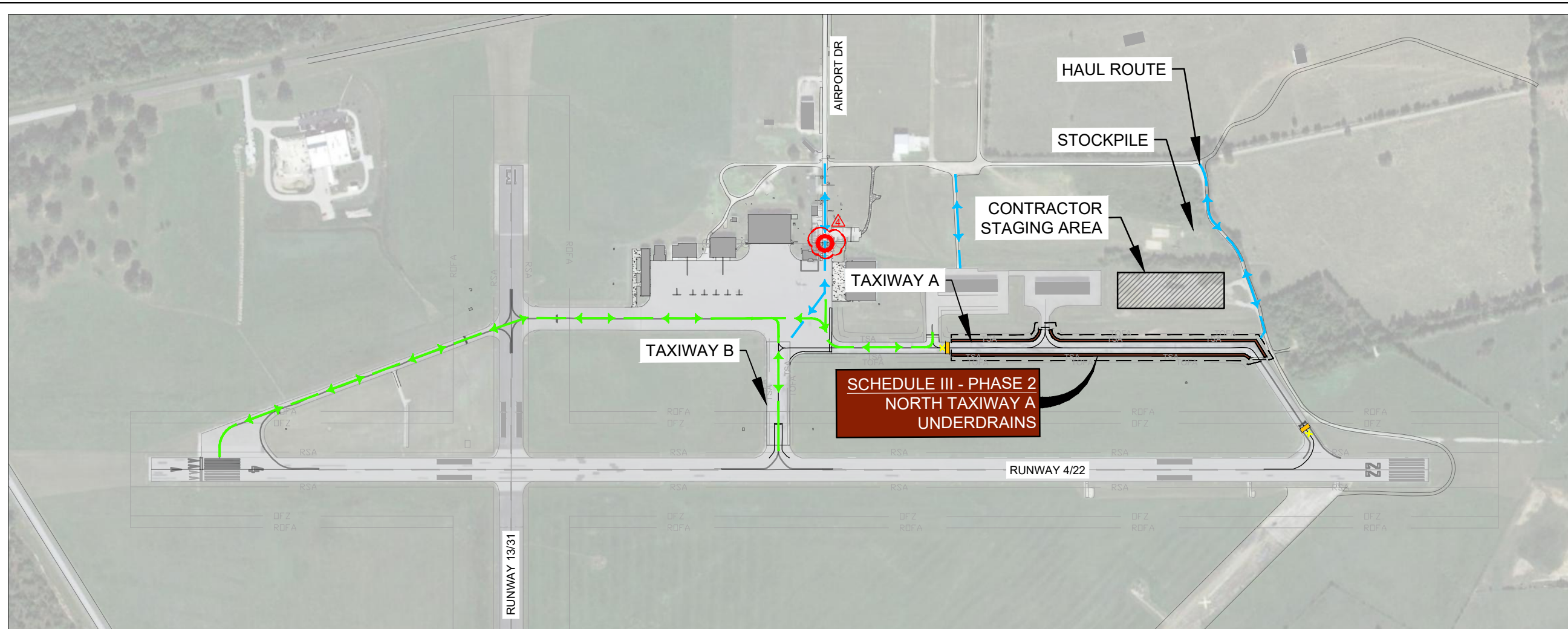
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| DR: V.S.B. | 1 | L.K.K. | 08/05/2025 | ISSUED FOR BID |
| CH: C.L.G. | 1 | L.K.K. | 09/23/2025 | ADDENDUM NO. 4 |
| APP: L.K.K. | | | | |

TAXIWAY A RECONSTRUCTION AND TAXIWAY CONNECTOR REHABILITATION

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| CONSTRUCTION SAFETY INDIVIDUAL PHASING PLAN - SCHEDULE III PHASE 1 | |
| MoDOT PROJ. NO. 23-056A-1 | WOOLPERT PROJ. NO. 10016991.00 |

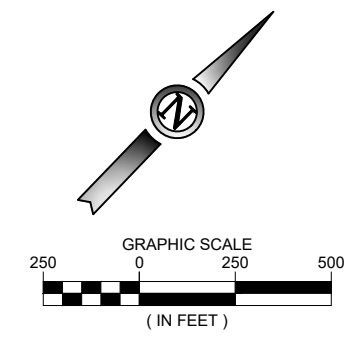
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SHEET NO. 17 of 54

