

ADDENDUM NO. 1

COVER SHEET

08/29/2025

CITY OF ROLLA
TAXIWAY A RECONSTRUCTION AND TAXIWAY CONNECTOR REHABILITATION
at
ROLLA NATIONAL AIRPORT (VIH)
VICHY, MO

MoDOT Project # 23-056A-1

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DATE: 08/29/2025

ADDENDUM NO. 1

**CITY OF ROLLA
TAXIWAY A RECONSTRUCTION AND TAXIWAY CONNECTOR REHABILITATION
at
ROLLA NATIONAL AIRPORT (VIH)
VICHY, MO**

MoDOT Project # 23-056A-1

TO: All Plan-holders of Record

The following addendum items supplement, clarify, modify, change, replace, delete from or add to, the requirements of the contract documents for this project. The articles contained in the addendum take precedence over the requirements of the previously published contract documents. Where any article of the contract specifications or any detail of the contract drawings is modified or any paragraph, subparagraph or clause thereof is modified or deleted by the articles contained in this addendum, the unaltered provisions of that article, paragraph, subparagraph or clause shall remain in effect.

PREPARED BY: Woolpert, Inc.
931 Wildwood Drive, Suite 101
Jefferson City, MO 65109

CERTIFIED BY:



Laura Koonce, Project Manager

ADDENDUM BEGINS

PROJECT MANUAL UPDATES

1. Page 8 of 396 – Engineer’s Certifications Part 8: FAA Technical Specifications

Revision: Removed specification to Item P-601 Crack Repair.

Justification: Crack repair will now follow the specification outlined in Item P-101, *Preparation and Removal of Existing Pavements*.

Revision: Relocated Items D-705 and D-751 to follow the Item C-110 specification.

Justification: The items were reordered to align with the sequence in which the specifications were attached.

2. Page 11 of 396 – Approximate Quantities of Major Bid Items Table

Revision: Updated quantity for Item P-101b Asphalt Pavement Removal – Partial Depth, Schedule I.

Justification: The revised quantity accounts for the expanded butt joint removal area, extending to the proposed taper limits on Taxiway A.

Revision: Added Item P-101c Joint and Crack Repair to Schedule II.

Justification: Crack repair will be accounted for under new Item P-101c Joint and Crack Repair. This item has been included in case crack repair below mill and fill area is needed. If no crack repair is required, the item will not be performed.

Revision: Corrected quantities for Item P-152a Unclassified Excavation in Schedules II and III.

Justification: The quantity listed under Schedule II was a typo; no earthwork is required for the mill and fill. Additionally, the quantity for Schedule III was previously omitted and has now been included.

Revision: Updated quantity for Item P-208a Aggregate Base Course, 8 Inches in Schedule I.

Justification: The change reflects a reduced overbuild width, resulting from a modification to the depth of the underdrain structure.

Revision: Updated quantity for P-208b Geotextile Fabric in Schedule I.

Justification: The adjustment reflects a reduced overbuild width due to a change in the depth of the underdrain structure.

Revision: Updated quantity for Item P-401a Asphalt Surface Course in Schedules I and II.

Justification: The adjustment reflects an increase in butt joint area along Taxiway A near the apron, and the removal of butt joints at Taxiway B.

Revision: Updated quantity for Item P-603a Bituminous Tack Coat in Schedule I.

Justification: The adjustment accounts for the increased butt joint area along Taxiway A near the apron.

Revision: Updated quantity for Item P-620b Permanent Airport Pavement Marking, Yellow in Schedule I.

Justification: The revision accounts for additional yellow paint needed for runway lead-in lines and taxiway centerlines that will be obliterated or covered during construction.

3. Page 12 of 396 – Approximate Quantities of Major Bid Items Table (Continued)

Revision: Updated quantity for Item D-705b Install 6-Inch Non-Perforated Polyethylene Pipe in Schedule III.

Justification: The revision accounts for additional outfalls and the increased pipe length needed to reach the appropriate surface elevation for outfall.

Revision: Updated quantity for Item D-751a Install 6-Inch Underdrain Cleanout in Schedule III.

Justification: The revision reflects the replacement of cleanouts with inspection pits.

Revision: Updated quantity for Item D-751b Install 18x18 Inch Inspection Pit in Schedule III.

Justification: The revision reflects the addition of inspection pits that replaced the originally planned cleanouts.

Revision: Added Item T-905a Topsoil.

Justification: Topsoil was not included in the original bid proposal and has now been added.

Revision: Corrected item description for Item L-110a Install 1-2" SCH 40 PVC Conduit, Direct Earth Buried (DBE).

Justification: The abbreviation "DBE" was a typo; the correct abbreviation is "DEB" for Direct Earth Buried.

Revision: Corrected item description for Item L-125c Install L-861T LED Taxiway Edge Light, Complete.

Justification: The next item to come after L-125a should be L-125b. The item labeled as L-125c is a typo.

4. Page 25 of 396 – Sales and Use Taxes

Revision: Updated section regarding the sponsor's tax exemption number.

Justification: The tax exemption number was not included in the original documentation but will be provided after the award.

5. Page 80 of 396 – Schedule I – Taxiway A Reconstruction Bid Proposal Sheet

Revision: Updated quantity for Item P-101b Asphalt Pavement Removal – Partial Depth.

Justification: The revised quantity accounts for the expanded butt joint removal area, extending to the proposed taper limits on Taxiway A.

Revision: Updated quantity for Item P-208a Aggregate Base Course, 8 Inches.

Justification: The change reflects a reduced overbuild width, resulting from a modification to the depth of the underdrain structure.

Revision: Updated quantity for P-208b Geotextile Fabric.

Justification: The adjustment reflects a reduced overbuild width due to a change in the depth of the underdrain structure.

Revision: Updated quantity for Item P-401a Asphalt Surface Course.

Justification: The adjustment reflects an increase in butt joint area along Taxiway A near the apron.

Revision: Updated quantity for Item P-603a Bituminous Tack Coat.

Justification: The adjustment accounts for the increased butt joint area along Taxiway A near the apron.

Revision: Updated quantity for Item P-620b Permanent Airport Pavement Marking, Yellow.

Justification: The revision accounts for additional yellow paint needed for runway lead-in lines and taxiway centerlines that will be obliterated or covered during construction.

Revision: Added Item T-905a Topsoil.

Justification: Topsoil was not included in the original bid proposal and has now been added.

Revision: Corrected item description for Item L-110a Install 1-2" SCH 40 PVC Conduit, Direct Earth Buried (DBE).

Justification: The abbreviation "DBE" was a typo; the correct abbreviation is "DEB" for Direct Earth Buried.

Revision: Corrected item description for Item L-125c Install L-861T LED Taxiway Edge Light, Complete.

Justification: The next item to come after L-125a should be L-125b. The item labeled as L-125c is a typo.

6. Page 81 of 396 – Schedule II – Taxiway B Rehabilitation Bid Proposal Sheet

Revision: Added Item P-101c Joint and Crack Repair.

Justification: Crack repair will be accounted for under new Item P-101c Joint and Crack Repair. This item has been included in case crack repair below mill and fill area is needed. If no crack repair is required, the item will not be performed.

Revision: Updated quantity for Item P-401a Asphalt Surface Course.

Justification: The adjustment reflects the removal of butt joints at Taxiway B.

7. Page 82 of 396 – Schedule III – Taxiway A Underdrains Bid Proposal Sheet

Revision: Updated quantity for Item D-705b Install 6-Inch Non-Perforated Polyethylene Pipe in Schedule III.

Justification: The revision accounts for additional outfalls and the increased pipe length needed to reach the appropriate surface elevation for outfall.

Revision: Updated quantity for Item D-751a Install 6-Inch Underdrain Cleanout in Schedule III.

Justification: The revision reflects the replacement of cleanouts with inspection pits.

Revision: Updated quantity for Item D-751b Install 18x18 Inch Inspection Pit in Schedule III.

Justification: The revision reflects the addition of inspection pits that replaced the originally planned cleanouts.

8. Page 86 of 396 – Approximate Quantities of Major Bid Items Table

Revision: Updated quantity for Item P-101b Asphalt Pavement Removal – Partial Depth, Schedule I.

Justification: The revised quantity accounts for the expanded butt joint removal area, extending to the proposed taper limits on Taxiway A.

Revision: Added Item P-101c Joint and Crack Repair to Schedule II.

Justification: Crack repair will be accounted for under new Item P-101c Joint and Crack Repair. This item has been included in case crack repair below mill and fill area is needed. If no crack repair is required, the item will not be performed.

Revision: Corrected quantities for Item P-152a Unclassified Excavation in Schedules II and III.

Justification: The quantity listed under Schedule II was a typo; no earthwork is required for the mill and fill. Additionally, the quantity for Schedule III was previously omitted and has now been included.

Revision: Updated quantity for Item P-208a Aggregate Base Course, 8 Inches in Schedule I.

Justification: The change reflects a reduced overbuild width, resulting from a modification to the depth of the underdrain structure.

Revision: Updated quantity for P-208b Geotextile Fabric in Schedule I.

Justification: The adjustment reflects a reduced overbuild width due to a change in the depth of the underdrain structure.

Revision: Updated quantity for Item P-401a Asphalt Surface Course in Schedules I and II.

Justification: The adjustment reflects an increase in butt joint area along Taxiway A near the apron, and the removal of butt joints at Taxiway B.

Revision: Updated quantity for Item P-603a Bituminous Tack Coat in Schedule I.

Justification: The adjustment accounts for the increased butt joint area along Taxiway A near the apron.

9. Page 87 of 396 – Approximate Quantities of Major Bid Items Table

Revision: Updated quantity for Item P-620b Permanent Airport Pavement Marking, Yellow in Schedule I.

Justification: The revision accounts for additional yellow paint needed for runway lead-in lines and taxiway centerlines that will be obliterated or covered during construction.

Revision: Updated quantity for Item D-705b Install 6-Inch Non-Perforated Polyethylene Pipe in Schedule III.

Justification: The revision accounts for additional outfalls and the increased pipe length needed to reach the appropriate surface elevation for outfall.

Revision: Updated quantity for Item D-751a Install 6-Inch Underdrain Cleanout in Schedule III.

Justification: The revision reflects the replacement of cleanouts with inspection pits.

Revision: Updated quantity for Item D-751b Install 18x18 Inch Inspection Pit in Schedule III.

Justification: The revision reflects the addition of inspection pits that replaced the originally planned cleanouts.

Revision: Added Item T-905a Topsoil.

Justification: Topsoil was not included in the original bid proposal and has now been added.

10. Page 88 of 396 – Approximate Quantities of Major Bid Items Table

Revision: Corrected item description for Item L-110a Install 1-2" SCH 40 PVC Conduit, Direct Earth Buried (DBE).

Justification: The abbreviation "DBE" was a typo; the correct abbreviation is "DEB" for Direct Earth Buried.

Revision: Corrected item description for Item L-125c Install L-861T LED Taxiway Edge Light, Complete.

Justification: The next item to come after L-125a should be L-125b. The item labeled as L-125c is a typo.

11. Page 96 of 396 – Warranty Bond Form

Revision: Added sample Warranty Bond Form following page 96.

Justification: The sample was included to serve as a reference guide for preparing the Warranty Bond.

12. Page 172 of 396 – Liquidated Damages

Revision: Updated liquidated damages cost per calendar day for the Construction Manager and Resident Engineer.

Justification: These costs have been included for clarification.

13. Page 224 of 396 – Construction Safety and Phasing Plan Cover Sheet

Revision: Removed reference to Bid Package No. 2 and added "Issued for Bid" to the title.

Justification: Bid Package No. 2 is not applicable to this document, as no phasing is associated with it.

14. Page 257 of 396 – Table of Contents Technical Specifications

Revision: Added a Table of Contents for the specifications section.

Justification: This addition improves navigation by allowing easier location of technical specifications in sequential order.

15. Page 262 of 396 – Item C-100 Contractor Quality Control Program

Revision: Added payment unit type to the specification.

Justification: The specification previously did not state the unit by which this item is to be paid; this addition clarifies the payment structure.

16. Page 281 of 396 – Item D-705 Pipe Underdrains for Airports

Revision: Removed “(mm pipe)” from the item description under the payment section.

Justification: This is not relevant since the pipe is being measured in linear feet.

17. Page 288 of 396 – Item P-101 Preparation/Removal of Existing Pavements

Revision: Added Specification Item P-101 – *Preparation/Removal of Existing Pavements* after page 288.

Justification: This specification was not included in the original project manual and is necessary for project completeness and clarity.

18. Page 313 of 396 – Item P-208 Aggregate Base Course

Revision: Removed reference to *Appendix P*.

Justification: The appendix is not applicable to this project manual.

19. Page 315 of 396 – Item P-401 Asphalt Mix Pavement

Revision: Removed the requirement for aircraft gross weights of 60,000 pounds or more from the Coarse Aggregate Material Requirements table.

Justification: The project is designed for aircraft with gross weights under 60,000 pounds, making this requirement unnecessary.

20. Page 319 of 396 – Item P-401 Asphalt Mix Pavement

Revision: Removed the Asphalt Pavement Analyzer (APA) and corresponding note from Table 1. Asphalt Design Criteria.

Justification: The APA is not required for projects involving aircraft with gross weights under 60,000 pounds.

21. Page 341 of 396 – Item P-605 Joint Sealants for Pavements

Revision: Added Specification Item P-605 – *Joint Sealants for Pavements* after page 341.

Justification: This specification was missing from the original project manual and is necessary for completeness.

22. Page 347 of 396 – Item P-610 Concrete for Miscellaneous Structures

Revision: Removed reference to *Appendix P*.

Justification: The appendix is not applicable to this project manual.

23. Page 350 of 396 – Item P-620 Runway and Taxiway Marking

Revision: Added temporary paint application rate.

Justification: To clarify that the application rate for temporary pavement markings differs from that of permanent markings.

24. Page 364 of 396 – Item T-905 Topsoil

Revision: Updated Section 905-3.4 Placing Topsoil.

Justification: Modified the specified depth for topsoil placement to better align with project requirements.

25. Page 365 of 396 – Item T-905 Topsoil

Revision: Updated Method of Measurement and Basis of Payment sections.

Justification: The method of measurement was revised to be by area instead of volume. The appropriate payment item and measurement detail was added. Additionally, references to topsoil being obtained off-site were removed to reflect the utilization of stockpiling and placement of on-site material.

CONSTRUCTION DRAWING UPDATES

1. Sheet G003 – General Notes (Sheet 3 of 54)

Revision: Updated notes throughout the entire sheet.

Justification: Several notes were outdated or contradictory and have been revised for consistency and accuracy.

2. Sheet G004 – Summary of Approximate Quantities (Sheet 4 of 54)

Revision: Updated quantity for Item P-101b Asphalt Pavement Removal – Partial Depth, Schedule I.

Justification: The revised quantity accounts for the expanded butt joint removal area, extending to the proposed taper limits on Taxiway A.

Revision: Added Item P-101c Joint and Crack Repair to Schedule II.

Justification: Crack repair will be accounted for under new Item P-101c Joint and Crack Repair. This item has been included in case crack repair below mill and fill area is needed. If no crack repair is required, the item will not be performed.

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Justification: The revision reflects the replacement of cleanouts with inspection pits.

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Justification: The abbreviation "DBE" was a typo; the correct abbreviation is "DEB" for Direct Earth Buried.

Revision: Corrected item description for Item L-125c Install L-861T LED Taxiway Edge Light, Complete.

Justification: The next item to come after L-125a should be L-125b. The item labeled as L-125c is a typo.

Revision: Updated Detail 1 Earthwork Calculations Detail (Excavation).

Justification: Numbered items were clarified as either paid or incidental to Items P-152a and T-905a. Additionally, a note was added to specify the depths for stripped and placed topsoil.

3. Sheet C100 – Demolition Plan Taxiway A (Sheet 20 of 54)

Revision: Extended butt joint limits to align with asphalt pavement construction.

Justification: The previously shown butt joint stopped short of the new taper edge. This update ensures the joint extends to the correct location for proper pavement transition.

Revision: Added temporary removal of markings leading to closed areas.

Justification: All taxilane centerlines and runway lead-in lines leading to closed taxiway areas must be obliterated or covered in accordance with FAA Advisory Circular 150/5370-2G.

4. Sheet C101 – Demolition Plan Taxiway A (Sheet 21 of 54)

Revision: Added temporary removal of markings leading to closed areas.

Justification: All taxilane centerlines and runway lead-in lines leading to closed taxiway areas must be obliterated or covered in accordance with FAA Advisory Circular 150/5370-2G. Additionally, a matchline was added to reference the viewport containing the remainder of the lead-in line for clarity and continuity.

5. Sheet C102 – Demolition Plan Taxiway B (Taxiway Connector) - (Sheet 22 of 54)

Revision: Removed saw cuts and butt joints for Taxiway B.

Justification: This area will undergo a mill and fill process, eliminating the need for butt joints or saw cutting along the taxiway connector.

Revision: Added temporary removal of markings leading to closed areas.

Justification: All taxilane centerlines and runway lead-in lines leading to closed taxiway areas must be obliterated or covered in accordance with FAA Advisory Circular 150/5370-2G.

Revision: Added viewport for paint obliteration/cover of runway lead-in line from Runway 4/22 to Taxiway A.

Justification: A viewport and matchline were added to clearly show the obliteration or covering of the runway lead-in line for construction purposes.

6. Sheet C200 – Geometry Plan Taxiway A (STA. 00+00 – 10+50) - (Sheet 23 of 54)

Revision: Extended butt joint limits to match asphalt pavement construction.

Justification: The butt joint shown previously stopped short of where the new taper edge will be.

7. Sheet C202 – Geometry Plan Taxiway B (Taxiway Connector) - (Sheet 25 of 54)

Revision: Removed butt joints for Taxiway B.

Justification: Since this will be a mill and fill, there is no need for butt joints along the taxiway connector.

8. Sheet C220 – Pavement Typical Sections - (Sheet 26 of 54)

Revision: Slope labels added to detail A Taxiway A Pavement Section (North-South)

Justification: The transverse slope of the pavement section was not called out.

Revision: Updated Cement Treated Subgrade (CTS) item on detail A Taxiway A Pavement Section (North-South)

Justification: The 18" of CTS was not called out under the exact item to be paid.

Revision: Note added to detail A Taxiway A Pavement Section (North-South)

Justification: This is to clarify the width of overbuild that is needed for the pavement section.

9. Sheet C300 – Grading and Spot Elevation Plan Taxiway A (STA. 00+00 – 05+00) - (Sheet 27 of 54)

Revision: Proposed surface updated to meet FAA standards.

Justification: The surface has been updated to meet TSA/TOFA grading standards per AC 150/5300-13B.

Revision: Added spot elevations every 50 feet.

Justification: Spot elevations are needed every 50 feet at the edge lines and centerlines of Taxiway A.

10. Sheet C301 – Grading and Spot Elevation Plan Taxiway A (STA. 05+00 – 10+00) - (Sheet 28 of 54)

Revision: Proposed surface updated to meet FAA standards.

Justification: The surface has been updated to meet TSA/TOFA grading standards per AC 150/5300-13B.

Revision: Added spot elevations every 50 feet.

Justification: Spot elevations are needed every 50 feet at the edge lines and centerlines of Taxiway A.

11. Sheet C302 – Grading and Spot Elevation Plan Taxiway A (STA. 10+00 – 15+00) - (Sheet 29 of 54)

Revision: Proposed surface updated to meet FAA standards.

Justification: The surface has been updated to meet TSA/TOFA grading standards per AC 150/5300-13B.

Revision: Added spot elevations every 50 feet.

Justification: Spot elevations are needed every 50 feet at the edge lines and centerlines of Taxiway A.

12. Sheet C303 – Grading and Spot Elevation Plan Taxiway A (STA. 15+00 – 19+00) - (Sheet 30 of 54)

Revision: Proposed surface updated to meet FAA standards.

Justification: The surface has been updated to meet TSA/TOFA grading standards per AC 150/5300-13B.

Revision: Added spot elevations every 50 feet.

Justification: Spot elevations are needed every 50 feet at the edge lines and centerlines of Taxiway A.

13. Sheet C401 – Taxiway A Plan and Profile STA. 06+50 – 13+00 - (Sheet 33 of 54)

Revision: Added existing and proposed grade labels to profile stationing.

Justification: These labels were blank on some of the profiles.

14. Sheet C402 – Taxiway A Plan and Profile STA. 13+00 – 19+50 - (Sheet 34 of 54)

Revision: Added existing and proposed grade labels to profile stationing.

Justification: These labels were blank on some of the profiles.

15. Sheet C530 – Underdrain Plan Taxiway A (STA. 00+00 – 05+00) - (Sheet 37 of 54)

Revision: Replaced previous cleanout, CO NO. 11 with inspection pit.

Justification: An inspection pit is needed at this location.

Revision: Added outfall to previous inspection pit, IP NO. 4.

Justification: The inspection pit needed a location to outfall.

Revision: Adjusted inverts elevations for all cleanouts and inspection pits.

Justification: The invert elevations were raised to better meet the existing conditions.

Revision: Renamed all cleanouts and inspection pits.

Justification: The numbering sequence has been updated to reflect the IP and CO location changes.

16. Sheet C531 – Underdrain Plan Taxiway A (STA. 05+00 – 10+00) - (Sheet 38 of 54)

Revision: Replaced previous cleanout, CO NO. 1 with inspection pit.

Justification: An inspection pit is needed at this location. The structure was also shifted north to match the construction phasing.

Revision: Added matchline at STA. 05+00 and updated sheet number for matchline at STA. 10+00.

Justification: The matchline was incorrect on either side of the sheet.

Revision: Adjusted inverts elevations for all cleanouts and inspection pits.

Justification: The invert elevations were raised to better meet the existing conditions. A new outfall was added to previous IP NO. 6.

Revision: Renamed all cleanouts and inspection pits.

Justification: The numbering sequence has been updated to reflect the IP and CO location changes.

17. Sheet C532 – Underdrain Plan Taxiway A (STA. 10+00 – 15+00) - (Sheet 39 of 54)

Revision: Added matchline at STA. 10+00 and updated sheet number for matchline at STA. 15+00.

Justification: The matchline was incorrect on either side of the sheet.

Revision: Replaced previous cleanouts, CO NO. 3, 21, and 22 with inspection pits.

Justification: Inspection pits were needed at these locations.

Revision: Adjusted inverts elevations for all cleanouts and inspection pits.

Justification: The invert elevations were raised to better meet the existing conditions.

Revision: Renamed all cleanouts and inspection pits.

Justification: The numbering sequence has been updated to reflect the IP and CO location changes.

18. Sheet C533 – Underdrain Plan Taxiway A (STA. 15+00 – 19+00) - (Sheet 40 of 54)

Revision: Added matchline at STA. 15+00 and removed matchline at STA. 19+00.

Justification: The matchline was incorrect on either side of the sheet.

Revision: Replaced previous IP NO. 8 with a cleanout.

Justification: This was replaced with a cleanout because it was the start of a line.

Revision: Adjusted inverts elevations for all cleanouts and inspection pits.

Justification: The invert elevations were raised to better meet the existing conditions.

Revision: Renamed all cleanouts and inspection pits.

Justification: The numbering sequence has been updated to reflect the IP and CO location changes.

19. Sheet C550 – Underdrain Details and Notes - (Sheet 41 of 54)

Revision: Updated Detail 1B numbering.

Justification: The numbering was revised to follow a logical sequencing order.

Revision: Updated Detail 2 *Underdrain Trench Section (Adjacent to Pavement) Detail*

Justification: This was revised to match the adjusted invert elevations.

20. Sheet C700 – Pavement Marking Plan Taxiway A - (Sheet 42 of 54)

Revision: Updated reference to detail on sheet C720.

Justification: Detail 4 was removed because it was the same as detail 1.

Revision: Added yellow paint for taxilane centerline between Taxiway A and the Taxiway Connector.

Justification: This was added to show the repainting of the taxilane centerline which was obliterated/covered for construction.

21. Sheet C701 – Pavement Marking Plan Taxiway A - (Sheet 43 of 54)

Revision: Updated reference to detail on sheet C720.

Justification: Detail 4 was removed because it was the same as detail 1.

Revision: Added yellow paint for runway lead-in line from Runway 4/22 to Taxiway A.

Justification: This was added to show the repainting of the runway lead-in line which was obliterated/covered for construction. A matchline referencing a viewport with the remainder of the lead-in line was also added.

22. Sheet C702 – Pavement Marking Plan Taxiway B Connector - (Sheet 44 of 54)

Revision: Updated reference to detail on sheet C720.

Justification: Detail 4 was removed because it was the same as detail 1.

Revision: Added yellow paint for taxilane centerline between Taxiway A and the Taxiway Connector.

Justification: This was added to show the repainting of the taxilane centerline which was obliterated/covered for construction.

Revision: Added viewport for yellow paint runway lead-in line from Runway 4/22 to Taxiway A.

Justification: A viewport and matchline were added to show the repainting of the runway lead-in line which was obliterated/covered for construction.

Revision: Updated the dimension from Runway 4/22 centerline to the runway holding position.

Justification: The previous distance was not to the runway holding position edge.

23. Sheet C720 – Pavement Marking Details and Notes - (Sheet 45 of 54)

Revision: Added dimensions to black borders for details 1 and 2.

Justification: This was to call out the width of the black borders.

Revision: Added note to details 1 and 3.

Justification: Needed clarification regarding black borders on asphalt pavement.

Revision: Added black paint to detail 3.

Justification: This is the correct detail. Refer to added note for paint on asphalt pavement.

Revision: Detail 4 removed.

Justification: Detail 4 was removed because it was the same as detail 1.

CLARIFICATIONS / RFI RESPONSES

1. **Question:** Items L-125a and L-125c are not shown in Bid Package No. 1 Page 135 of PDF. P-152a is shown in Bid Package 1 Schedule III but not in contract documents PDF page 141.

Answer: All bid proposal sheets have been updated to include all items.

PRE-BID MEETING AGENDA AND SIGN-IN SHEET

The pre-bid meeting minutes, sign-in sheet, and planholder's list are attached to this addendum.

Questions will be accepted via written format to the Project Manager until 4:00 P.M. (CST) Friday, September 5, 2025.

ADDENDUM ENDS



Federal Provisions

State Provisions

Local Provisions

PART 6: WAGE RATES

Davis Bacon Wage Rates (or State)

PART 7: SAFETY DOCUMENTS

Construction Safety and Phasing Plan (CSPP)

Construction Safety and Phasing Compliance Document (CSPCD)

PART 8: FAA TECHNICAL SPECIFICATIONS

ITEM C-100 – CONTRACTOR QUALITY CONTROL PROGRAM (CQCP)

ITEM C-102 – TEMPORARY AIR AND WATER POLLUTIONS, SOIL EROSION, AND SILTATION CONTROL

ITEM C-105 – MOBILIZATION

ITEM C-110 – PERCENTAGE OF MATERIAL WITHIN SPECIFICATION LIMIT (PWL)

ITEM D-705 PIPE UNDERDRAINS FOR AIRPORTS

ITEM D-751 MANHOLES, CATCH BASINS, INLETS AND INSPECTION HOLES

ITEM P-101 PREPARATION AND REMOVAL OF EXISTING PAVEMENTS

ITEM P-151 CLEARING AND GRUBBING

ITEM P-152 EXCAVATION, SUBGRADE, AND EMBANKMENT

ITEM P-153 CONTROLLED LOW-STRENGTH MATERIAL (CLSM)

ITEM P-156 CEMENT TREATED SUBGRADE

ITEM P-208 AGGREGATE BASE COURSE

ITEM P-401 ASPHALT MIX PAVEMENT

ITEM P-603 EMULSIFIED ASPHALT TACK COAT

ITEM P-605 JOINT SEALANTS FOR PAVEMENTS

ITEM P-610 CONCRETE FOR MISCELLANEOUS STRUCTURES

ITEM P-620 RUNWAY AND TAXIWAY MARKING

ITEM T-901 SEEDING

ITEM T-905 TOPSOIL



			SCHEDULE I TAXIWAY A RECONSTRUCTION	SCHEDULE II TAXIWAY B REHABILITATION	SCHEDULE III TAXIWAY A UNDERDRAINS
Item No.	Item Description	Unit	Quantity	Quantity	Quantity
C-100a	Contractor's Quality Control Program (CQCP)	LS	1	1	-
C-102a	Temporary Erosion Control	LS	1	-	-
C-105a	Mobilization	LS	1	1	1
P-101a	Pavement Removal - Full Depth	SY	11,580	-	-
P-101b	Asphalt Pavement Removal - Partial Depth	SY	400	5,180	-
P-101c	Joint and Crack Repair	LF	-	2500	-
P-152a	Unclassified Excavation	CY	910	-	80
P-156a	Cement Treated Subgrade, 18 Inches	SY	8430	-	-
P-156b	Cement	TON	280	-	-
P-208a	Aggregate Base Course, 8 Inches	CY	2050	-	-
P-208b	Geotextile Fabric	SY	9130	-	-
P-401a	Asphalt Surface Course	TON	2090	760	-
P-603a	Bituminous Tack Coat	GAL	1340	760	-
P-620a	Temporary Airport Pavement Marking	SF	3480	640	-
P-620b	Permanent Airport Pavement Marking, Yellow	SF	2320	640	-
P-620c	Permanent Airport Pavement Marking, Black	SF	1440	-	-
P-620d	Pavement Marking Obliteration	SF	-	580	-



D-705a	Install 6-Inch Perforated Polyethylene Pipe	LF	-	-	3900
D-705b	Install 6-Inch Non-Perforated Polyethylene Pipe	LF	-	-	900
D-751a	Install 6-Inch Underdrain Cleanout	EA	-	-	20
D-751b	Install 18x18 Inch Inspection Pit	EA	-	-	12
T-901a	Seeding with Hydromulch	AC	2.5	-	-
T-905a	Topsoil	SY	11,010	-	-
L-108a	Install #8 AWG, L-824C, 5000 Volt Wire	LF	5100		-
L-108b	Install #6 AWG Bare Copper Counterpoise, Including Ground Rods	LF	4100	-	-
L-110a	Install 1-2" SCH 40 PVC Conduit, Direct Earth Buried (DEB)	LF	3900	-	-
L-110b	Install 1-2" SCH 40 HDPE Conduit, Directionally Bored	LF	190		
L-125a	Remove Stake Mounted Taxiway Edge Light, Complete	EA	33	-	-
L-125b	Install L-861T LED Taxiway Edge Light, Complete	EA	55	-	-

Pre-Bid Conference. A VOLUNTARY pre-bid conference for this project will be held on Tuesday, August 19, 2025 at 9:00 a.m., in the Airport terminal building conference room at the Rolla National Airport, 631 Airport Drive, Vichy, MO 65580.

Bid Conditions. All bidders shall make arrangements with the Rolla National Airport to examine the site to become familiar with all site conditions prior to submitting their bid.

The bidder is required to provide all information as required within the Contract Documents. The bidder is required to bid on all items of every schedule or as otherwise detailed in the Instructions to Bidders.



- b) The Contractor shall provide a one (1) year warranty (4 years for LED fixtures) against defective materials or workmanship in the work covered under the contract effective upon the date of final acceptance by the Sponsor. Final acceptance will be considered as the date of the final acceptance letter or as the date of the final inspection meeting if no letter is prepared.
- c) The Contractor shall provide a warranty bond to cover the one (1) year warranty period (4 years for LED fixtures) as a condition of final acceptance of the project by the Sponsor as a part of the work at no additional cost to the Sponsor.

25) Signing of Agreement

- a) When Sponsor issues a Notice of Award to the Successful Bidder, it shall be accompanied by the unexecuted counterparts of the Agreement along with the other Contract Documents as identified in the Agreement. Within 30 days thereafter, Successful Bidder shall execute and deliver the required number of counterparts of the Agreement (and any bonds and insurance documentation required to be delivered by the Contract Documents) to Sponsor. Within ten days thereafter, Sponsor shall deliver one fully executed counterpart of the Agreement to Successful Bidder.

26) Notice to Proceed

- a) Work may not start under any awarded contract until a written Notice to Proceed is issued by the Sponsor. The Sponsor may issue the Notice to Proceed any time after the contract is signed and, if required, insurance and bonds have been provided.

27) Sales and Use Taxes

- a) Sponsor is exempt from Missouri state sales and use taxes on materials and equipment to be incorporated in the Work. (Exemption No. to be provided after award). Said taxes shall not be included in the Bid.

28) Bid Protests

- a) Bidders are notified, that in accordance with FAA policy, bid protests based on an allegedly defective bid solicitation, shall be in writing and received by the Sponsor prior to the bid opening.
- b) For bid protests based on an alleged improper evaluation of bids, a protest must be received by the Sponsor in writing within 10 days after the Notice of Award to the winning bidder is issued. It is the responsibility of the protesting bidder to keep apprised of when the Notice of Award is issued by calling or emailing the Sponsor for updates.

29) Bidder Questions

- a) If a Bidder finds discrepancies in, or omissions from, the Contract Documents, or if he is in doubt as to their meaning, he shall at once notify the Engineer in writing. Such notification must be made at least seven (7) consecutive calendar days prior to the bid date. Neither the Sponsor nor the Engineer will accept telephone calls regarding questions about the Contract Documents. All inquiries must be in writing. All interpretations of the Contract Documents will be issued via addenda to all bidders. All addenda issued will become a part of the Contract. The Sponsor will not be responsible for any other explanation or interpretation of the Contract Documents.

The questions should be delivered to Laura Koonce, Woolpert, Inc., at the Pre-Bid Conference, or emailed to laura.koonce@woolpert.com.

