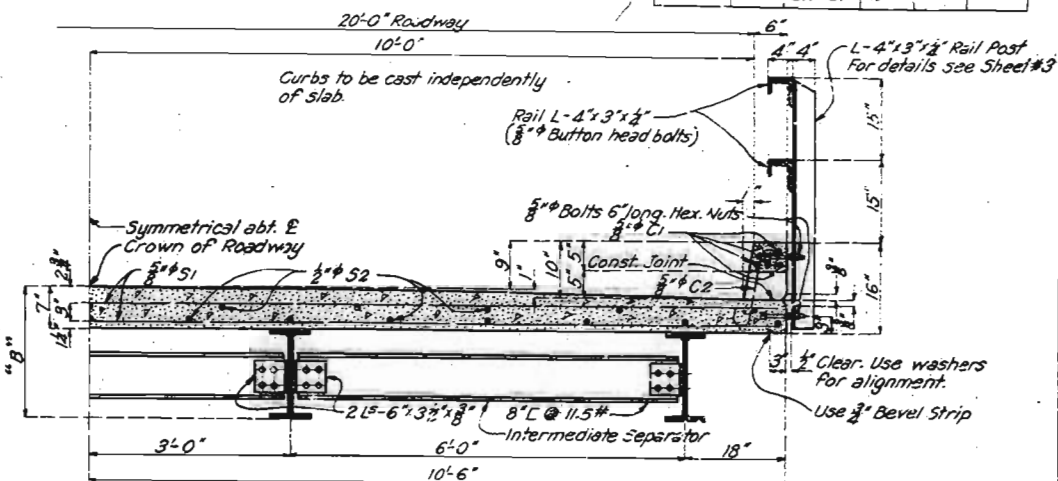
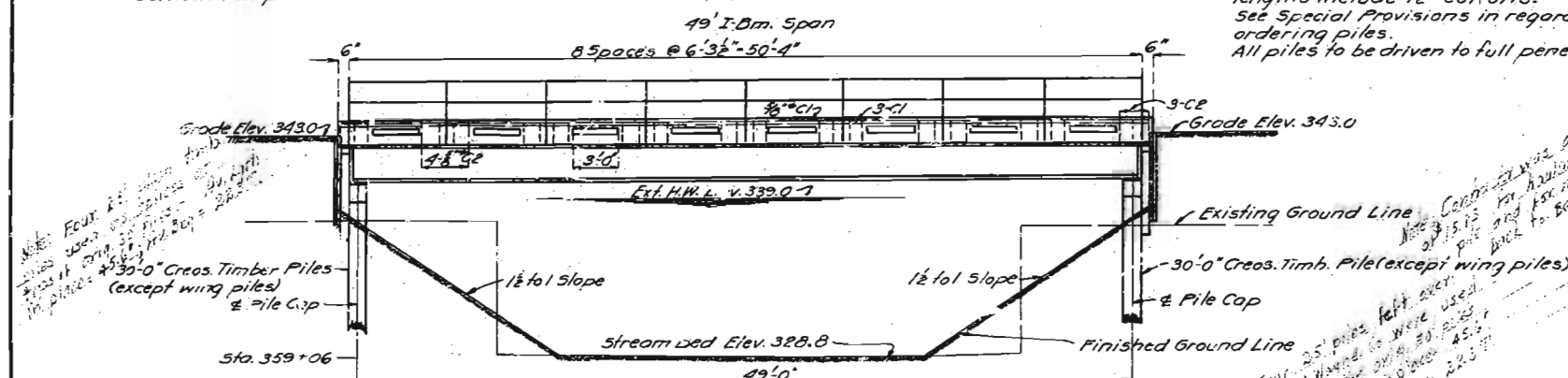


FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEET
5	NO.	SN-S/	19		

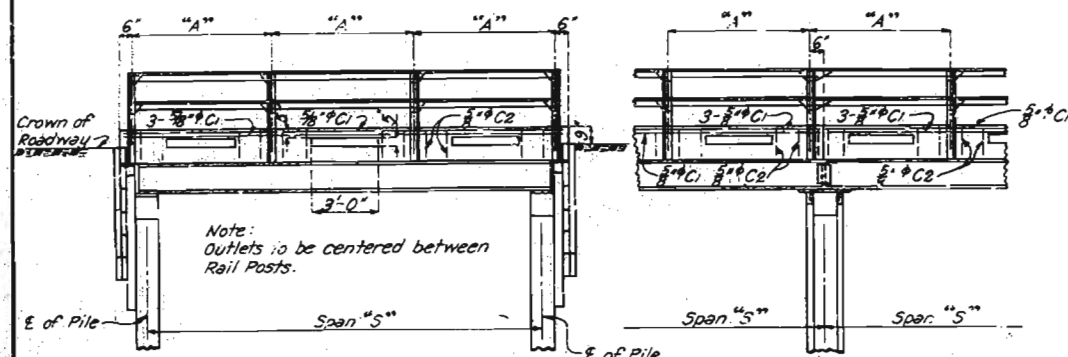
DIMENSION "B"	
SPAN "A"	"B"
15'-0"	2'-0"
17'-0"	2'-1"
19'-0"	2'-2"
21'-0"	2'-3"
23'-0"	2'-3 1/2"
25'-0"	2'-4"
27'-0"	2'-4 1/2"
29'-0"	2'-4 3/4"
31'-0"	2'-5"
33'-0"	2'-5 1/4"
35'-0"	2'-5 1/2"
37'-0"	2'-5 3/4"
39'-0"	2'-6"
41'-0"	2'-6 1/4"
43'-0"	2'-6 1/2"
45'-0"	2'-6 3/4"
47'-0"	2'-7"
49'-0"	2'-7 1/4"
51'-0"	2'-7 1/2"
53'-0"	2'-7 3/4"
55'-0"	2'-8"
57'-0"	2'-8 1/4"
59'-0"	2'-8 1/2"
61'-0"	2'-8 3/4"
63'-0"	2'-9"
65'-0"	2'-9 1/4"
67'-0"	2'-9 1/2"
69'-0"	2'-9 3/4"
71'-0"	2'-10"
73'-0"	2'-10 1/4"
75'-0"	2'-10 1/2"
77'-0"	2'-10 3/4"
79'-0"	2'-11"
81'-0"	2'-11 1/4"
83'-0"	2'-11 1/2"
85'-0"	2'-11 3/4"
87'-0"	2'-12"
89'-0"	2'-12 1/4"
91'-0"	2'-12 1/2"
93'-0"	2'-12 3/4"
95'-0"	2'-13"
97'-0"	2'-13 1/4"
99'-0"	2'-13 1/2"
101'-0"	2'-13 3/4"
103'-0"	2'-14"
105'-0"	2'-14 1/4"
107'-0"	2'-14 1/2"
109'-0"	2'-14 3/4"
111'-0"	2'-15"
113'-0"	2'-15 1/4"
115'-0"	2'-15 1/2"
117'-0"	2'-15 3/4"
119'-0"	2'-16"
121'-0"	2'-16 1/4"
123'-0"	2'-16 1/2"
125'-0"	2'-16 3/4"
127'-0"	2'-17"
129'-0"	2'-17 1/4"
131'-0"	2'-17 1/2"
133'-0"	2'-17 3/4"
135'-0"	2'-18"
137'-0"	2'-18 1/4"
139'-0"	2'-18 1/2"
141'-0"	2'-18 3/4"
143'-0"	2'-19"
145'-0"	2'-19 1/4"
147'-0"	2'-19 1/2"
149'-0"	2'-19 3/4"
151'-0"	2'-20"
153'-0"	2'-20 1/4"
155'-0"	2'-20 1/2"
157'-0"	2'-20 3/4"
159'-0"	2'-21"
161'-0"	2'-21 1/4"
163'-0"	2'-21 1/2"
165'-0"	2'-21 3/4"
167'-0"	2'-22"
169'-0"	2'-22 1/4"
171'-0"	2'-22 1/2"
173'-0"	2'-22 3/4"
175'-0"	2'-23"
177'-0"	2'-23 1/4"
179'-0"	2'-23 1/2"
181'-0"	2'-23 3/4"
183'-0"	2'-24"
185'-0"	2'-24 1/4"
187'-0"	2'-24 1/2"
189'-0"	2'-24 3/4"
191'-0"	2'-25"
193'-0"	2'-25 1/4"
195'-0"	2'-25 1/2"
197'-0"	2'-25 3/4"
199'-0"	2'-26"
201'-0"	2'-26 1/4"
203'-0"	2'-26 1/2"
205'-0"	2'-26 3/4"
207'-0"	2'-27"
209'-0"	2'-27 1/4"
211'-0"	2'-27 1/2"
213'-0"	2'-27 3/4"
215'-0"	2'-28"
217'-0"	2'-28 1/4"
219'-0"	2'-28 1/2"
221'-0"	2'-28 3/4"
223'-0"	2'-29"
225'-0"	2'-29 1/4"
227'-0"	2'-29 1/2"
229'-0"	2'-29 3/4"
231'-0"	2'-30"
233'-0"	2'-30 1/4"
235'-0"	2'-30 1/2"
237'-0"	2'-30 3/4"
239'-0"	2'-31"
241'-0"	2'-31 1/4"
243'-0"	2'-31 1/2"
245'-0"	2'-31 3/4"
247'-0"	2'-32"
249'-0"	2'-32 1/4"
251'-0"	2'-32 1/2"
253'-0"	2'-32 3/4"
255'-0"	2'-33"
257'-0"	2'-33 1/4"
259'-0"	2'-33 1/2"
261'-0"	2'-33 3/4"
263'-0"	2'-34"
265'-0"	2'-34 1/4"
267'-0"	2'-34 1/2"
269'-0"	2'-34 3/4"
271'-0"	2'-35"
273'-0"	2'-35 1/4"
275'-0"	2'-35 1/2"
277'-0"	2'-35 3/4"
279'-0"	2'-36"



Note: Depth of outside stringers will in some cases be a fraction of an inch less than that of inside stringers and in order to keep bottom of slab horizontal it will be necessary to haunch slab down to top of outside stringers.