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CONSTRUCTION PHASING NOTES

| SCHEDULE III / PHASE 2 | MAJOR WORK TO BE COMPLETED | IMPACTS ON OPERATIONS | NOTES |
|---|--|--|--|
| <p>DURATION</p> <p>PHASE 2: CONCURRENT WITH SCHEDULE I PHASE 2</p> <p>FOR SCHEDULE III:</p> <ul style="list-style-type: none"> PHASE 2 IS NOT CONCURRENT WITH PHASE 1 <p>CONTRACTOR ACCESS TIMES</p> <p>24 HOUR ACCESS TO APPROVED WORK AREAS</p> <p>ALL AIRPORT OPERATIONS AREAS SHALL REMAIN OPEN AND UNAFFECTED DURING THIS SCHEDULE WITH THE FOLLOWING EXCEPTIONS:</p> <ul style="list-style-type: none"> TAXIWAY A WILL BE PARTIALLY CLOSED THROUGHOUT THE DURATIONS OF SCHEDULE I, PHASES 1 AND 2 TO PROVIDE ACCESS TO THE ADJOINING HANGAR | <p>SITE PREPARATION</p> <p>1. EROSION CONTROL MEASURES</p> <p>EARTHWORK</p> <p>1. UNCLASSIFIED EXCAVATION</p> <p>2. TRENCHING AND BACKFILLING</p> <p>UTILITY</p> <p>1. UNDERDRAINS AND STRUCTURES</p> <p>SITE RECLAMATION</p> <p>1. SEEDING</p> <p>2. EROSION CONTROL MEASURES</p> | <p>1. THE CONTRACTOR SHALL GIVE RIGHT OF WAY TO ALL AIRCRAFT AND EMERGENCY VEHICLES AT ALL TIMES.</p> <p>2. CONTRACTOR TO MAINTAIN EMERGENCY VEHICLE ACCESS TO RUNWAY AT ALL TIMES.</p> <p>3. ALL NECESSARY TAXIWAY LIGHTS AND SIGNS SHALL BE COVERED FOR TEMPORARY TAXIWAY CLOSURES. CONTRACTOR METHOD FOR COVERING OF LIGHTS AND SIGNS SHALL BE APPROVED BY THE ENGINEER AND SHALL PREVENT DAMAGE TO THE EQUIPMENT AND SHALL BE SECURELY FASTENED TO PREVENT FREEING OF COVER FROM WIND.</p> <p>4. DURING PAVEMENT MARKING APPLICATIONS AND CURE TIMES, CONSTRUCTION BARRICADES SHALL BE INSTALLED AS DIRECTED BY ENGINEER FOR AREAS THAT ARE TEMPORARILY CLOSED TO TRAFFIC.</p> <p>5. NO WORK, OPEN EXCAVATIONS, EQUIPMENT, STOCKPILES, OR PERSONNEL ARE ALLOWED IN THE SAFETY AREAS OR OBJECT FREE AREAS FOR ANY ACTIVE TAXIWAY OR SAFETY AREAS FOR AN ACTIVE RUNWAY WHEN THE AIRPORT IS OPEN.</p> <p>6. ALL SAFETY AREAS AND OBJECT FREE AREAS FOR ACTIVE AIRPORT PAVEMENTS ARE OFF LIMITS TO THE CONTRACTOR WITHOUT PRIOR APPROVAL FROM AIRPORT OPERATIONS.</p> <p>7. PRIOR TO OPENING A RUNWAY OR TAXIWAY, ALL EXCAVATIONS AND TRENCHES IN THE SAFETY AREAS SHALL BE BACKFILLED AND COMPACTED TO P-152 SPECIFICATIONS. GRADING WITHIN THE SAFETY AREA SHALL CONFORM TO AC 150/5300-13B, CHAPTER 3.</p> <p>8. CONTRACTOR TO PROVIDE RUNWAY CLOSURE MARKERS AND INSTALLATION OF MARKERS. RUNWAY CLOSURE MARKERS SHALL BE INSTALLED ON RUNWAY 4/22 DURING CLOSURES. CONTRACTOR IS RESPONSIBLE FOR MAINTENANCE OF RUNWAY X'S.</p> <p>9. BARRICADES SHALL BE INSTALLED PER PHASING PLANS AND AS REQUIRED BY AC 150/5370-2G OR AS DIRECTED BY THE ENGINEER.</p> | <p>10. ALL STOCKPILES OR EQUIPMENT ADJACENT TO OBJECT FREE AREAS SHALL BE MARKED AND LIGHTED PER AC 150/5370-2G.</p> <p>11. THE CONTRACTOR SHALL PROVIDE A TRAFFIC CONTROL PLAN APPROVED BY THE AIRPORT AND LOCAL JURISDICTION, AND A WORK PLAN FOR THIS SCHEDULE OF WORK.