BID PACKAGE NO.1
TAXIWAY A RECONSTRUCTION AND TAXIWAY B REHABILITATION

SCHEDULE I - TAXIWAY A RECONSTRUCTION						
Item No.	Description		Units	Estimated Quantity	Unit Price	Total
C-100a	Contractor's Quality Control Program (CQCP)	at the unit price of: _____ dollars and _____ cents.	LS	1	\$	\$
C-102a	Temporary Erosion Control	at the unit price of: _____ dollars and _____ cents.	LS	1	\$	\$
C-105a	Mobilization	at the unit price of: _____ dollars and _____ cents.	LS	1	\$	\$
P-101a	Pavement Removal - Full Depth	at the unit price of: _____ dollars and _____ cents.	SY	11,580	\$	\$
P-101b	Asphalt Pavement Removal - Partial Depth	at the unit price of: _____ dollars and _____ cents.	SY	400	\$	\$
P-152a	Unclassified Excavation	at the unit price of: _____ dollars and _____ cents.	CY	910	\$	\$
P-156a	Cement Treated Subgrade, 18 Inches	at the unit price of: _____ dollars and _____ cents.	SY	8,430	\$	\$
P-156b	Cement	at the unit price of: _____ dollars and _____ cents.	TON	280	\$	\$
P-208a	Aggregate Base Course, 8 Inches	at the unit price of: _____ dollars and _____ cents.	CY	2,050	\$	\$
P-208b	Geotextile Fabric	at the unit price of: _____ dollars and _____ cents.	SY	9,130	\$	\$
P-401a	Asphalt Surface Course	at the unit price of: _____ dollars and _____ cents.	TON	2,090	\$	\$
P-603a	Bituminous Tack Coat	at the unit price of: _____ dollars and _____ cents.	GAL	1,340	\$	\$
P-620a	Temporary Airport Pavement Marking	at the unit price of: _____ dollars and _____ cents.	SF	3,480	\$	\$
P-620b	Permanent Airport Pavement Marking, Yellow	at the unit price of: _____ dollars and _____ cents.	SF	2,320	\$	\$
P-620c	Permanent Airport Pavement Marking, Black	at the unit price of: _____ dollars and _____ cents.	SF	1,440	\$	\$
T-901a	Seeding with Hydromulch	at the unit price of: _____ dollars and _____ cents.	AC	2.50	\$	\$
T-905a	Topsoil	at the unit price of: _____ dollars and _____ cents.	SY	11,010	\$	\$
L-108a	Install #8 AWG, L-824C, 5000 Volt Wire	at the unit price of: _____ dollars and _____ cents.	LF	5,100	\$	\$
L-108b	Install #6 AWG Bare Copper Counterpoise, Including Ground Rods	at the unit price of: _____ dollars and _____ cents.	LF	4,100	\$	\$
L-110a	Install 1-2" SCH 40 PVC Conduit, Direct Earth Buried (DEB)	at the unit price of: _____ dollars and _____ cents.	LF	3,900	\$	\$
L-110b	Install 1-2" SCH 40 HDPE Conduit, Directionally Bored	at the unit price of: _____ dollars and _____ cents.	LF	190	\$	\$
L-125a	Remove Stake Mounted Taxiway Edge Light, Complete	at the unit price of: _____ dollars and _____ cents.	EA	33	\$	\$
L-125b	Install L-861T LED Taxiway Edge Light, Complete	at the unit price of: _____ dollars and _____ cents.	EA	55	\$	\$

SCHEDULE I TOTAL \$

BID PACKAGE NO.1
TAXIWAY A RECONSTRUCTION AND TAXIWAY B REHABILITATION

SCHEDULE II - TAXIWAY B REHABILITATION						
Item No.	Description		Units	Estimated Quantity	Unit Price	Total
C-100a	Contractor's Quality Control Program (CQCP)	at the unit price of: _____ dollars and _____ cents.	LS	1	\$	\$
C-105a	Mobilization	at the unit price of: _____ dollars and _____ cents.	LS	1	\$	\$
P-101b	Asphalt Pavement Removal - Partial Depth	at the unit price of: _____ dollars and _____ cents.	SY	5,180	\$	\$
P-101c	Joint and Crack Repair	at the unit price of: _____ dollars and _____ cents.	LF	2,500	\$	\$
P-401a	Asphalt Surface Course	at the unit price of: _____ dollars and _____ cents.	TON	760	\$	\$
P-603a	Bituminous Tack Coat	at the unit price of: _____ dollars and _____ cents.	GAL	760	\$	\$
P-620a	Temporary Airport Pavement Marking	at the unit price of: _____ dollars and _____ cents.	SF	640	\$	\$
P-620b	Permanent Airport Pavement Marking, Yellow	at the unit price of: _____ dollars and _____ cents.	SF	640	\$	\$
P-620d	Pavement Marking Obliteration	at the unit price of: _____ dollars and _____ cents.	SF	580	\$	\$

SCHEDULE II TOTAL \$ _____

BID PACKAGE NO.1
TAXIWAY A RECONSTRUCTION AND TAXIWAY B REHABILITATION

SCHEDULE III - TAXIWAY A UNDERDRAINS						
Item No.	Description	at the unit price of: _____ dollars and _____ cents.	Units	Estimated Quantity	Unit Price	Total
C-105a	Mobilization	at the unit price of: _____ dollars and _____ cents.	LS	1	\$	\$
P-152a	Unclassified Excavation	at the unit price of: _____ dollars and _____ cents.	CY	80	\$	\$
D-705a	Install 6-Inch Perforated Polyethylene Pipe	at the unit price of: _____ dollars and _____ cents.	LF	3,900	\$	\$
D-705b	Install 6-Inch Non-Perforated Polyethylene Pipe	at the unit price of: _____ dollars and _____ cents.	LF	900	\$	\$
D-751a	Install 6-Inch Underdrain Cleanout	at the unit price of: _____ dollars and _____ cents.	EA	20	\$	\$
D-751b	Install 18x18 Inch Inspection Pit	at the unit price of: _____ dollars and _____ cents.	EA	12	\$	\$

SCHEDULE III TOTAL \$ _____



			SCHEDULE I TAXIWAY A RECONSTRUCTION	SCHEDULE II TAXIWAY B REHABILITATION	SCHEDULE III TAXIWAY A UNDERDRAINS
Item No.	Item Description	Unit	Quantity	Quantity	Quantity
C-100a	Contractor's Quality Control Program (CQCP)	LS	1	1	-
C-102a	Temporary Erosion Control	LS	1	-	-
C-105a	Mobilization	LS	1	1	1
P-101a	Pavement Removal - Full Depth	SY	11,580	-	-
P-101b	Asphalt Pavement Removal - Partial Depth	SY	400	5180	-
P-101c	Joint and Crack Repair	LF	-	2500	-
P-152a	Unclassified Excavation	CY	910	-	80
P-156a	Cement Treated Subgrade, 18 Inches	SY	8430	-	-
P-156b	Cement	TON	280	-	-
P-208a	Aggregate Base Course, 8 Inches	CY	2050	-	-
P-208b	Geotextile Fabric	SY	9130	-	-
P-401a	Asphalt Surface Course	TON	2090	760	-
P-603a	Bituminous Tack Coat	GAL	1340	760	-



P-620a	Temporary Airport Pavement Marking	SF	3480	640	-
P-620b	Permanent Airport Pavement Marking, Yellow	SF	2320	640	-
P-620c	Permanent Airport Pavement Marking, Black	SF	1440	-	-
P-620d	Pavement Marking Obliteration	SF	-	580	-
D-705a	Install 6-Inch Perforated Polyethylene Pipe	LF	-	-	3900
D-705b	Install 6-Inch Non-Perforated Polyethylene Pipe	LF	-	-	900
D-751a	Install 6-Inch Underdrain Cleanout	EA	-	-	20
D-751b	Install 18x18 Inch Inspection Pit	EA	-	-	12
T-901a	Seeding with Hydromulch	AC	2.5	-	-
T-905a	Topsoil	SY	11,010	-	-
L-108a	Install #8 AWG, L-824C, 5000 Volt Wire	LF	5100		-



L-108b	Install #6 AWG Bare Copper Counterpoise, Including Ground Rods	LF	4100	-	-
L-110a	Install 1-2" SCH 40 PVC Conduit, Direct Earth Buried (DEB)	LF	3900	-	-
L-110b	Install 1-2" SCH 40 HDPE Conduit, Directionally Bored	LF	190		
L-125a	Remove Stake Mounted Taxiway Edge Light, Complete	EA	33	-	-
L-125b	Install L-861T LED Taxiway Edge Light, Complete	EA	55	-	-

The amount of money appropriated will be equal to or in excess of the contract amount as forth in the notice(s) to proceed. Change orders requiring additional compensable work to be performed, which cause the aggregate amount payable under the contract to exceed the amount appropriated for the original contract, are prohibited unless the contractor is given written assurance by Sponsor that lawful appropriations to cover costs of the additional work have been made or unless such work is covered under a remedy granting provision of the contract. Notwithstanding anything to the contrary in the Contract Documents the Contractor hereby acknowledges and agrees that Sponsor's performance under the contract is subject to receipt of funds from the FAA and further is subject to annual appropriation by the Sponsor in accordance with a budget adopted by the City of Rolla. Sponsor may issue multiple Notice(s) to Proceed in incremental stages as funding becomes available.

Inasmuch as this Contract is executed pursuant to the laws of the State of Missouri, pertaining to airports and payment of the contract unit price shall be made solely from special account established for this project.

ARTICLE 4: PAYMENT BY SPONSOR. It is hereby further agreed that, at the completion of the work and its acceptance by the Sponsor, all sums due the Contractor by reason of his faithful performance of the work, taking into consideration additions to or deductions from the Contract price by reason of alterations or modifications of the original Contract or by reason of "Extra Work" authorized under this Contract, will be paid the Contractor by the Sponsor after said completion and acceptance.



Warranty Bond Form

Contractor Name: [Full formal name of Contractor] Address (principal place of business): [Address of Contractor's principal place of business]	Surety Name: [Full formal name of Surety] Address (principal place of business): [Insert address of Surety's principal place of business]
Owner Name: [Full formal name of Owner] Address (principal place of business): [Address of Owner's principal place of business]	Construction Contract Description (name and location): [Owner's project/contract name, and location of the project] Contract Price: [Amount from Contract] Effective Date of Contract: [Date from Contract] Contract's Date of Substantial Completion: [Date from Contract]
Bond Bond Amount: [Amount] Date of Bond: [Date] Modifications to this Bond form: <input type="checkbox"/> None <input type="checkbox"/> See Paragraph 9 Bond Period: Commencing 364 days after Substantial Completion of the Work under the Construction Contract, and continuing until [insert number of years, typically either two or three] years after such Substantial Completion.	
Surety and Contractor, intending to be legally bound hereby, subject to the terms set forth herein, do each cause this Warranty Bond to be duly executed by an authorized officer, agent, or representative.	
Contractor as Principal	Surety
By: <u>(Full formal name of Contractor)</u> (Signature)	By: <u>(Full formal name of Surety) (corporate seal)</u> (Signature) (Attach Power of Attorney)
Name: _____ (Printed or typed)	Name: _____ (Printed or typed)
Title: _____	Title: _____
Attest: _____ (Signature)	Attest: _____ (Signature)
Name: _____ (Printed or typed)	Name: _____ (Printed or typed)
Title: _____	Title: _____
Notes: (1) Provide supplemental execution by any additional parties, such as joint venturers. (2) Any singular reference to Contractor, Surety, Owner, or other party is considered plural where applicable.	



1. The Contractor and Surety, jointly and severally, bind themselves, their heirs, executors, administrators, successors, and assigns to the Owner for the performance of the Construction Contract's Correction Period Obligations. The Construction Contract is incorporated herein by reference.
2. If the Contractor performs the Correction Period Obligations, the Surety and the Contractor shall have no obligation under this Warranty Bond.
3. If Owner gives written notice to Contractor and Surety during the Bond Period of Contractor's obligation under the Correction Period Obligations, and Contractor does not fulfill such obligation, then Surety shall be responsible for fulfillment of such Correction Period Obligations. Surety shall either fulfill the Correction Period Obligations itself, through its agents or contractors, or, in the alternative, Surety may waive the right to fulfill the Correction Period Obligations itself, and reimburse the Owner for all resulting costs incurred by Owner in performing Contractor's Correction Period Obligations, including but not limited to correction, removal, replacement, and repair costs.
4. The Surety's liability is limited to the amount of this Warranty Bond. Renewal or continuation of the Warranty Bond will not modify such amount, unless expressly agreed to by Surety in writing.
5. The Surety shall have no liability under this Warranty Bond for obligations of the Contractor that are unrelated to the Construction Contract. No right of action will accrue on this Warranty Bond to any person or entity other than the Owner or its heirs, executors, administrators, successors, and assigns.
6. Any proceeding, legal or equitable, under this Warranty Bond may be instituted in any court of competent jurisdiction in the location in which the Work or part of the Work is located and must be instituted within two years after the Surety refuses or fails to perform its obligations under this Warranty Bond.
7. Written notice to the Surety, the Owner, or the Contractor must be mailed or delivered to the address shown in this Warranty Bond.
8. Definitions
 - 8.1. *Construction Contract*—The agreement between the Owner and Contractor identified on the cover page of this Warranty Bond, including all Contract Documents and changes made to the agreement and the Contract Documents.
 - 8.2. *Contract Documents*—All the documents that comprise the agreement between the Owner and Contractor.
 - 8.3. *Correction Period Obligations*—The duties, responsibilities, commitments, and obligations of the Contractor with respect to correction or replacement of defective Work, as set forth in the Construction Contract's Correction Period clause, EJCDC® C-700, Standard General Conditions of the Construction Contract (2018), Paragraph 15.08, as duly modified.
 - 8.4. *Substantial Completion*—As defined in the Construction Contract.
 - 8.5. *Work*—As defined in the Construction Contract.
9. Modifications to this Bond are as follows: **[Describe modification or enter "None"]**



Local laws, ordinances, rules, and regulations, and building and construction codes bearing on the conduct of the work.

LIQUIDATED DAMAGES

Subject to the provisions of the Contract Documents, the Sponsor shall be entitled to liquidated damages as anticipated damages to the Sponsor for failure of the Contractor to complete the work within the specified contract time. Liquidated damages are to serve as compensation for the Sponsor's non-use and incurred costs related to work extending beyond the contract time. The Contractor shall be assessed a liquidated damage for each day that the work remains uncompleted beyond the contract period. Further, each phase of work under the project may have different liquidated damage amounts, as outlined in General Provisions Section 80-08 FAILURE TO COMPLETE ON TIME.

As part of liquidated damages, the Contractor further agrees to pay the Sponsor for the costs associated for the construction manager/Engineer in connection with the failure of the Contractor to complete the work within the specified contract time as follows: **\$2,220/calendar day(s)** for the construction manager and **\$2,520/calendar day(s)** for each additional resident engineer plus any incurred expenses (per diem, lodging, etc.). The Contractor also agrees to pay for any services of the Engineer and its subcontractors arising from the failure of the Contractor to furnish materials or equipment in conformance with the Contract Documents necessitating redesign, retesting, or additional review time by the Engineer and their subcontractors or the failure of the Contractor to complete the work within the specified contract time. Such services shall be paid at the standard hourly rates of Engineer and its Subcontractors.

AIRPORT IMPROVEMENT PROGRAM

The work in this contract is included in Airport Improvement Program (AIP) Project Number 23-056A-1 which is being undertaken and accomplished by the Sponsor in accordance with the terms and conditions of a grant agreement between the Sponsor and the United States, under the Airport Improvement Act per Chapter 471 of Title 49 of the United States Code (U.S.C.), as amended by the airport, and the Airway Safety and Capacity Expansion Act of 1987, pursuant to which the United States has agreed to pay a certain percentage of the associated project costs that are determined to be allowable under said Act. The Contractor shall note that the United States is not a party to this contract and no reference in this contract to the FAA or any representative thereof, or to any rights granted to the FAA or any representative thereof, or the United States, by the contract, make the United States a party to this contract.

DBE ADMINISTRATION

1. Eligibility of DBE's:

Those firms currently certified as DBE's by the State Department of Transportation are eligible to participate as DBE's on this contract. A list of these firms can be obtained from the State, the consulting engineer, or the Sponsor. Previous acceptance of a DBE by the FAA, State or Sponsor does not ensure acceptance on this project.

2. Counting DBE Participation Towards DBE Goals:

DBE participation toward attainment of the goal will be computed based on the subcontract prices agreed to between the contractor and subcontractors for the contract items or portions of items being sublet, as shown on the DBE Participation Form and attachments. Credit will only be given for use of DBE's that are certified or accepted according to this specification. DBE participation shall be counted toward meeting the DBE goal in accordance with the following:

Rolla National Airport

Bid Pkg No.1: Taxiway A Reconstruction and Taxiway Connector Rehabilitation

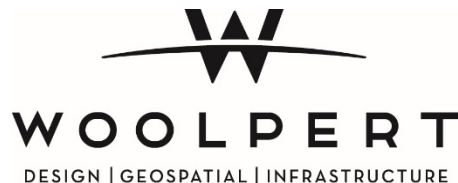
MoDOT Project No: 23-056A-1

Construction Safety and Phasing Plan **Issued for Bid - Addendum No. 1**

Prepared For:

Rolla National Airport
Maries County
631 Airport Road
Vichy, MO 65580

Prepared By:



931 Wildwood Drive, Suite 101
Jefferson City, Missouri 65109
(573) 636-3200

August 5, 2025

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C-105	Mobilization
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<u>SECTION</u>	<u>TITLE</u>
L-108	Underground Power Cable for Airports
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process into control. The requirements for corrective action shall include both general requirements for operation of the CQCP as a whole, and for individual items of work contained in the technical specifications.

The CQCP shall detail how the results of QC inspections and tests will be used for determining the need for corrective action and shall contain clear rules to gauge when a process is out of control and the type of correction to be taken to regain process control.

When applicable or required by the technical specifications, the Contractor shall establish and use statistical QC charts for individual QC tests. The requirements for corrective action shall be linked to the control charts.

100-11 Inspection and/or observations by the RPR. All items of material and equipment are subject to inspection and/or observation by the RPR at the point of production, manufacture or shipment to determine if the Contractor, producer, manufacturer or shipper maintains an adequate QC system in conformance with the requirements detailed here and the applicable technical specifications and plans. In addition, all items of materials, equipment and work in place shall be subject to inspection and/or observation by the RPR at the site for the same purpose.

Inspection and/or observations by the RPR does not relieve the Contractor of performing QC inspections of either on-site or off-site Contractor's or subcontractor's work.

100-12 Noncompliance.

a. The Resident Project Representative (RPR) will provide written notice to the Contractor of any noncompliance with their CQCP. After receipt of such notice, the Contractor must take corrective action.

b. When QC activities do not comply with either the CQCP or the contract provisions or when the Contractor fails to properly operate and maintain an effective CQCP, and no effective corrective actions have been taken after notification of non-compliance, the RPR will recommend the Owner take the following actions:

- (1) Order the Contractor to replace ineffective or unqualified QC personnel or subcontractors and/or
- (2) Order the Contractor to stop operations until appropriate corrective actions are taken.

METHOD OF MEASUREMENT

100-13 Basis of measurement and payment. Contractor Quality Control Program (CQCP) is for the personnel, tests, facilities and documentation required to implement the CQCP. The CQCP will be paid as a lump sum with the following schedule of partial payments:

- a. With first pay request, 25% with approval of CQCP and completion of the Quality Control (QC)/Quality Assurance (QA) workshop.
- b. When 25% or more of the original contract is earned, an additional 25%.
- c. When 50% or more of the original contract is earned, an additional 20%.
- d. When 75% or more of the original contract is earned, an additional 20%
- e. After final inspection and acceptance of project, the final 10%.

BASIS OF PAYMENT

100-14 Payment will be made under:

Item C-100a Contractor Quality Control Program (CQCP) – per lump sum

be measured separately. All fittings shall be included in the footage as typical pipe sections in the pipeline being measured.

705-4.2 The quantity of porous backfill shall considered incidental to pipe underdrains, complete.

705-4.3 The quantity of filter fabric shall considered incidental to pipe underdrains, complete.

705-4.4. The quantity of pipe underdrains shall considered incidental to pipe underdrains, complete.

BASIS OF PAYMENT

705-5.1 Payment will be made at the contract unit price per linear foot (meter) for pipe underdrains of the type, class, and size designated.

705-5.2 POROUS BACKFILL.

a. Porous backfill No. 1 shall be considered incidental to pipe underdrains, complete.

b. Porous Backfill No. 2 shall be considered incidental to pipe underdrains, complete.

705-5.3. FILTER FABRIC. Filter fabric shall be considered incidental to pipe underdrains, complete.

705-5.4 PIPE UNDERDRAINS, COMPLETE. Pipe underdrains, complete (including porous backfill and filter fabric) shall be made at the contract unit price per linear foot (meter) complete (including porous backfill and filter fabric).

These prices shall be full compensation for furnishing all materials and for all preparation, excavation, and installation of these materials, and for all labor, equipment, tools, and incidentals necessary to complete the item.

Payment will be made under:

Item D-705a	Install 6-Inch Perforated Polyethylene Pipe - per linear foot (meter)
Item D-705b	Install 6-Inch Non-Perforated Polyethylene Pipe - per linear foot (meter)

REFER TO APPENDIX P FOR ITEM DESCRIPTIONS.

REFERENCES

The publications listed below form a part of this specification to the extent referenced. The publications are referred to within the text by the basic designation only.

ASTM International (ASTM)

Item P-101 Preparation/Removal of Existing Pavements

DESCRIPTION

101-1 This item shall consist of preparation of existing pavement surfaces for overlay, surface treatments, removal of existing pavement, and other miscellaneous items. The work shall be accomplished in accordance with these specifications and the applicable plans.

EQUIPMENT AND MATERIALS

101-2 All equipment and materials shall be specified here and in the following paragraphs or approved by the Resident Project Representative (RPR). The equipment shall not cause damage to the pavement to remain in place.

CONSTRUCTION

101-3.1 Removal of existing pavement.

The Contractor's removal operation shall be controlled to not damage adjacent pavement structure, and base material, cables, utility ducts, pipelines, or drainage structures which are to remain under the pavement.

a. Concrete pavement removal. Full depth saw cuts shall be made perpendicular to the slab surface. The Contractor shall saw through the full depth of the slab including any dowels at the joint, removing the pavement and installing new dowels as shown on the plans and per the specifications. Where the perimeter of the removal limits is not located on the joint and there are no dowels present, the perimeter shall be saw cut to the full depth of the pavement. The pavement inside the saw cut shall be removed by methods which will not cause distress in the pavement which is to remain in place. If the material is to be wasted on the airport site, it shall be reduced to a maximum size as directed by the RPR. Concrete slabs that are damaged by under breaking shall be repaired or removed and replaced as directed by the RPR.

The edge of existing concrete pavement against which new pavement abuts shall be protected from damage at all times. Spall and underbreak repair shall be in accordance with the plans. Any underlaying material that is to remain in place, shall be recompact and/or replaced as shown on the plans. Adjacent areas damaged during repair shall be repaired or replaced at the Contractor's expense.

b. Asphalt pavement removal. Asphalt pavement to be removed shall be cut to the full depth of the asphalt pavement around the perimeter of the area to be removed.

c. Repair or removal of Base, Subbase, and/or Subgrade. All failed material including surface, base course, subbase course, and subgrade shall be removed and repaired as shown on the plans or as directed by the RPR. Materials and methods of construction shall comply with the applicable sections of these specifications. Any damage caused by Contractor's removal process shall be repaired at the Contractor's expense.

101-3.2 Preparation of joints and cracks prior to overlay/surface treatment. Remove all vegetation and debris from cracks to a minimum depth of 1 inch (25 mm). If extensive vegetation exists, treat the specific area with a concentrated solution of a water-based herbicide approved by the RPR. Fill all cracks greater than 1/4 inch (6 mm wide) with a crack sealant per ASTM D6690. The crack sealant, preparation, and application shall be compatible with the surface treatment/overlay to be used. To minimize contamination of the asphalt with the crack sealant,

underfill the crack sealant a minimum of 1/8 inch (3 mm), not to exceed ¼ inch (6 mm). Any excess joint or crack sealer shall be removed from the pavement surface.

Wider cracks (over 1-1/2 inch wide (38 mm)), along with soft or sunken spots, indicate that the pavement or the pavement base should be repaired or replaced as stated below.

Cracks and joints may be filled with a mixture of emulsified asphalt and aggregate. The aggregate shall consist of limestone, volcanic ash, sand, or other material that will cure to form a hard substance. The combined gradation shall be as shown in the following table.

Gradation

Sieve Size	Percent Passing
No. 4 (4.75 mm)	100
No. 8 (2.36 mm)	90-100
No. 16 (1.18 mm)	65-90
No. 30 (600 µm)	40-60
No. 50 (300 µm)	25-42
No. 100 (150 µm)	15-30
No. 200 (75 µm)	10-20

Up to 3% cement can be added to accelerate the set time. The mixture shall not contain more than 20% natural sand without approval in writing from the RPR.

The proportions of asphalt emulsion and aggregate shall be determined in the field and may be varied to facilitate construction requirements. Normally, these proportions will be approximately one part asphalt emulsion to five parts aggregate by volume. The material shall be poured or placed into the joints or cracks and compacted to form a voidless mass. The joint or crack shall be filled to within +0 to -1/8 inches (+0 to -3 mm) of the surface. Any material spilled outside the width of the joint shall be removed from the pavement surface prior to constructing the overlay. Where concrete overlays are to be constructed, only the excess joint material on the pavement surface and vegetation in the joints need to be removed.

101-3.3 Removal of Foreign Substances/contaminates prior to overlay and remarking. Removal of foreign substances/contaminates from existing pavement that will affect the bond of the new treatment shall consist of removal of rubber, fuel spills, oil, crack sealer, at least 90% of paint, and other foreign substances from the surface of the pavement. Areas that require removal are designated on the plans and as directed by the RPR in the field during construction.

High-pressure water, cold milling, rotary grinding, and/or sandblasting may be used. If chemicals are used, they shall comply with the state's environmental protection regulations. Removal methods used shall not cause major damage to the pavement, or to any structure or utility within or adjacent to the work area. Major damage is defined as changing the properties of the pavement, removal of asphalt causing the aggregate to ravel, or removing pavement over 1/8 inch (3 mm) deep. If it is deemed by the RPR that damage to the existing pavement is caused by operational error, such as permitting the application method to dwell in one location for too long, the Contractor shall repair the damaged area without compensation and as directed by the RPR.

Removal of foreign substances shall not proceed until approved by the RPR. Water used for high-pressure water equipment shall be provided by the Contractor at the Contractor's expense. No material shall be deposited on the pavement shoulders. All wastes shall be disposed of in areas indicated in this specification or shown on the plans.

101-3.4 Concrete spall or failed asphaltic concrete pavement repair.

a. Repair of concrete spalls in areas to be overlaid with asphalt. The Contractor shall repair all spalled concrete as shown on the plans or as directed by the RPR. The perimeter of the repair shall be saw cut a minimum of 2 inches (50 mm) outside the affected area and 2 inches (50 mm) deep. The deteriorated material shall be removed to a depth where the existing material is firm or cannot be easily removed with a geologist pick. The removed area shall be filled with asphalt mixture with aggregate sized appropriately for the depth of the patch. The material shall be compacted with equipment approved by the RPR until the material is dense and no movement or marks are visible. The material shall not be placed in lifts over 4 inches (100 mm) in depth. This method of repair applies only to pavement to be overlaid.

b. Asphalt pavement repair. The Contractor shall repair all spalled concrete as shown on the plans or as directed by the RPR. The failed areas shall be removed as specified in paragraph 101-3.1b. All failed material including surface, base course, subbase course, and subgrade shall be removed. Materials and methods of construction shall comply with the applicable sections of these specifications.

101-3.5 Cold milling. Milling shall be performed with a power-operated milling machine or grinder, capable of producing a uniform finished surface. The milling machine or grinder shall operate without tearing or gouging the underlying surface. The milling machine or grinder shall be equipped with grade and slope controls, and a positive means of dust control. All millings shall be removed and disposed off Airport property. If the Contractor mills or grinds deeper or wider than the plans specify, the Contractor shall replace the material removed with new material at the Contractor's Expense.

a. Patching. The milling machine shall be capable of cutting a vertical edge without chipping or spalling the edges of the remaining pavement and it shall have a positive method of controlling the depth of cut. The RPR shall layout the area to be milled with a straightedge in increments of 1-foot (30 cm) widths. The area to be milled shall cover only the failed area. Any excessive area that is milled because the Contractor doesn't have the appropriate milling machine, or areas that are damaged because of his negligence, shall be repaired by the Contractor at the Contractor's Expense.

b. Profiling, grade correction, or surface correction. The milling machine shall have a minimum width of 7 feet and it shall be equipped with electronic grade control devices that will cut the surface to the grade specified. The tolerances shall be maintained within +0 inch and -1/4 inch (+0 mm and -6mm) of the specified grade. The machine must cut vertical edges and have a positive method of dust control. The machine must have the ability to remove the millings or cuttings from the pavement and load them into a truck. All millings shall be removed and disposed of off the airport.

c. Clean-up. The Contractor shall sweep the milled surface daily and immediately after the milling until all residual materials are removed from the pavement surface. Prior to paving, the Contractor shall wet down the milled pavement and thoroughly sweep and/or blow the surface to remove loose residual material. Waste materials shall be collected and removed from the pavement surface and adjacent areas by sweeping or vacuuming. Waste materials shall be removed and disposed off Airport property.

101-3.6. Preparation of asphalt pavement surfaces prior to surface treatment. Existing asphalt pavements to be treated with a surface treatment shall be prepared as follows:

a. Patch asphalt pavement surfaces that have been softened by petroleum derivatives or have failed due to any other cause. Remove damaged pavement to the full depth of the damage and replace with new asphalt pavement similar to that of the existing pavement in accordance with paragraph 101-3.4b.

b. Repair joints and cracks in accordance with paragraph 101-3.2.

c. Remove oil or grease that has not penetrated the asphalt pavement by scrubbing with a detergent and washing thoroughly with clean water. After cleaning, treat these areas with an oil spot primer.

d. Clean pavement surface immediately prior to placing the surface treatment so that it is free of dust, dirt, grease, vegetation, oil or any type of objectionable surface film.

101-3.7 Maintenance. The Contractor shall perform all maintenance work necessary to keep the pavement in a satisfactory condition until the full section is complete and accepted by the RPR. The surface shall be kept clean and free from foreign material. The pavement shall be properly drained at all times. If cleaning is necessary or if the pavement becomes disturbed, any work repairs necessary shall be performed at the Contractor's expense.

101-3.8 Preparation of Joints in Rigid Pavement prior to resealing. Prior to application of sealant material, clean and dry the joints of all scale, dirt, dust, old sealant, curing compound, moisture and other foreign matter. The Contractor shall demonstrate, in the presence of the RPR, that the method used cleans the joint and does not damage the joint.

101-3.8.1 Removal of Existing Joint Sealant. All existing joint sealants will be removed by plowing or use of hand tools. Any remaining sealant and or debris will be removed by use of wire brushes or other tools as necessary. Resaw joints removing no more than 1/16 inch (2 mm) from each joint face. Immediately after sawing, flush out joint with water and other tools as necessary to completely remove the slurry.

101-3.8.2 Cleaning prior to sealing. Immediately before sealing, joints shall be cleaned by removing any remaining laitance and other foreign material. Allow sufficient time to dry out joints prior to sealing. Joint surfaces will be surface-dry prior to installation of sealant.

101-3.8.3 Joint sealant. Joint material and installation will be in accordance with Item P-605.

101-3.9 Preparation of Cracks in Flexible Pavement prior to sealing. Prior to application of sealant material, clean and dry the joints of all scale, dirt, dust, old sealant, curing compound, moisture and other foreign matter. The Contractor shall demonstrate, in the presence of the RPR, that the method used cleans the cracks and does not damage the pavement.

101-3.9.1 Preparation of Crack. Widen crack with router or random crack saw by removing a minimum of 1/16 inch (2 mm) from each side of crack. Immediately before sealing, cracks will be blown out with a hot air lance combined with oil and water-free compressed air.

101-3.9.2 Removal of Existing Crack Sealant. Existing sealants will be removed by routing or random crack saw. Following routing or sawing any remaining debris will be removed by use of a hot lance combined with oil and water-free compressed air.

101-3.9.3 Crack Sealant. Crack sealant material and installation will be in accordance with Item P-605.

101-3.9.4 Removal of Pipe and other Buried Structures.

- a. **Removal of Existing Pipe Material.** Not used.
- b. **Removal of Inlets/Manholes.** Not used.

METHOD OF MEASUREMENT

101-4.1 Pavement removal. The unit of measurement for pavement removal shall be the number of square yards (square meters) removed by the Contractor. Any pavement removed outside the limits of removal because the pavement was damaged by negligence on the part of the Contractor shall not be included in the measurement for payment. No direct measurement or payment shall be made for saw cutting. Saw cutting shall be incidental to pavement removal. Dowel bar installation shall be incidental to pavement removal.

101-4.2 Joint and crack repair. Joint and crack repair shall be considered incidental to pavement removal.

101-4.3 Removal of Foreign Substances/contaminates. The foreign substances/contaminates removal shall be considered incidental to pavement removal.

101-4.4 Spalled and failed asphalt pavement repair. Failed asphalt pavement repair shall be considered incidental to pavement removal.

101-4.5 Concrete Spall Repair. Concrete spall repair is not anticipated but shall be considered incidental to pavement removal. The location and average depth of the patch shall be determined and agreed upon by the RPR and the Contractor.

101-4.6 Cold milling. Cold milling shall be considered incidental to pavement removal. . The location and average depth of the cold milling shall be as shown on the plans. If the initial cut does not correct the condition, the Contractor shall re-mill the area and will be paid for the total depth of milling.

101-4.7 Removal of Pipe and other Buried Structures. Not required.

BASIS OF PAYMENT

101-5.1 Payment. Payment shall be made at contract unit price for the unit of measurement as specified above. This price shall be full compensation for furnishing all materials and for all preparation, hauling, and placing of the material and for all labor, equipment, tools, and incidentals necessary to complete this item.

Item P 101a	Pavement Removal - Full Depth - per square yard (square meter)
Item P 101b	Pavement Removal – Partial Depth - per square yard (square meter)
Item P 101c	Joint and Crack Repair - per linear foot (meter)

REFERENCES

The publications listed below form a part of this specification to the extent referenced. The publications are referred to within the text by the basic designation only.

Advisory Circulars (AC)

AC 150/5380-6	Guidelines and Procedures for Maintenance of Airport Pavements.
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ASTM International (ASTM)

ASTM D6690	Standard Specification for Joint and Crack Sealants, Hot Applied, for Concrete and Asphalt Pavements
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END OF ITEM P-101

Payment will be made under:

Item P-208a	Aggregate Base Course, 8 Inches - per cubic yard (cubic meter)
Item P-208b	Geotextile Fabric - per square yard (square meter)

REFERENCES

The publications listed below form a part of this specification to the extent referenced. The publications are referred to within the text by the basic designation only.