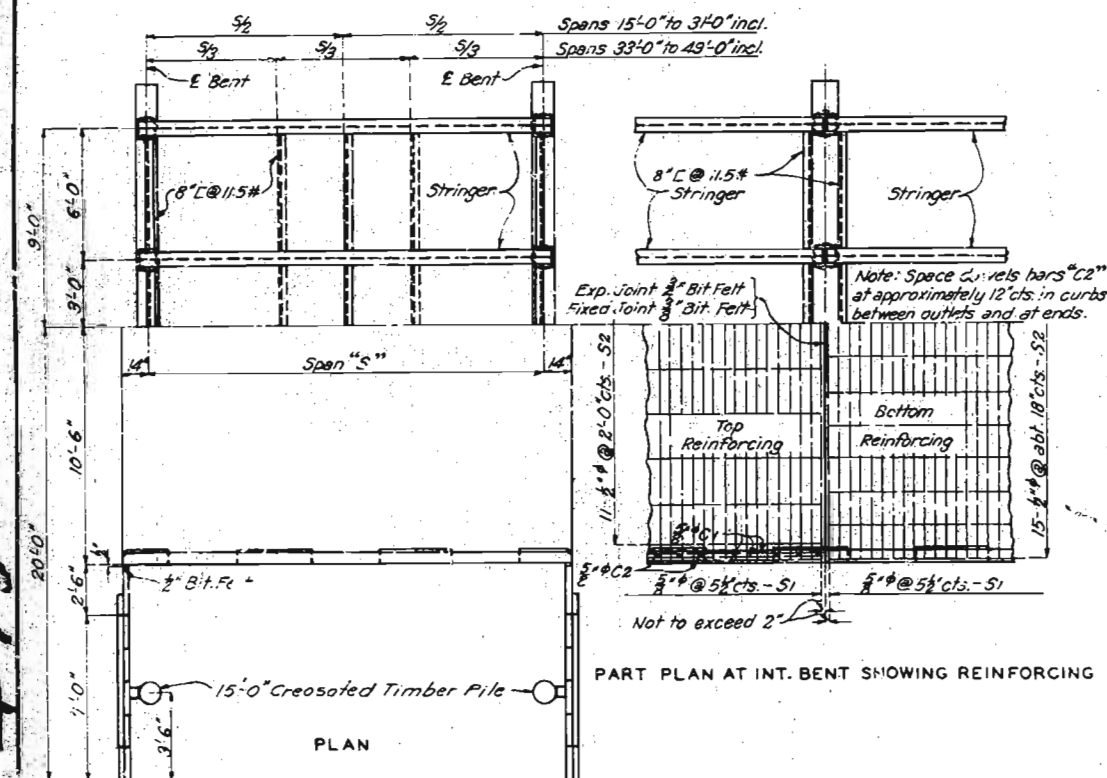


GENERAL ELEVATION



ELEVATION

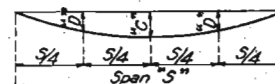
PART ELEVATION AT INT. BENT



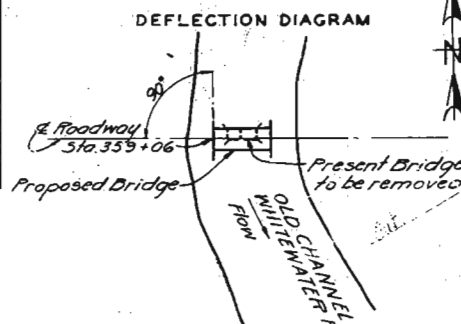
PART PLAN AT INT. BENT SHOWING REINFORCING

HANDRAIL SPACING					
SPAN "S"	NO. PANELS	"A" DIMENSIONS			
		SINGLE SPAN	1ST END SPAN	ALL INT. SPANS	LAST END SPAN
15'-0"	3	5'-5 1/2" abt.	5'-0 1/2" abt.	5'-0"	5'-4 1/4" abt.
17'-0"	3	6'-1 1/2" abt.	5'-8 1/2" abt.	5'-8"	6'-0 1/8" abt.
19'-0"	3	6'-9 1/2" abt.	6'-4 1/4" abt.	6'-4"	6'-0 7/8" abt.
21'-0"	4	5'-7"	5'-3 1/2"	5'-3"	5'-6 1/2"
23'-0"	4	6'-1"	5'-9 1/2"	5'-9"	6'-0 3/4"
25'-0"	4	6'-7"	6'-3 3/8"	6'-3"	5'-6 5/8"
27'-0"	5	5'-8"	5'-5 1/2" abt.	5'-4 1/2" abt.	5'-7 1/2" abt.
29'-0"	5	6'-0 1/2" abt.	5'-4"	5'-3 3/8" abt.	6'-0 1/2" abt.
31'-0"	5	6'-5 1/2" abt.	6'-2 1/2" abt.	6'-2 1/2" abt.	6'-5 1/2" abt.
33'-0"	6	5'-8 1/2" abt.	5'-6 1/2" abt.	5'-6"	5'-8 1/2" abt.
35'-0"	6	6'-0 1/2" abt.	5'-10 1/2" abt.	5'-10"	6'-0 1/2" abt.
37'-0"	6	6'-4 1/2" abt.	6'-2 1/2" abt.	6'-2"	6'-4 1/2" abt.
39'-0"	7	5'-9 1/2" abt.	5'-7 1/4" abt.	5'-6 1/2" abt.	5'-8 1/2" abt.
41'-0"	7	6'-0 1/2" abt.	5'-10 1/2" abt.	5'-10 1/2" abt.	6'-0 1/2" abt.
43'-0"	7	6'-4"	6'-2"	6'-1 1/2" abt.	6'-3 1/2" abt.
45'-0"	8	5'-9 1/2"	5'-7 1/4"	5'-7 1/4"	5'-9 1/2"
47'-0"	8	6'-0 1/2"	5'-10 1/2"	5'-10 1/2"	6'-0 1/2"
49'-0"	8	6'-3 3/8"			

DEFLECTION		
SPANS	"C"	"O"
15'-0"	1/8"	5/8"
17'-0"	1/8"	5/8"
19'-0"	1/8"	5/8"
21'-0"	1/8"	5/8"
23'-0"	1/8"	5/8"
25'-0"	1/8"	5/8"
27'-0"	1/8"	5/8"
29'-0"	1/8"	5/8"
31'-0"	1/8"	5/8"
33'-0"	1/8"	5/8"
35'-0"	1/8"	5/8"
37'-0"	1/8"	5/8"
39'-0"	1/8"	5/8"
41'-0"	1/8"	5/8"
43'-0"	1/8"	5/8"
45'-0"	1/8"	5/8"
47'-0"	1/8"	5/8"
49'-0"	1/8"	5/8"



Note: Floor slab to be brought to grade and dead load deflection taken care of by increasing slab thickness. Depth of slab at outside face of curb to be kept uniform and bottom surface of slab warped between curb and outside beam to obtain required thickness at beam. Payment will be allowed for additional concrete required for thickening slab. This additional concrete is included in "Estimated Quantities."