</p> <p>12. THE CONTRACTOR SHALL HAVE A SWEEPER ON SITE AT ALL TIMES TO CLEAN DEBRIS FROM HAUL ROUTES, CONSTRUCTION ACCESS POINTS, OR AREAS ADJACENT TO CONSTRUCTION.</p> <p>13. THE CONTRACTOR SHALL KEEP ALL CONSTRUCTION TRAFFIC LIMITED TO THE APPROVED HAUL ROUTES AS SHOWN ON THE PLANS OR AS APPROVED BY THE ENGINEER. THE CONTRACTOR IS RESPONSIBLE FOR ANY DAMAGE CAUSED ALONG THE HAUL ROUTES OR CONSTRUCTION ACCESS ROUTES TO THE PROJECT SITE. HAUL ROADS WILL BE MAINTAINED AND RESTORED TO THEIR ORIGINAL CONDITION BY THE CONTRACTOR AT NO EXPENSE TO THE SPONSOR.</p> <p>14. PRIOR TO ACCEPTANCE AND OPENING OF AN AREA TO AIRCRAFT, ALL FINAL GRADING MUST BE COMPLETED. THIS INCLUDES ROCK REMOVAL, FINE GRADING, AND SEED PLACEMENT.</p> <p>15. ACCESS TO WORK AREAS WILL BE BY HAUL ROUTES ONLY.</p> <p>16. UNICOM - MONITOR 123.0 MHZ AT ALL TIMES.</p> <p>17. ALL COMPONENTS OF THE AIRFIELD LIGHTING SYSTEM OUTSIDE OF THE PROJECT AREA SHALL BE OPERATIONAL AT THE END OF EACH WORK SHIFT AND FOR EVERY PERIOD OF LOW VISIBILITY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING ALL TEMPORARY JUMPERS AND OTHER EQUIPMENT NECESSARY TO MAINTAIN AN OPERATIONAL SYSTEM DURING CONSTRUCTION. TEMPORARY JUMPERS SHALL BE CONSIDERED INCIDENTAL TO THE VARIOUS BID ITEMS. ALL TEMPORARY JUMPERS SHALL BE INSTALLED IN CONDUIT.</p> <p>18. PRIOR TO BEGINNING ELECTRICAL DEMOLITION THE CONTRACTOR SHALL PROVIDE A PLAN TO THE ENGINEER DETAILING THE INSTALLATION OF ALL REQUIRED TEMPORARY JUMPERS. THE PLAN SHALL SHOW THE LOCATIONS AND INSTALLATION TECHNIQUES OF ALL JUMPERS AND</p> <p>19. MATERIAL STORAGE WITHIN THE PROJECT LIMITS MUST BE APPROVED BY THE ENGINEER.</p> <p>20. HAUL ROUTES TO AND FROM THE PROJECT SITE MUST BE APPROVED BY THE ENGINEER.</p> <p>21. OPERATIONS WITHIN THE AIRPORT OPERATIONS AREA (A.O.A.) MUST BE APPROVED BY THE ENGINEER. SUFFICIENT NOTICE OF THE OPERATIONS MUST BE GIVEN TO THE ENGINEER.</p> <p>22. SEE SAFETY PLAN FOR SPECIFIC SAFETY REQUIREMENTS.</p> <p>23. ALL MATERIAL BESIDES MILLINGS ARE TO BE HAULED OFF SITE. MILLINGS ARE TO BE STOCKPILED IN THE CONTRACTOR STAGING AREA FOR AIRPORT USE.</p> <p>24. ALL LEAD-IN LINES TO CLOSED TAXIWAY AREAS MUST BE OBLITERATED OR COVERED PER AC 150/5370-2G OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION. REMOVAL IS CONSIDERED INCIDENTAL TO ITEM P-620.</p> |

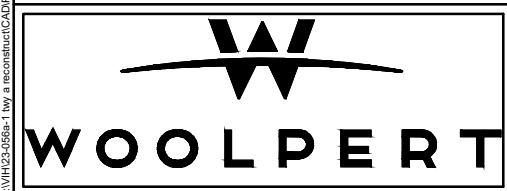


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| LAURA K. KOONCE | 2022012014 | 08/05/2025 |
| NAME | REG. NO. | DATE |

FOR AND ON BEHALF OF WOOLPERT, INC.



| DR: V.S.B. | ISSUE RECORD | | | |
|-------------|--------------|------------|------|----------------|
| | NO. | BY | DATE | DESCRIPTION |
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| | L.K.K. | 09/23/2025 | | ADDENDUM NO. 4 |
| CH: C.L.G. | | | | |
| APP: L.K.K. | | | | |

TAXIWAY A RECONSTRUCTION AND TAXIWAY CONNECTOR REHABILITATION

CONSTRUCTION SAFETY INDIVIDUAL PHASING PLAN - SCHEDULE III PHASE 2

MoDOT PROJ. NO. 23-056A-1

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