ASTM International (ASTM)

ASTM C29	Standard Test Method for Bulk Density ("Unit Weight") and Voids in Aggregate
ASTM C88	Standard Test Method for Soundness of Aggregates by Use of Sodium Sulfate or Magnesium Sulfate
ASTM C117	Standard Test Method for Materials Finer than 75- μ m (No. 200) Sieve in Mineral Aggregates by Washing
ASTM C131	Standard Test Method for Resistance to Degradation of Small-Size Coarse Aggregate by Abrasion and Impact in the Los Angeles Machine
ASTM C136	Standard Test Method for Sieve or Screen Analysis of Fine and Coarse Aggregates
ASTM C142	Standard Test Method for Clay Lumps and Friable Particles in Aggregates
ASTM D75	Standard Practice for Sampling Aggregates
ASTM D698	Standard Test Methods for Laboratory Compaction Characteristics of Soil Using Standard Effort (12,400 ft-lbf/ft ³ (600 kN-m/m ³))
ASTM D1556	Standard Test Method for Density and Unit Weight of Soil in Place by the Sand-Cone Method
ASTM D2167	Standard Test Method for Density and Unit Weight of Soil in Place by the Rubber Balloon Method
ASTM D2487	Standard Practice for Classification of Soils for Engineering Purposes (Unified Soil Classification System)
ASTM D3665	Standard Practice for Random Sampling of Construction Materials
ASTM D4318	Standard Test Methods for Liquid Limit, Plastic Limit, and Plasticity Index of Soils
ASTM D4491	Standard Test Methods for Water Permeability of Geotextiles by Permittivity
ASTM D4643	Standard Test Method for Determination of Water Content of Soil and Rock by Microwave Oven Heating

Item P-401 Asphalt Mix Pavement

DESCRIPTION

401-1.1 This item shall consist of pavement courses composed of mineral aggregate and asphalt binder mixed in a central mixing plant and placed on a prepared base or stabilized course in accordance with these specifications and shall conform to the lines, grades, thicknesses, and typical cross-sections shown on the plans. Each course shall be constructed to the depth, typical section, and elevation required by the plans and shall be rolled, finished, and approved before the placement of the next course.

MATERIALS

401-2.1 AGGREGATE. Aggregates shall consist of crushed stone, crushed gravel, crushed slag, screenings, natural sand, and mineral filler, as required. The aggregates should have no known history of detrimental pavement staining due to ferrous sulfides, such as pyrite. Coarse aggregate is the material retained on the No. 4 (4.75 mm) sieve. Fine aggregate is the material passing the No. 4 (4.75 mm) sieve.

- a. Coarse aggregate.** Coarse aggregate shall consist of sound, tough, durable particles, free from films of matter that would prevent thorough coating and bonding with the asphalt material and free from organic matter and other deleterious substances. Coarse aggregate material requirements are given in the table below.

Coarse Aggregate Material Requirements

Material Test	Requirement	Standard
Resistance to Degradation	Loss: 40% maximum	ASTM C131
Soundness of Aggregates by Use of Sodium Sulfate or Magnesium Sulfate	Loss after 5 cycles: 12% maximum using Sodium sulfate - or - 18% maximum using magnesium sulfate	ASTM C88
Clay lumps and friable particles	1.0% maximum	ASTM C142
Percentage of Fractured Particles	For pavements designed for aircraft gross weights less than 60,000 pounds (27200 kg): Minimum 50% by weight of particles with at least two fractured faces and 65% with at least one fractured face ¹	ASTM D5821

- Type and amount of Anti-strip agent when used.
- Asphalt Pavement Analyzer (APA) results.
- Date the JMF was developed. Mix designs that are not dated or which are from a prior construction season shall not be accepted.

Table 1. Asphalt Design Criteria

Test Property	Value	Test Method
Number of gyrations	50	
Air voids (%)	3.5	ASTM D3203
Percent voids in mineral aggregate (VMA), minimum	See Table 2	ASTM D6995
Tensile Strength Ratio (TSR) ¹	not less than 80 at a saturation of 70-80%	ASTM D4867

¹ Test specimens for TSR shall be compacted at 7 ± 1.0 % air voids. In areas subject to freeze-thaw, use freeze-thaw conditioning in lieu of moisture conditioning per ASTM D4867.

The mineral aggregate shall be of such size that the percentage composition by weight, as determined by laboratory sieves, will conform to the gradation or gradations specified in Table 2 when tested in accordance with ASTM C136 and ASTM C117.

The gradations in Table 2 represent the limits that shall determine the suitability of aggregate for use from the sources of supply; be well graded from coarse to fine and shall not vary from the low limit on one sieve to the high limit on the adjacent sieve, or vice versa.

Item P-605 Joint Sealants for Pavements

DESCRIPTION

605-1.1 This item shall consist of providing and installing a resilient and adhesive joint sealing material capable of effectively sealing joints in pavement; joints between different types of pavements; and cracks in existing pavement.

MATERIALS

605-2.1 Joint sealants. Joint sealant materials shall meet the requirements of **ASTM D6690**.

Each lot or batch of sealant shall be delivered to the jobsite in the manufacturer's original sealed container. Each container shall be marked with the manufacturer's name, batch or lot number, the safe heating temperature, and shall be accompanied by the manufacturer's certification stating that the sealant meets the requirements of this specification.

605-2.2 Backer rod. The material furnished shall be a compressible, non-shrinking, non-staining, non-absorbing material that is non-reactive with the joint sealant in accordance with ASTM D5249. The backer-rod material shall be $25\% \pm 5\%$ larger in diameter than the nominal width of the joint.

605-2.3 Bond breaking tapes. Provide a bond breaking tape or separating material that is a flexible, non-shrinkable, non-absorbing, non-staining, and non-reacting adhesive-backed tape. The material shall have a melting point at least 5°F (3°C) greater than the pouring temperature of the sealant being used when tested in accordance with ASTM D789. The bond breaker tape shall be approximately 1/8 inch (3 mm) wider than the nominal width of the joint and shall not bond to the joint sealant.

CONSTRUCTION METHODS

605-3.1 Time of application. Joints shall be sealed as soon after completion of the curing period as feasible and before the pavement is opened to traffic, including construction equipment. The pavement temperature shall be 50°F (10°C) and rising at the time of application of the poured joint sealing material. Do not apply sealant if moisture is observed in the joint.

605-3.2 Equipment. Machines, tools, and equipment used in the performance of the work required by this section shall be approved before the work is started and maintained in satisfactory condition at all times. Submit a list of proposed equipment to be used in performance of construction work including descriptive data, **10** days prior to use on the project.

a. Tractor-mounted routing tool. Provide a routing tool, used for removing old sealant from the joints, of such shape and dimensions and so mounted on the tractor that it will not damage the sides of the joints. The tool shall be designed so that it can be adjusted to remove the old material to varying depths as required. The use of V-shaped tools or rotary impact routing devices will not be permitted. Hand-operated spindle routing devices may be used to clean and enlarge random cracks.

b. Concrete saw. Provide a self-propelled power saw, with water-cooled diamond or abrasive saw blades, for cutting joints to the depths and widths specified.

c. Sandblasting equipment. Sandblasting is not allowed.

d. Waterblasting equipment. The Contractor must demonstrate waterblasting equipment including the pumps, hose, guide and nozzle size, under job conditions, before approval in accordance with paragraph 605-3.3. The Contractor shall demonstrate, in the presence of the RPR, that the method cleans the joint and does not damage the joint.

e. Hand tools. Hand tools may be used, when approved, for removing defective sealant from a crack and repairing or cleaning the crack faces. Hand tools should be carefully evaluated for potential spalling effects prior to approval for use.

f. Hot-poured sealing equipment. The unit applicators used for heating and installing ASTM D6690 joint sealant materials shall be mobile and shall be equipped with a double-boiler, agitator-type kettle with an oil medium in the outer space for heat transfer; a direct-connected pressure-type extruding device with a nozzle shaped for inserting in the joint to be filled; positive temperature devices for controlling the temperature of the transfer oil and sealant; and a recording type thermometer for indicating the temperature of the sealant. The applicator unit shall be designed so that the sealant will circulate through the delivery hose and return to the inner kettle when not in use.

g. Cold-applied, single-component sealing equipment. Not applicable.

605-3.3 Preparation of joints. Pavement joints for application of material in this specification must be dry, clean of all scale, dirt, dust, curing compound, and other foreign matter. The Contractor shall demonstrate, in the presence of the RPR, that the method cleans the joint and does not damage the joint.

a. Sawing. All joints shall be sawed in accordance with specifications and plan details. Immediately after sawing the joint, the resulting slurry shall be completely removed from joint and adjacent area by flushing with a jet of water, and by use of other tools as necessary.

b. Sealing. Immediately before sealing, the joints shall be thoroughly cleaned of all remaining laitance, curing compound, filler, protrusions of hardened concrete, old sealant and other foreign material from the sides and upper edges of the joint space to be sealed. Cleaning shall be accomplished by tractor-mounted routing equipment, concrete saw, or waterblaster as specified in paragraph 605-3.2. The newly exposed concrete joint faces and the pavement surface extending a minimum of 1/2 inch (12 mm) from the joint edge shall be sandblasted clean. Sandblasting shall be accomplished in a minimum of two passes. One pass per joint face with the nozzle held at an angle directly toward the joint face and not more than 3 inches (75 mm) from it. After final cleaning and immediately prior to sealing, blow out the joints with compressed air and leave them completely free of debris and water. The joint faces shall be surface dry when the seal is applied.

c. Backer Rod. When the joint opening is of a greater depth than indicated for the sealant depth, plug or seal off the lower portion of the joint opening using a backer rod in accordance with paragraph 605-2.2 to prevent the entrance of the sealant below the specified depth. Take care to ensure that the backer rod is placed at the specified depth and is not stretched or twisted during installation.

d. Bond-breaking tape. Where inserts or filler materials contain bitumen, or the depth of the joint opening does not allow for the use of a backup material, insert a bond-separating tape breaker in accordance with paragraph 605-2.3 to prevent incompatibility with the filler materials and three-sided adhesion of the sealant. Securely bond the tape to the bottom of the joint opening so it will not float up into the new sealant.

605-3.4 Installation of sealants. Joints shall be inspected for proper width, depth, alignment, and preparation, and shall be approved by the RPR before sealing is allowed. Sealants shall be installed in accordance with the following requirements:

Immediately preceding, but not more than 50 feet (15 m) ahead of the joint sealing operations, perform a final cleaning with compressed air. Fill the joints from the bottom up to 1/4 inch (6 mm) \pm 1/16 inch (2 mm) below the top of pavement surface; or bottom of groove for grooved pavement. Remove and discard excess or spilled sealant from the pavement by approved methods. Install the sealant in such a manner as to prevent the formation of voids and entrapped air. In no case shall gravity methods or pouring pots be used to install the sealant material. Traffic shall

not be permitted over newly sealed pavement until authorized by the RPR. When a primer is recommended by the manufacturer, apply it evenly to the joint faces in accordance with the manufacturer's instructions. Check the joints frequently to ensure that the newly installed sealant is cured to a tack-free condition within the time specified.

605-3.5 Inspection. The Contractor shall inspect the joint sealant for proper rate of cure and set, bonding to the joint walls, cohesive separation within the sealant, reversion to liquid, entrapped air and voids. Sealants exhibiting any of these deficiencies at any time prior to the final acceptance of the project shall be removed from the joint, wasted, and replaced as specified at no additional cost to the airport.

605-3.6 Clean-up. Upon completion of the project, remove all unused materials from the site and leave the pavement in a clean condition.

METHOD OF MEASUREMENT

605-4.1 Joint sealing material shall be considered incidental to Item P-101.

BASIS OF PAYMENT

605-5.1 Payment for joint sealing material shall be considered incidental to Item P-101. The price shall be full compensation for furnishing all materials, for all preparation, delivering, and placing of these materials, and for all labor, equipment, tools, and incidentals necessary to complete the item.

REFERENCES

The publications listed below form a part of this specification to the extent referenced. The publications are referred to within the text by the basic designation only.

ASTM International (ASTM)

ASTM D789	Standard Test Method for Determination of Relative Viscosity of Polyamide (PA)
ASTM D5249	Standard Specification for Backer Material for Use with Cold- and Hot-Applied Joint Sealants in Portland-Cement Concrete and Asphalt Joints
ASTM D6690	Standard Specification for Joint and Crack Sealants, Hot Applied, for Concrete and Asphalt

Advisory Circulars (AC)

AC 150/5340-30	Design and Installation Details for Airport Visual Aids
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END ITEM P-605

preparation, delivery, installation, and curing of these materials, and for all labor, equipment, tools, and incidentals necessary to complete the item.

REFERENCES

The publications listed below form a part of this specification to the extent referenced. The publications are referred to within the text by the basic designation only.

ASTM International (ASTM)

ASTM A615	Standard Specification for Deformed and Plain Carbon-Steel Bars for Concrete Reinforcement
ASTM C31	Standard Practice for Making and Curing Concrete Test Specimens in the Field
ASTM C33	Standard Specification for Concrete Aggregates
ASTM C39	Standard Test Method for Compressive Strength of Cylindrical Concrete Specimens
ASTM C94	Standard Specification for Ready-Mixed Concrete
ASTM C136	Standard Test Method for Sieve or Screen Analysis of Fine and Coarse Aggregates
ASTM C114	Standard Test Methods for Chemical Analysis of Hydraulic Cement
ASTM C136	Standard Test Method for Sieve Analysis of Fine and Coarse Aggregates
ASTM C143	Standard Test Method for Slump of Hydraulic-Cement Concrete
ASTM C150	Standard Specification for Portland Cement
ASTM C171	Standard Specification for Sheet Materials for Curing Concrete
ASTM C231	Standard Test Method for Air Content of Freshly Mixed Concrete by the Pressure Method
ASTM C260	Standard Specification for Air-Entraining Admixtures for Concrete
ASTM C311	Standard Test Methods for Sampling and Testing Fly Ash or Natural Pozzolans for Use in Portland-Cement Concrete
ASTM C494	Standard Specification for Chemical Admixtures for Concrete

Item P-620 Runway and Taxiway Marking

DESCRIPTION

620-1.1 This item shall consist of the preparation and painting of numbers, markings, and stripes on the surface of runways, taxiways, and aprons, in accordance with these specifications and at the locations shown on the plans, or as directed by the Resident Project Representative (RPR). The terms “paint” and “marking material” as well as “painting” and “application of markings” are interchangeable throughout this specification.

MATERIALS

620-2.1 MATERIALS ACCEPTANCE. The Contractor shall furnish manufacturer’s certified test reports, for materials shipped to the project. The certified test reports shall include a statement that the materials meet the specification requirements. This certification along with a copy of the paint manufacturer’s surface preparation; marking materials, including adhesion, flow promoting and/or floatation additive; and application requirements must be submitted and approved by the Resident Project Representative (RPR) prior to the initial application of markings. The reports can be used for material acceptance or the RPR may perform verification testing. The reports shall not be interpreted as a basis for payment. The Contractor shall notify the RPR upon arrival of a shipment of materials to the site. All material shall arrive in sealed containers that are easily quantifiable for inspection by the RPR.

620-2.2 MARKING MATERIALS.

TABLE 1. MARKING MATERIALS

Paint ¹				Glass Beads ²	
Type	Color	Fed Std. 595 Number	Application Rate Maximum	Type	Application Rate Minimum
Waterborne Type I	Yellow	33538 or 33655	115 ft ² /gal (2.8 m ² /l)	Type I, Gradation A	7 lb/gal (0.85 kg/l)
Waterborne Type I	Black	37038	115 ft ² /gal (2.8 m ² /l)	Not used	Not used
Temporary Marking Waterborne Type I	Same as Above	Same as Above	230 ft ² /gal (5.6 m ² /l)	Not Used	Not Used

¹ See paragraph 620-2.2a

² See paragraph 620-2.2b

Suitable equipment necessary for proper preparation and treatment of the ground surface, stripping of topsoil, and for the handling and placing of all required materials shall be on hand, in good condition, and approved by the RPR before the various operations are started.

905-3.2 PREPARING THE GROUND SURFACE. Immediately prior to dumping and spreading the topsoil on any area, the surface shall be loosened by discs or spike-tooth harrows, or by other means approved by the RPR, to a minimum depth of 2 inches (50 mm) to facilitate bonding of the topsoil to the covered subgrade soil. The surface of the area to be topsoiled shall be cleared of all stones larger than 2 inches (50 mm) in any diameter and all litter or other material which may be detrimental to proper bonding, the rise of capillary moisture, or the proper growth of the desired planting. Limited areas, as shown on the plans, which are too compact to respond to these operations shall receive special scarification.

Grades on the area to be topsoiled, which have been established by others as shown on the plans, shall be maintained in a true and even condition. Where grades have not been established, the areas shall be smooth-graded and the surface left at the prescribed grades in an even and compacted condition to prevent the formation of low places or pockets where water will stand.

905-3.3 OBTAINING TOPSOIL. Prior to the stripping of topsoil from designated areas, any vegetation, briars, stumps and large roots, rubbish or stones found on such areas, which may interfere with subsequent operations, shall be removed using methods approved by the RPR. Heavy sod or other cover, which cannot be incorporated into the topsoil by discing or other means shall be removed.

When suitable topsoil is available on the site, the Contractor shall remove this material from the designated areas and to the depth as directed by the RPR. The topsoil shall be spread on areas already tilled and smooth-graded, or stockpiled in areas approved by the RPR. Any topsoil stockpiled by the Contractor shall be rehandled and placed without additional compensation. Any topsoil that has been stockpiled on the site by others, and is required for topsoil purposes, shall be removed and placed by the Contractor. The sites of all stockpiles and areas adjacent thereto which have been disturbed by the Contractor shall be graded if required and put into a condition acceptable for seeding.

When suitable topsoil is secured off the airport site, the Contractor shall locate and obtain the supply, subject to the approval of the RPR. The Contractor shall notify the RPR sufficiently in advance of operations in order that necessary measurements and tests can be made. The Contractor shall remove the topsoil from approved areas and to the depth as directed. The topsoil shall be hauled to the site of the work and placed for spreading, or spread as required. Any topsoil hauled to the site of the work and stockpiled shall be rehandled and placed without additional compensation.

905-3.4 PLACING TOPSOIL. The topsoil shall be evenly spread on the prepared areas to a uniform depth of 3 inches (50 mm) after compaction, unless otherwise shown on the plans or stated in the special provisions. Spreading shall not be done when the ground or topsoil is frozen, excessively wet, or otherwise in a condition detrimental to the work. Spreading shall be carried on so that turving operations can proceed with a minimum of soil preparation or tilling.

After spreading, any large, stiff clods and hard lumps shall be broken with a pulverizer or by other effective means, and all stones or rocks (2 inches (50 mm) or more in diameter), roots, litter, or any foreign matter shall be raked up and disposed of by the Contractor. After spreading is completed, the topsoil shall be satisfactorily compacted by rolling with a cultipacker or by other means approved by the

RPR. The compacted topsoil surface shall conform to the required lines, grades, and cross-sections. Any topsoil or other dirt falling upon pavements as a result of hauling or handling of topsoil shall be promptly removed.

METHOD OF MEASUREMENT

905-4.1 Topsoil obtained on the site shall be measured by the number of square yards (square meters) of topsoil measured in its original position and stripped or excavated. Topsoil stockpiled by others and removed for topsoil by the Contractor shall be measured by the number of square yards (square meters) of topsoil measured in the stockpile. Topsoil shall be measured by area in square yards (square meters) computed by the method of end areas.

BASIS OF PAYMENT

905-5.1 Payment will be made at the contract unit price per square yard (square meter) for topsoil (obtained on the site). This price shall be full compensation for furnishing all materials and for all preparation, placing, and spreading of the materials, and for all labor, equipment, tools, and incidentals necessary to complete the item.

Payment will be made under:

Item T-905a	Topsoil - per square yard (square meter)
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REFERENCES

The publications listed below form a part of this specification to the extent referenced. The publications are referred to within the text by the basic designation only.

ASTM International (ASTM)

ASTM C117	Materials Finer than 75 μ m (No. 200) Sieve in Mineral Aggregates by Washing
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Advisory Circulars (AC)

AC 150/5200-33	Hazardous Wildlife Attractants on or Near Airports
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FAA/United States Department of Agriculture

Wildlife Hazard Management at Airports, A Manual for Airport Personnel
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END OF ITEM T-905

1. IF DURING THE CONSTRUCTION PROCESS, CONDITIONS ARE ENCOUNTERED WHICH COULD INDICATE A SITUATION THAT IS NOT IDENTIFIED IN THE PLANS OR SPECIFICATIONS, OR REPRESENT A SIGNIFICANT DIFFERENCE BETWEEN THE CONTRACT DOCUMENTS AND FIELD CONDITIONS, THE CONTRACTOR SHALL CONTACT THE ENGINEER IMMEDIATELY.

11. PRIOR TO OPENING OR CLOSING A TAXIWAY, THE CONTRACTOR MUST, THROUGH THE AIRPORT, GIVE NOTICE USING THE NOTICE TO AIRMEN (NOTAM) SYSTEM OF PROPOSED LOCATION, TIME AND DATE OF COMMENCEMENT OF CONSTRUCTION AND THE DURATION OF THE CLOSURE.
12. THIS PROJECT WILL GENERATE QUANTITIES OF ASPHALT MILLINGS. THE CONTRACTOR SHALL COORDINATE WITH THE SPONSOR AND/OR THE ENGINEER FOR PLACEMENT LOCATIONS. THE MILLINGS WILL BE PLACED AND ROLLED IN ON-SITE LOCATIONS DESIGNATED BY THE SPONSOR AND/OR THE ENGINEER IN ACCORDANCE WITH ITEM P-101. THE PLACEMENT OF MILLINGS SHALL BE CONSIDERED INCIDENTAL TO ITEMS P-101. ALL MILLINGS PLACED WILL RECEIVE A TACK COAT APPLICATION IN ACCORDANCE WITH P-603.

13. DESIGNS CONTAINED HEREIN ARE BASED ON SPECIFIED EQUIPMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY REDESIGN FOR EQUIPMENT SUBSTITUTIONS TO THE APPROVED SPECIFICATIONS AT NO ADDITIONAL COST TO THE SPONSOR. THE CONTRACTOR SHALL PROVIDE ALL MATERIAL, SUBMITTALS, SAMPLES AND DESIGN DRAWINGS FOR THE ENGINEER'S APPROVAL A MINIMUM OF SEVEN (7) DAYS PRIOR TO ORDERING.
14. THE AIRPORT RESERVES THE RIGHT TO LOAD, HAUL, AND STOCKPILE, WITH THEIR OWN EQUIPMENT, ANY AND/OR ALL ASPHALT MILLINGS GENERATED FROM DEMOLITION OPERATIONS.
15. ALL MATERIALS, WORKMANSHIP, AND CONSTRUCTION OF PUBLIC IMPROVEMENTS SHALL MEET OR EXCEED THE STANDARDS AND SPECIFICATIONS SET FORTH IN THE PROJECT MANUAL FOR THIS PROJECT, F&A STANDARDS, AND MISSOURI STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, AND APPLICABLE STATE AND FEDERAL REGULATIONS. WHERE THERE IS A CONFLICT BETWEEN THESE PLANS AND THE SPECIFICATIONS, OR ANY APPLICABLE STANDARDS, THE HIGHER QUALITY STANDARD SHALL APPLY. ALL WORK SHALL BE INSPECTED AND APPROVED BY THE DESIGN ENGINEER.
16. DIMENSIONING FOR LAYOUTS AND CONSTRUCTION ARE NOT TO BE SCALED FROM ANY DRAWINGS. IF PERTINENT DIMENSIONS ARE NOT SHOWN, CONTACT THE ENGINEER FOR CLARIFICATION AND RECORD DIMENSIONS ON AS-BUILT DRAWINGS.
17. TOPSOIL SHALL BE REMOVED AND STOCKPILED PRIOR TO GRADING OPERATIONS. PAYMENT FOR RE-HANDLING OF TOPSOIL SHALL NOT BE MEASURED FOR PAYMENT.
18. ALL WASTE GENERATED FROM CLEARING AND GRUBBING SHALL BE REMOVED OFF SITE AND ALL ASSOCIATED COST SHALL BE CONSIDERED INCIDENTAL TO THIS PROJECT.
19. CONTRACTOR SHALL HAVE A COPY OF THE CURRENT F&A ADVISORY CIRCULAR AC 15033401 (CURRENT VERSION) "STANDARDS FOR AIRPORT MARKINGS" ON SITE AT ALL TIMES. ANY DISCREPANCY BETWEEN INFORMATION SHOWN ON THE PLAN SHEETS AND THE ADVISORY CIRCULAR SHALL BE COORDINATED WITH THE ENGINEER FOR DIRECTION.

1. FOURTEEN (14) DAYS PRIOR TO THE BEGINNING OF WORK, THE CONTRACTOR SHALL SUBMIT A QUALITY CONTROL PLAN WHICH INCLUDES A WORK SCHEDULE AND PROPOSED CONSTRUCTION METHODS CONSISTENT WITH THE PHASING PLAN STATED IN THE DESIGN.

2. THE CONTRACTOR SHALL HAVE A MINIMUM OF ONE (1) CURRENT COPY OF THE APPROVED PLANS (INCLUDING ANY CHANGE ORDERS, SUPPLEMENTAL AGREEMENTS, FIELD DIRECTIVES, ETC.), ONE (1) CURRENT COPY OF THE APPROPRIATE STANDARDS AND SPECIFICATIONS, AND A COPY OF ANY PERMITS AND EXTENSION AGREEMENTS NEEDED FOR THE JOB, ON SITE AT ALL TIMES.

1. PRELIMINARY PERMITTING INFORMATION WILL BE SUBMITTED BY THE ENGINEER PRIOR TO AWARD OF CONTRACT. SPECIFIC ITEMS THAT WILL NEED TO BE COMPLETED BY THE CONTRACTOR INCLUDE BUT ARE NOT LIMITED TO SUPPLYING NECESSARY BONDING, PAYMENT OF ALL FEES, REVIEW OF ALL CALCULATIONS AND ASSUMPTIONS MADE BY THE ENGINEER PRIOR TO AWARD. THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS INCLUDING, BUT NOT LIMITED TO, AN FAA 7460-1 NOTICE OF PROPOSED CONSTRUCTION OR ALTERATION, A NPDES STORMWATER PERMIT, AND A FUGITIVE DUST PERMIT. THE CONTRACTOR SHALL BE RESPONSIBLE TO PAY FOR THE COST TO OBTAIN ALL PERMITS.

2. THE CONTRACTOR SHALL SUBMIT A COPY OF ALL PERMITS REQUIRED FOR THE PROJECT TO THE ENGINEER, FOR HIS REVIEW.

1. THE CONTRACTOR IS REQUIRED TO OBTAIN A WATER METER FROM ROLLA MUNICIPAL UTILITIES FOR ACCESS TO CONSTRUCTION WATER FROM AN ON AIRPORT LOCATION. THE CONTACT PHONE NUMBER IS (573) 364-1572. A WATER SOURCE ON AIRPORT PROPERTY AND THE PRICE OF WATER WILL BE COORDINATED AT THE TIME OF BIDDING.

2. DURING CONSTRUCTION, THE CONTRACTOR SHALL MINIMIZE DISTURBANCES TO ALL CONSTRUCTION AREAS AND ACCESS ROADS. THIS INCLUDES EQUIPMENT AND VEHICULAR TRAFFIC CREATED IN ANY PAVEMENTS, ANY HAUILLAGE ROADS, OR ANY INFILDS/SAFETY AREAS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO EXISTING FACILITIES OR ROADS. REPAIRS SHALL BE MADE AT NO ADDITIONAL COST TO THE SPONSOR AND TO THE SATISFACTION OF THE SUPERVISOR. NO ROAD OR INFILDS/SAFETY AREAS SHALL BE CLOSED OR TRAFFIC SHALL BE DIVERTED TO AN OFF-ROAD PAVEMENT. COSTS ASSOCIATED WITH HAIL, BRIDGE CONSTRUCTION WILL BE CONSIDERED INCIDENTAL TO MOBILIZATION.
3. BEFORE ESTABLISHING SITE ACCESS AND HAIL ROADS, THE CONTRACTOR SHALL OBTAIN APPROVAL FROM THE ENGINEER. WHEN POSSIBLE, ACCESS/HAIL ROUTES SHALL UTILIZE EXISTING ROADS. THE CONTRACTOR SHALL MAINTAIN AIRPORT SECURITY AT ALL TIMES.
4. ALL DAMAGE TO UTILITIES, PAVEMENT, EQUIPMENT, OR STRUCTURES FROM CONSTRUCTION ACTIVITIES SHALL BE IMMEDIATELY REPORTED TO THE RESIDENT ENGINEER. THE RESIDENT ENGINEER SHALL DETERMINE WHETHER REPAIR OR REPLACEMENT IS NECESSARY. ALL REPAIR METHODS SHALL BE SUBMITTED TO THE RESIDENT ENGINEER FOR REVIEW AND APPROVAL PRIOR TO INITIATING THE WORK. REPAIRS SHALL BE MADE AT NO ADDITIONAL COST TO THE SPONSOR AND TO THE APPROVAL OF THE ENGINEER.
5. CONTRACTOR SHALL EXAMINE THE EXISTING PAVEMENTS THAT WILL BE USED FOR HAULING OF MATERIAL AND EQUIPMENT, AND DETERMINE THE PAVEMENTS ABILITY TO WITHSTAND CONTRACTOR REPAIR OPERATIONS WITHOUT CAUSING DAMAGE TO THE PAVEMENT. ANY DAMAGE CAUSED BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR TO THE APPROVAL OF THE ENGINEER AND AT NO ADDITIONAL COST TO THE SPONSOR.

6. CONTRACTOR SHALL BE REQUIRED TO PROVIDE NON-POTABLE WATER FOR CONSTRUCTION PURPOSES. CONTRACTOR SHALL BE RESPONSIBLE FOR STORAGE OF NON-POTABLE WATER ANY STRUCTURES ERECTED IN SUPPORT OF WATERING OPERATIONS SHALL MEET FAR PART 77 CLEARANCES FOR ALL DUST AND BE APPROPRIATELY LIT AS A HAZARD TO THE FLYING PUBLIC. NON-POTABLE WATER USED FOR P-152 OR CURT CONTROL SHALL BE INCIDENTAL TO THE PROJECT BID ITEMS.
7. DURING ANY NIGHTTIME OPERATIONS ALL AREA LIGHTING SHALL FACED IN DIRECTIONS AS DIRECTED BY THE ENGINEER. AT ANY TIME THAT LIGHT PLANTS BE LEFT RUNNING WHEN CONSTRUCTION OPERATIONS ARE NOT IN PROCESS.
8. ALL AREAS THAT ARE DISTURBED BY CONTRACTOR OPERATIONS, SHALL BE SEEDDED PER 7-501 SEEDING AND C-102 EROSION CONTROL. SEEDING AND EROSION CONTROL COSTS SHALL BE PAID SEPARATELY THEIR RESPECTIVE BID ITEMS.

9. ALL CONTRACTOR EMPLOYEES SHALL BE REQUIRED TO PARK IN THE CONTRACTORS DESIGNATED STAGING AREA ONLY AND SHALL BE DRIVEN TO THE PROJECT SITE BY DESIGNATED CONSTRUCTION VEHICLES

10. CRAWLER TRACKED VEHICLES SHALL NOT BE ALLOWED ON PAVED SURFACES. TRACKED VEHICLES MUST BE MOVED

11. WHENEVER CONSTRUCTION TRAFFIC IS REQUIRED TO CROSS AN ACTIVE RUNWAY, TAXIWAY, TAXILANE, OR INTERRUPT NORMAL TRAFFIC FLOW ON APRONS OR RAMPS, THE CONTRACTOR SHALL PROVIDE FLAGGERS AT THE CROSSING(S) AS REQUIRED BY THE CONSTRUCTION PHASING DRAWINGS OR AS DIRECTED BY THE ENGINEER OR THE AIRPORT (INCIDENTAL TO ITEM C-105).

1. PRIOR TO COMMENCING WORK, IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO CONTACT THE APPROPRIATE UTILITY AGENCIES, UTILITY NOTIFICATION CENTERS, AND TO FIELD VERIFY THE LOCATIONS AND DEPTHS, THROUGH UTILITY LOCATING DEVICES AND POTHOLES, OF ALL EXISTING UTILITIES WITHIN THE PROJECT LIMITS, STAGING, AND Haul Rte AREAS.
2. THE EXISTING UTILITY LOCATIONS SHOWN ON THE PLANS ARE APPROXIMATE AND SHALL NOT BE SCALED FOR EXACT LOCATION. LOCATION OF EXISTING DUCT BANK, CIRCULATING, UTILITIES AND STRUCTURES SHOWN ON THESE DRAWINGS IS FOR INFORMATION ONLY. INFORMATION IS NOT WARRANTED TO BE EXACT. NOR IS IT WARRANTED THAT ALL OF THESE ITEMS ARE SHOWN.
3. CONTRACTOR SHALL CONTACT AND COORDINATE WITH THE APPROPRIATE UTILITY AGENCIES WHEN WORKING ON OR WITHIN THE PROXIMITY OF AN AGENCIES UTILITY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RELOCATE EXISTING UTILITIES TO AVOID THE PROPOSED CONSTRUCTION.