LOCATION SKETCH

TABLE OF STRINGERS							
SPAN "S"	PER PLANS		PERMISSIBLE SUBSTITUTIONS				
	CARNegie BEAMS		STANDARD I-BEAMS		BETHLEHEM BEAMS		
	Inside	Outside	Inside	Outside	Inside	Outside	Outside
15'-0"	14" @ 30"	14" @ 30"	12" @ 40.8"	12" @ 35.5"	14" @ 30"	14" @ 30"	14" @ 30"
17'-0"	14" @ 33 1/4"	14" @ 33 1/4"	15" @ 42.9"	15" @ 42.9"	14" @ 33 1/4"	14" @ 33 1/4"	14" @ 33 1/4"
19'-0"	16" @ 37"	16" @ 37"	15" @ 42.9"	15" @ 42.9"	16" @ 37"	16" @ 37"	16" @ 37"
21'-0"	16" @ 40"	16" @ 37"	15" @ 50"	15" @ 45.5"	16" @ 40"	16" @ 37"	16" @ 37"
23'-0"	16" @ 45"	16" @ 40"	18" @ 54.7"	18" @ 54.7"	16" @ 45"	16" @ 40"	16" @ 40"
25'-0"	18" @ 47 1/2"	18" @ 47 1/2"	18" @ 54.7"	18" @ 54.7"	18" @ 47 1/2"	18" @ 47 1/2"	18" @ 47 1/2"
27'-0"	18" @ 52"	18" @ 47 1/2"	18" @ 60"	18" @ 54.7"	18" @ 52"	18" @ 47 1/2"	18" @ 47 1/2"
29'-0"	20" @ 55"	20" @ 55"	20" @ 65 1/4"	20" @ 65 1/4"	20" @ 55"	20" @ 55"	20" @ 55"
31'-0"	21" @ 58"	21" @ 58"	20" @ 70"	20" @ 65 1/4"	22" @ 58"	22" @ 58"	22" @ 58"
33'-0"	21" @ 52"	21" @ 58"	20" @ 75"	20" @ 70"	22" @ 62"	22" @ 58"	22" @ 58"
35'-0"	21" @ 67"	21" @ 62"	20" @ 81.4"	20" @ 75"	22" @ 57"	22" @ 62"	22" @ 62"
37'-0"	24" @ 70"	24" @ 70"	24" @ 79.9"	24" @ 79.9"	24" @ 70"	24" @ 70"	24" @ 70"
39'-0"	24" @ 74"	24" @ 70"	24" @ 79.9"	24" @ 79.9"	24" @ 74"	24" @ 70"	24" @ 70"
41'-0"	24" @ 81"	24" @ 74"	24" @ 85"	24" @ 79.9"	24" @ 81"	24" @ 74"	24" @ 74"
43'-0"	26" @ 85"	24" @ 81"	24" @ 100"	24" @ 90"	26" @ 85"	26" @ 81"	26" @ 81"
45'-0"	27" @ 85"	27" @ 85"	24" @ 105.9"	24" @ 100"	26" @ 85"	26" @ 85"	26" @ 85"
47'-0"	27" @ 91"	27" @ 85"	24" @ 105.9"	24" @ 105.9"	28" @ 91"	26" @ 85"	26" @ 85"
49'-0"	27" @ 91"	27" @ 91"	24" @ 115"	24" @ 105.9"	28" @ 91"	28" @ 91"	28" @ 91"

See Sheet 2 For Final Chart

ESTIMATED QUANTITIES			
ITEM	SUPERSTR.	SUBSTR.	TOTAL
Bridge Excavation Class 1 Cu. Yds.		30	30
Bridge Excavation Class 2 Cu. Yds.		0	0
Concrete 1:2:4 mix "B" Cu. Yds.		0	0
Concrete 1:2:3 1/2 mix "X" Cu. Yds.	25.3		25.3
Fabricated Structural Steel Lbs.	21090		21090
Reinforcing Steel Lbs.	6350		6350
Creasoted Timber Piles Lin. Ft.		200	200
Creasoted Timber Pile Caps Lin. Ft.		12	12
Timber (See Special Provisions) F.B.M.		159	159

Note: Bridge excavation will be allowed for end bents within horizontal limits shown and noted on "Half Plan of End Bent" sheet 2-0. This excavation will be computed from existing ground line to bottom end of 6'-6" backing supports.
Excavation required to place superstructure allowing 2'-0" below bottom of beams and 4'-0" outside of curb lines will be paid for at unit price bid for roadway excavation.

GENERAL NOTES:

Loading: One 10 Ton Truck, 80% of weight on rear axle, 30% impact, 14'-0" wheel base, 5'-0" gage, 10' tire.

All concrete to be 1:2 1/2:3 mix, 4" x 1 1/2".

Exposed edges to be beveled 3/4" where no other bevel is noted.

All timber to be crosscut Douglas Fir of the West Coast Region, Close-grained Structural Grade; crosscut Southern Yellow Pine, Dense Structural Square Edge and Sound Grade; or untreated California Redwood, Prime Structural Grade. All timber rough full-sawn except as noted in timber bill for pile caps. Slight variations in sawing to be in accordance with grading rules. All treated timber to be cut to lengths, shaped and treated as shown before treating. Backing plank are all billed 6' long and are to be fitted and cut in the field.

Field holes for drift pins shall be field bored 3 3/8". Unless otherwise noted all other field holes in timber shall be field bored 3/4".

When bolts with countersunk heads are indicated on plans cut washers shall be used under heads. O.G. washers shall be used under heads of all other bolts and under nuts of all bolts.

Number of bolts, drift pins, nails and washers given exact; no allowance made for excess. Cost of substructure hardware to be included in price bid for timber in place.

I-Beams with fastenings, spacers, handrail, handrail posts with fastenings, clip angles and cap plate on end bent with fastenings, will be paid for as structural steel. Cost of metallic edge moulding will be included in unit bid price for concrete.

Rivets 3/4", holes 1 1/4", except in handrail where rivets shall be 5/8", holes 1 1/8".

Nails 1/2", holes 3/4", except in handrail where rivets shall be 5/8", holes 1 1/8".

Field bolts 1/2", except handrail, shall be machine bolts. Field connecting plate bolts shall be 3/8" button head bolts, 1/2" holes.

Detail shop drawings shall be submitted to the State Highway Department in duplicate and shall be approved before steel is fabricated.

See Special Provisions in regard to permissible substitution of beams.

Where bituminous felt is used in expansion or partition joints in concrete stretch felt in vertical joint securely to one face of concrete with copper wire.

Paint: Shop, none. Field, contact surfaces of steel to be painted. No other paint to be applied by contractor. All paint required will be furnished by the Missouri State Highway Department.

BRIDGE OVER OLD CHANNEL WHITEWATER RIVER

STATE ROAD FROM DRUM TO DELTA
ABOUT 0.7 MILE WEST OF DELTA
PROJECT NO. SN-SI STA. 359 + 06

CAPE GIRARDEAU COUNTY

SUBMITTED BY W. Davis DATE 7/1/54
APPROVED BY H. Carter CHIEF ENGINEER

B.M. Elev. 341.88 - Nail in Root of 15" Elm.
45' left of Sta. 359+90.

Sheet No. 1 of 3

FINISHE

FINISHED

S-29

Des. gnd. Nov. 1929 By F.W.H.
 Drawn Mar. 1930 By R.J.G.
 Traced Dec. 1931 By R.J.G.
 Checked Dec. 1931 By J.T.M.

Assembled June 1932 By H.E.U.
 Checked July 1932 By J.T.M.

Note: This drawing is not to scale.
Follow dimensions.

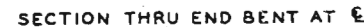
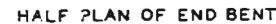
493

FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
5	MO.	SN-5/	19		



Note: Any irregularity in alignment of piling in end bents to be corrected by facing one surface of the 6"x6" backing support or by grading the thickness of the backing support such as to place the surface of the backing in a true plane and eliminate any strain on the backing plank.

Splice in backing plank to be made at center of 6"x6" backing support and to be alternated on the two intermediate supports.



Note: Omit sway bracing when "E" is less than 5'-0".



BACKING SUPPORT

Note: Holes in backing support to be $\frac{3}{4}" \phi$.

Note: Reinforcing bars in each span to be banded and tagged separately.

Note: Pile caps to be classified as bears and stringers.
All other timber to be classified as joist and plank.

**** 525 to exact depth of 11 1/2".**

§ Omit when "E" is less than 5'-0"

Note: Bolts to be threaded 8" and sawed off $\frac{1}{2}$ " outside of nut after nut has been tightened.

* 2 Washers plus washers for fill required for alignment of railposts.

FINAL QUANTITIES		
Item		Quan.
Class I Bridge Exc.	Cu. Yd.	10
Class II Concrete	" "	25.3
Reb. Str. Steel	lbs.	2,150
Reinforcing Steel	"	6,350
Graded and Paved in Place	Sq. Ft.	346
" " Cut-outs	" "	32
" Timber	P.C.M.	1,592
Temporary Bridge	Sq. Ft.	180
Removal of Old Bridge	L. Yards	1
Driveway State Furnished Trucks	Days	55
Paved Timber Frame in Place	"	107
Moving piles and driving piles	Feet	4,123

FINISHED

Designed Nov. 1929 By F.W.H.
 Drawn Dec. 1930 By R.J.G. Assembled June 1932 By H.E.U.
 Traced Dec. 1931 By R.J.G. Checked July 1932 By J.H.M.
 Checked Dec. 1931 By J.H.M.

DETAILS OF BEARING ON PILE CAP

Note: This drawing is not to scale.
Follow dimensions.