4. ANY INTERRUPTION OF ANY EXISTING SYSTEM OR UTILITY SERVICE SHALL BE COORDINATED AND APPROVED BY THE AIRPORT AND THE AUTHORITY, AGENCY, OR UTILITY HAVING JURISDICTION, PRIOR TO STARTING WORK INCLUDING CONTACTING THE AIRPORT AND FAA.
5. THE CONTRACTOR IS RESPONSIBLE FOR DAMAGES TO EXISTING UTILITIES, REPAIRS DEEMED NECESSARY BY THE ENGINEER WILL BE COMPLETED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE SPONSOR. SEE SECTION 50 OF THE CONTRACT DOCUMENTS AND THE "CONSTRUCTION STAKING AND LAYOUT" NOTES CONTAINED IN THESE GENERAL NOTES SHEETS FOR ADDITIONAL NOTES REGARDING UTILITY LOCATES.
6. THE CONTRACTOR SHALL COORDINATE AND COOPERATE WITH THE CITY, COUNTY, AND ALL UTILITY COMPANIES INVOLVED, WITH REGARD TO RELOCATIONS OR ADJUSTMENTS OF EXISTING UTILITIES DURING CONSTRUCTION, AND TO AVOID THE THE SAME AS FAR AS POSSIBLE IN A TIMELY FASHION AND WITH A MINIMUM DISRUPTION OF SERVICE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING ALL PARTIES AFFECTED BY ANY DISRUPTION OF ANY SERVICE.
7. WHERE NEW DUCT BANKS AND/OR OTHER UTILITIES ARE NEAR EXISTING UTILITIES, THE CONTRACTOR SHALL HAND EXCAVATE AROUND THE EXISTING UTILITIES IN ORDER TO AVOID DAMAGE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR IMMEDIATELY REPAIRING ANY UTILITY DAMAGED DURING CONSTRUCTION.
8. THE CONTRACTOR SHALL SEQUENCE INSTALLATION OF UTILITIES IN SUCH A MANNER AS TO MINIMIZE POTENTIAL UTILITY CONFLICTS. IN GENERAL, STORM SEWER AND SANITARY SEWER SHOULD BE CONSTRUCTED PRIOR TO INSTALLATION OF THE WATER LINES AND DRY UTILITIES.
9. CONTRACTOR SHOULD EXPECT TO ENCOUNTER WATER IN LIGHT CANS, JUNCTION CANS AND STRUCTURES. CONTRACTOR RESPONSIBLE FOR Dewatering AT NO ADDITIONAL COST TO THE OWNER.
10. THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING ALL UNDERGROUND UTILITIES PRIOR TO BEGINNING CONSTRUCTION. THE CONTRACTOR MAY USE THE FOLLOWING TOLL FREE TELEPHONE NUMBER PROVIDED BY "NORTHSTAR CALL SYSTEM, INC." - 800-DIG-RT-1. THIS NUMBER IS APPLICABLE WHEREVER WITHIN THE STATE OF MISSOURI.

1. THE CONTRACTOR SHALL SUBMIT A DETAILED LISTING OF ALL SUBMITTALS (E.G. MIX DESIGNS, MATERIAL CERTIFICATION, AND PRODUCT INFORMATION) AND SHOP DRAWINGS REQUIRED BY THE TECHNICAL SPECIFICATIONS.
2. THE CONTRACTOR SHALL REVIEW THE CONTRACT DOCUMENTS SECTION 10-05 FOR SUBMITTAL SCHEDULE REQUIREMENTS.
3. THE CONTRACTOR SHALL PROVIDE MATERIAL SUBMITTALS FOR THE ENGINEER'S APPROVAL AT LEAST SEVEN (10) DAYS PRIOR TO ORDERING.

1. THE CONTRACTOR IS RESPONSIBLE FOR COORDINATING AND PROVIDING ALL REQUIRED TRAFFIC CONTROL FOR THE PROJECT'S ACCESS LOCATIONS, INCLUDING ANY REQUIREMENTS OF MODOT, TOWN OF ROLLA, OR MARIES COUNTY. ALL ASSOCIATED COSTS ARE INCIDENTAL TO CONSTRUCTION AND WILL NOT BE PAID SEPARATELY.

2. THE CONTRACTOR SHALL SUBMIT A TRAFFIC CONTROL PLAN IN ACCORDANCE WITH LOCAL JURISDICTIONAL REQUIREMENTS FOR APPROVAL PRIOR TO THE START OF ANY CONSTRUCTION ACTIVITIES. CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING AND MAINTAINING ANY AND ALL TRAFFIC CONTROL DEVICES.

1. ALL MATERIALS MUST BE OBTAINED FROM AN UNDESIGNATED SOURCE UNLESS OTHERWISE IDENTIFIED IN THE PLANS OR SPECIFICATIONS.
2. ALL WASTE MATERIALS SHALL BE REMOVED FROM THE AIRPORT PROPERTY AT NO COST TO THE SPONSOR UNLESS OTHERWISE DIRECTED BY THE SPONSOR.

1. THE STRUCTURES SHOWN OR DEFINED IN THE CONTRACT DOCUMENTS AND PLANS HAS BEEN DESIGNED ONLY FOR LOADS ANTICIPATED ON THE STRUCTURE DURING ITS SERVICE LIFE. CONTRACTOR SHALL PROVIDE ALL REQUIRED ENGINEERING AND OTHER MEASURES TO ACHIEVE THE MEANS, METHODS, AND SEQUENCES OF WORK. REQUIRED ENGINEERING MAY INCLUDE, BUT IS NOT LIMITED TO:

- DESIGN FOR FORMWORK, SHORING, AND RESHORING
- DESIGN OF CONCRETE MIXES
- ERECTION PROCEDURES WHICH ADDRESS STABILITY OF THE FRAME DURING CONSTRUCTION
- WELD PROCEDURES
- DESIGN OF TEMPORARY BRACING OF WALLS FOR WIND, SEISMIC, OR SOIL LOADS
- SURVEYING TO VERIFY CONSTRUCTION TOLERANCES
- EVALUATION OF TEMPORARY CONSTRUCTION LOADS ON STRUCTURE DUE TO EQUIPMENT AND MATERIALS
- STRUCTURAL ENGINEERING TO RESIST ANY OTHER LOADS NOT IDENTIFIED ON DESIGN DRAWINGS.

1. REFER TO SHEET C220 FOR TYPICAL SECTIONS.
2. REFER TO SHEET C300 - C304 FOR SPOT ELEVATIONS.
3. THE DEPTH OF EXISTING UTILITIES ARE APPROXIMATE AND WERE DETERMINED ASSUMING A BURIAL DEPTH OF 2'-0". ACTUAL DEPTHS MAY VARY AND SHOULD BE FIELD VERIFIED.
4. IN THE EVENT OF ANY CONFLICT WITHIN THESE PLANS, THE INFORMATION IN THE PROFILES SHALL GOVERN OVER THE SPOT ELEVATIONS AND CROSS SECTIONS.
5. CONTRACTOR TO VERIFY EXISTING TIE/CONNECTION POINTS PRIOR TO CONSTRUCTION AND NOTIFY THE ENGINEER OF ANY DISCREPANCIES.

1. LIMITS OF GRADING ARE APPROXIMATE AND DO NOT CONSTITUTE LIMITS OF DISTURBANCE. CONTRACTOR SHALL BE RESPONSIBLE TO RESTORE ALL ADDITIONAL AREAS DISTURBED BY CONSTRUCTION OPERATIONS AT NO COST TO SPONSOR. THIS INCLUDES, BUT IS NOT LIMITED TO, MINOR GRADING, TOPSOILING, TEMPORARY AND PERMANENT EROSION CONTROL MEASURES.
2. ANY ADDITIONAL ITEMS REQUIRED FOR INSTALLATION OR MAINTENANCE OF TEMPORARY OR PERMANENT EROSION CONTROL MEASURES SHALL NOT BE PAID SEPARATELY BUT SHALL BE INCIDENTAL TO VARIOUS ITEMS.

3. ALL AREAS THAT ARE DISTURBED BY CONTRACTOR OPERATIONS SHALL BE SEEDED PER T-601 SEEDING AND C-102 EROSION CONTROL. ALL EROSION CONTROL (INCLUDING EXCISLOR LOGS, EROSION PROTECTION BLANKETS, SILT FENCE AND INLET PROTECTION) COSTS ARE INCIDENTAL TO CONSTRUCTION AND WILL NOT BE PAID SEPARATELY.
4. TEMPORARY AND PERMANENT EROSION CONTROL PRACTICES SHALL BE MAINTAINED AND REPAIRED BY THE CONTRACTOR DURING THE CONSTRUCTION PHASES AS NEEDED TO ENSURE CONTINUED PERFORMANCE OF THEIR INTENDED FUNCTION.
5. SILT FENCE IS TO BE INSTALLED PRIOR TO START OF CLEARING AND GRUBBING OPERATIONS.
6. ANY SEDIMENT TRACKED ONTO PAVED SECTIONS, REGARDLESS OF LOCATION OR QUANTITY, SHALL BE IMMEDIATELY CLEANED.
7. ALL LIMITS OF SEEDING AND HYDRAULIC MULCHING ARE APPROXIMATE.
8. ALL DISTURBED GROUND SHALL BE SEEDED AND HYDRAULIC MULCHED WITHIN 14 DAYS AFTER COMPLETION OF FINISHED GRADE.
9. ALL TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES SHALL BE REMOVED AND DISPOSED WITHIN 30 DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED, OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED, WHICHEVER OCCURS EARLIEST, OR AS AUTHORIZED BY THE LOCAL GOVERNING JURISDICTION. TRAPPED SEDIMENT AND DISTURBED SOIL AREAS RESULTING FROM THE DISPOSAL OF TEMPORARY MEASURES MUST BE RETURNED TO FINAL PLAN GRADES AND PERMANENTLY STABILIZED TO PREVENT FURTHER SOIL EROSION.

1. DURING CONSTRUCTION, THE CONTRACTOR SHALL COMPLY WITH FAA ADVISORY CIRCULAR (AC) 150/5370-2 (CURRENT VERSION), "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION".
2. THE CONTRACTOR SHALL REVIEW THE CONSTRUCTION SAFETY AND PHASING PLAN (CSPP) CONTAINED IN THE CONTRACT DOCUMENTS, AND PREPARE FOR APPROVAL BY THE ENGINEER, A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) PRIOR TO NOTICE TO PROCEED, AS REQUIRED PER ADVISORY CIRCULAR (AC) 150/5370-2 (CURRENT VERSION), "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION".

4. ALL VEHICLES AND EQUIPMENT WORKING REGULARLY ON THE PROJECT SITE SHALL BE EQUIPPED WITH STANDARD FAA MARKINGS PER FAA ADVISORY CIRCULAR 150/521-5. (CURRENT VERSION) OR BE ESCORTED BY A PROTECTED LIGHTED PILOT OR PILOTS. ALL EQUIPMENT OPERATING IN THE AIRPORT PERMETER FENCE MUST BE USED DURING DAYTIME OPERATIONS OR A FLASHING BEACON MAY BE USED AT ANY TIME. FAILURE TO PROVIDE SUCH MARKINGS OR ESCORT FOR ANY EQUIPMENT INSIDE THE AIRPORT PERMETER FENCE WILL PRECLUDE THAT EQUIPMENT FROM OPERATING INSIDE THE AIRPORT PERMETER FENCE. ALL EQUIPMENT SHALL BE PROXIMITY MONITORED BY THE CONTRACTOR. BACKUP ALARMS SHALL BE ADJUSTED FOR SURROUNDING NOISE LEVELS. TRITON HARK-ALARM OR APPROVED EQUIVA.
5. THE CONTRACTOR SHALL PROVIDE THREE TAXIWAY CLOSURE MARKERS (TCM) AND TWO LIGHTED RUNWAY CLOSURE MARKERS (RCM). RCMs ARE PORTABLE, TRAILER MOUNTED, DIESEL GENERATOR POWERED UNITS, CAPABLE OF PROVIDING LIGHTED PIVOT POINTS TO THE RUNWAY. THE CONTRACTOR SHALL PROVIDE THE FOLLOWING SPECIFICATION L-893. THE CONTRACTOR SHALL PLACE THE TCM'S IN FRONT OF THE TAXIWAY A AND TAXIWAY B. THE CONTRACTOR SHALL PROVIDE THE TCM'S TO THE PROJECT. LIGHT CHANGES SHALL TAKE PLACE WHENEVER THE CLOSURE TO AIR TRAFFIC. THE CONTRACTOR SHALL MAKE SURE THE TCM'S ARE PROPERLY ANCHORED, SO THEY CANNOT MOVE IN HEAVY WINDS. THE CONTRACTOR SHALL FURNISH ALL DIESEL FUELS, LIGHT CHANGES, FILTERS, LAMP'S, AND BATTERIES. THE CONTRACTOR SHALL PROVIDE THE TCM'S TO THE PROJECT. THE CONTRACTOR SHALL PROVIDE 150 HOURS OF OPERATION. SEE RUNWAY CLOSURE MARKING (RCM) AND TAXIWAY CLOSURE MARKING (TCM) NOTES ON SHEET G050 FOR ADDITIONAL INFORMATION. TCM'S SHALL BE TRANSFERRED AND STORED ON AN IMPROVED SURFACE ADJACENT ASSOCIATED WITH USE, OPERATION, AND MAINTENANCE OF TCM'S. TCM'S SHALL BE INCLUDED IN THE PROJECT.
6. ALL VEHICLES AND EQUIPMENT WORKING ON THE SITE SHALL BE EQUIPPED WITH STANDARD FAA MARKINGS PER FAA ADVISORY CIRCULAR 150/521-5. (CURRENT VERSION) OR BE ESCORTED BY A PROTECTED LIGHTED PILOT OR PILOTS. ALL EQUIPMENT OPERATING WITHIN THE AIRPORT'S PERMETER FENCE NOT PROPERLY MARKED OR ESCORTED MAY NOT OPERATE ON THE SITE AND MUST BE REMOVED IMMEDIATELY. ANY DELAY OR COST TO CONTRACTOR OPERATIONS FROM THE REMOVAL OF UNMARKED OR IMPROPERLY MARKED EQUIPMENT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. FAA VEHICLE FLAGS (3 FOOT BY 3 FOOT ORANGE AND WHITE) MAY BE USED DURING DAYTIME HOURS. FLASHING LIGHTS MAY BE USED AT ANY TIME. BACKUP ALARMS ARE REQUIRED AND SHALL BE PROXIMITY MONITORED AND ADJUSTED FROM SURROUNDING NOISE LEVELS. SEE THE CONSTRUCTION SAFETY AND PHASING PLAN (CSP) FOR MORE DETAILS.
7. CONTRACTOR SHALL MAINTAIN AIRPORT PERMETER SECURITY FOR THE DURATION OF THE PROJECT. ANY REVISIONS TO FENCE ALIGNMENT SHALL BE COORDINATED WITH ENGINEER FOR APPROVAL AT LEAST ONE WEEK PRIOR TO CONSTRUCTION. ALL COSTS SHALL BE INCIDENTAL TO PROJECT BID AMOUNT.
8. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL ASPECTS OF SAFETY INCLUDING, BUT NOT LIMITED TO, EXCAVATION, TRENCHING, SHORING, TRAFFIC CONTROL, AND SECURITY.

1. THE CONTRACTOR SHALL COMPLY WITH ALL TERMS AND CONDITIONS OF THE MISSOURI PERMIT FOR STORM WATER DISCHARGE, THE STORM WATER MANAGEMENT PLAN, THE EROSION CONTROL PLAN, AND ALL REQUIREMENTS OF THE LOCAL DRAINAGE AUTHORITY.

4. ALL STRUCTURAL EROSION CONTROL MEASURES SHALL BE INSTALLED, AT THE LIMITS OF CONSTRUCTION, PRIOR TO ANY OTHER GROUND-DISTURBING ACTIVITY. ALL EROSION CONTROL MEASURES SHALL BE MAINTAINED IN GOOD PRIORITY BY THE CONTRACTOR, UNTIL SUCH TIME AS THE ENTIRE DISTURBED AREA IS STABILIZED WITH HARD SURFACE OR LANDSCAPING.
3. CONTRACTOR SHALL MAINTAIN POSITIVE DUST CONTROL DURING THE ENTIRE PROJECT DURATION. THE METHOD OF DUST CONTROL EMPLOYED DURING ALL PHASES SHALL BE SUBMITTED FOR APPROVAL BY THE ENGINEER. DUST CONTROL SHALL BE EMPLOYED DURING ANY PROJECT SHUTDOWN PERIODS, WINTER OR OTHERWISE. PAYMENT FOR THIS WORK SHALL BE INCIDENTAL TO THE VARIOUS ITEMS OF WORK, AND NO SEPARATE PAYMENT WILL BE MADE.
4. ANY EROSION CONTROL FACILITY DAMAGED OR DESTROYED PREMATURELY, BY ANY MEANS, SHALL BE IMMEDIATELY REPAIRED BY THE CONTRACTOR.
5. A WATER TRUCK SHALL BE KEPT ON SITE AT ALL TIMES DURING EARTHWORK ACTIVITIES FOR DUST ABATEMENT.
6. THE STORMWATER BMPs SHOWN IN THE ISSUED FOR CONSTRUCTION EROSION CONTROL SHEETS ARE TO BE USED AS A GUIDE FOR THE CONTRACTOR WHEN DEVELOPING HIGHER STORMWATER MANAGEMENT PLAN. FIELD CONDITIONS MAY WARRANT MORE, LESS OR DIFFERENT BMP INSTALLATION. IT IS THE CONTRACTOR'S RESPONSIBILITY TO DEVELOP A STORMWATER AND EROSION CONTROL PLAN THAT MEETS ALL LOCAL, STATE, AND FEDERAL REQUIREMENTS ASSOCIATED WITH THE STORMWATER PERMIT.
7. SILT AND SEDIMENT SHALL BE REMOVED AFTER EACH SUBSTANTIAL RAINFALL.
8. NEGATIVE IMPACTS TO DOWNSTREAM AREAS CAUSED BY GRADING ARE TO BE MONITORED AND CORRECTED BY THE CONTRACTOR. ANY OFF-SITE CLEAN-UP, DIRECTED BY THE PUBLIC WORKS INSPECTOR, INCLUDING STREET CLEANING, SHALL BE COMPLETED WITHIN 24-HOURS OF WRITTEN INSTRUCTION, OR RISK CONSTRUCTION STOPPAGE.
9. TEMPORARY EROSION CONTROL MEASURES SHALL NOT BE REMOVED UNTIL SUCH TIME AS ALL TRIBUTARY-DISTURBED AREAS ARE SUFFICIENTLY STABILIZED IN THE OPINION OF THE PUBLIC WORKS INSPECTOR OR RESIDENT ENGINEER, TO MINIMIZE EROSION POTENTIAL.
10. WHEN TEMPORARY EROSION CONTROL MEASURES ARE REMOVED, THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE CLEAN-UP AND REMOVAL OF ALL SEDIMENT AND DEBRIS FROM ALL DRAINAGE AND OTHER PUBLIC FACILITIES.
11. ALL AREAS FOR SEEDING SHALL BE TILLED TO BREAK UP ROOTING RESTRICTIVE LAYERS, HAVE A MINIMUM OF 4" OF TOPSOIL REAPPLIED, AND THEN BE HARROWED, AND ROLLED OR PACKED, TO PREPARE THE REQUIRED FIRM SEED BED.

1. ALL STATED QUANTITIES ARE CONSIDERED APPROXIMATE. ACTUAL QUANTITIES WILL BE DETERMINED BY THE ENGINEER FROM WORK IN-PLACE.
2. ACTUAL RATES OF APPLICATION WILL BE DETERMINED BY THE ENGINEER.
3. THE PROJECT PAY ITEMS PROVIDED ARE TO BE INCLUSIVE OF ALL WORK TO BE PERFORMED AS SHOWN IN THE CONTRACT DOCUMENTS. ALL WORK NOT IDENTIFIED WITH A SPECIFIC PAY ITEM IS TO BE CONSIDERED REQUIRED WORK TO COMPLETE THE PROJECT, AND IS TO BE INCIDENTAL TO THE COST OF PROJECT PAY ITEMS PROVIDED.
4. ALL PAVEMENT REMOVAL SHALL BE MEASURED AND PAID TO NEAT LINE DIMENSIONS.

5. IF THE CONTRACTOR CHOOSES TO OVERBUILD PAVEMENT LAYERS BEYOND THE DIMENSIONS SHOWN ON THE PLANS FOR CONSTRUCTIBILITY, NO PAYMENT WILL BE MADE FOR THIS ADDITIONAL MATERIAL.
6. PIPE LENGTHS SHOWN ON PLANS ARE FROM CENTER OF STRUCTURE TO CENTER OF STRUCTURE.
7. THE FOLLOWING RATES WERE USED TO CALCULATE ESTIMATED QUANTITIES:
 - 7.1. BITUMINOUS PAVEMENT COURSE AT THE RATE OF 155 LBS PER CUBIC FOOT.
 - 7.2. ASPHALT ROTOMILLING IS BASED ON SQUARE YARD REGARDLESS OF ASPHALT DEPTH.
 - 7.3. EMULSIFIED BITUMINOUS TACK COAT AT THE RATE OF 0.15 GAL. PER SQUARE YARD PER LIFT.
 - 7.4. FOR OVERLAY PAVEMENT BITUMINOUS TACK COAT AT 0.15 GALLONS PER SQUARE YARD PER LIFT (UNDILUTED).

2. TWO WEEKS PRIOR TO START OF CONSTRUCTION, THE CONTRACTOR SHALL PROVIDE THE ENGINEER WITH A PRE-CONSTRUCTION SURVEY VERIFYING EXISTING ELEVATIONS OF ALL PAVEMENT AREAS AND OTHER CRITICAL AREAS DETERMINED BY THE ENGINEER. THE SURVEY SHALL BE PERFORMED USING SPECIFIED PROJECT CONTROL AND SHALL BE PROVIDED TO THE ENGINEER IN A DIGITAL AND PRINTED FORMAT. THE SURVEY SHALL BE PROVIDED TO THE ENGINEER IN ELECTRONIC FORMAT THAT IS ACCEPTABLE TO THE ENGINEER. THIS SURVEY WILL BE USED TO DETERMINE THE EXISTING ELEVATIONS OF THE PROJECT AREAS. THE PRE-CONSTRUCTION SURVEY SHALL BE PERFORMED BY A STATE LICENSED LAND SURVEYOR. SEE SECTION 5.0 OF THE CONTRACT DOCUMENTS AND THE "CONSTRUCTION STAKING AND LAYOUT" NOTES CONTAINED IN THESE GENERAL NOTES FOR ADDITIONAL SURVEY INFORMATION.

CONSTRUCTION STAKING AND LAYOUT:

1. DRAINAGE SWALES SLOPE STAKES AND FLOW LINE BLUE TOPS AT 50-FOOT (15-M) STATIONS.
2. SUBGRADE BLUE TOPS AT 50-FOOT STATIONS WITH A 50-FOOT OFFSET DISTANCE (MAXIMUM) AND AT THE EDGE OF PAVEMENT.
3. SUBBASE AND BASE COURSE BLUE TOPS AT 50-FOOT STATIONS WITH A 50-FOOT OFFSET DISTANCE (MAXIMUM) AND AT THE EDGE OF PAVEMENT.
4. PAVEMENT AREAS:
 - 4.1. EDGE OF PAVEMENT HUBS AND TACKS (FOR STRINGLINE BY CONTRACTOR) AT 100-FOOT STATIONS
 - 4.2. BETWEEN LOTS AT 25-FOOT STATIONS FOR RUNWAYS (EACH PAVING LANE WIDTH), TAXIWAYS (EACH PAVING LANE WIDTH), AND HOLDING AREAS (EACH PAVING LANE WIDTH)
 - 4.3. AFTER FINISH PAVING OPERATIONS AT 50-FOOT STATIONS (FOR GRADE ACCEPTANCE VERIFICATION) AT ALL PAVED AREAS AT THE EDGE OF EACH PAVING LANE AND ALL GRADE BREAKS PRIOR TO NEXT PAVING LOT
 - 4.4. SHOULDER AND SAFETY AREA BLUE TOPS AT 50-FOOT STATIONS AND AT ALL BREAK POINTS WITH MAXIMUM OF 50-FOOT OFFSETS
5. REQUIRED VERIFICATION/AS-BUILT SURVEY SHALL BE PROVIDED ELECTRONICALLY IN AN ENGINEER APPROVED FORMAT AND SHALL INCLUDE POINT NUMBER, NORTHING, EASTING, ELEVATION, AND DESCRIPTION (PNEZD, COMMA DELIMITED FORMAT).
6. THE CONTRACTOR SHALL PROVIDE VERIFICATION SURVEY TO THE ENGINEER FOR ALL LOCATIONS WHERE PROPOSED CONSTRUCTION WILL TIE INTO ANY EXISTING STRUCTURES AND PAVEMENTS. THIS SURVEY SHALL BE USED FOR VERIFICATION OF EXISTING CONDITIONS AND SHALL BE SUBMITTED PRIOR TO COMMENCING CONSTRUCTION ACTIVITIES IN THE AREAS OF THE EXISTING INFRASTRUCTURE. THIS SURVEY SHALL BE CONSIDERED INCIDENTAL TO CONSTRUCTION OPERATIONS AND SHALL BE PROVIDED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE SPONSOR. WORK IN THESE AREAS SHALL NOT BE ALLOWED TO COMMENCE UNTIL THIS SURVEY VERIFICATION HAS BEEN SUPPLIED BY THE CONTRACTOR TO THE ENGINEER AND THE ENGINEER HAS PROVIDED ACCEPTANCE, BASED ON A TIMELY REVIEW OF THE VERIFICATION SURVEY.

7. IN ADDITION TO ALL REQUIRED UTILITY LOCATES, THE CONTRACTOR SHALL BE REQUIRED TO VERIFY THE ELEVATIONS OF ALL UTILITY CROSSINGS BEFORE COMMENCING CONSTRUCTION OPERATIONS. FOR EXAMPLE, BEFORE THE CONTRACTOR BEGINS WORK ON A PROPOSED STORM DRAIN, THE ENGINEER, BOTH TOP AND BOTTOM, OF ALL UTILITIES THAT CROSS THE PROPOSED PIPE SHALL BE VERIFIED AND PROVIDED TO THE CONTRACTOR. THIS VERIFICATION SHALL BE CONSIDERED A PART OF THE CONTRACTOR'S OBLIGATION TO VERIFY THE UTILITY LOCATIONS. THE CONTRACTOR SHALL BE RESPONSIBLE TO THE SPONSOR. WORK IN THESE AREAS SHALL NOT BE ALLOWED TO COMMENCE UNTIL THESE UTILITY VERIFICATIONS HAVE BEEN SUPPLIED BY THE CONTRACTOR TO THE ENGINEER AND THE ENGINEER HAS PROVIDED ACCEPTANCE, BASED ON A TIMELY REVIEW OF THE VERIFICATION SURVEY.
8. AREAS WHERE EXCAVATIONS OR EMBANKMENTS ARE TO BE CONSTRUCTED, THE CONTRACTOR SHALL PROVIDE THE ENGINEER WITH A MAINTENANCE SURVEY. THIS SURVEY SHALL INCLUDE THE DETERMINATION OF FINAL EARTHWORK QUANTITIES FOR PAYMENT. THE CONTRACTOR SHALL FURNISH THE INITIAL SURVEY BEFORE CONSTRUCTION OPERATIONS COMMENCE AND THE FINAL SURVEY AFTER CONSTRUCTION OPERATIONS HAVE CONCLUDED TO THE ENGINEER FOR QUANTITY DETERMINATION. IN PAVEMENT AREAS, THE FINAL SURFACE SHALL BE THE TOP OF APPROVED GRADE. SURVEYS SHALL MATCH PROPER SPOTS AND SHOTS TO ACCURATELY REPRESENT BOTH INITIAL AND FINAL SURFACES. IF ENGINEER DETERMINES THAT THE SUBMITTED SURVEY IS DEFICIENT IN ADDITIONAL DETAILING SURVEYED SURFACES, THE CONTRACTOR SHALL PERFORM ADDITIONAL SURVEY TO THE SATISFACTION OF THE ENGINEER. SURVEY SHALL BE CONSIDERED INCIDENTAL TO CONSTRUCTION OPERATIONS AND SHALL BE PROVIDED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE SPONSOR. SHOULD THE CONTRACTOR FAIL TO PROVIDE THESE INITIAL AND FINAL SURVEYS TO THE ENGINEER, THE EXISTING AND PROPOSED DESIGN SURFACE DATAS THAT ARE SHOWN IN THE PLANS SHALL BE USED FOR QUANTITY DETERMINATION.
9. ADDITIONAL STAKES OR MARKINGS SHALL BE REQUIRED AT AN INTERVAL TO CLEARLY DEFINE GRADES FOR SUB-GRADE AND MAINTENANCE. ADDITIONAL STAKES SHALL BE REQUIRED FOR STRUCTURES INCLUDING AND INCLUDING. BASES FOR PAVEMENTS; ADDITIONAL STAKING AND CONTROLS SHALL BE PLACED AS NEEDED FOR CONSTRUCTION TO MEET THE DESIGN AS REQUIRED BY THE SPECIFICATIONS OR SHOWN ON THE DRAWINGS. IN ADDITION TO LOCATIONS STATED ABOVE, STAKING FOR LAYOUT AND SURVEY FOR GRADE VERIFICATIONS SHALL BE PROVIDED AT LOCATIONS OF ALL SPOT ELEVATIONS WHEN PROVIDED FOR IN THE PLANS.
10. ON ALL PAVEMENT LIFTS AND MILLED SURFACES, CONTRACTOR SHALL SPRAY PAINT ON THE PAVEMENT SURFACES FILL PATCHES AND CRACKS. CONTRACTOR CAN USUALLY VERIFY PAVEMENT GRADES AND THICKNESSES. FILL LOCATIONS SHALL MATCH ALL SPOT ELEVATIONS AND STAKING AND LAYOUT LOCATIONS DISCUSSED IN THIS SECTION.
11. THE ESTABLISHMENT OF SURVEY CONTROL, AND/OR REESTABLISHMENT OF SURVEY CONTROL SHALL BE BY A STATE LICENSED LAND SURVEYOR.
12. CONTROLS AND STAKES DISTURBED OR SUSPECT OF HAVING BEEN DISTURBED SHALL BE CHECKED AND/OR RESET AS DIRECTED BY THE ENGINEER WITHOUT ADDITIONAL COST TO THE OWNER.

1. CONTRACTOR SHALL BE RESPONSIBLE FOR PROPER OFF-SITE DISPOSAL OF ALL WASTE MATERIALS GENERATED FROM THE PROJECT. ALL COSTS ARE INCIDENTAL TO RESPECTIVE REMOVAL BID ITEM.
2. DEMOLITION OF EXISTING PAVEMENTS SHALL BE PERFORMED WITHIN THE PHASING REQUIREMENTS AS DEPICTED IN THE CONSTRUCTION SAFETY DRAWINGS.
3. EXISTING SEALANT DAMAGED DURING OBLITERATION OF EXISTING MARKINGS SHALL BE REPAIRED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE SPONSOR.
4. ALL REMOVAL NORTHINGS AND EASTINGS ARE APPROXIMATE.

1. ALL PAVEMENT TO BE SAW CUT PRIOR TO FULL DEPTH REMOVAL. PAVEMENT REMOVAL SHALL BE MEASURED AND PAID TO NEAT LINE DIMENSIONING.
2. THE CONTRACTOR SHALL SUBMIT A PROCEDURE FOR REMOVING EXISTING PAVEMENT AT THE CORNERS OF THE PARTIAL REMOVAL AREA TO THE ENGINEER NO LATER THAN SEVEN (7) DAYS PRIOR TO THE START OF ROTMILLING OPERATIONS.
3. FULL DEPTH CONCRETE OR ASPHALT PAVEMENT REMOVAL INCLUDES THE REMOVAL OF ANY UNDERLYING ASPHALT AND/OR CONCRETE PAVEMENT, BASE AND SUBBASE LAYERS, GEOTEXTILE FABRIC, AND ANY STABILIZED SUBGRADES.
4. FULL DEPTH AND PARTIAL PAVEMENT REMOVAL SHALL BE PAID FOR BY THE SQUARE YARD REGARDLESS OF DEPTH. PARTIAL DEPTH VARIES FROM 2" TO 2.9". SEE SHEETS C300-C304 FOR FINAL GRADE ELEVATIONS. MILLING DEPTH SHALL BE SUCH THAT EXISTING GEOTEXTILE FABRIC SHALL BE REMOVED PRIOR TO PAVING, IF ENCOUNTERED.
5. FULL DEPTH PAVEMENT REMOVAL PAID FOR BY THE SQUARE YARD REGARDLESS OF DEPTH. DEPTH VARIES FROM 2.0" TO 12.0".

6. CONTRACTOR MAY ELECT MILL ALTERNATE BUTT JOINT WIDTH TO ACCOMMODATE PAVING EQUIPMENT. ADDITIONAL MILLING AREA AND ASPHALT MATERIAL WILL NOT BE DIRECTLY PAID FOR, BUT WILL BE INCIDENTAL TO THE CONSTRUCTION OF THE BUTT JOINT.
7. CONTRACTOR SHALL SEAL ALL CRACKS IN EXISTING PAVEMENT AFTER MILLING OPERATIONS HAVE CONCLUDED AND PRIOR TO ASPHALT PAVING.
8. ALL TAXIPAY PAINT IS TO BE REMOVED DURING ROTOMILLING (INCIDENTAL TO ROTOMILLING EXCEPT WHERE SHOWN ON THE PLANS). ANY PAINT DAMAGED OUTSIDE OF REMOVAL LIMITS IS TO BE RESTORED TO ITS ORIGINAL CONDITION AT THE EXPENSE OF THE CONTRACTOR.
9. CONTRACTOR SHALL PROVIDE SURVEY DATA TO THE ENGINEER IN ACCORDANCE WITH SECTION 50-07, THAT ENSURES THE MINIMUM OVERLAY THICKNESS WILL BE CONSTRUCTED. AREAS THAT ARE DEFICIENT FROM THE MINIMUM OVERLAY THICKNESS SHALL BE PARTIALLY MILLED AT THE DIRECTION OF THE ENGINEER TO ENSURE THE MINIMUM MAT THICKNESS IS ACHIEVED.

1. ANY PAYMENT DAMAGED DURING REMOVAL OUTSIDE THE PROPOSED REMOVAL LIMITS SHALL BE SQUARED OFF, BY SAW CUTTING, TO THE SATISFACTION OF THE ENGINEER. ALL COSTS ASSOCIATED WITH THE ADDITIONAL REMOVAL AND RECONSTRUCTION SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
2. CONTRACTOR SHALL PROTECT ALL AIRFIELD ELECTRICAL AND LIGHT SYSTEMS AND ANY OTHER UTILITY DURING CONSTRUCTION.
3. CONTRACTOR SHALL PROTECT ALL UNDERGROUND AND OVERHEAD UTILITIES AT ALL TIMES UNLESS OTHERWISE NOTED. CONTRACTOR SHALL FIELD VERIFY ALL UTILITY LOCATIONS AND DEPTHS PRIOR TO CONSTRUCTION.
4. ANY DAMAGE TO EXISTING PAINT SHALL BE REPAIRED BY CONTRACTOR AT NO EXPENSE TO THE SPONSOR. RESTORED PAINT SHALL BE TO THE SATISFACTION OF THE ENGINEER AND SHALL INCLUDE GLASS BEADS AS REQUIRED.

1. UNLESS OTHERWISE SPECIFIED, ALL TAXIWAY MARKINGS SHALL BE YELLOW IN ACCORD WITH SPECIFICATION P-620.
2. CONTRACTOR WILL BE REQUIRED TO REPAINT ANY MARKINGS THAT ARE OUTSIDE THE PROJECT WORK LIMITS WHICH ARE DAMAGED BY THE CONTRACTORS OPERATIONS. REPAINTING OF THESE DAMAGED AREAS WILL BE AT THE CONTRACTORS EXPENSE.
3. PAINT SHOULD BE STORED IN A CLIMATE-CONTROLLED ENVIRONMENT IN ORDER TO MEET MANUFACTURERS RECOMMENDED TEMPERATURES BEFORE IT IS APPLIED. MATERIAL THAT DOES NOT MEET REQUIRED TEMPERATURE REQUIREMENTS WILL BE WARNED TO THE MINIMUM TEMPERATURE FOR 24 HOURS BEFORE IT IS APPLIED OR AS APPROVED BY THE RESIDENT ENGINEER.
4. PERMANENT APPLICATION OF PAINT WILL BE APPLIED 30 DAYS AFTER THE INITIAL TEMPORARY APPLICATION. RATES OF APPLICATION SHALL BE AS SPECIFIED IN SECTION P-620.
5. ALL PERMANENT MARKINGS SHALL BE APPLIED DURING DAYLIGHT HOURS ONLY.
6. GLASS BEADS SHALL BE APPLIED TO ALL TEMPORARY AND PERMANENT PAVEMENT MARKINGS. APPLICATION RATES AND GLASS BEAD TYPE SHALL BE AS SPECIFIED IN ITEM P-620. GLASS BEADS SHALL NOT BE APPLIED TO BLACK PAINT.
7. UNLESS OTHERWISE SPECIFIED, ALL TAXIWAY MARKINGS ARE 6" WIDE (YELLOW).
8. FINAL PAINT LAYOUT WILL BE CONFIRMED BY THE AIRPORT PRIOR TO CONSTRUCTION.

9. THE CONTRACTOR SHALL MATCH THE EXISTING PLAN LAYOUT. EXISTING MARKINGS SHALL BE SURVEY VERIFIED PRIOR TO PAVEMENT REMOVAL.
10. CONTRACTOR SHALL HAVE A COPY OF THE CURRENT F.A. ADVISORY CIRCULAR AC 1505340-1 (CURRENT VERSION) "STANDARDS FOR AIRPORT MARKINGS" ON SITE AT ALL TIMES. ANY DISCREPANCY BETWEEN INFORMATION SHOWN ON THE PLAN SHEET AND THE F.A. ADVISORY CIRCULAR SHALL BE COORDINATED WITH THE ENGINEER FOR DIRECT ACTION.
11. IF CONTRACTOR USES STENCILS FOR SURFACE PAINTED HOLD SIGNS, GAPS BETWEEN STENCILS ARE PROHIBITED.
12. ANY DISCREPANCIES BETWEEN THESE PLANS AND THE ACTUAL STRIPING PRESENT IN THE FIELD SHALL BE BROUGHT TO THE ATTENTION OF RESIDENT ENGINEER.
13. SURFACE PAINTED HOLD SIGNS SHALL BECOME A TEMPORARY APPLICATION AND A PERMANENT APPLICATION 30 DAYS AFTER THE DATE OF COMPLETION OF THE PROJECT.


1. PAINT OBLITERATION SHALL BE ACCOMPLISHED BY WATER OR SAND BLASTING OR ANOTHER APPROVED METHOD. TEMPORARY MARKING REMOVAL REQUIRED FOR COORDINATION OF PHASES MAY BE ACCOMPLISHED BY PAINTING OVER THE EXISTING MARKING WITH BLACK PAINT.

2. NO OBSTRUCTION OF MARKINGS ARE REQUIRED INSIDE PAVEMENT REMOVAL LIMITS.
3. CONTRACTOR WILL BE REQUIRED TO OBLITERATE EXISTING MARKINGS (E.G. TAXIWAY LEAD-IN LINES BACK TO THE RUNWAY HOLD BAR) WHICH ARE NOT WITHIN THE LIMITS OF THE PROPOSED CONSTRUCTION. OBLITERATION WILL BE ACCOMPLISHED IN ACCORDANCE WITH P-620 OF THE SPECIFICATIONS. ALL EXISTING TAXIWAY MARKINGS WILL BE OBLITERATED USING WATER BLASTING.
4. ANY DAMAGE DONE DURING MARKING OBLITERATION OPERATIONS TO PAVEMENTS OR PAVEMENT JOINT/CRACK SEALS SHALL BE REPAIRED BY THE CONTRACTOR, AT HIS EXPENSE, AND TO THE APPROVAL OF THE ENGINEER.
5. ALL MARKINGS SHALL BE OBLITERATED PRIOR TO CRACK SEALING.



WOOLPERT

ROLLA
NATIONAL AIRPORT
CITY OF ROLLA, MISSOURI

DES: F.Z.D.	ISSUE RECORD			
DR: N.B.B.	NO.	BY	DATE	DESCRIPTION
CH: C.L.G.	1	L.K.K.	08/05/2025	ISSUED FOR BID
APP: L.K.K.		L.K.K.	08/29/2025	ADDENDUM NO. 1

TAXIWAY A RECONSTRUCTION AND TAXIWAY CONNECTOR REHABILITATION

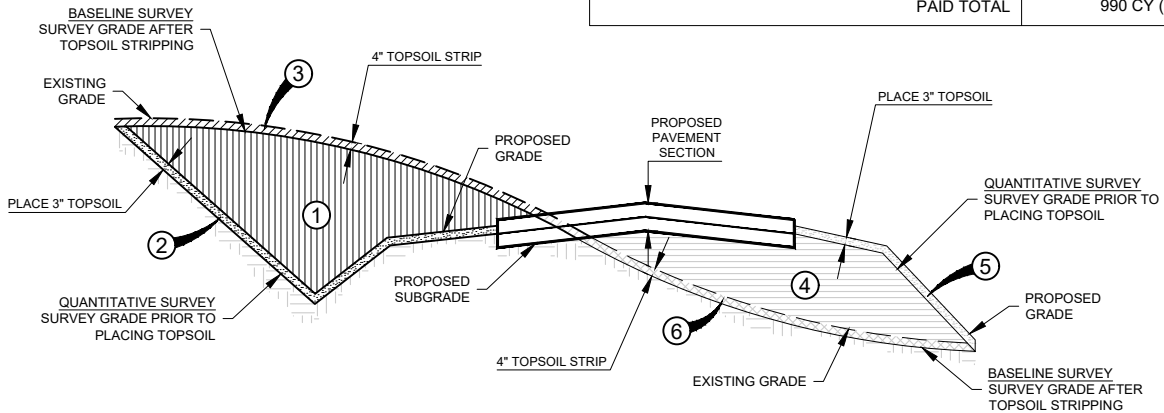
GENERAL NOTES		SHEET NAME G003
		SHEET NO. 3 of 54
toDOT PROJ. NO. 23-056A-1	WOOLPERT PROJ. NO. 10016991.00	

ENTIRE SHEET HAS BEEN REVISED AS PART OF ADDENDUM NO. 1

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SUMMARY OF APPROXIMATE QUANTITIES								
ITEM NO.	ITEM DESCRIPTION	UNITS	SCHEDULE I		SCHEDULE II		SCHEDULE III	
			ESTIMATE	AS BUILT	ESTIMATE	AS BUILT	ESTIMATE	AS BUILT
C-100a	Contractor's Quality Control Program (CQCP)	LS	1		1		0	
C-102a	Temporary Erosion Control	LS	1		0		0	
C-105a	Mobilization	LS	1		1		1	
P-101a	Pavement Removal - Full Depth	SY	11,580		0		0	
P-101b	Asphalt Pavement Removal - Partial Depth	SY	400		5,180		0	
P-101c	Joint and Crack Repair	LF	0		2,500		0	
P-152a	Unclassified Excavation	CY	910		0		80	
P-156a	Cement Treated Subgrade, 18 Inches	SY	8,430		0		0	
P-156b	Cement	TON	280		0		0	
P-208a	Aggregate Base Course, 8 Inches	CY	2,050		0		0	
P-208b	Geotextile Fabric	SY	9,130		0		0	
P-401a	Asphalt Surface Course	TON	2,090		760		0	
P-603a	Bituminous Tack Coat	GAL	1,340		760		0	
P-620a	Temporary Airport Pavement Marking	SF	3,480		640		0	
P-620b	Permanent Airport Pavement Marking, Yellow	SF	2,320		640		0	
P-620c	Permanent Airport Pavement Marking, Black	SF	1,440		0		0	
P-620d	Pavement Marking Obliteration	SF	0		580		0	
D-705a	Install 6-Inch Perforated Polyethylene Pipe	LF	0		0		3,900	
D-705b	Install 6-Inch Non-Perforated Polyethylene Pipe	LF	0		0		900	
D-751a	Install 6-Inch Underdrain Cleanout	EA	0		0		20	
D-751b	Install 18x18 Inch Inspection Pit	EA	0		0		12	
T-901a	Seeding with Hydromulch	AC	2.50		0		0	
T-905a	Topsoil	SY	11,010		0		0	
L-108a	Install #8 AWG, L-824C, 5000 Volt Wire	LF	5,100		0		0	
L-108b	Install #6 AWG Bare Copper Counterpoise, Including Ground Rods	LF	4,100		0		0	
L-110a	Install 1-2" SCH 40 PVC Conduit, Direct Earth Buried (DEB)	LF	3,900		0		0	
L-110b	Install 1-2" SCH 40 HDPE Conduit, Directionally Bored	LF	190		0		0	
L-125a	Remove Stake Mounted Taxiway Edge Light, Complete	EA	33		0		0	
L-125b	Install L-861T LED Taxiway Edge Light, Complete	EA	55		0		0	

EARTHWORK SUMMARY		
AREA DESCRIPTION	CUT (CY)	FILL (CY)
SCHEDULE I - TAXIWAY A RECONSTRUCTION		
TAXIWAY A - SOUTH	520	20
TAXIWAY A - NORTH	390	950
SCHEDULE I SUBTOTAL	910	970
SCHEDULE II - TAXIWAY B REHABILITATION		
TAXIWAY B	0	0
SCHEDULE II SUBTOTAL	0	0
SCHEDULE III - TAXIWAY A UNDERDRAINS		
UNDERDRAINS	80	0
SCHEDULE III SUBTOTAL	80	0
PROJECT TOTAL	990	970
PAID TOTAL	990 CY (CUT)	



TOTAL EXCAVATION

- ① EXCAVATION PAID AS P-152a UNCLASSIFIED EXCAVATION
- ② TOPSOIL (PLACED) INCIDENTAL TO ITEM T-905a TOPSOIL
- ③ TOPSOIL (STRIPPED) PAID AS T-905a TOPSOIL

TOTAL EMBANKMENT

- ④ EMBANKMENT INCIDENTAL TO P-152a UNCLASSIFIED EXCAVATION
- ⑤ TOPSOIL (PLACED) INCIDENTAL TO ITEM T-905a TOPSOIL
- ⑥ TOPSOIL (STRIPPED) PAID AS T-905a TOPSOIL

1

EARTHWORK CALCULATIONS DETAIL (EXCAVATION)
NOT TO SCALE
NOTE: TOPSOIL ASSUMED TO BE STRIPPED AT 4 INCHES DEEP AND PLACED AT APPROX. 3 INCHES DEEP

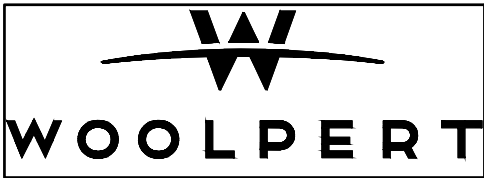
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THESE DRAWINGS ARE FOR PURPOSES ONLY. THEY WERE PREPARED BY OR UNDER THE SUPERVISION OF:

LAURA K. KOONCE202201201408/05/2025

NAMEREG. NO.DATE

FOR AND ON BEHALF OF WOOLPERT, INC.



DES: F.Z.D.	ISSUE RECORD			
	NO.	BY	DATE	DESCRIPTION
DR: N.B.B.	1	L.K.K.	08/05/2025	ISSUED FOR BID
	1	L.K.K.	08/29/2025	ADDENDUM NO. 1
CH: C.L.G.				
APP: L.K.K.				

TAXIWAY A RECONSTRUCTION AND
TAXIWAY CONNECTOR REHABILITATION

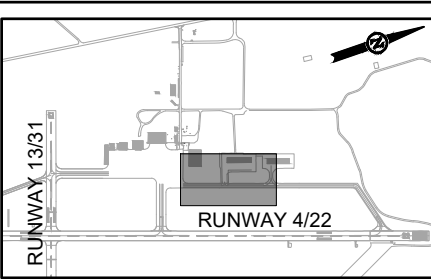
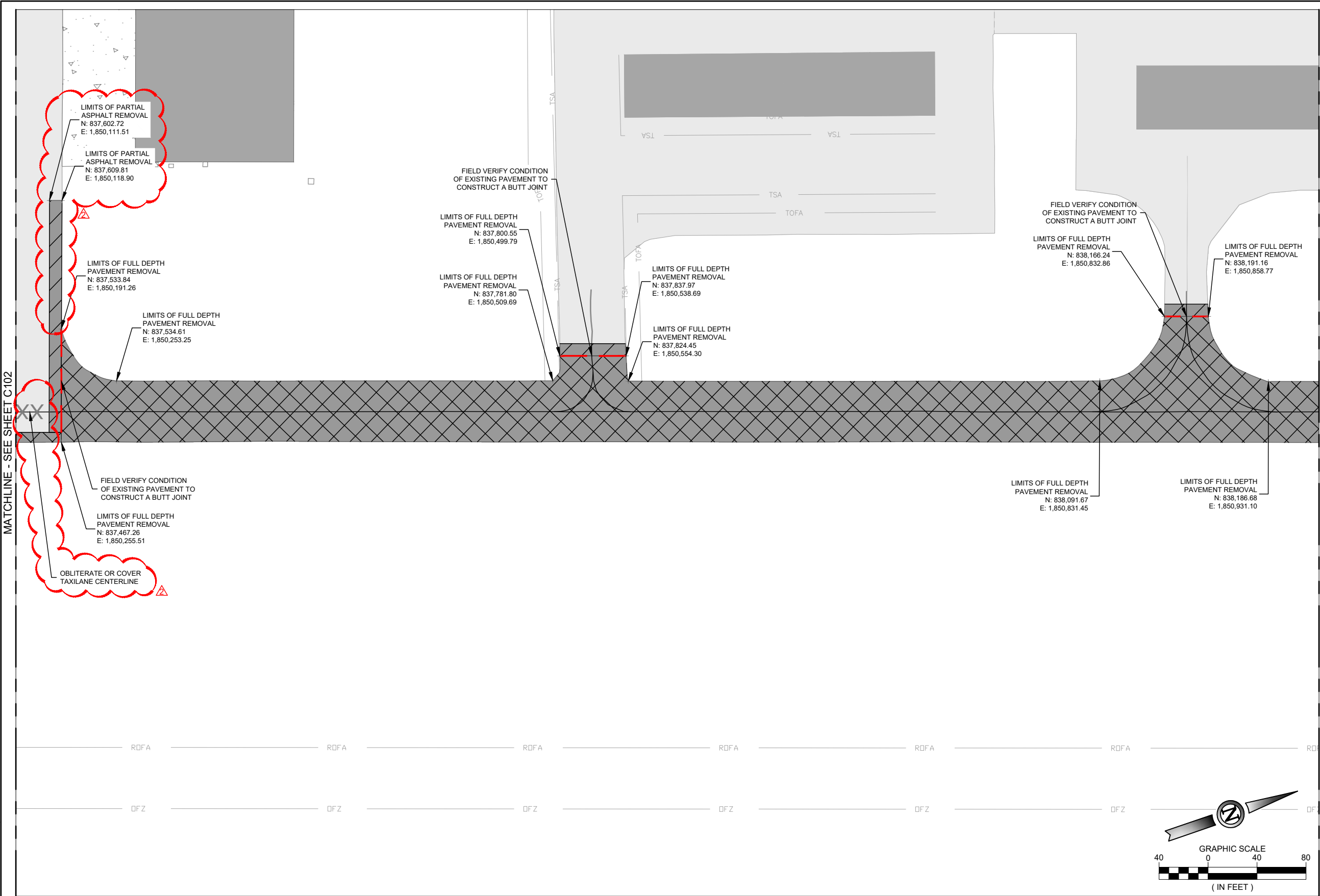
SUMMARY OF
APPROXIMATE QUANTITIES

MoDOT PROJ. NO. 23-056A-1	WOOLPERT PROJ. NO. 10016991.00
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SHEET NAME G004
SHEET NO. 4 of 54

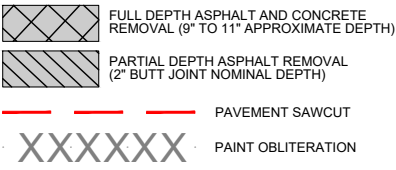
⚠ ENTIRE SHEET HAS BEEN REVISED AS PART OF ADDENDUM NO. 1

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KEYMAP

DEMOLITION LEGEND



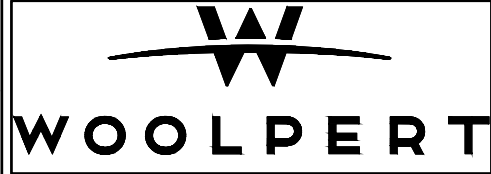
GENERAL DEMOLITION NOTES

- CONTRACTOR SHALL BE RESPONSIBLE FOR PROPER OFF-SITE DISPOSAL OF ALL WASTE MATERIALS GENERATED FROM THE PROJECT. ALL COSTS ARE INCIDENTAL TO RESPECTIVE REMOVAL BID ITEM.
 - CONTRACTOR SHALL HAUL OFF-SITE ALL MILLINGS AND EXCAVATED MATERIALS.
 - DEMOLITION OF EXISTING PAVEMENTS SHALL BE PERFORMED WITHIN THE PHASING REQUIREMENTS AS DEPICTED IN THE CONSTRUCTION SAFETY DRAWINGS.
 - EXISTING SEALANT DAMAGED DURING OBLITERATION OF EXISTING MARKINGS SHALL BE REPAIRED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE SPONSOR.
 - ALL REMOVAL NORTHINGS AND EASTINGS ARE APPROXIMATE.
- #### ASPHALT REMOVAL
- ALL PAVEMENT TO BE SAW CUT PRIOR TO FULL DEPTH REMOVAL. PAVEMENT REMOVAL SHALL BE MEASURED AND PAID TO NEAT LINE DIMENSIONING.
 - THE CONTRACTOR SHALL SUBMIT A PROCEDURE FOR REMOVING EXISTING PAVEMENT AT THE CORNERS OF THE PARTIAL REMOVAL AREA TO THE ENGINEER NO LATER THAN SEVEN (7) DAYS PRIOR TO THE START OF ROTOMILLING OPERATIONS.
 - FULL DEPTH CONCRETE OR ASPHALT PAVEMENT REMOVAL INCLUDES THE REMOVAL OF ANY UNDERLYING ASPHALT AND/OR CONCRETE PAVEMENT, BASE AND SUBBASE LAYERS, GEOTEXTILE FABRIC, AND ANY STABILIZED SUBGRADES.
 - PARTIAL PAVEMENT REMOVAL SHALL BE PAID FOR BY THE SQUARE YARD REGARDLESS OF DEPTH. MILLING DEPTH VARIES FROM APPROXIMATELY 1.5\"/>

ISSUED FOR BID

THESE DRAWINGS ARE FOR PURPOSES ONLY. THEY WERE PREPARED BY OR UNDER THE SUPERVISION OF:

LAURA K. KOONCE	2022012014	08/05/2025
NAME	REG. NO.	DATE
FOR AND ON BEHALF OF WOOLPERT, INC.		



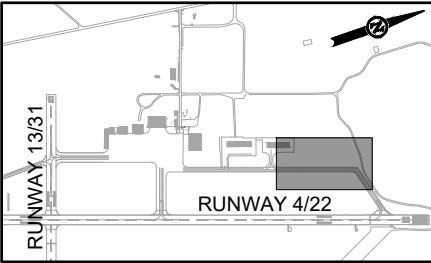
DES: F.Z.D.	ISSUE RECORD			
DR: N.B.B.	NO.	BY	DATE	DESCRIPTION
CH: C.L.G.	1	L.K.K.	08/05/2025	ISSUED FOR BID
APP: L.K.K.	2	L.K.K.	08/29/2025	ADDENDUM NO. 1

TAXIWAY A RECONSTRUCTION AND
TAXIWAY CONNECTOR REHABILITATION





DEMOLITION PLAN
TAXIWAY A

MoDOT PROJ. NO. 23-056A-1	WOOLPERT PROJ. NO. 10016991.00
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SHEET NAME
C100
SHEET NO.
20 of 54



DEMOLITION LEGEND




	FULL DEPTH ASPHALT AND CONCRETE REMOVAL (9" TO 11" APPROXIMATE DEPTH)
	PARTIAL DEPTH ASPHALT REMOVAL (2" BUTT JOINT NOMINAL DEPTH)
	PAVEMENT SAWCUT
	PAINT OBLITERATION

1. CONTRACTOR SHALL BE RESPONSIBLE FOR PROPER OFF-SITE DISPOSAL OF ALL WASTE MATERIALS GENERATED FROM THE PROJECT. ALL COSTS ARE INCIDENTAL TO RESPECTIVE REMOVAL BID ITEM.
2. CONTRACTOR SHALL HAUL OFF-SITE ALL MILLINGS AND EXCAVATED MATERIALS.
3. DEMOLITION OF EXISTING PAVEMENTS SHALL BE PERFORMED WITHIN THE PHASING REQUIREMENTS AS DEPICTED IN THE CONSTRUCTION SAFETY DRAWINGS.
4. EXISTING SEALANT DAMAGED DURING OBLITERATION OF EXISTING MARKINGS SHALL BE REPAIRED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE SPONSOR.

1. ALL PAVEMENT TO BE SAW CUT PRIOR TO FULL DEPTH REMOVAL. PAVEMENT REMOVAL SHALL BE MEASURED AND PAID TO NEAT LINE DIMENSIONING.
2. THE CONTRACTOR SHALL SUBMIT A PROCEDURE FOR REMOVAL OF EXISTING PAVEMENT AT THE CORNERS OF THE PARTIAL REMOVAL AREA TO THE ENGINEER NO LATER THAN SEVEN (7) DAYS PRIOR TO THE START OF ROTOMILLING OPERATIONS.
3. FULL DEPTH CONCRETE OR ASPHALT PAVEMENT REMOVAL INCLUDES THE REMOVAL OF ANY UNDERLYING ASPHALT AND/OR CONCRETE PAVEMENT, BASE AND SUBBASE LAYERS, GEOTEXTILE FABRIC, AND ANY STABILIZED SUBGRADES.
4. PARTIAL PAVEMENT REMOVAL SHALL BE PAID FOR BY THE SQUARE YARD REGARDLESS OF DEPTH. MILLING DEPTH VARIES FROM APPROXIMATELY 1.5" TO 2.5". SEE SHEETS C300-C302 FOR FINAL GRADE ELEVATIONS.
5. FULL DEPTH PAVEMENT REMOVAL PAID FOR BY THE SQUARE YARD REGARDLESS OF DEPTH. DEPTH VARIES FROM APPROXIMATELY 8.0" TO 12.0" AND INCLUDES ASPHALT AND CONCRETE. SEE SHEETS G020 - G023 FOR MORE DETAILS.

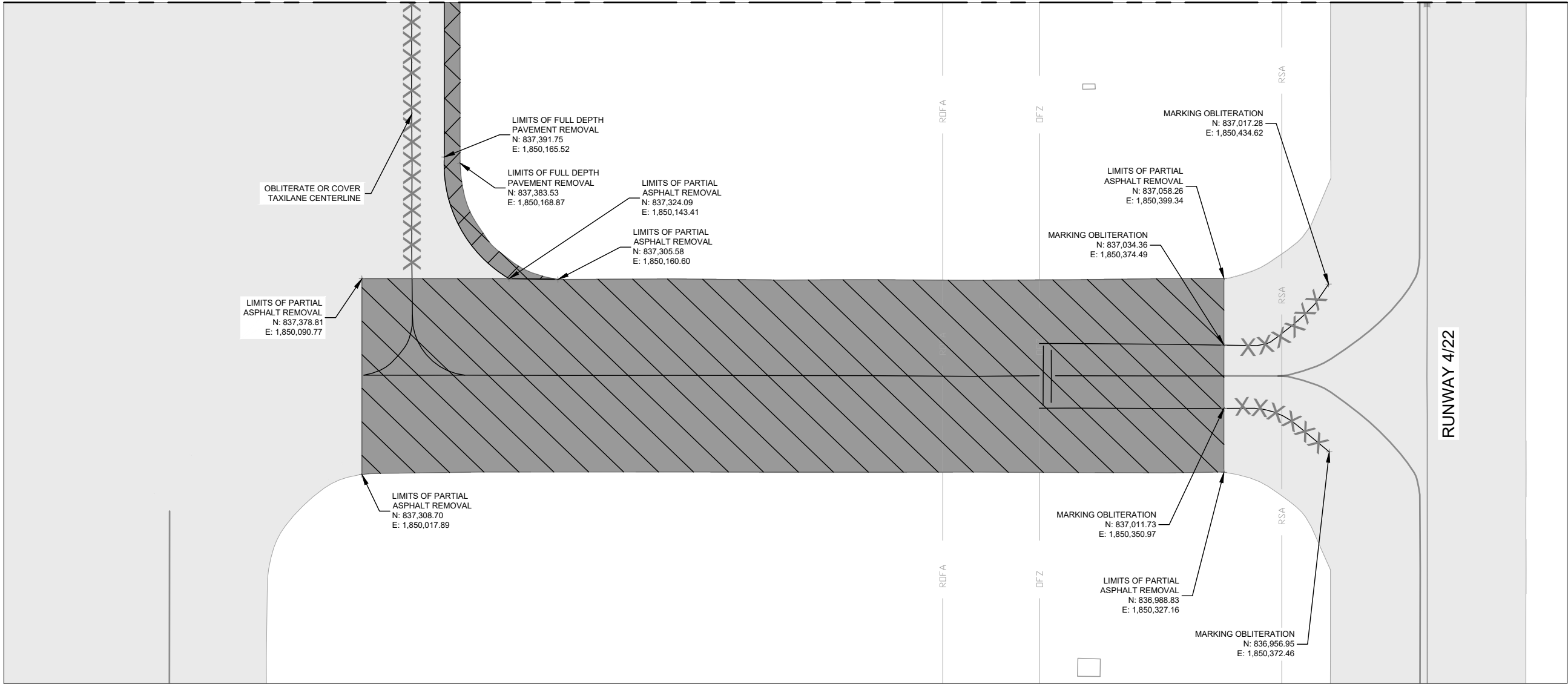
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UNDER THE SUPERVISION OF:

LAURA K. KOONCE	2022012014	08/05/2022
NAME	REG. NO.	DATE
FOR AND ON BEHALF OF WOOLPERT, INC.		

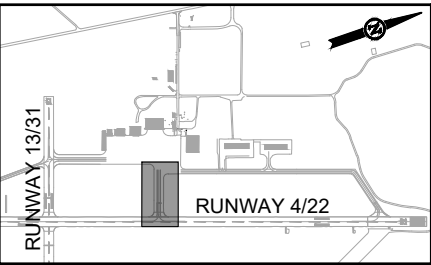
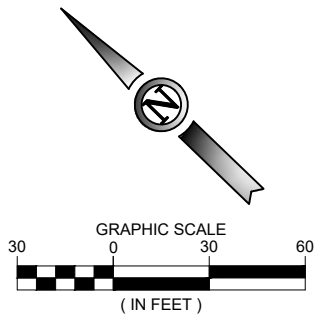
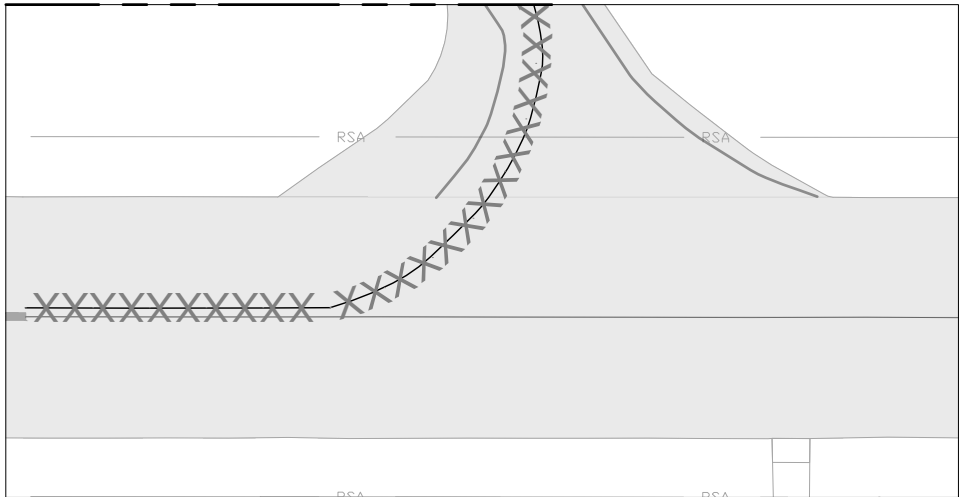
 WOOLPERT	 ROLLA NATIONAL AIRPORT CITY OF ROLLA, MISSOURI	DES: F.Z.D.	ISSUE RECORD				TAXIWAY A RECONSTRUCTION AND TAXIWAY CONNECTOR REHABILITATION	DEMOLITION PLAN TAXIWAY A		SHEET NAME
		DR: N.B.B.	NO.	BY	DATE	DESCRIPTION				C101
		CH: C.L.G.	1	L.K.K.	08/05/2025	ISSUED FOR BID				
		APP: L.K.K.		L.K.K.	08/29/2025	ADDENDUM NO. 1				
		MoDOT PROJ. NO. 23-056A-1		WOOLPERT PROJ. NO. 10016991.00		21 of 54				

Plotted August 28, 2025 @ 3:53 PM by Borzu, Nicole
\\VH23-056a-1 \nwv a reconstruct\CAD\PLANS\10-VIH-23-056A-1-C100-DEMO.dwg

MATCHLINE - SEE SHEET C100

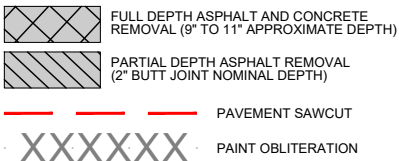


MATCHLINE - SEE SHEET C101



KEYMAP

DEMOLITION LEGEND



GENERAL DEMOLITION NOTES

1. CONTRACTOR SHALL BE RESPONSIBLE FOR PROPER OFF-SITE DISPOSAL OF ALL WASTE MATERIALS GENERATED FROM THE PROJECT. ALL COSTS ARE INCIDENTAL TO RESPECTIVE REMOVAL BID ITEM.
2. CONTRACTOR SHALL HAUL OFF-SITE ALL MILLINGS AND EXCAVATED MATERIALS.
3. DEMOLITION OF EXISTING PAVEMENTS SHALL BE PERFORMED WITHIN THE PHASING REQUIREMENTS AS DEPICTED IN THE CONSTRUCTION SAFETY DRAWINGS.
4. EXISTING SEALANT DAMAGED DURING OBLITERATION OF EXISTING MARKINGS SHALL BE REPAIRED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE SPONSOR.
5. ALL REMOVAL NORTHINGS AND EASTINGS ARE APPROXIMATE.

ASPHALT REMOVAL

1. ALL PAVEMENT TO BE SAW CUT PRIOR TO FULL DEPTH REMOVAL. PAVEMENT REMOVAL SHALL BE MEASURED AND PAID TO NEAT LINE DIMENSIONING.
2. THE CONTRACTOR SHALL SUBMIT A PROCEDURE FOR REMOVING EXISTING PAVEMENT AT THE CORNERS OF THE PARTIAL REMOVAL AREA TO THE ENGINEER NO LATER THAN SEVEN (7) DAYS PRIOR TO THE START OF ROTOMILLING OPERATIONS.
3. FULL DEPTH CONCRETE OR ASPHALT PAVEMENT REMOVAL INCLUDES THE REMOVAL OF ANY UNDERLYING ASPHALT AND/OR CONCRETE PAVEMENT, BASE AND SUBBASE LAYERS, GEOTEXTILE FABRIC, AND ANY STABILIZED SUBGRADES.
4. PARTIAL PAVEMENT REMOVAL SHALL BE PAID FOR BY THE SQUARE YARD REGARDLESS OF DEPTH. MILLING DEPTH VARIES FROM APPROXIMATELY 1.5\"/>
5. FULL DEPTH PAVEMENT REMOVAL PAID FOR BY THE SQUARE YARD REGARDLESS OF DEPTH. DEPTH VARIES FROM APPROXIMATELY 8.0\"/>

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NAME	REG. NO.	DATE
FOR AND ON BEHALF OF WOOLPERT, INC.		