Sheet No. 2 of 3.

S-291

MISSOURI STATE HIGHWAY DEPARTMENT

FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
5	MO.	SN-51	19		

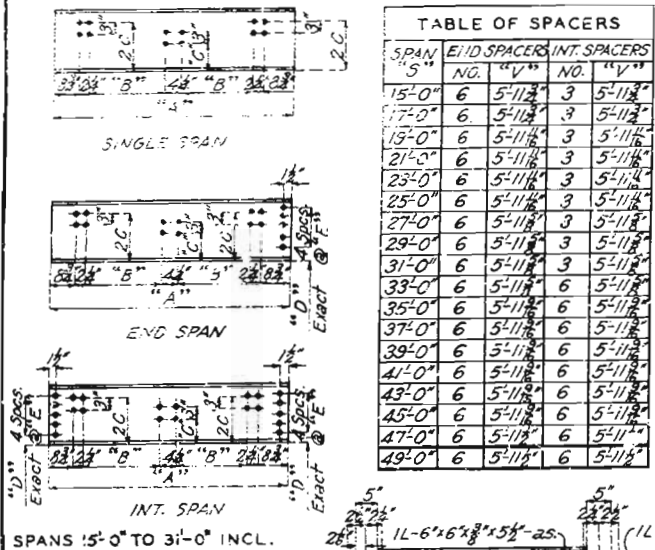


TABLE OF SPACERS

SPAN "S"	NO.	"V"	NO.	"V"
15'-0"	6	5-11/2"	3	5-11/2"
17'-0"	6	5-11/2"	3	5-11/2"
19'-0"	6	5-11/2"	3	5-11/2"
21'-0"	6	5-11/2"	3	5-11/2"
23'-0"	6	5-11/2"	3	5-11/2"
25'-0"	6	5-11/2"	3	5-11/2"
27'-0"	6	5-11/2"	3	5-11/2"
29'-0"	6	5-11/2"	3	5-11/2"
31'-0"	6	5-11/2"	3	5-11/2"
33'-0"	6	5-11/2"	3	5-11/2"
35'-0"	6	5-11/2"	3	5-11/2"
37'-0"	6	5-11/2"	3	5-11/2"
39'-0"	6	5-11/2"	3	5-11/2"
41'-0"	6	5-11/2"	3	5-11/2"
43'-0"	6	5-11/2"	3	5-11/2"
45'-0"	6	5-11/2"	3	5-11/2"
47'-0"	6	5-11/2"	3	5-11/2"
49'-0"	6	5-11/2"	3	5-11/2"

TABLE OF STRINGERS

SPAN "S"	NO.	SIZE	SINGLE SPAN	END SPAN	INT. SPAN	"C"	"D"	"E"
15'-0"	4	14" C.B. @ 30"	15'-8"	6'-8"	15'-3"	6'-6"	14'-11"	5'-2"
17'-0"	4	14" C.B. @ 33"	17'-8"	7'-8"	17'-3"	7'-6"	16'-11"	5'-2"
19'-0"	4	16" C.B. @ 37"	19'-8"	8'-8"	19'-3"	8'-6"	18'-11"	5'-2"
21'-0"	2	16" C.B. @ 37"	21'-8"	9'-8"	21'-3"	9'-6"	20'-11"	5'-2"
23'-0"	2	16" C.B. @ 40"	23'-8"	10'-8"	23'-3"	10'-6"	22'-11"	5'-2"
25'-0"	4	16" C.B. @ 40"	25'-8"	11'-8"	25'-3"	11'-6"	24'-11"	5'-2"
27'-0"	2	18" C.B. @ 42"	27'-8"	12'-8"	27'-3"	12'-6"	26'-11"	5'-2"
29'-0"	2	18" C.B. @ 45"	29'-8"	13'-8"	29'-3"	13'-6"	28'-11"	5'-2"
31'-0"	4	20" C.B. @ 55"	31'-8"	14'-8"	31'-3"	14'-6"	30'-11"	5'-2"
33'-0"	2	21" C.B. @ 52"	33'-8"	15'-8"	33'-3"	15'-6"	32'-11"	5'-2"
35'-0"	2	21" C.B. @ 62"	35'-8"	16'-8"	35'-3"	16'-6"	34'-11"	5'-2"
37'-0"	4	24" C.B. @ 70"	37'-8"	17'-8"	37'-3"	17'-6"	36'-11"	5'-2"
39'-0"	2	24" C.B. @ 74"	39'-8"	18'-8"	39'-3"	18'-6"	38'-11"	5'-2"
41'-0"	2	24" C.B. @ 74"	41'-8"	19'-8"	41'-3"	19'-6"	40'-11"	5'-2"
43'-0"	2	24" C.B. @ 81"	43'-8"	20'-8"	43'-3"	20'-6"	42'-11"	5'-2"
45'-0"	4	27" C.B. @ 85"	45'-8"	21'-8"	45'-3"	21'-6"	44'-11"	5'-2"
47'-0"	4	27" C.B. @ 85"	47'-8"	22'-8"	47'-3"	22'-6"	46'-11"	5'-2"
49'-0"	4	27" C.B. @ 91"	49'-8"	23'-8"	49'-3"	23'-6"	48'-11"	5'-2"

TABLE OF RAIL ANGLE DIMENSIONS

SPAN	MARK	"L"	"M"	"N"	"O"	"P"	"R"	"T"	"U"	"V"	"W"	"X"	"Y"	"Z"
15'-0"	RA1	16'-7"	5'-0"	0	0	0	0	0	5'-5"	0	0	0	5'-0"	0
17'-0"	"	18'-7"	5'-0"	0	0	0	0	0	6'-1"	0	0	0	5'-0"	0
19'-0"	"	20'-7"	5'-0"	0	0	0	0	0	6'-7"	0	0	0	5'-0"	0
21'-0"	"	22'-7"	5'-2"	0	0	0	5'-4"	12"	5'-7"	0	0	0	5'-2"	0
23'-0"	"	24'-7"	5'-8"	0	0	0	5'-10"	23"	6'-1"	0	0	0	5'-8"	0
25'-0"	"	26'-7"	6'-2"	0	0	0	6'-4"	23"	6'-7"	0	0	0	6'-2"	0
27'-0"	"	28'-7"	5'-3"	0	0	0	5'-5"	23"	5'-8"	23"	5'-5"	0	5'-3"	0
29'-0"	"	30'-7"	5'-7"	0	0	0	5'-10"	23"	6'-0"	23"	5'-10"	0	5'-7"	0
31'-0"	"	32'-7"	6'-0"	0	0	0	6'-2"	23"	6'-5"	23"	6'-0"	0	6'-0"	0
33'-0"	"	34'-7"	5'-3"	5'-5"	23"	0	0	0	5'-8"	23"	5'-8"	23"	5'-3"	0
35'-0"	"	36'-7"	5'-7"	5'-9"	23"	0	0	0	6'-1"	23"	6'-1"	23"	5'-7"	0
37'-0"	"	38'-7"	5'-11"	6'-1"	23"	0	0	0	6'-4"	23"	6'-4"	23"	5'-11"	0
39'-0"	"	40'-7"	5'-4"	5'-6"	23"	0	0	23"	5'-9"	23"	5'-6"	23"	5'-4"	0
41'-0"	"	42'-7"	5'-7"	5'-9"	23"	0	0	23"	6'-0"	23"	5'-9"	23"	5'-7"	0
43'-0"	"	44'-7"	5'-11"	6'-1"	23"	0	0	23"	6'-4"	23"	6'-1"	23"	5'-11"	0
45'-0"	"	46'-7"	5'-4"	5'-6"	23"	23"	5'-6"	23"	5'-9"	23"	5'-6"	23"	5'-4"	0
47'-0"	"	48'-7"	5'-7"	5'-9"	23"	23"	5'-9"	23"	6'-0"	23"	5'-9"	23"	5'-7"	0
49'-0"	"	50'-7"	5'-10"	6'-0"	23"	23"	6'-0"	23"	6'-3"	23"	6'-0"	23"	5'-10"	0

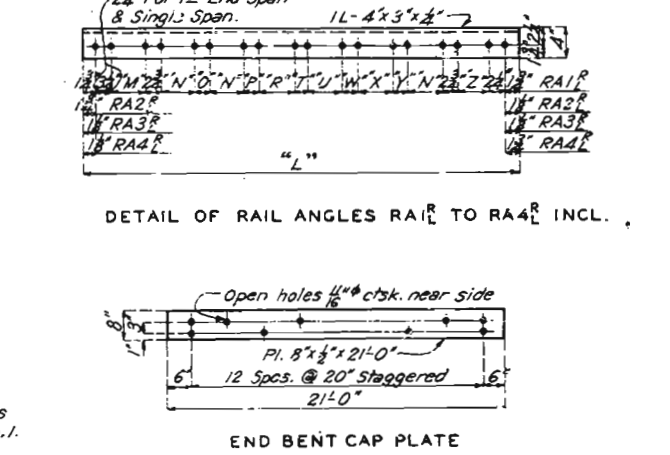
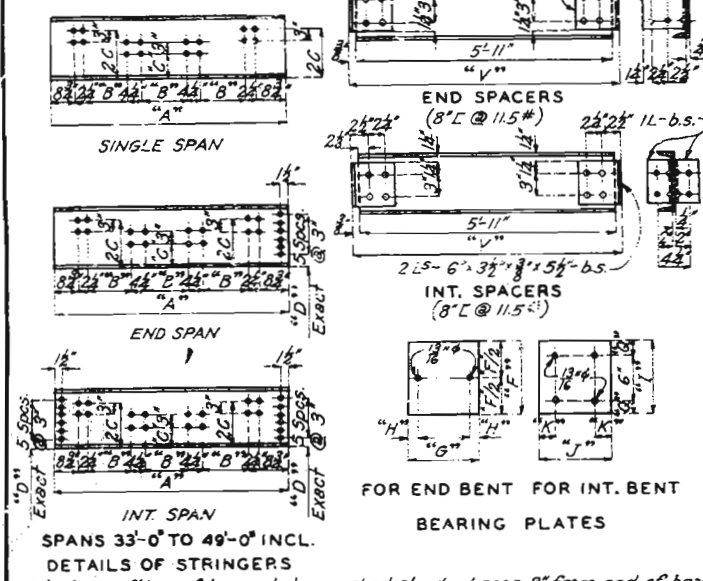
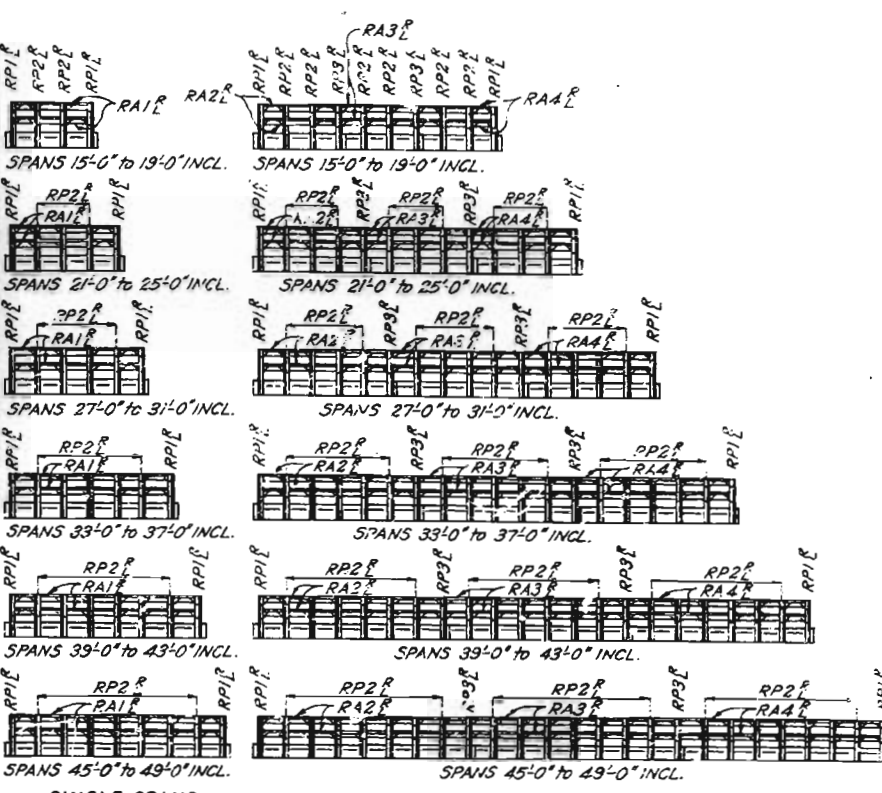
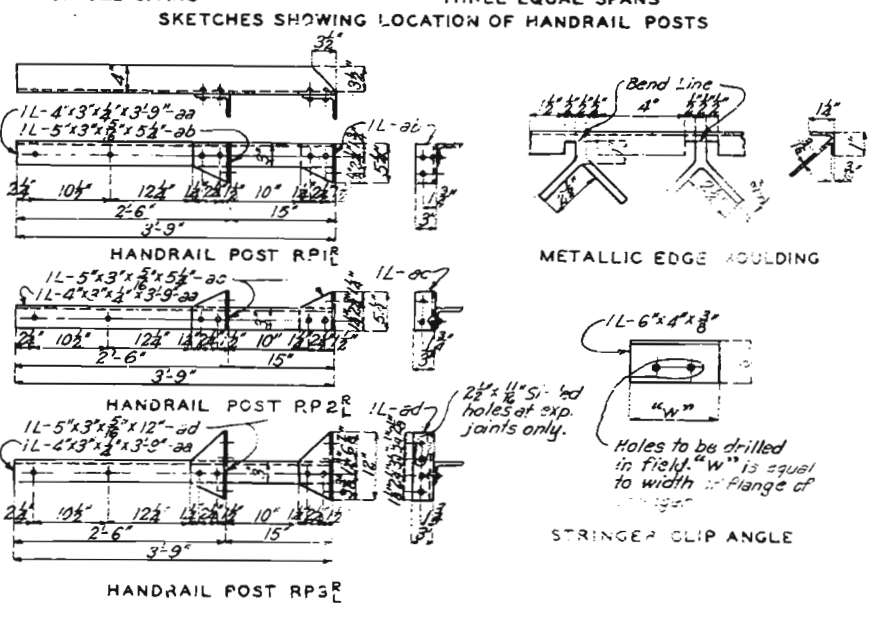
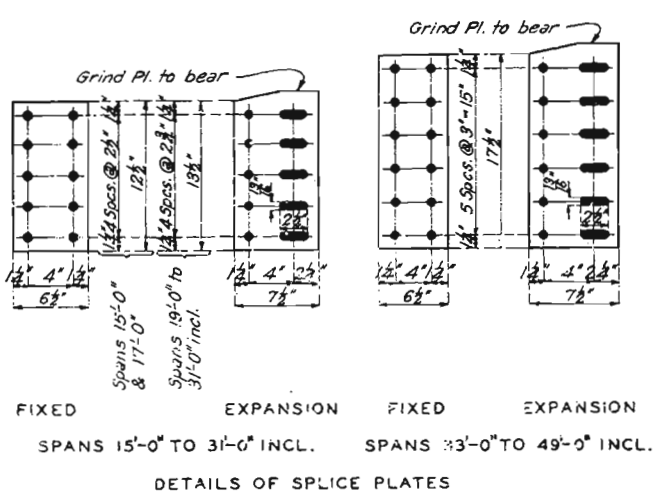


TABLE OF BEARING PLATES

SPAN "S"	NO. PLATES	STRINGER	AT END BENTS	THICK.	"G"	"H"	THICK.	"I"	"J"	"K"	"Q"
15'-0"	4	All	10"	11"	13"	3"	10"	11"	13"	2"	
17'-0"	4	"	10"	11"	13"	3"	10"	11"	13"	2"	
19'-0"	4	"	10"	12"	13"	3"	10"	12"	13"	2"	
21'-0"	2	Inside	10"	12"	13"	3"	10"	12"	13"	2"	
23'-0"	2	Outside	10"	12"	13"	3"	10"	12"	13"	2"	
25'-0"	4	All	10"	12"	13"	3"	10"	12"	13"	2"	
27'-0"	4	"	10"	12"	13"	3"	10"	12"	13"	2"	
29'-0"	4	"	10"	12"	13"	3"	10"	12"	13"	2"	
31'-0"	4	"	10"	13"	13"	3"	10"	13"	14"	2"	
33'-0"	2	"	11"	13"	13"	3"	11"	13"	14"	2"	
35'-0"	2	"	11"	13"	13"	3"	11"	13"	15"	2"	
37'-0"	2	"	11"	13"	13"	3"	11"	13"	15"	2"	
39'-0"	4	"	11"	13"	13"	3"	11"	13"	15"	2"	
41'-0"	2	"	11"	13"	13"	3"	11"	13"	15"	2"	
43'-0"	2	Outside	11"	14"	13"	3"	11"	14"	15"	2"	
45'-0"	4	"	11"	14"	13"	3"	11"	14"	15"	2"	
47'-0"	4	"	11"	14"	13"	3"	11"	14"	15"	2"	
49'-0"	4	All	11"	14"	13"	3"	11"	14"	15"	2"	




Designed Nov. 1929 By F.W.H.
Drawn Dec. 1930 By R.J.G.
Traced Dec. 1931 By R.J.G.
Checked Dec. 1931 By J.M.P.


Assembled June 1932 By M.E.U.
Checked July 1932 By J.M.P.