ENTIRE SHEET HAS BEEN REVISED AS PART OF ADDENDUM NO. 1



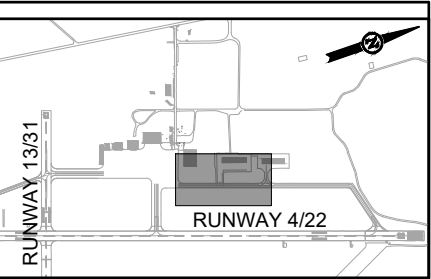
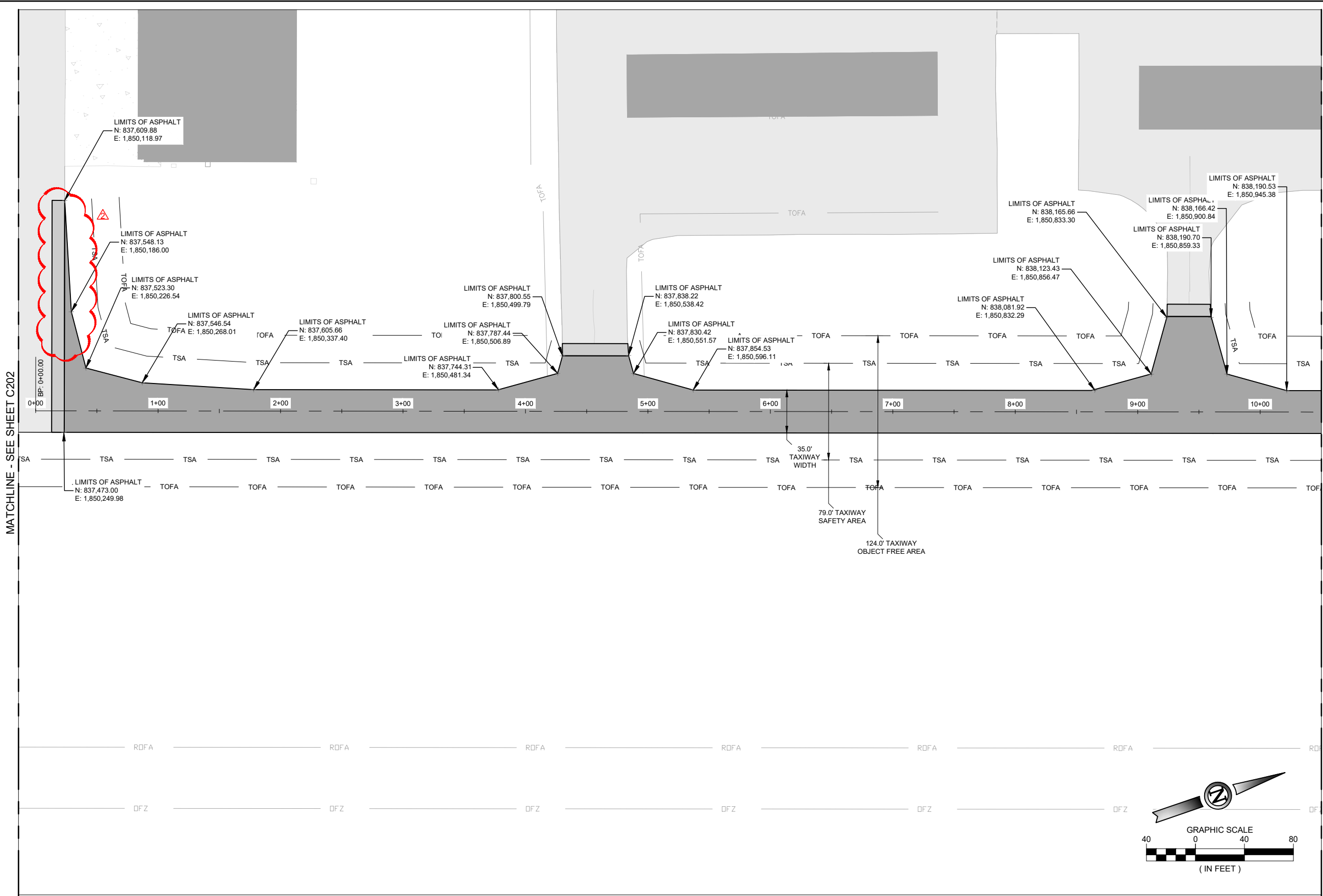
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	NO.	BY	DATE	DESCRIPTION
DR: N.B.B.	1	L.K.K.	08/05/2025	ISSUED FOR BID
	2	L.K.K.	08/29/2025	ADDENDUM NO. 1
CH: C.L.G.				
APP: L.K.K.				

TAXIWAY A RECONSTRUCTION AND
TAXIWAY CONNECTOR REHABILITATION

DEMOLITION PLAN
TAXIWAY B (TAXIWAY CONNECTOR)

MoDOT PROJ. NO. 23-056A-1	WOOLPERT PROJ. NO. 10016991.00	SHEET NAME C102
		SHEET NO. 22 of 54

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\\NP22-056A-1\My Documents\CADD\PLANS\25\25056A-1-C200-GMTT.dwg



KEYMAP

ASPHALT MILL AND FILL
(2" APPROXIMATE DEPTH)

ASPHALT FULL DEPTH
RECONSTRUCTION

- #### GEOMETRY NOTES
- CONTRACTOR TO USE SURVEY CONTROL POINTS AS SHOWN ON SHEET G005 SURVEY CONTROL LAYOUT.
 - ALL LINE AND CURVE CALLOUTS ARE AT EDGE OF ASPHALT UNLESS OTHERWISE NOTED.
 - THE GEOMETRY SHOWN ON THESE SHEETS REPRESENT THEORETICAL PAVEMENT EDGES FOR TAXIWAYS BASED ON ADVISORY CIRCULAR FOR AIRPORT DESIGN.
 - SEE SHEETS G050 THRU G058 FOR CONSTRUCTION PHASING.
 - SEE SHEETS C220 FOR TYPICAL PAVEMENT SECTIONS.
 - SEE SHEETS C700 THRU C720 FOR PAVEMENT MARKING INFORMATION.
 - ANY PAVEMENT DAMAGE DURING CONSTRUCTION OUTSIDE THE PROPOSED PROJECT REMOVAL LIMITS SHALL BE REPAIRED TO THE SATISFACTION OF THE ENGINEER. ALL COSTS ASSOCIATED WITH RECONSTRUCTION SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
 - CONTRACTOR SHALL LOCATE AND PROTECT ALL EXISTING UTILITIES.

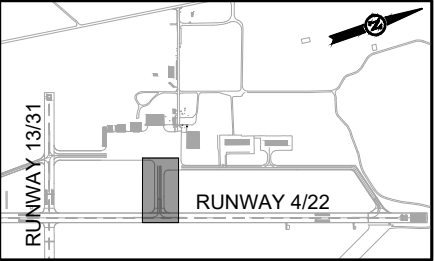
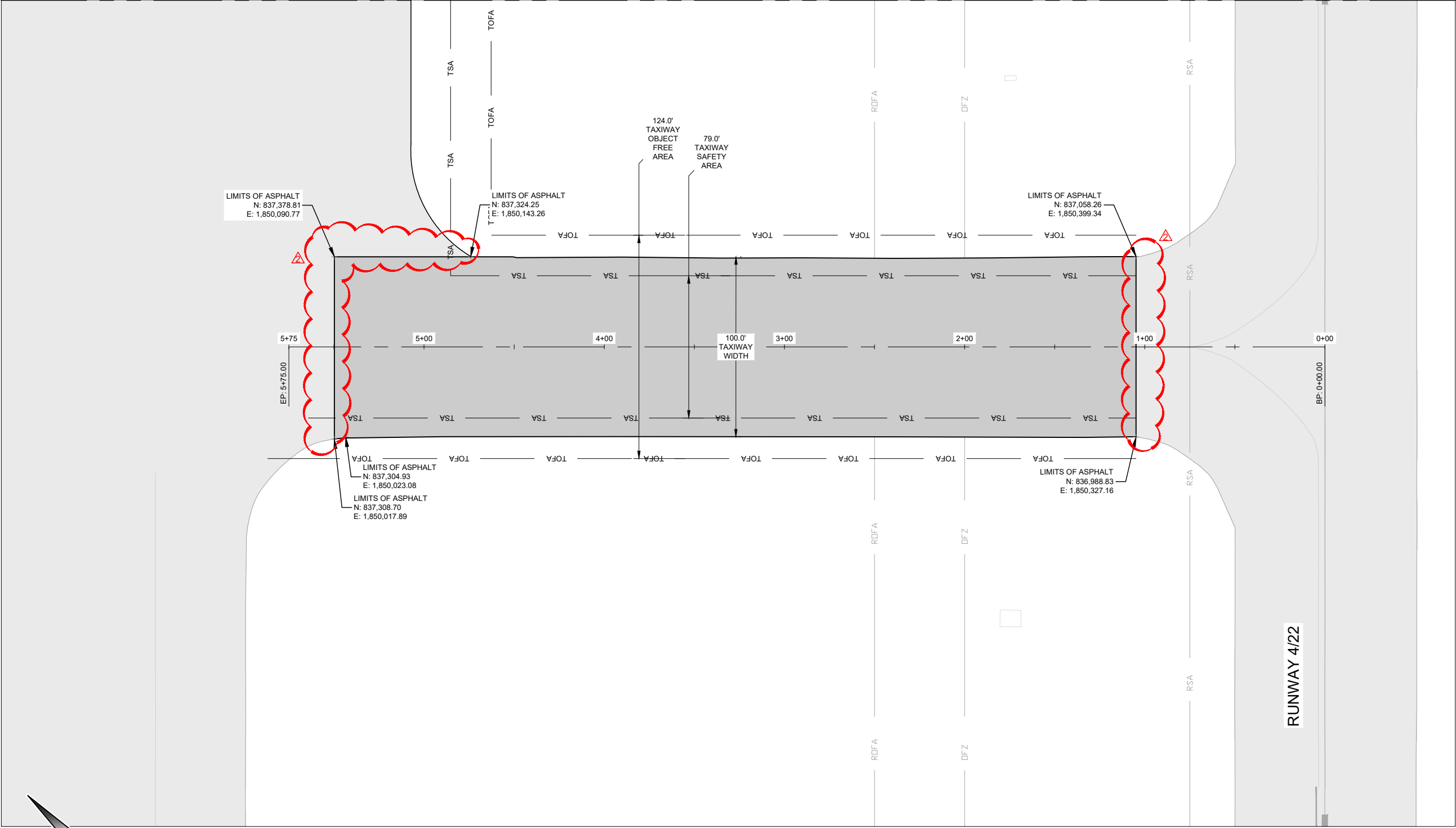
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NAME	REG. NO.	DATE
FOR AND ON BEHALF OF WOOLPERT, INC.		

DES: F.Z.D.	ISSUE RECORD				TAXIWAY A RECONSTRUCTION AND TAXIWAY CONNECTOR REHABILITATION	GEOMETRY PLAN TAXIWAY A (STA. 00+00 - 10+50)	MoDOT PROJ. NO. 23-056A-1	WOOLPERT PROJ. NO. 10016991.00	SHEET NAME
DR: N.B.B.	NO.	BY	DATE	DESCRIPTION					C200
CH: C.L.G.	1	L.K.K.	08/05/2025	ISSUED FOR BID					SHEET NO. 23 of 54
APP: L.K.K.		L.K.K.	08/29/2025	ADDENDUM NO. 1					

MATCHLINE - SEE SHEET C200



KEYMAP

GEOMETRY LEGEND

- ASPHALT MILL AND FILL (2" APPROXIMATE DEPTH)
- ASPHALT FULL DEPTH RECONSTRUCTION

GEOMETRY NOTES

- CONTRACTOR TO USE SURVEY CONTROL POINTS AS SHOWN ON SHEET G005 SURVEY CONTROL LAYOUT.
- ALL LINE AND CURVE CALLOUTS ARE AT EDGE OF ASPHALT UNLESS OTHERWISE NOTED.
- THE GEOMETRY SHOWN ON THESE SHEETS REPRESENT THEORETICAL PAVEMENT EDGES FOR TAXIWAYS BASED ON ADVISORY CIRCULAR FOR AIRPORT DESIGN.
- SEE SHEETS G050 THRU G058 FOR CONSTRUCTION PHASING.
- SEE SHEETS C220 FOR TYPICAL PAVEMENT SECTIONS.
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- CONTRACTOR SHALL LOCATE AND PROTECT ALL EXISTING UTILITIES.

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NAME	REG. NO.	DATE
FOR AND ON BEHALF OF WOOLPERT, INC.		



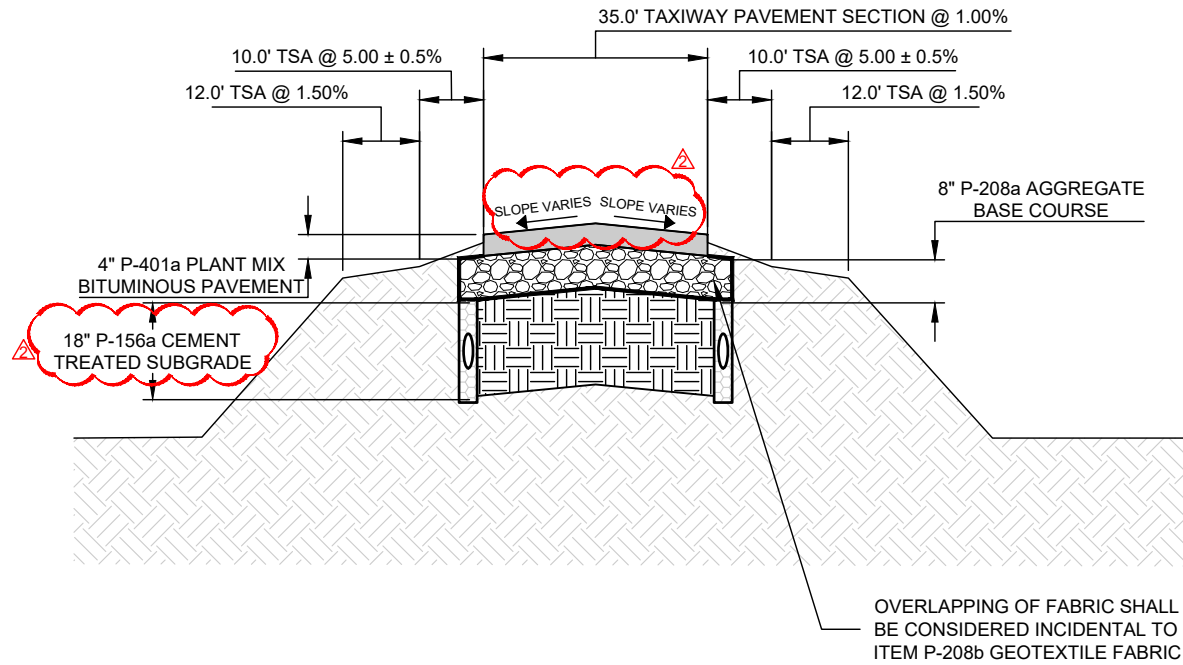
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	NO.	BY	DATE	DESCRIPTION
DR: N.B.B.	1	L.K.K.	08/05/2025	ISSUED FOR BID
	2	L.K.K.	08/29/2025	ADDENDUM NO. 1
CH: C.L.G.				
APP: L.K.K.				

TAXIWAY A RECONSTRUCTION AND
TAXIWAY CONNECTOR REHABILITATION

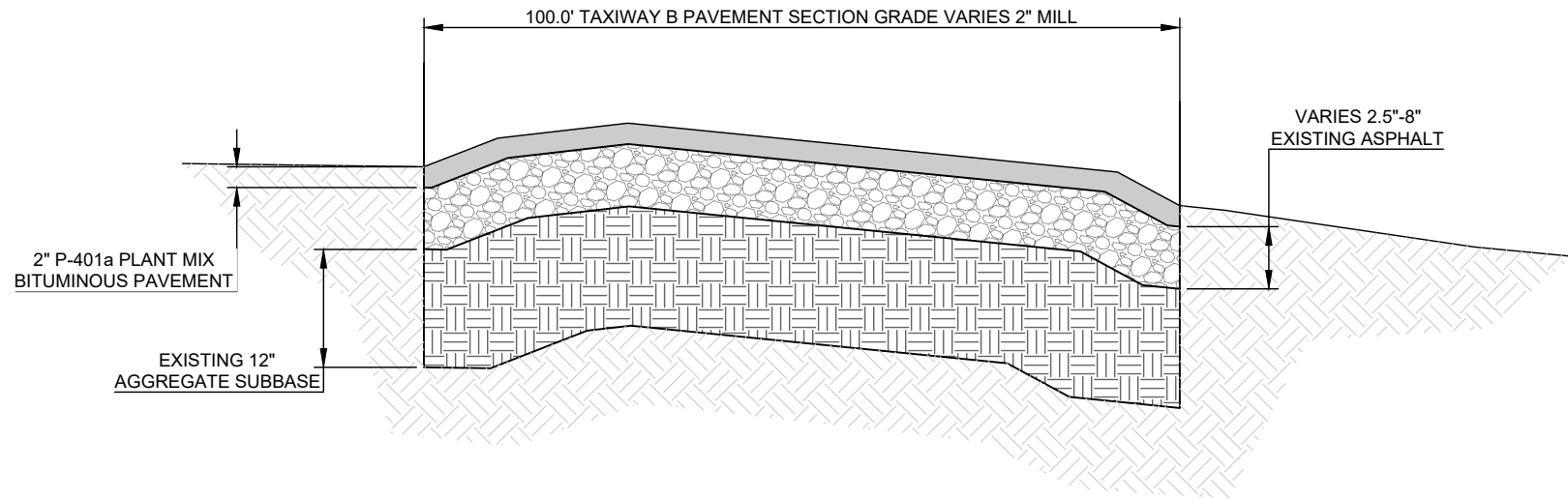
GEOMETRY PLAN
TAXIWAY B (TAXIWAY CONNECTOR)

MoDOT PROJ. NO. 23-056A-1	WOOLPERT PROJ. NO. 10016991.00
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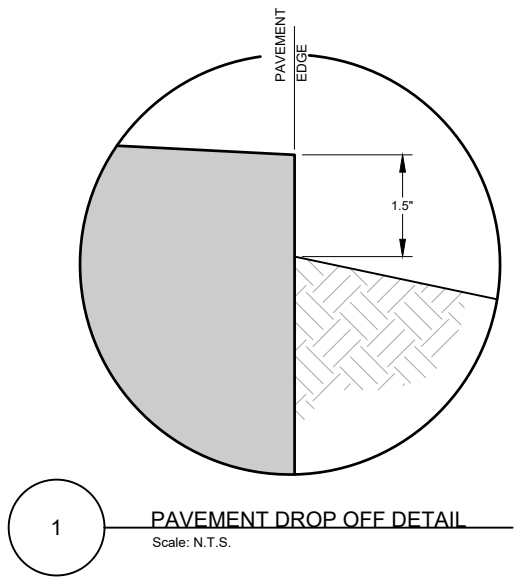
SHEET NAME
C202
SHEET NO.
25 of 54



A TAXIWAY A PAVEMENT SECTION (NORTH - SOUTH)
 NTS
 NOTE: TAXIWAY A OVERBUILD WIDTH IS 1'



B TAXIWAY B PAVEMENT SECTION REAHBILITATION (WEST - EAST)
 NTS



ISSUED FOR BID

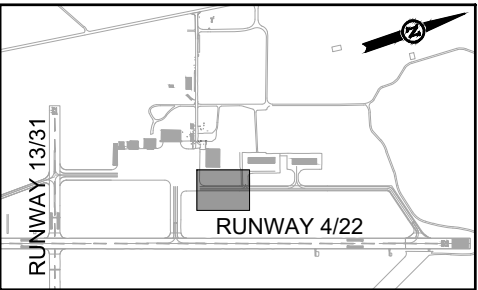
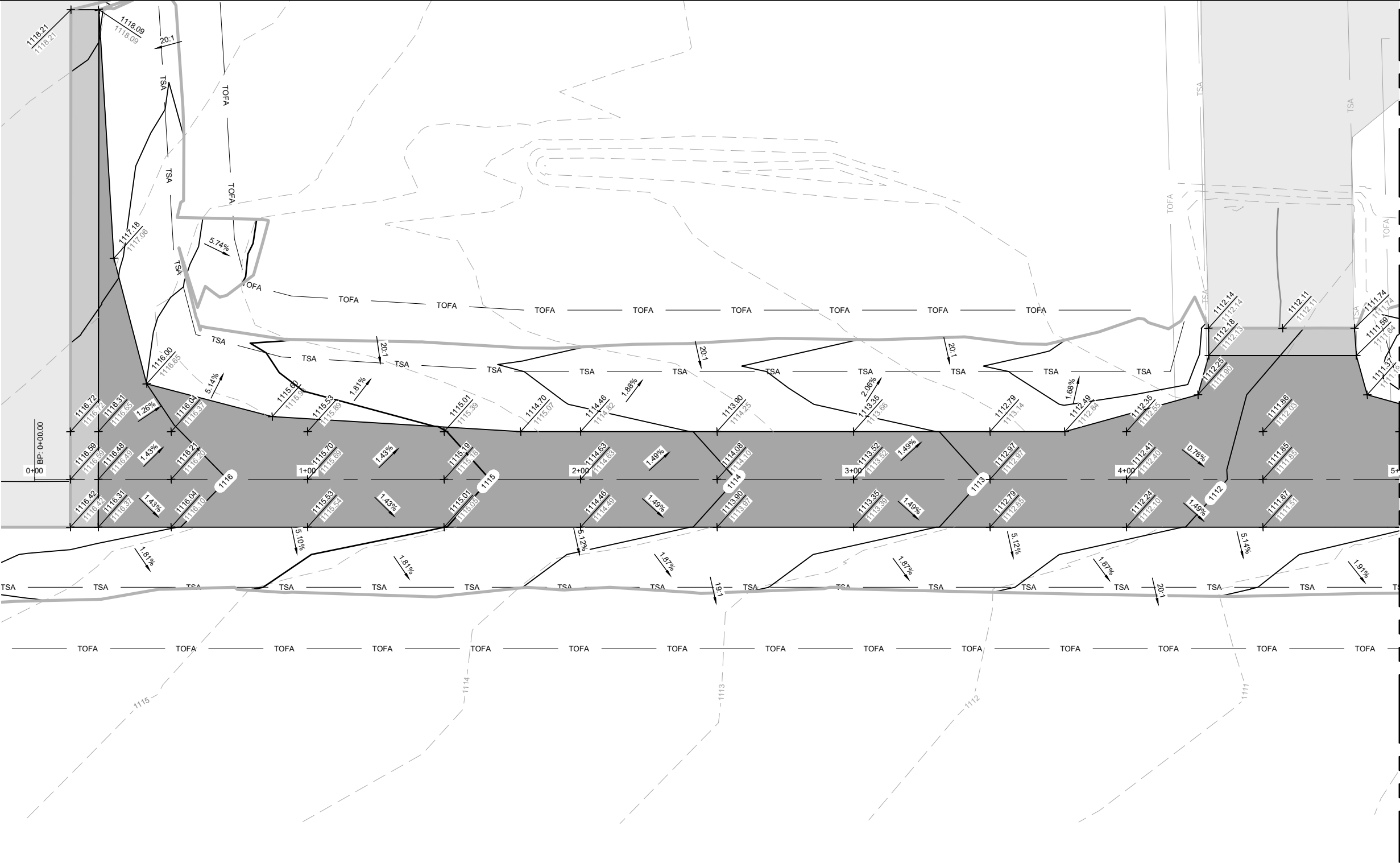
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LAURA K. KOONCE	2022012014	08/05/2025
NAME	REG. NO.	DATE

FOR AND ON BEHALF OF WOOLPERT, INC.

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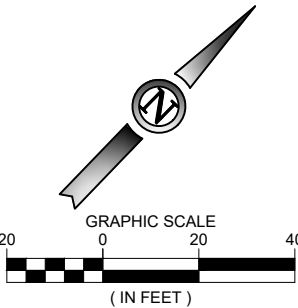
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KEYMAP

GRADING LEGEND

- ASPHALT MILL AND FILL (2" APPROXIMATE DEPTH)
- PAVEMENT FULL DEPTH RECONSTRUCTION
- EXISTING MAJOR CONTOUR
- EXISTING MINOR CONTOUR
- PROPOSED MAJOR CONTOUR
- PROPOSED MINOR CONTOUR
- APPROX. GRADING LIMITS
- SWALE FLOW LINE
- SPOT ELEVATION
- GB GRADE BREAK
- MP MID POINT
- LP LOW POINT
- HP HIGH POINT
- EC END OF CURVE



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LAURA K. KOONCE 2022012014 08/05/2025
NAME REG. NO. DATE
FOR AND ON BEHALF OF WOOLPERT, INC.

GRADING NOTES

- LIMITS OF GRADING ARE APPROXIMATE AND DO NOT CONSTITUTE LIMITS OF DISTURBANCE. THE CONTRACTOR SHALL BE RESPONSIBLE TO RESTORE ALL AREAS DISTURBED BY CONSTRUCTION OPERATIONS OUTSIDE OF THE GRADING LIMITS, INCLUDING ENGINEERED IDENTIFIED STAGING AREAS, STOCKPILES AREAS, AND HAUL ROUTES. ALL RESTORATION SHALL BE AT CONTRACTOR'S EXPENSE AND INCLUDES, BUT IS NOT LIMITED TO, MINOR GRADING, TEMPORARY AND PERMANENT EROSION CONTROL MEASURES.
- CONTRACTOR SHALL USE CAUTION AND PROTECT ALL EXISTING UNDERGROUND UTILITIES.
- IN THE EVENT OF ANY CONFLICT WITHIN THESE PLANS, THE INFORMATION IN THE PROFILES SHALL GOVERN OVER THE SPOT ELEVATIONS AND CROSS SECTIONS.

- THE TOP 4-INCHES OF TOPSOIL WITHIN ALL GRADING LIMITS SHALL BE STRIPPED AND STOCKPILED FOR PLACEMENT AFTER FINAL GRADING OPERATIONS. PAYMENT FOR TOPSOIL SHALL BE PAID PER ITEM P-152 (T-905). NO PAYMENT FOR STOCKPILING AND REHANDLING OF TOPSOIL MATERIAL SHALL BE PAID BUT SHALL BE INCIDENTAL TO ITEM P-152 (T-905).
- PRIOR TO THE START OF GRADING OPERATIONS, ALL LIMITS OF GRADING SHALL BE CLEARED AND GRUBBED PER SECTION P-151.
- ALL EXCESS EXCAVATION SHALL BE HAULED OFFSITE TO AN UNDISCLOSED LOCATION DETERMINED BY THE CONTRACTOR.
- SEE SHEETS C220 FOR TYPICAL SECTIONS.

SPOT ELEVATION NOTES

- ALL SPOT ELEVATIONS ARE AT FINISHED GRADE (PAVEMENT OR DIRT) UNLESS NOTED OTHERWISE.
- THE CONTRACTOR SHALL REPAIR ALL AREAS DISTURBED BY THEIR OPERATIONS OUTSIDE OF THE GRADING LIMITS AT THEIR OWN EXPENSE.
- SEE RUNWAY AND TAXIWAY PLAN AND PROFILE SHEETS C400 - C450 FOR ADDITIONAL INFORMATION.

ENTIRE SHEET HAS BEEN REVISED AS PART OF ADDENDUM NO. 1



DES: F.Z.D.	ISSUE RECORD			
	NO.	BY	DATE	DESCRIPTION
DR: N.B.B.	1	L.K.K.	08/05/2025	ISSUED FOR BID
CH: C.L.G.		L.K.K.	08/29/2025	ADDENDUM NO. 1
APP: L.K.K.				

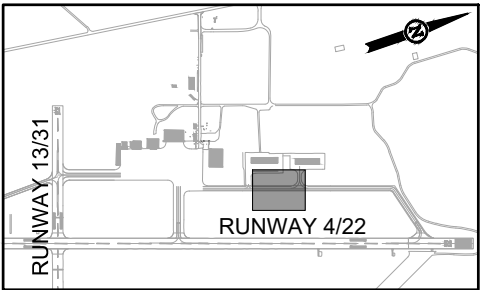
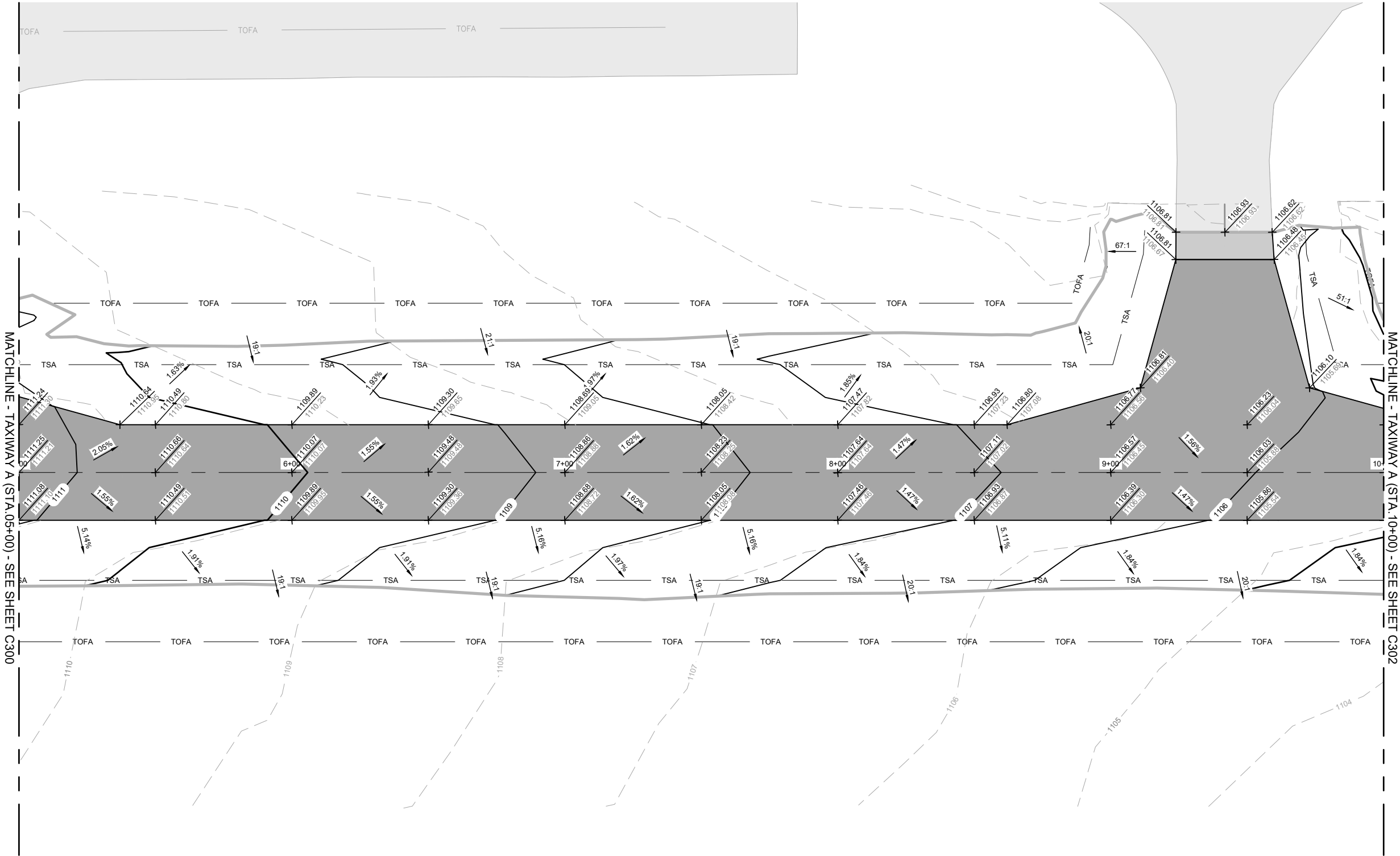
TAXIWAY A RECONSTRUCTION AND
TAXIWAY CONNECTOR REHABILITATION

GRADING AND SPOT ELEVATION PLAN
TAXIWAY A (STA. 00+00 - 05+00)

MoDOT PROJ. NO. 23-056A-1
WOOLPERT PROJ. NO. 10016991.00

SHEET NAME
C300
SHEET NO.
27 of 54

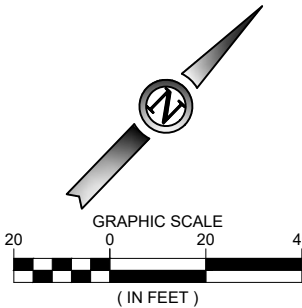
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KEYMAP

GRADING LEGEND

- ASPHALT MILL AND FILL (2" APPROXIMATE DEPTH)
- PAVEMENT FULL DEPTH RECONSTRUCTION
- EXISTING MAJOR CONTOUR
- EXISTING MINOR CONTOUR
- PROPOSED MAJOR CONTOUR
- PROPOSED MINOR CONTOUR
- APPROX. GRADING LIMITS
- SWALE FLOW LINE
- SPOT ELEVATION
- GRADE BREAK
- MID POINT
- LOW POINT
- HIGH POINT
- END OF CURVE



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NAME	REG. NO.	DATE
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- CONTRACTOR SHALL USE CAUTION AND PROTECT ALL EXISTING UNDERGROUND UTILITIES.
- IN THE EVENT OF ANY CONFLICT WITHIN THESE PLANS, THE INFORMATION IN THE PROFILES SHALL GOVERN OVER THE SPOT ELEVATIONS AND CROSS SECTIONS.

- THE TOP 4-INCHES OF TOPSOIL WITHIN ALL GRADING LIMITS SHALL BE STRIPPED AND STOCKPILED FOR PLACEMENT AFTER FINAL GRADING OPERATIONS. PAYMENT FOR TOPSOIL SHALL BE PAID PER ITEM P-152 (T-905). NO PAYMENT FOR STOCKPILING AND REHANDLING OF TOPSOIL MATERIAL SHALL BE PAID BUT SHALL BE INCIDENTAL TO ITEM P-152 (T-905).
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- SEE SHEETS C220 FOR TYPICAL SECTIONS.

SPOT ELEVATION NOTES

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ENTIRE SHEET HAS BEEN REVISED AS PART OF ADDENDUM NO. 1



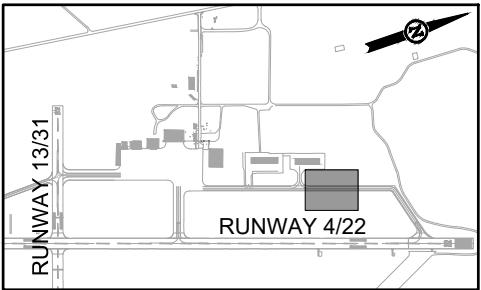
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CH: C.L.G.		L.K.K.	08/29/2025	ADDENDUM NO. 1
APP: L.K.K.				

TAXIWAY A RECONSTRUCTION AND
TAXIWAY CONNECTOR REHABILITATION

GRADING AND SPOT ELEVATION PLAN
TAXIWAY A (STA. 05+00 - 10+00)

MoDOT PROJ. NO. 23-056A-1	WOOLPERT PROJ. NO. 10016991.00
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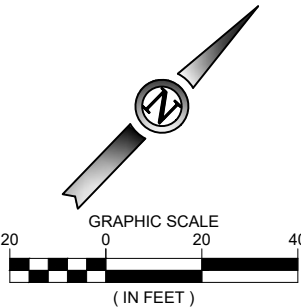
SHEET NAME
C301
SHEET NO.
28 of 54



KEYMAP

GRADING LEGEND

- ASPHALT MILL AND FILL (2" APPROXIMATE DEPTH)
- PAVEMENT FULL DEPTH RECONSTRUCTION
- EXISTING MAJOR CONTOUR
- EXISTING MINOR CONTOUR
- PROPOSED MAJOR CONTOUR
- PROPOSED MINOR CONTOUR
- APPROX. GRADING LIMITS
- SWALE FLOW LINE
- SPOT ELEVATION
- GRADE BREAK
- MID POINT
- LOW POINT
- HIGH POINT
- END OF CURVE



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LAURA K. KOONCE	2022012014	08/05/2025
NAME	REG. NO.	DATE
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ENTIRE SHEET HAS BEEN REVISED AS PART OF ADDENDUM NO. 1



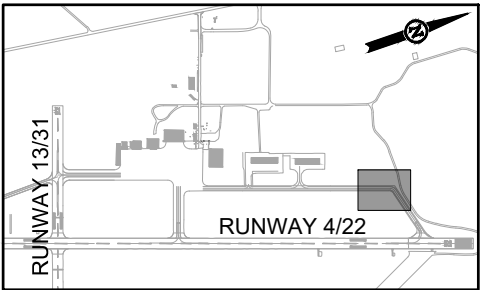
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CH: C.L.G.		L.K.K.	08/29/2025	ADDENDUM NO. 1
APP: L.K.K.				

TAXIWAY A RECONSTRUCTION AND TAXIWAY CONNECTOR REHABILITATION

GRADING AND SPOT ELEVATION PLAN TAXIWAY A (STA. 10+00 - 15+00)

MoDOT PROJ. NO. 23-056A-1	WOOLPERT PROJ. NO. 10016991.00
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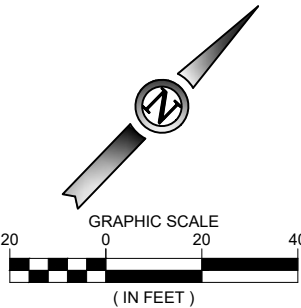
SHEET NAME
C302
SHEET NO.
29 of 54



KEYMAP

GRADING LEGEND

- ASPHALT MILL AND FILL (2" APPROXIMATE DEPTH)
- PAVEMENT FULL DEPTH RECONSTRUCTION
- EXISTING MAJOR CONTOUR
- EXISTING MINOR CONTOUR
- PROPOSED MAJOR CONTOUR
- PROPOSED MINOR CONTOUR
- APPROX. GRADING LIMITS
- SWALE FLOW LINE
- SPOT ELEVATION
- GB GRADE BREAK
- MP MID POINT
- LP LOW POINT
- HP HIGH POINT
- EC END OF CURVE



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LAURA K. KOONCE	2022012014	08/05/2025
NAME	REG. NO.	DATE
FOR AND ON BEHALF OF WOOLPERT, INC.		

GRADING NOTES

- LIMITS OF GRADING ARE APPROXIMATE AND DO NOT CONSTITUTE LIMITS OF DISTURBANCE. THE CONTRACTOR SHALL BE RESPONSIBLE TO RESTORE ALL AREAS DISTURBED BY CONSTRUCTION OPERATIONS OUTSIDE OF THE GRADING LIMITS, INCLUDING ENGINEERED IDENTIFIED STAGING AREAS, STOCKPILES AREAS, AND HAUL ROUTES. ALL RESTORATION SHALL BE AT CONTRACTOR'S EXPENSE AND INCLUDES, BUT IS NOT LIMITED TO, MINOR GRADING, TEMPORARY AND PERMANENT EROSION CONTROL MEASURES.
- CONTRACTOR SHALL USE CAUTION AND PROTECT ALL EXISTING UNDERGROUND UTILITIES.
- IN THE EVENT OF ANY CONFLICT WITHIN THESE PLANS, THE INFORMATION IN THE PROFILES SHALL GOVERN OVER THE SPOT ELEVATIONS AND CROSS SECTIONS.

- THE TOP 4-INCHES OF TOPSOIL WITHIN ALL GRADING LIMITS SHALL BE STRIPPED AND STOCKPILED FOR PLACEMENT AFTER FINAL GRADING OPERATIONS. PAYMENT FOR TOPSOIL SHALL BE PAID PER ITEM P-152 (T-905). NO PAYMENT FOR STOCKPILING AND REHANDLING OF TOPSOIL MATERIAL SHALL BE PAID BUT SHALL BE INCIDENTAL TO ITEM P-152 (T-905).
- PRIOR TO THE START OF GRADING OPERATIONS, ALL LIMITS OF GRADING SHALL BE CLEARED AND GRUBBED PER SECTION P-151.
- ALL EXCESS EXCAVATION SHALL BE HAULED OFFSITE TO AN UNDISCLOSED LOCATION DETERMINED BY THE CONTRACTOR.
- SEE SHEETS C220 FOR TYPICAL SECTIONS.

SPOT ELEVATION NOTES

- ALL SPOT ELEVATIONS ARE AT FINISHED GRADE (PAVEMENT OR DIRT) UNLESS NOTED OTHERWISE.
- THE CONTRACTOR SHALL REPAIR ALL AREAS DISTURBED BY THEIR OPERATIONS OUTSIDE OF THE GRADING LIMITS AT THEIR OWN EXPENSE.
- SEE RUNWAY AND TAXIWAY PLAN AND PROFILE SHEETS C400 - C450 FOR ADDITIONAL INFORMATION.

ENTIRE SHEET HAS BEEN REVISED AS PART OF ADDENDUM NO. 1



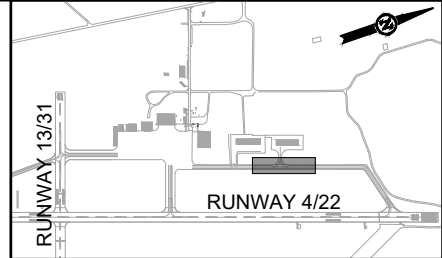
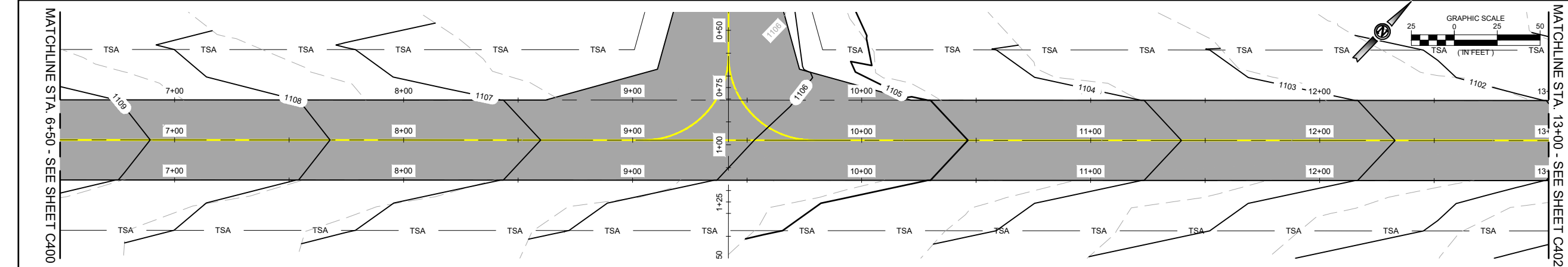
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	NO.	BY	DATE	DESCRIPTION
DR: N.B.B.	1	L.K.K.	08/05/2025	ISSUED FOR BID
CH: C.L.G.	2	L.K.K.	08/29/2025	ADDENDUM NO. 1
APP: L.K.K.				

TAXIWAY A RECONSTRUCTION AND TAXIWAY CONNECTOR REHABILITATION

GRADING AND SPOT ELEVATION PLAN TAXIWAY A (STA. 15+00 - 19+00)

MoDOT PROJ. NO. 23-056A-1	WOOLPERT PROJ. NO. 10016991.00
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SHEET NAME C303
SHEET NO. 30 of 54



KEYMAP

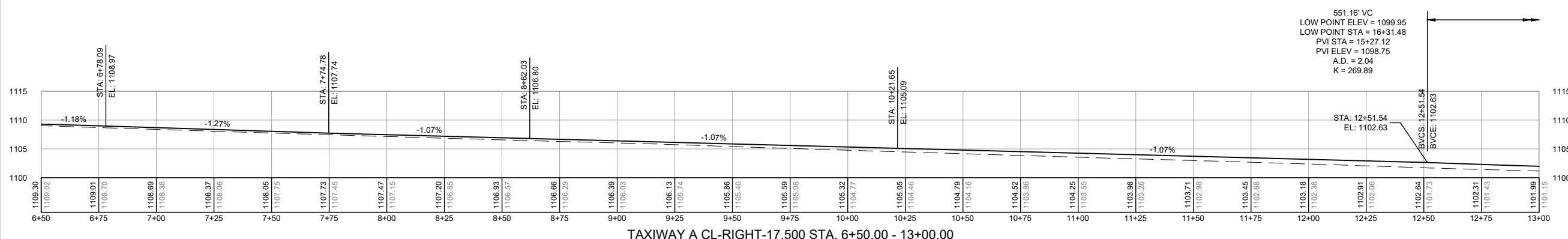
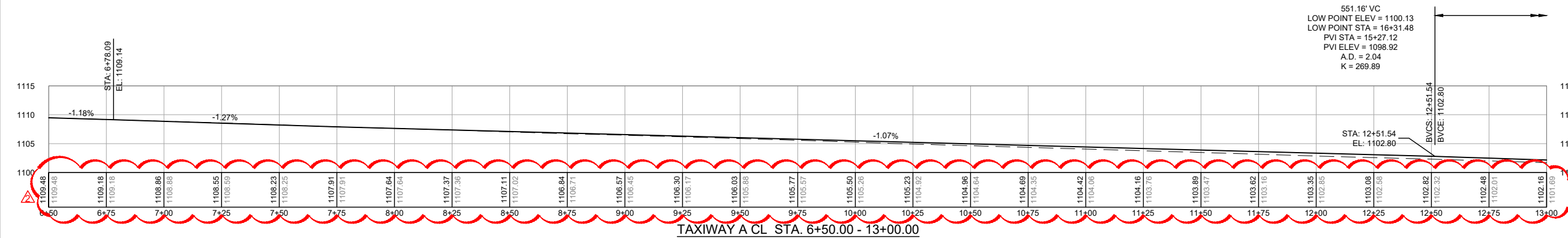
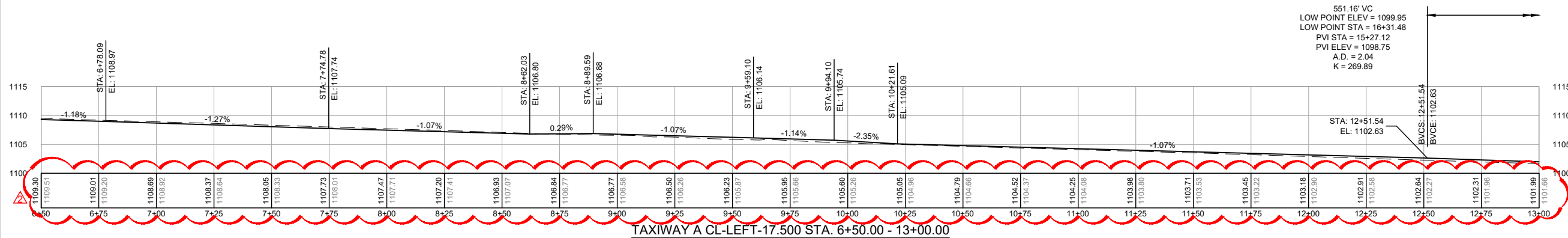
PLAN LEGEND

- ASPHALT MILL AND FILL (2" APPROXIMATE DEPTH)
- ASPHALT FULL DEPTH RECONSTRUCTION (TO BE DONE BY OTHERS)
- PROPOSED YELLOW PAINT
- PROPOSED BLACK PAINT

- EXISTING MAJOR CONTOUR
- EXISTING MINOR CONTOUR
- PROPOSED MAJOR CONTOUR
- PROPOSED MINOR CONTOUR

PROFILE LEGEND

- PROPOSED GRADE
- EXISTING GROUND
- PROFILE BAND
- EXISTING ELEVATION
- PROPOSED ELEVATION



- ### PLAN AND PROFILE NOTES
- REFER TO SHEET C220 FOR TYPICAL SECTIONS.
 - REFER TO SHEET C300-C304 FOR GRADING AND SPOT ELEVATIONS.
 - THE DEPTH OF EXISTING UTILITIES WAS DETERMINED ASSUMING A BURIAL DEPTH OF 2'-0". ACTUAL DEPTHS MAY VARY AND SHOULD BE FIELD VERIFIED.
 - IN THE EVENT OF ANY CONFLICT WITHIN THESE PLANS, THE INFORMATION IN THE PROFILES SHALL GOVERN OVER THE SPOT ELEVATIONS AND CROSS SECTIONS.
 - CONTRACTOR TO VERIFY EXISTING TIE POINTS PRIOR TO CONSTRUCTION AND NOTIFY THE ENGINEER OF ANY DISCREPANCIES.
 - PROFILES ARE SHOWN FOR 25' AND 50' OFFSET OF TAXIWAY A AND THE TAXIWAY CONNECTOR CENTERLINE. SEE SPOT ELEVATIONS FOR TAXIWAY A AND THE TAXIWAY CONNECTOR EDGE ELEVATIONS AT 25' AND 50' OFFSETS.

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LAURA K. KOONCE	2022012014	08/05/2025
NAME	REG. NO.	DATE
FOR AND ON BEHALF OF WOOLPERT, INC.		



DES: F.Z.D.	ISSUE RECORD			
	NO.	BY	DATE	DESCRIPTION
DR: N.B.B.	1	L.K.K.	08/05/2025	ISSUED FOR BID
	2	L.K.K.	08/29/2025	ADDENDUM NO. 1
CH: C.L.G.				
APP: L.K.K.				

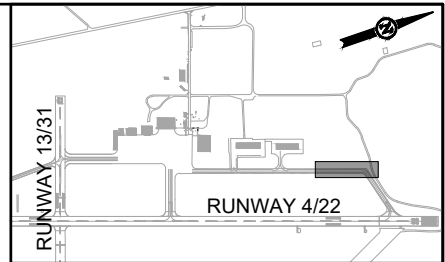
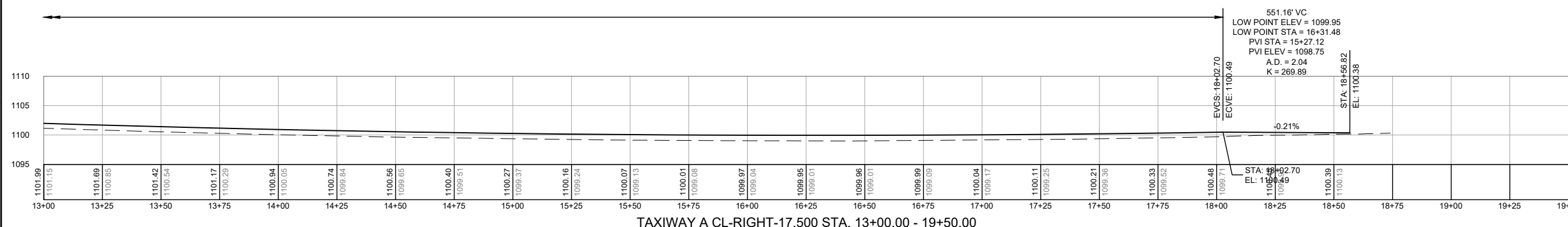
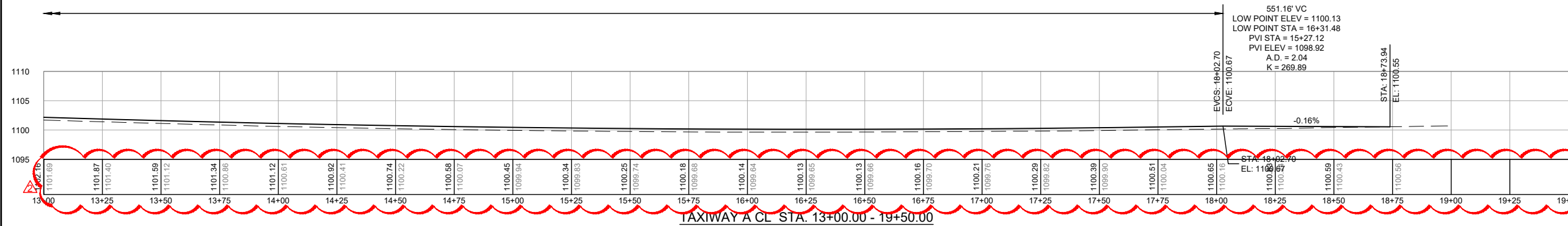
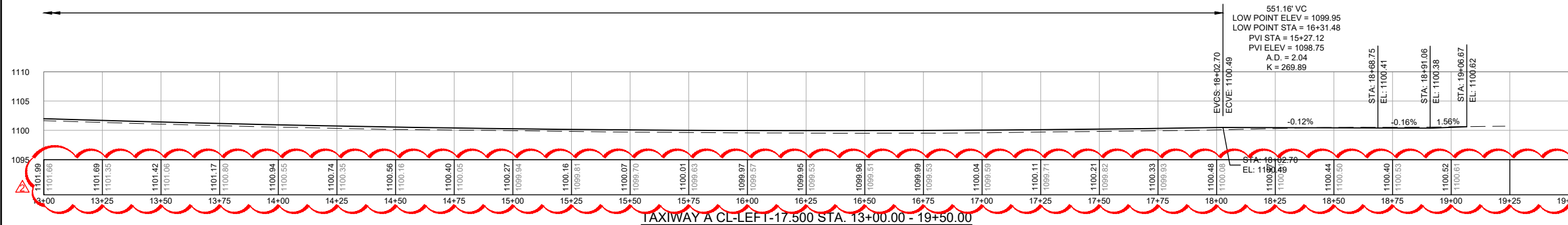
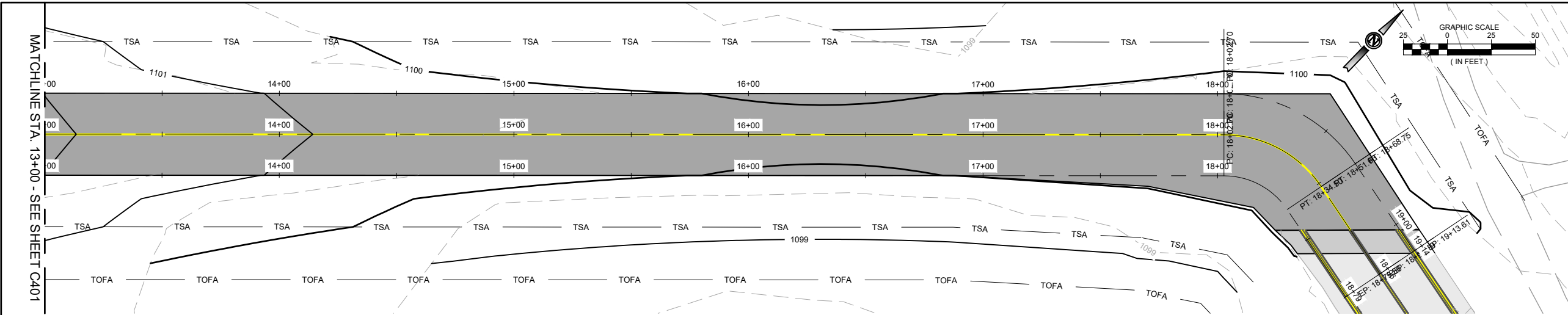
TAXIWAY A RECONSTRUCTION AND TAXIWAY CONNECTOR REHABILITATION

TAXIWAY A PLAN AND PROFILE STA. 06+50 - STA. 13+00

MoDOT PROJ. NO. 23-056A-1	WOOLPERT PROJ. NO. 10016991.00
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SHEET NAME C401
SHEET NO. 33 of 54

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\\NP22-056A-1\My Documents\CADD\PLANS\Sub-VH-23-056A-1-C402-PROF.dwg



KEYMAP

PLAN LEGEND

- ASPHALT MILL AND FILL (2" APPROXIMATE DEPTH)
- ASPHALT FULL DEPTH RECONSTRUCTION (TO BE DONE BY OTHERS)
- PROPOSED YELLOW PAINT
- PROPOSED BLACK PAINT
- EXISTING MAJOR CONTOUR
- EXISTING MINOR CONTOUR
- PROPOSED MAJOR CONTOUR
- PROPOSED MINOR CONTOUR

PROFILE LEGEND

- PROPOSED GRADE
- EXISTING GROUND
- PROFILE BAND
- EXISTING ELEVATION
- PROPOSED ELEVATION

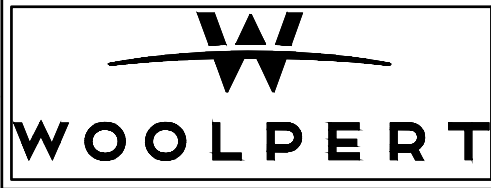
PLAN AND PROFILE NOTES

- REFER TO SHEET C220 FOR TYPICAL SECTIONS.
- REFER TO SHEET C300-C304 FOR GRADING AND SPOT ELEVATIONS.
- THE DEPTH OF EXISTING UTILITIES WAS DETERMINED ASSUMING A BURIAL DEPTH OF 2'-0". ACTUAL DEPTHS MAY VARY AND SHOULD BE FIELD VERIFIED.
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LAURA K. KOONCE 2022012014 08/05/2025
NAME REG. NO. DATE
FOR AND ON BEHALF OF WOOLPERT, INC.



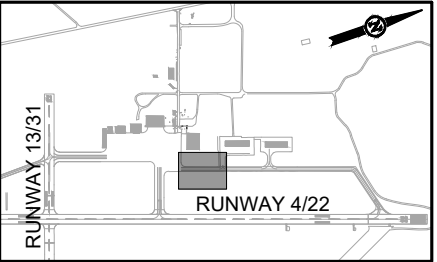
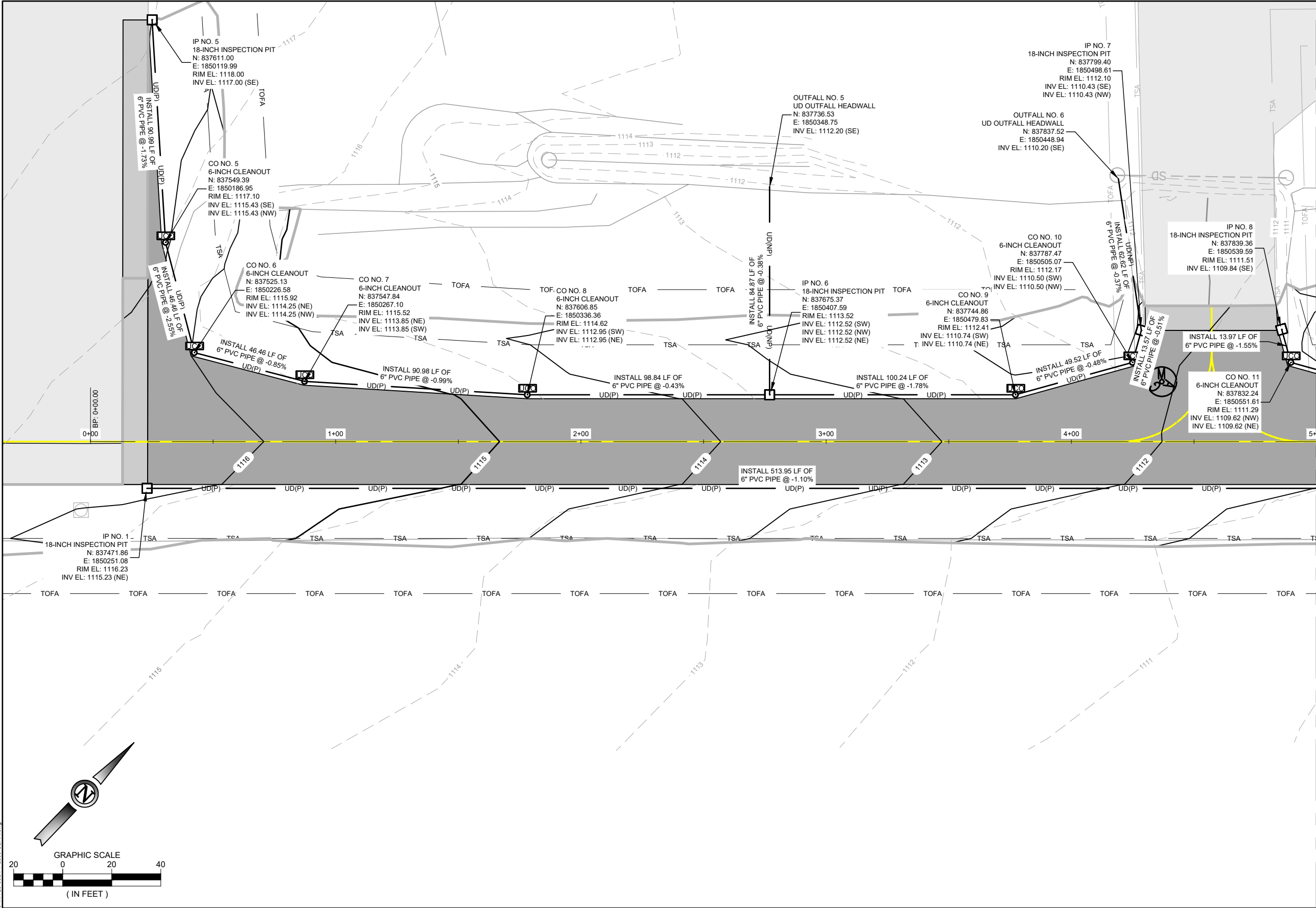
DES: F.Z.D.	ISSUE RECORD			
	NO.	BY	DATE	DESCRIPTION
DR: N.B.B.	1	L.K.K.	08/05/2025	ISSUED FOR BID
	2	L.K.K.	08/29/2025	ADDENDUM NO. 1
CH: C.L.G.				
APP: L.K.K.				

TAXIWAY A RECONSTRUCTION AND
TAXIWAY CONNECTOR REHABILITATION

TAXIWAY A PLAN AND PROFILE
STA. 13+00 - STA.19+50

MoDOT PROJ. NO. 23-056A-1 WOOLPERT PROJ. NO. 10016991.00

SHEET NAME C402
SHEET NO. 34 of 54



KEYMAP

UNDERDRAIN LEGEND

	ASPHALT FULL DEPTH RECONSTRUCTION (TO BE DONE BY OTHERS)
	PROPOSED YELLOW PAINT
	PROPOSED BLACK PAINT
	EXISTING MAJOR CONTOUR
	EXISTING MINOR CONTOUR
	PROPOSED MAJOR CONTOUR
	PROPOSED MINOR CONTOUR
	SWALE FLOW LINE
	PERFORATED UNDERDRAIN PIPE
	NON-PERFORATED UNDERDRAIN PIPE
	UNDERDRAIN CLEANOUT
	UNDERDRAIN INSPECTION PIT

- #### UNDERDRAIN NOTES
- REFER TO SHEET C220 FOR TYPICAL SECTIONS.
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 - SLOPES ARE CALCULATED FROM CENTER TO CENTER OF STRUCTURE.
 - CONTRACTOR TO COORDINATE UNDERDRAIN CROSSINGS WITH ELECTRICAL AND OTHER UTILITIES IN THE VICINITY TO ELIMINATE POTENTIAL CONFLICTS.

ISSUED FOR BID

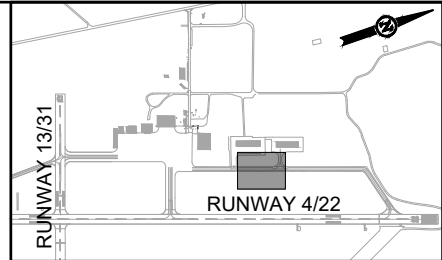
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LAURA K. KOONCE	2022012014	08/05/2025
NAME	REG. NO.	DATE
FOR AND ON BEHALF OF WOOLPERT, INC.		

DES: N.B.B.	ISSUE RECORD				TAXIWAY A RECONSTRUCTION AND TAXIWAY CONNECTOR REHABILITATION	UNDERDRAIN PLAN TAXIWAY A (STA. 00+00 - 05+00)	MoDOT PROJ. NO. 23-056A-1	WOOLPERT PROJ. NO. 10016991.00	SHEET NAME
DR: V.S.B.	NO.	BY	DATE	DESCRIPTION					C530
CH: C.L.G.	1	L.K.K.	08/05/2025	ISSUED FOR BID					SHEET NO. 37 of 54
APP: L.K.K.	1	L.K.K.	08/29/2025	ADDENDUM NO. 1					

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ENTIRE SHEET HAS BEEN REVISED AS PART OF ADDENDUM NO. 1



KEYMAP

UNDERDRAIN LEGEND

ASPHALT FULL DEPTH RECONSTRUCTION (TO BE DONE BY OTHERS)

PROPOSED YELLOW PAINT

PROPOSED BLACK PAINT

EXISTING MAJOR CONTOUR

EXISTING MINOR CONTOUR

PROPOSED MAJOR CONTOUR

PROPOSED MINOR CONTOUR

SWALE FLOW LINE

PERFORATED UNDERDRAIN PIPE

NON-PERFORATED UNDERDRAIN PIPE

UNDERDRAIN CLEANOUT

UNDERDRAIN INSPECTION PIT

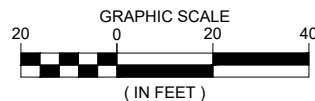
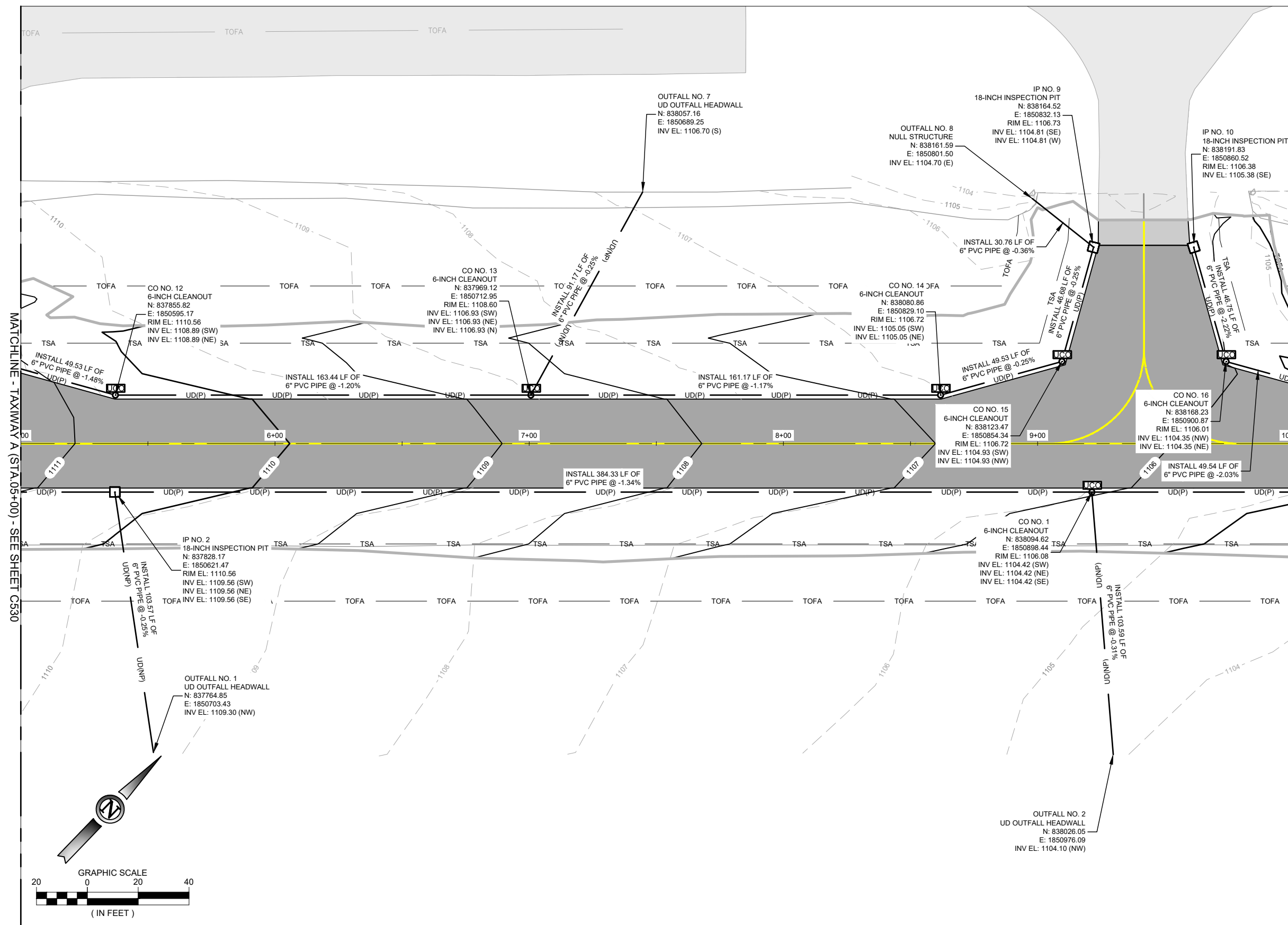
UNDERDRAIN NOTES

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NAME	REG. NO.	DATE
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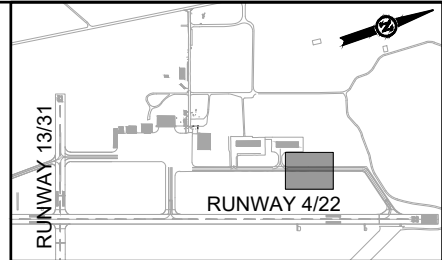
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DR: V.S.B.	1	L.K.K.	08/05/2025	ISSUED FOR BID
CH: C.L.G.	2	L.K.K.	08/29/2025	ADDENDUM NO. 1
APP: L.K.K.				