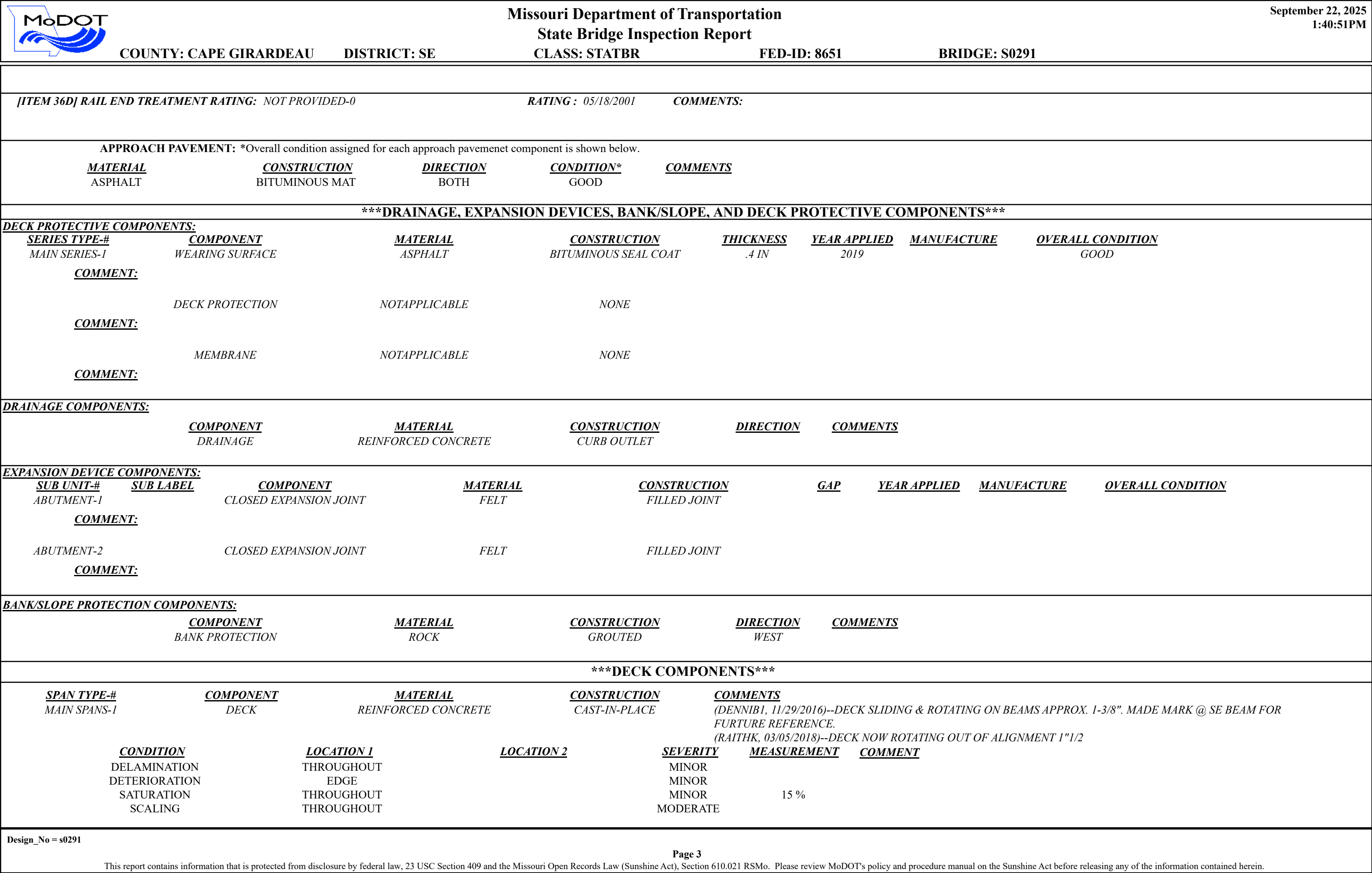
Note: This drawing is not to scale.
Follow dimensions.

Sheet No. 3 of 3.

BRIDGE OVER OLD CHANNEL WHITEWATER RIVER
STATE ROAD FROM DRUM TO DELTA
ABOUT 0.7 MILE WEST OF DELTA
PROJECT NO. SN-51 STA. 359+06
CAPE GIRARDEAU COUNTY
DATE 4/2/32
DESIGNED BY J.H. Carter
CHECKED BY J.H. Carter
FINISHED
S-291

		<div>Missouri Department of Transportation</div> <div>State Bridge Inspection Report</div>				<div>September 22, 2025</div> <div>1:40:51PM</div>							
COUNTY: CAPE GIRARDEAU		DISTRICT: SE		CLASS: STATBR		FED-ID: 8651		BRIDGE: S0291					
GENERAL STRUCTURE INFORMATION							***BRIDGE INSPECTION INFORMATION***						
<div>ROUTE: RTNE</div> <div>FEATURE: WHITEWATER RVR</div> <div>STATUS: P-POSTLOAD</div> <div>LOG MILE: 15.924</div> <div>DETOUR: 22.00 MILES</div> <div>NHS: NO</div> <div>BUILT: 1932</div> <div>REHAB:</div> <div>LOCATION: S 5 T 29 R 12 E</div> <div>LATITUDE: 37 12 34.57 (DMS)</div> <div>LONGITUDE: 89 44 53.15 (DMS)</div>		<div># SPANS: 1</div> <div>LANES ON: 1</div> <div>LANES UNDER: 0</div> <div>COMPASS DIRECTION: WEST to EAST</div> <div>DIRECTION OF TRAFFIC: 1-LN/2-WAY</div> <div>FUNCTIONAL CLASS: RL-MAJOR COLLECTOR</div> <div>NBI OWNER: MODOT</div> <div>NBI MAINTAINED: MODOT</div> <div>MAINTENANCE DISTRICT: SE</div> <div>MAINTENANCE COUNTY: CAPE GIRARDEAU</div> <div>SUB AREA: 7H01</div>		<div>PLACE CODE: 78280 WELCH</div> <div>LENGTH: 52 FT 0 IN</div> <div>MAXIMUM SPAN: 41 FT 0 IN</div> <div>APPROACH ROADWAY: 20 FT 0 IN</div> <div>CURB TO CURB: 20 FT 0 IN</div> <div>OUT TO OUT: 21 FT 0 IN</div> <div>AADT: 2155</div> <div>AADT YEAR: 2024</div> <div>AADT TRUCK: 3.3%</div> <div>FUTURE AADT: 3340</div> <div>FUTURE AADT YEAR: 2044</div>		<div>DATE: 11/04/2024</div> <div>RESPONSIBILITY: DISTRICT</div> <div>FREQUENCY: 12</div> <div>CALCULATED INTERVAL**: 12</div> <div>TEAM LEADER: STEVE RIGHTNOWAR</div> <div>ELEMENT: NO</div> <div>INSPECTOR 2:</div> <div>INSPECTOR 4:</div> <div>INSPECTOR 3:</div> <div>** When calculated interval exceeds the frequency, a justification comment per BIRM is required.</div>							
						GENERAL INSPECTION COMMENTS							
FRACTURE CRITICAL INSPECTION INFORMATION					***INDEPTH INSPECTION INFORMATION***								
<div>DATE:</div> <div>FREQUENCY:</div> <div>TEAM LEADER:</div> <div>INSPECTOR 2:</div> <div>RESPONSIBILITY:</div> <div>CALCULATED INTERVAL**:</div> <div>INSPECTOR 3:</div> <div>INSPECTOR 4:</div> <div>CATEGORY:</div> <div>NBI:</div> <div>METHOD:</div> <div>** When calculated interval exceeds the frequency, a justification comment per BIRM is required.</div>					<div>DATE:</div> <div>FREQUENCY:</div> <div>TEAM LEADER:</div> <div>INSPECTOR 2:</div> <div>RESPONSIBILITY:</div> <div>CALCULATED INTERVAL**:</div> <div>INSPECTOR 3:</div> <div>INSPECTOR 4:</div> <div>CATEGORY:</div> <div>NBI:</div> <div>METHOD:</div> <div>** When calculated interval exceeds the frequency, a justification comment per BIRM is required.</div>								
FRACTURE CRITICAL INSPECTION COMMENTS					INDEPTH INSPECTION COMMENTS								
SPECIAL INSPECTION INFORMATION					***UNDERWATER INSPECTION INFORMATION***								
<div>DATE: 02/28/2018</div> <div>FREQUENCY: 999</div> <div>TEAM LEADER:</div> <div>INSPECTOR 2: JESSE ELSEMAN</div> <div>RESPONSIBILITY: BRIDGEDIV</div> <div>CALCULATED INTERVAL**:</div> <div>INSPECTOR 3: KEVIN RAITHEL</div> <div>INSPECTOR 4:</div> <div>CATEGORY: QUALITY ASSURANCE</div> <div>NBI: NO</div> <div>METHOD:</div> <div>** When calculated interval exceeds the frequency, a justification comment per BIRM is required.</div>					<div>DATE: 11/04/2024</div> <div>FREQUENCY: 60</div> <div>TEAM LEADER: STEVE RIGHTNOWAR</div> <div>INSPECTOR 2:</div> <div>RESPONSIBILITY: DISTRICT</div> <div>CALCULATED INTERVAL**: 12</div> <div>INSPECTOR 3:</div> <div>INSPECTOR 4:</div> <div>CATEGORY: DRY</div> <div>NBI: NO</div> <div>METHOD: VISUAL</div> <div>** When calculated interval exceeds the frequency, a justification comment per BIRM is required.</div>								
SPECIAL INSPECTION COMMENTS					UNDERWATER INSPECTION COMMENTS								
OTHER SPECIAL INSPECTIONS					OTHER UNDERWATER INSPECTIONS								
<u>DATE</u>	<u>FREQUENCY</u>	<u>CATEGORY</u>	<u>NBI</u>	<u>CALCULATED INTERVAL</u>	<u>RESPONSIBILITY</u>	<u>METHOD</u>	<u>DATE</u>	<u>FREQUENCY</u>	<u>CATEGORY</u>	<u>NBI</u>	<u>CALCULATED INTERVAL</u>	<u>RESPONSIBILITY</u>	<u>METHOD</u>
05/16/2017	999	DAMAGE POST INCIDENT	NO		DISTRICT								
05/05/2014	120	CHANNEL CROSS SECTIONS	NO		DISTRICT	WT TAPE							
Design_No = s0291													
<div>Page 1</div> <div>This report contains information that is protected from disclosure by federal law, 23 USC Section 409 and the Missouri Open Records Law (Sunshine Act), Section 610.021 RSMo. Please review MoDOT's policy and procedure manual on the Sunshine Act before releasing any of the information contained herein.</div>													

		<div>Missouri Department of Transportation</div> <div>State Bridge Inspection Report</div>			<div>September 22, 2025</div> <div>1:40:51PM</div>	
COUNTY: CAPE GIRARDEAU		DISTRICT: SE	CLASS: STATBR	FED-ID: 8651	BRIDGE: S0291	
STRUCTURE POSTING						
APPROVED CATEGORY: S-17		CL OF BR AND TRK OVR 16 T 15MPH ON BR EXCPT SNGL E UNT TRKS WT LMT 21 T OTHR TRKS WT LMT 37 T.				
Ton 1: 16		Ton 2: 21	Ton 3: 37			
COMMENTS:						
FIELD CATEGORY: S-17		CL OF BR AND TRK OVR 16 T 15MPH ON BR EXCPT SNGL E UNT TRKS WT LMT 21 T OTHR TRKS				
Ton 1: 16		Ton 2: 21	Ton 3: 37	PROBLEM:		PROBLEM DIRECTION:
COMMENTS:						
GENERAL COMMENTS/MAJOR RATED ITEMS						
GENERAL COMMENTS: (BOWDEJ1, 09/10/2008)--(41') SMP WF GDR SPAN						
[ITEM 58] DECK: 6-SATISFACTORY CONDITION			COMMENTS: (BLALOR1, 11/19/2013)--TRANS CRACKS, EDGE DETER			
RATING : 05/18/2001						
[ITEM 59] SUPER: 6-SATISFACTORY CONDITION			COMMENTS: (BLALOR1, 11/14/2012)--MINOR PK RUST TOP FLANGE-DECK LIFTING			
RATING : 11/14/2012						
[ITEM 60] SUB: 5-FAIR CONDITION			COMMENTS: (BLALOR1, 11/14/2012)--TIMBER PILE SPLICES			
RATING : 11/15/2019			(ROBINC3, 11/18/2015)--1 OF 4 PILE 50% HOLLOW IN BENT LINE			
			(HESSE, 07/11/2016)--WORK COMPLETED ON PILE. RAISE RATING NEXT INSPECTION			
			(ROBINC3, 11/15/2019)--PILE SPLICES			
[ITEM 61] BANK/CHANNEL: 6-WIDESPREAD MINOR DAMAGE			COMMENTS: (RAITHK, 03/05/2018)--LOWERED TO 6 RATING DUE TO THE MINOR EROSION OF STREAM BANK.			
RATING : 03/05/2018			(CHAPMM1, 11/14/2023)--STREAM MIGRATING TO WEST ABUT			
[ITEM 113] SCOUR: 8-STABLE FOR CALCULATED			COMMENTS:			
RATING : 05/18/2001						
EVALUATION TYPE :						
[ITEM 71] WATERWAY ADEQUACY: DECK ABOVE FLOOD ELEV			COMMENTS:			
RATING : 05/18/2001						
[ITEM 72] APPRRDWY ALIGNMENT: 7-GOOD			COMMENTS:			
RATING : 05/18/2001						
RAILING AND APPROACH PAVEMENT COMPONENTS AND RATINGS						
[ITEM 36A] BRIDGE RAILING RATING: DOESNT MEET CURRNT STND-0			RATING : 02/17/2004	COMMENTS:		
<u>MATERIAL</u>	<u>CONSTRUCTION</u>	<u>DIRECTION</u>	<u>COMMENTS</u>			
REINFORCED CONCRETE	CURB	BOTH				
<u>CONDITION</u>	<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>COMMENT</u>		
SPALLS	RANDOM		MINOR			
VERTICAL CRACKS	THROUGHOUT		MODERATE			
STEEL	ANGLE-DOUBLE	BOTH				
<u>CONDITION</u>	<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>COMMENT</u>		
COLLISION DAMAGE	THROUGHOUT		MINOR	(REHAGM, 03/19/2004)--SOUTH SIDE		
RUSTING	THROUGHOUT		MINOR			
[ITEM 36B] TRANSITION RAILING RATING: NOT PROVIDED-0			RATING : 05/18/2001	COMMENTS:		
[ITEM 36C] APPROACH RAILING RATING: NOT PROVIDED-0			RATING : 05/18/2001	COMMENTS:		
Design_No = s0291						
Page 2						
This report contains information that is protected from disclosure by federal law, 23 USC Section 409 and the Missouri Open Records Law (Sunshine Act), Section 610.021 RSMo. Please review MoDOT's policy and procedure manual on the Sunshine Act before releasing any of the information contained herein.						





Missouri Department of Transportation

State Bridge Inspection Report

September 22, 2025
1:40:51PM

COUNTY: CAPE GIRARDEAU DISTRICT: SE CLASS: STATBR FED-ID: 8651 BRIDGE: S0291


SPALLS	THROUGHOUT	MEDIUM	(SHUNAT1, 04/04/2018)--W/ REBAR EXPOSED
TRANSVERSE CRACKS	THROUGHOUT	MEDIUM	(ROBINC3, 12/12/2019)--W/RUST


SUPERSTRUCTURE COMPONENTS

<u>SERIES TYPE-#</u>	<u>SPAN TYPE</u>	<u>MATERIAL</u>	<u>CONSTRUCTION</u>	<u>LABEL</u>	<u>COMMENTS</u>
MAIN SERIES-1	SIMPLE SPAN	STEEL	WIDE FLANGE GIRDERS		
<u>SPAN</u>	<u>COMPOSITE INDICATOR</u>	<u>LENGTH</u>	<u>WEATHERING STEEL</u>	<u>COMMENTS</u>	
MAIN SPANS-1	NON-COMPOSITE	41 FT 0 IN	NO	(BLALOR1, 11/13/2014)--MINOR DECK POUNDING	
<u>CONDITION</u>	<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
AGAINST BACKWALL	AT GIRDERS		NOT APPLICABLE		(BLALOR1, 12/03/2013)--BOTH ABUTS
DECK LIFTING	THROUGHOUT		MINOR		(BLALOR1, 12/11/2012)--1/2"
PACK RUST	TOP FLANGE		MODERATE		(BLALOR1, 12/11/2012)--EXTERIOR BEAMS
RUSTING	BOTTOM FLANGE		MINOR		
RUSTING	TOP FLANGE		MINOR		

SUBSTRUCTURE COMPONENTS

<u>SUBSTRUCTURE</u>	<u>SKREW</u>	<u>LENGTH</u>	<u>MATERIAL</u>	<u>CONSTRUCTION</u>	<u>LABEL</u>	<u>COMMENTS</u>
ABUTMENT-1		20 FT 6 IN	TIMBER	NON-INTEGRAL		
	<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>
<u>ASSOCIATED COMPONENT</u>			<u>MATERIAL</u>	<u>CONSTRUCTION</u>		<u>COMMENT</u>
BEAM CAP			TIMBER	BEAM		
	<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>COMMENT</u>
	CRUSHING		RANDOM		MINOR	(ROBINC3, 11/18/2015)--PILE #2
	SPLITTING		THROUGHOUT		MINOR	
PILING			TIMBER	OTHER		
	<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>COMMENT</u>
	DECAYING		THROUGHOUT		MINOR	
	HOLLOW		THROUGHOUT		NOT APPLICABLE	(ROBINC3, 11/18/2015)--MODERATE - PILE #2 DRILLED 11/12/15
	REPLACE WITH H PILE		1 OF 4		NOT APPLICABLE	(BLALOR1, 12/11/2012)--NORTH EXT
	SPLITTING		THROUGHOUT		MINOR	
STRAIGHT WINGS			STEEL	OTHER		
	<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>COMMENT</u>
WING PILES			TIMBER	OTHER		
	<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>COMMENT</u>
	DECAYING		THROUGHOUT		HEAVY	
BACKWALL			TIMBER	PLANKS		
	<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>COMMENT</u>
FIXED BEARING			STEEL	FLAT PLATE		
	<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>COMMENT</u>
	RUSTING		THROUGHOUT		MINOR	
ABUTMENT-2		20 FT 6 IN	TIMBER	NON-INTEGRAL		
	<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>COMMENT</u>
<u>ASSOCIATED COMPONENT</u>			<u>MATERIAL</u>	<u>CONSTRUCTION</u>		
BEAM CAP			TIMBER	BEAM		
	<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>COMMENT</u>
	CRUSHING		INTERIOR COLUMN		MINOR	
	SPLITTING		THROUGHOUT		MINOR	
PILING			TIMBER	OTHER		
	<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>COMMENT</u>
	DECAYING		GROUND LINE		MINOR	

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Missouri Department of Transportation
Bridge Inventory and Inspection System
Structural Inventory & Appraisal Sheet

September 22, 2025
1:42:01pm

COUNTY : CAPE GIRARDEAU BRIDGE : S0291 REVIEW STATUS : APPROVED NBI STATUS : T
RECORD TYPE : ROUTE CARRIED 'ON' STRUCT RUN DATE : 7/16/2025 SUBMITTAL YEAR : 2025

GENERAL STRUCTURE INFORMATION			ROUTE DESIGNATION INFORMATION		
1	State	MISSOURI	5A	Record Type	ROUTE CARRIED 'ON' STRUCT
2	District	SE	5B	Route Signing Prefix	MO
3	County	CAPE GIRARDEAU	5C	Designated Level of Service	MAINLINE
8	Federal ID No.	8651	5D	Route Number	0000N
27	Year Built	1932	5E	Directional Suffix	NOT APPLICABLE
106	Year Reconstructed	0	7	Facility Carried	RT N E
42A	Type of Service On	HIGHWAY	12	Base Hwy. Network	NO
21	Structure Maintenance	STATE HIGHWAY AGENCY	13A	LRS Inventory Route No.	
22	Structure Owner	STATE HIGHWAY AGENCY	13B	Subroute No.	
33	Br. Median Code	NO MEDIAN	20	Toll Status	ON FREE ROAD
37	Historical Significance	NOT ELIGIBLE FOR NR OF HP	26	Functional Classification	07-RURAL MAJOR COLLECTOR
101	Parallel Struc Desg	NONE EXISTS	28A	Lanes on Structure	01
103	Temporary Structure	NOT TEMPORARY	100	STRAHNET Designation	RTE NOT A DEFENSE HWY
112	NBIS Bridge Length	YES	104	National Highway System	NOT ON NHS
			105	Federal Lands Highway	NOT APPLICABLE
			110	Designated Nat. Network	NO
STRUCTURE LOCATION INFORMATION			STRUCTURE TRAFFIC INFORMATION		
4	Place	WELCH	29	AADT	2155
	Code	78280	30	AADT Year	2024
9	Location	S 5 T 29 N R 12 E	102	Direction of Traffic	ONE LANE BRIDGE FOR 2-WAY
11	Milepoint	16.02 miles	109	AADT Truck Percent	3%
16	Latitude	37 D 12 M 35 S	114	Future AADT	3340
17	Longitude	89 D 44 M 53 S	115	Future AADT Year	2044
UNDERRECORD INFORMATION			STRUCTURE GEOMETRIC INFORMATION		
6	Features Intersected	WHITEWATER RVR	10	Inventory Rte. Vert. Clear	99 Ft. 99 In.
42B	Type of Service Under	WATERWAY	19	By pass Detour Length	21.88 miles
28B	Lanes Under Structure	00	32	Approach Roadway Width	20 Ft. 0 In.
54A	Vert. Clearance Ref.	N/A	34	Skew	0.00 Degrees
54B	Vert. Clearance	0 Ft. 0 In.	35	Struct. Flared	NO
55A	Rt. Lat Clear Ref.	N/A	47	Total Horiz. Clear	20 Ft. 0 In.
55B	Rt. Lat Clearance	0 Ft. 0 In.	48	Maximum Span Length	41 Ft. 0 In.
56	Left Lat Clearance	0 Ft. 0 In.	49	Structure Length	51 Ft. 10 In.
38	Navigation Control	PERMIT NOT REQ	50A	Left Curb/Sidewalk Width	0 Ft. 0 In.
39	Nav Vertical Clear	0 Ft. 0 In.	50B	Right Curb/Sidewalk Width	0 Ft. 0 In.
40	Nav Horizontal Clear	0 Ft. 0 In.	51	Curb to Curb Br. Width	20 Ft. 0 In.
111	Nav. Pier Protection		52	Deck Width (Out-Out)	20 Ft. 12 In.
116	Nav. Cl. Vert. Clear		53	Vert. Clearance Over Deck	99 Ft. 99 In.

Design_No = s0291 and Inventory_Appraisal_Submittal_Year = 2025



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LOAD RATING AND POSTING INFORMATION			MATERIAL/CONSTRUCTION INFORMATION		
31	Design Load	H 10	43A	Main Struc. Mat type	STEEL
41	Structure Status	POSTED FOR LOAD	43B	Main struc Constr. Type	STRINGER/MULTIBEAM - GRD
63	Oper. Rating Meth.	LOAD FACTOR	45	# of Main Spans	1
64	Operating Rating	19 Tons.	44A	Appr Struc. Mat type	000
65	Inventory Rating Meth	LOAD FACTOR	44B	Appr Struc. Cnstr. type	000
66	Inventory Rating	12 Tons.	46	# of Approach Span	0
70	Bridge Posting Code	30.0-39.9% BELOW	107	Deck Mat/Constr.	1 CONCRETE CIP
PROPOSED IMPROVEMENT INFORMATION			108A	Wear Surf Mat/Constr.	6 BITUMINOUS
Sufficiency Rating 40.6 Percent			108B	Membrane Mat/Constr.	0 NONE
Deficiency Rating STRUCTURAL			108C	Deck Protect Mat/Constr.	0 NONE
Funding Eligibility FULL			CONDITION RATING INFORMATION		
75A	Proposed Work	REPLACEMENT SUBSTND LOAD	58	Deck Cond. Rating	6
75B	Work Done By	Contract	59	Superstructure Cond. Rating	6
76	New Struc Length	75 Ft. 6 In.	60	Substructure Cond. Rating	5
94	Struc Improve Cost	\$ 480,000	61	Channel /Channel Protection Cond. Rating	6
95	Roadway Improve Cost	\$ 48,000	62	Culvert Cond. Rating	N
96	Total Project Cost	\$ 720,000	INSPECTION INFORMATION		
97	Year of Cost Estimates	2025	90	Gen. Insp Date	11 / 24
APPRAISAL RATING INFORMATION			91	Gen. Insp. Frequency	12 Months
36A	Br. Rail App. Rating	DOES NOT MEET ACCEPT STND	92A	Frac. Critical Inspection	N Months
36B	Transition Rail App. Rating	DOES NOT MEET ACCEPT STND	93A	Frac. Critical Insp. Date	
36C	Approach Rail App. Rating	DOES NOT MEET ACCEPT STND	92B	Underwater Inspection	N Months
36D	Rail End Treat. App. Rating	DOES NOT MEET ACCEPT STND	93B	Underwater Insp. Date	
67	Struc Eval App. Rating	2	92C	Special Inspection	N Months
68	Deck Geometry App. Rating	2	93C	Special Inspection Date	
69	Underclearance App. Rating	N	BORDER BRIDGE INFORMATION		
71	Waterway Adeq. App. Rating	8	98	Neighboring State Code	
72	Approach Road App. Rating	7	98B	Neighboring State % Respon	
113	Scour Assess App. Rating	8	99	Neighboring State Struc. No.	
APPROVED POSTING INFORMATION			FIELD POSTING INFORMATION		
Approved Posting Category S-17			Field Posting Category S-17		
Ton1 Ton2 Ton3			Ton1 Ton2 Ton3		
Tonnage Values for Posting Sign 16 21 37			Tonnage Values for Posting Sign 16 21 37		
General Text for Posting Sign			General Text for Posting Sign		
CL OF BR AND TRK OVR 16 T 15MPH ON BR EXCPT SNGL E UNT TRKS WT LMT 21 T OTHR TRKS WT LMT 37 T.			CL OF BR AND TRK OVR 16 T 15MPH ON BR EXCPT SNGL E UNT TRKS WT LMT 21 T OTHR TRKS WT LMT 37 T.		

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