TAXIWAY A RECONSTRUCTION AND
TAXIWAY CONNECTOR REHABILITATION

UNDERDRAIN PLAN
TAXIWAY A (STA. 05+00 - 10+00)

MoDOT PROJ. NO. 23-056A-1	WOOLPERT PROJ. NO. 10016991.00
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SHEET NAME
C531
SHEET NO.
38 of 54



KEYMAP

UNDERDRAIN LEGEND

- ASPHALT FULL DEPTH RECONSTRUCTION (TO BE DONE BY OTHERS)
- PROPOSED YELLOW PAINT
- PROPOSED BLACK PAINT
- EXISTING MAJOR CONTOUR
- EXISTING MINOR CONTOUR
- PROPOSED MAJOR CONTOUR
- PROPOSED MINOR CONTOUR
- SWALE FLOW LINE
- PERFORATED UNDERDRAIN PIPE
- NON-PERFORATED UNDERDRAIN PIPE
- UNDERDRAIN CLEANOUT
- UNDERDRAIN INSPECTION PIT

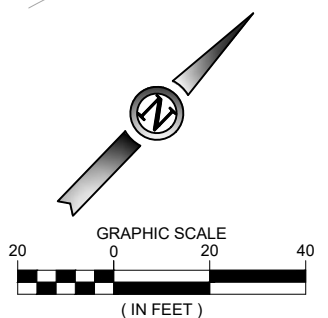
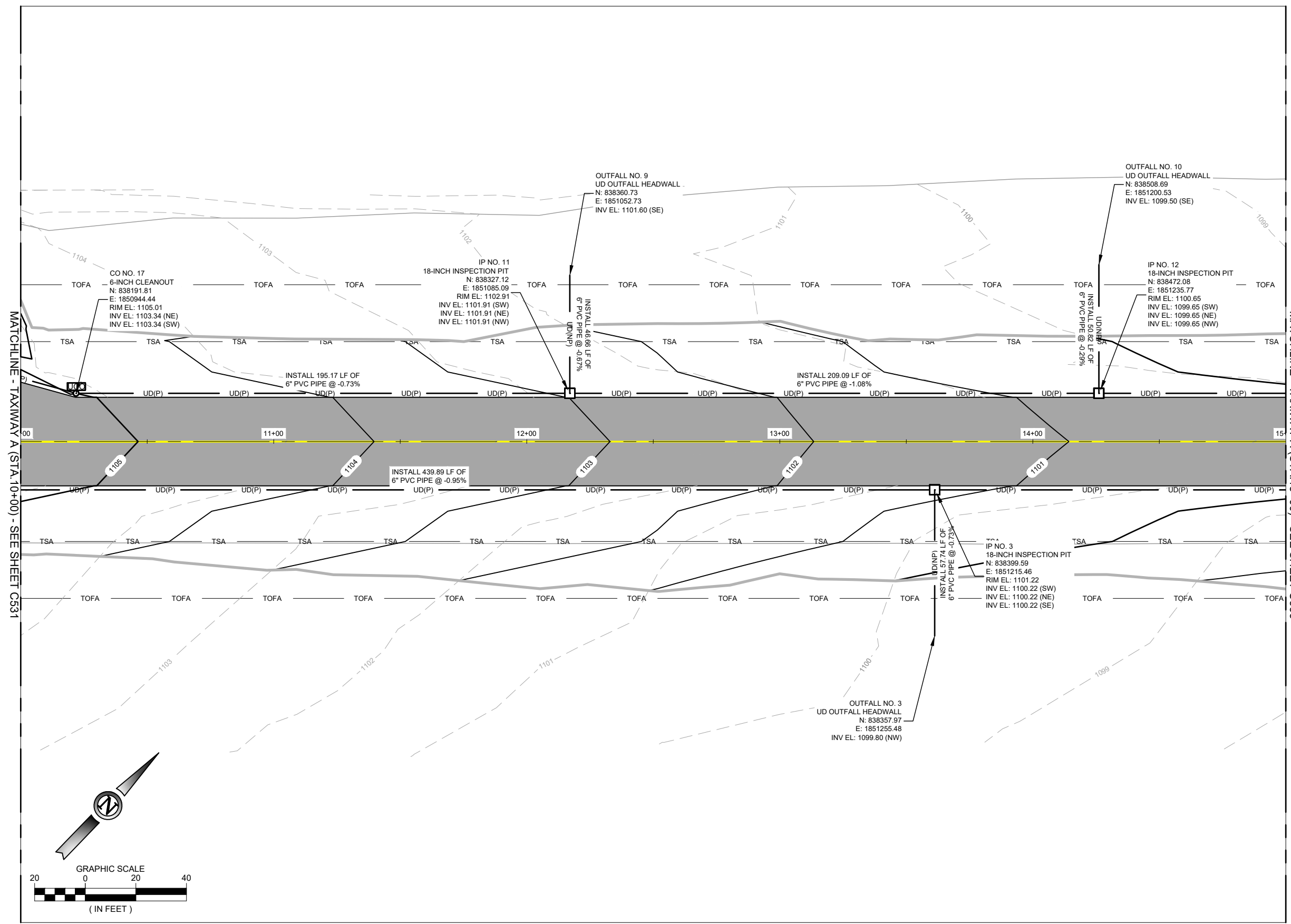
UNDERDRAIN NOTES

- REFER TO SHEET C220 FOR TYPICAL SECTIONS.
- REFER TO SHEETS C300 THRU C304 FOR SPOT ELEVATIONS.
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- SLOPES ARE CALCULATED FROM CENTER TO CENTER OF STRUCTURE.
- CONTRACTOR TO COORDINATE UNDERDRAIN CROSSINGS WITH ELECTRICAL AND OTHER UTILITIES IN THE VICINITY TO ELIMINATE POTENTIAL CONFLICTS.

ISSUED FOR BID

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LAURA K. KOONCE	2022012014	08/05/2025
NAME	REG. NO.	DATE
FOR AND ON BEHALF OF WOOLPERT, INC.		



ENTIRE SHEET HAS BEEN REVISED AS PART OF ADDENDUM NO. 1



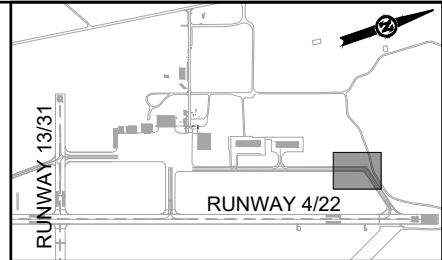
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DR: V.S.B.	NO.	BY	DATE	DESCRIPTION
CH: C.L.G.	1	L.K.K.	08/05/2025	ISSUED FOR BID
APP: L.K.K.		L.K.K.	08/29/2025	ADDENDUM NO. 1

TAXIWAY A RECONSTRUCTION AND TAXIWAY CONNECTOR REHABILITATION

UNDERDRAIN PLAN
TAXIWAY A (STA. 10+00 - 15+00)

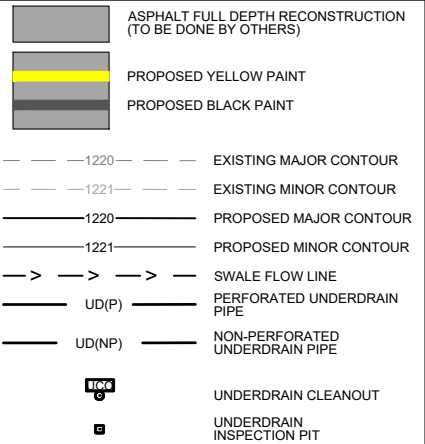
MoDOT PROJ. NO. 23-056A-1	WOOLPERT PROJ. NO. 10016991.00
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SHEET NAME
C532
SHEET NO.
39 of 54



KEYMAP

UNDERDRAIN LEGEND



UNDERDRAIN NOTES

- REFER TO SHEET C220 FOR TYPICAL SECTIONS.
- REFER TO SHEETS C300 THRU C304 FOR SPOT ELEVATIONS.
- THE DEPTH OF EXISTING UTILITIES ARE APPROXIMATE AND WERE DETERMINED ASSUMING A BURIAL DEPTH OF 2'-0". ACTUAL DEPTHS MAY VARY AND SHOULD BE FIELD VERIFIED.
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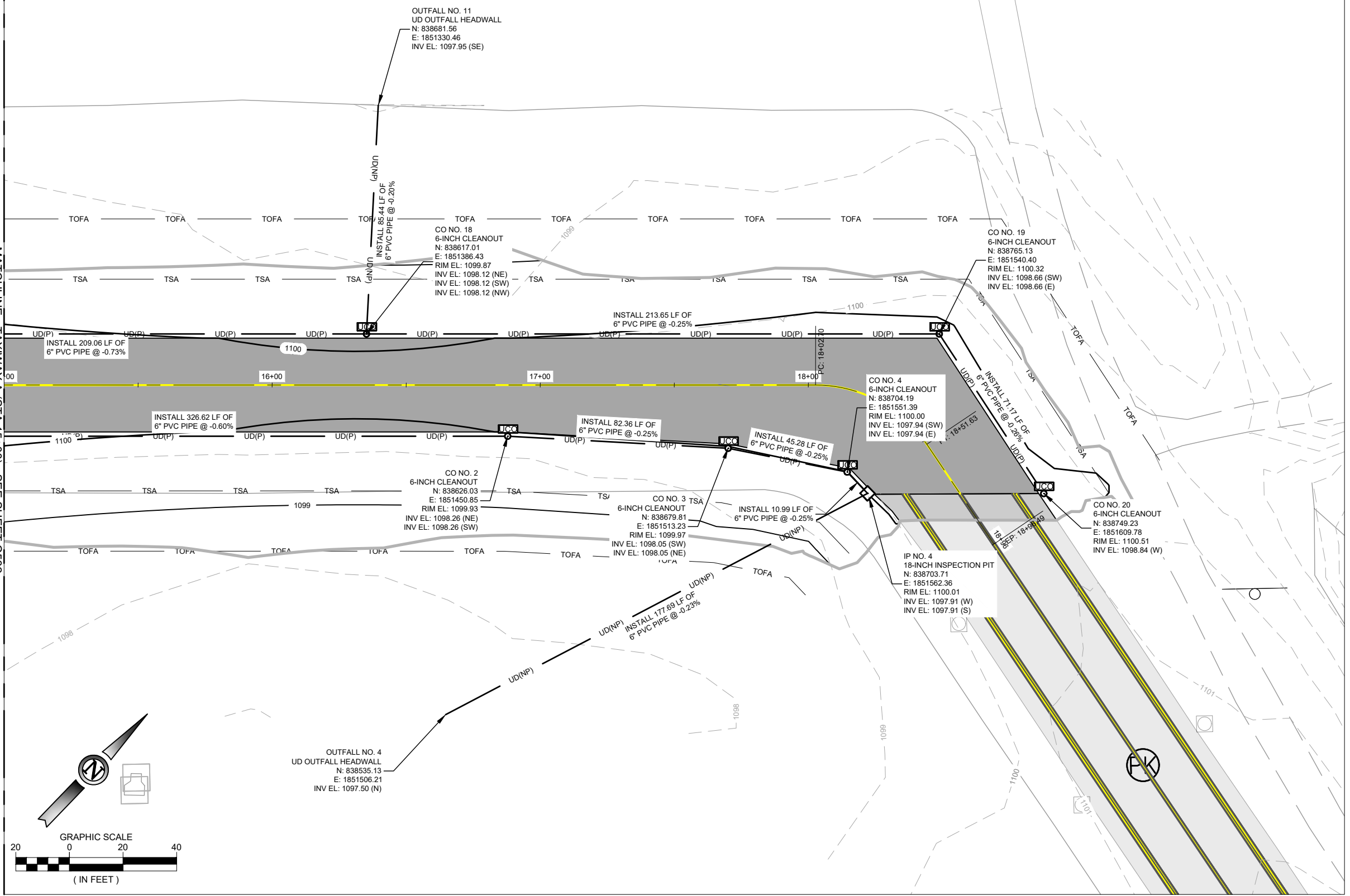
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LAURA K. KOONCE	2022012014	08/05/2025
NAME	REG. NO.	DATE
FOR AND ON BEHALF OF WOOLPERT, INC.		

ENTIRE SHEET HAS BEEN REVISED AS PART OF ADDENDUM NO. 1

MATCHLINE - TAXIWAY A (STA. 15+00) - SEE SHEET C532



DES: N.B.B.	ISSUE RECORD			
	NO.	BY	DATE	DESCRIPTION
DR: V.S.B.	1	L.K.K.	08/05/2025	ISSUED FOR BID
CH: C.L.G.	2	L.K.K.	08/29/2025	ADDENDUM NO. 1
APP: L.K.K.				

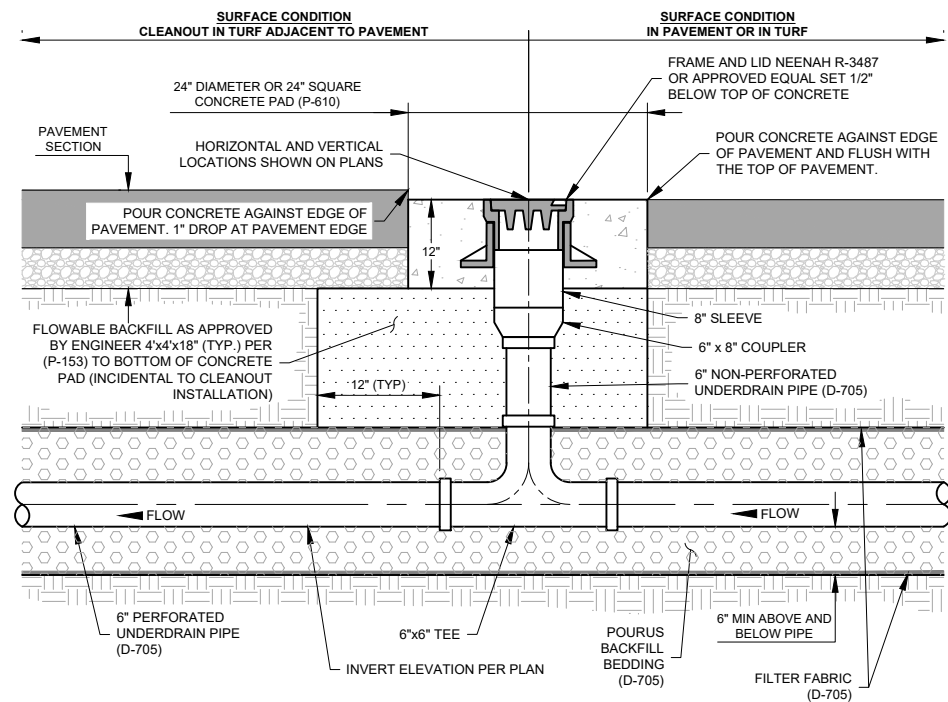
TAXIWAY A RECONSTRUCTION AND TAXIWAY CONNECTOR REHABILITATION

UNDERDRAIN PLAN
TAXIWAY A (STA. 15+00 - 19+00)

MoDOT PROJ. NO. 23-056A-1	WOOLPERT PROJ. NO. 10016991.00
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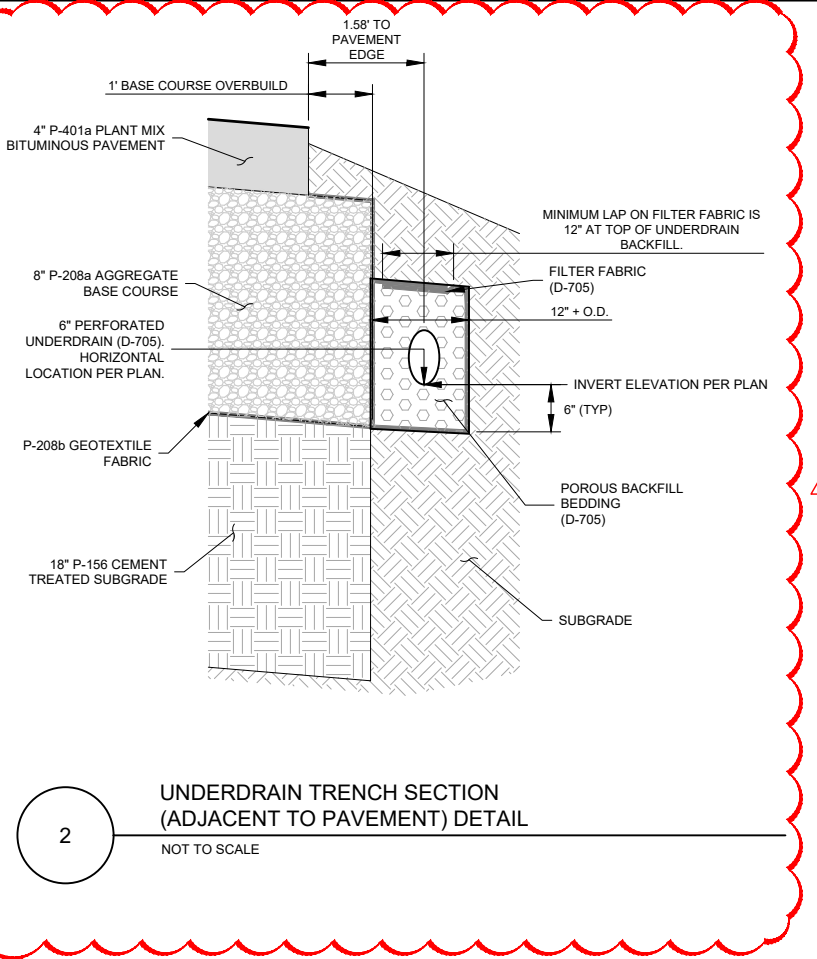
SHEET NAME C533
SHEET NO. 40 of 54

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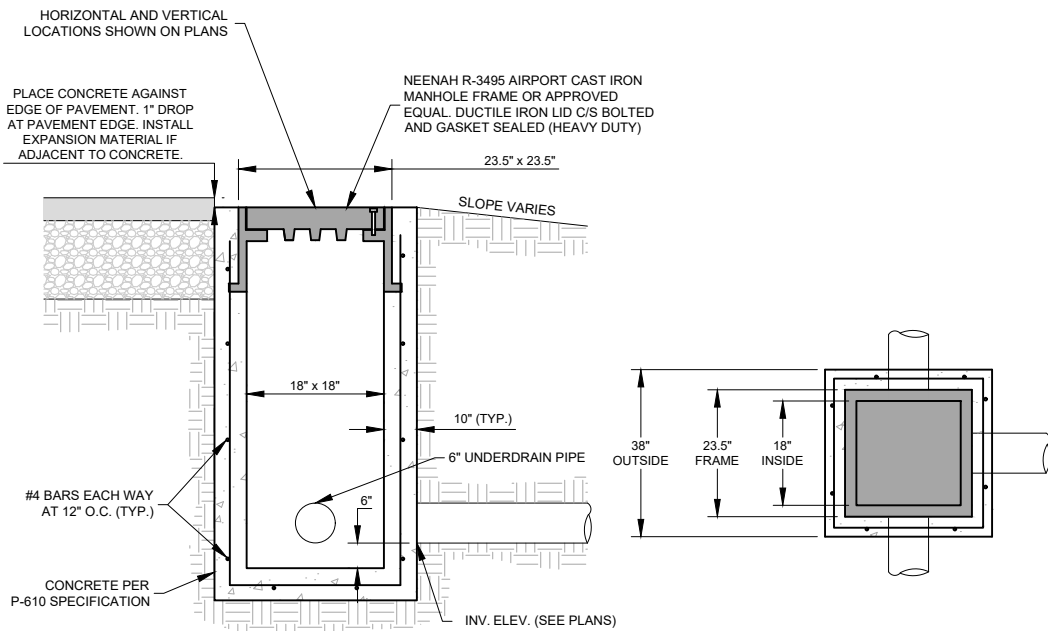
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NOT TO SCALE



UNDERDRAIN TRENCH SECTION (ADJACENT TO PAVEMENT) DETAIL

NOT TO SCALE

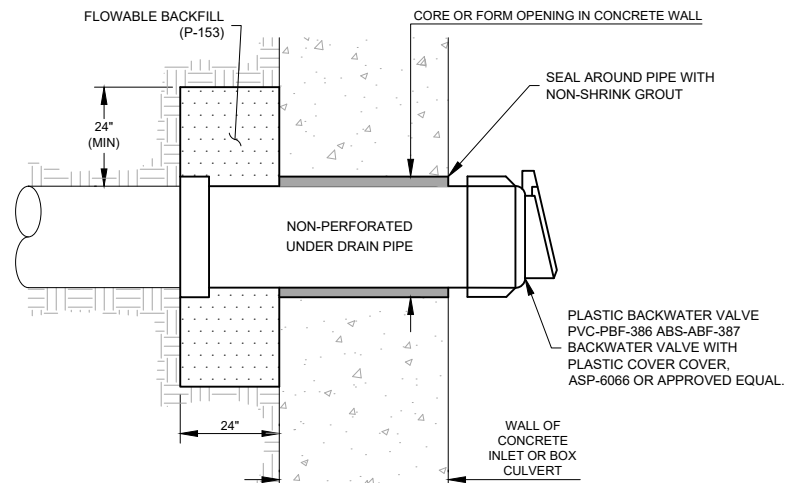


SECTION VIEW

PLAN VIEW

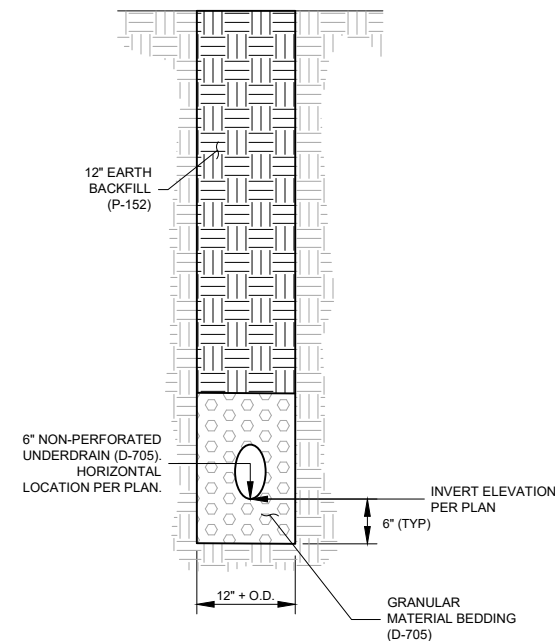
UNDERDRAIN INSPECTION PIT (IN TURF) DETAIL

NOT TO SCALE



UNDERDRAIN OUTFALL TO STRUCTURE DETAIL

NOT TO SCALE



UNDERDRAIN TRENCH SECTION (OUTFALL) DETAIL

NOT TO SCALE

ISSUED FOR BID

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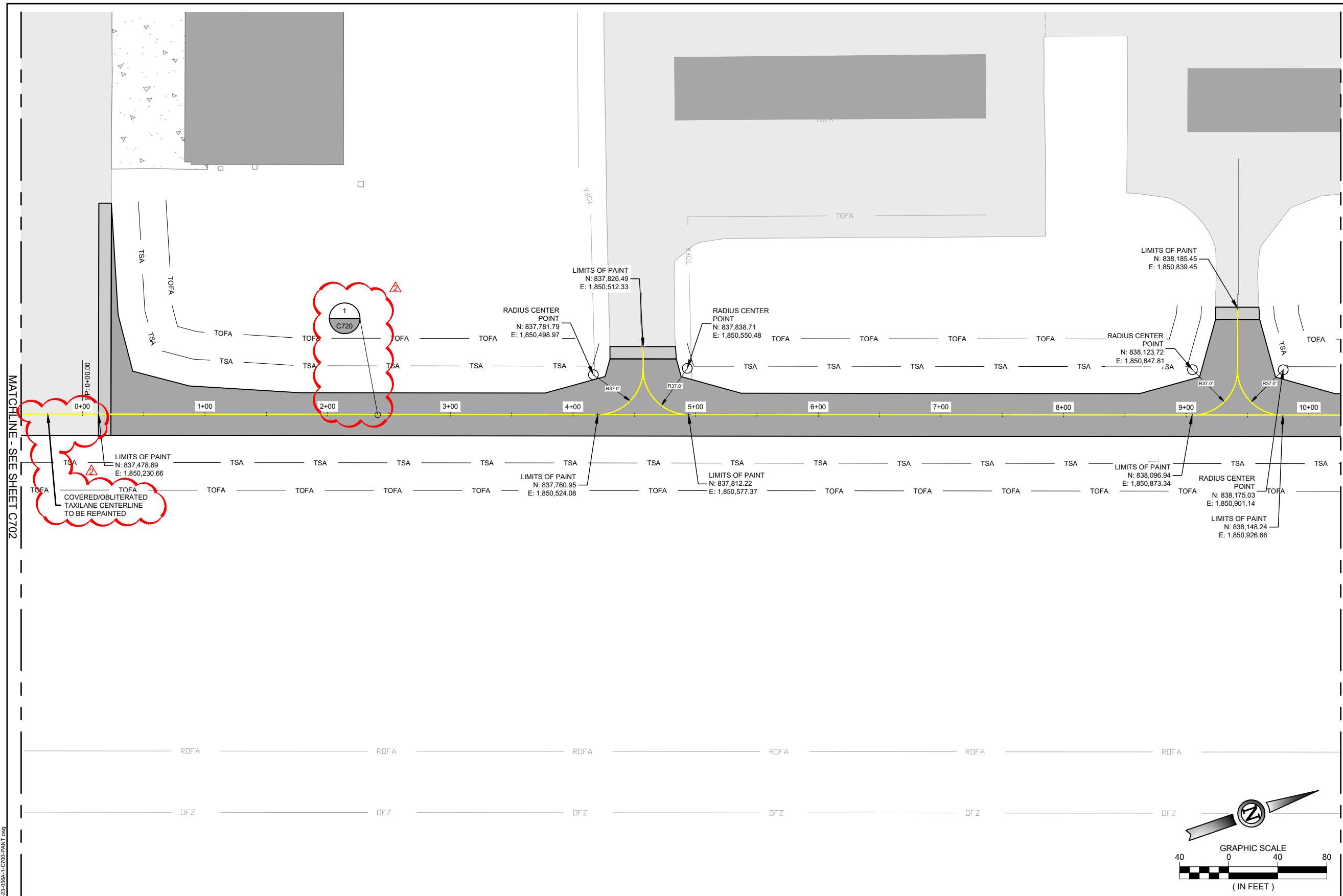
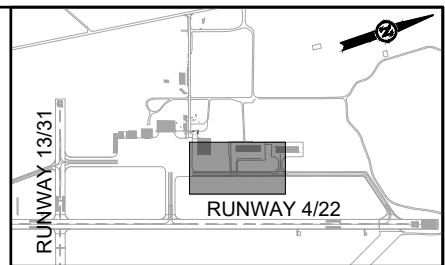
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NO.	BY	DATE	DESCRIPTION	
1	L.K.K.	08/05/2025	ISSUED FOR BID	
2	L.K.K.	08/29/2025	ADDENDUM NO. 1	

TAXIWAY A RECONSTRUCTION AND TAXIWAY CONNECTOR REHABILITATION

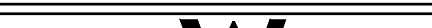

UNDERDRAIN DETAILS AND NOTES

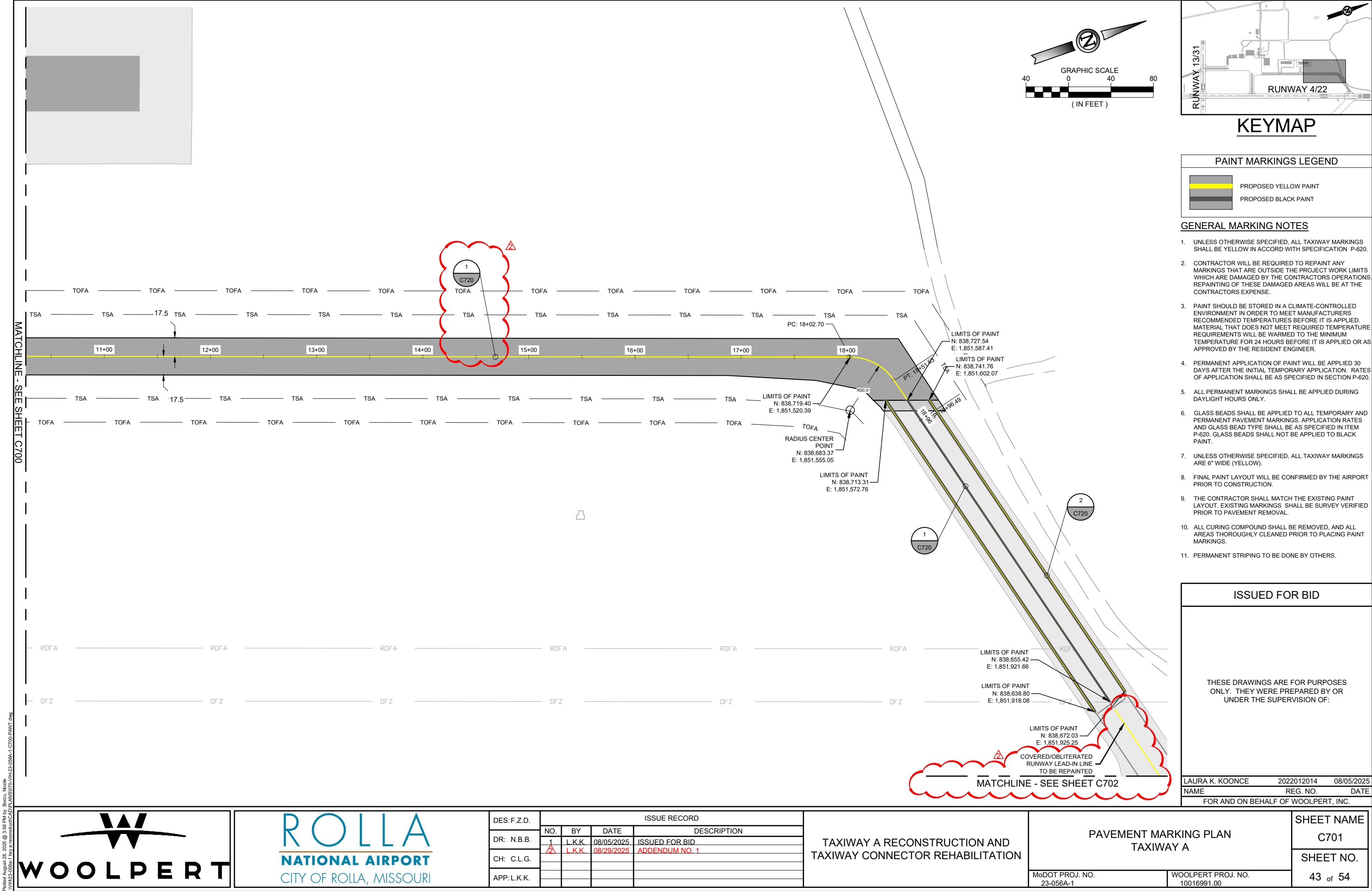
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SHEET NAME
C550
SHEET NO.
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ISSUED FOR BID

		DES: F.Z.D.	ISSUE RECORD				TAXIWAY A RECONSTRUCTION AND TAXIWAY CONNECTOR REHABILITATION	PAVEMENT MARKING PLAN TAXIWAY A		SHEET NAME	
		DR: N.B.B.	NO.	BY	DATE	DESCRIPTION				C700	
		CH: C.L.G.	1	L.K.K.	08/05/2025	ISSUED FOR BID		ADDENDUM NO. 1	MoDOT PROJ. NO. 23-056A-1	WOOLPERT PROJ. NO. 10016991.00	SHEET NO. 42 of 54
		APP: L.K.K.									



PAINT MARKINGS LEGEND

PROPOSED YELLOW PAINT

PROPOSED BLACK PAINT

- GENERAL MARKING NOTES
- UNLESS OTHERWISE SPECIFIED, ALL TAXIWAY MARKINGS SHALL BE YELLOW IN ACCORD WITH SPECIFICATION P-620.
 - CONTRACTOR WILL BE REQUIRED TO REPAINT ANY MARKINGS THAT ARE OUTSIDE THE PROJECT WORK LIMITS WHICH ARE DAMAGED BY THE CONTRACTORS OPERATIONS. REPAINTING OF THESE DAMAGED AREAS WILL BE AT THE CONTRACTORS EXPENSE.
 - PAINT SHOULD BE STORED IN A CLIMATE-CONTROLLED ENVIRONMENT IN ORDER TO MEET MANUFACTURERS RECOMMENDED TEMPERATURES BEFORE IT IS APPLIED. MATERIAL THAT DOES NOT MEET REQUIRED TEMPERATURE REQUIREMENTS WILL BE WARMED TO THE MINIMUM TEMPERATURE FOR 24 HOURS BEFORE IT IS APPLIED OR AS APPROVED BY THE RESIDENT ENGINEER.
 - PERMANENT APPLICATION OF PAINT WILL BE APPLIED 30 DAYS AFTER THE INITIAL TEMPORARY APPLICATION. RATES OF APPLICATION SHALL BE AS SPECIFIED IN SECTION P-620.
 - ALL PERMANENT MARKINGS SHALL BE APPLIED DURING DAYLIGHT HOURS ONLY.
 - GLASS BEADS SHALL BE APPLIED TO ALL TEMPORARY AND PERMANENT PAVEMENT MARKINGS. APPLICATION RATES AND GLASS BEAD TYPE SHALL BE AS SPECIFIED IN ITEM P-620. GLASS BEADS SHALL NOT BE APPLIED TO BLACK PAINT.
 - UNLESS OTHERWISE SPECIFIED, ALL TAXIWAY MARKINGS ARE 6" WIDE (YELLOW).
 - FINAL PAINT LAYOUT WILL BE CONFIRMED BY THE AIRPORT PRIOR TO CONSTRUCTION.
 - THE CONTRACTOR SHALL MATCH THE EXISTING PAINT LAYOUT. EXISTING MARKINGS SHALL BE SURVEY VERIFIED PRIOR TO PAVEMENT REMOVAL.
 - ALL CURING COMPOUND SHALL BE REMOVED, AND ALL AREAS THOROUGHLY CLEANED PRIOR TO PLACING PAINT MARKINGS.
 - PERMANENT STRIPING TO BE DONE BY OTHERS.

ISSUED FOR BID

THESE DRAWINGS ARE FOR PURPOSES ONLY. THEY WERE PREPARED BY OR UNDER THE SUPERVISION OF:

LAURA K. KOONCE	2022012014	08/05/2025
NAME	REG. NO.	DATE
FOR AND ON BEHALF OF WOOLPERT, INC.		

DES: F.Z.D.	ISSUE RECORD			
	NO.	BY	DATE	DESCRIPTION
DR: N.B.B.	1	L.K.K.	08/05/2025	ISSUED FOR BID
	2	L.K.K.	08/29/2025	ADDENDUM NO. 1
CH: C.L.G.				
APP: L.K.K.				

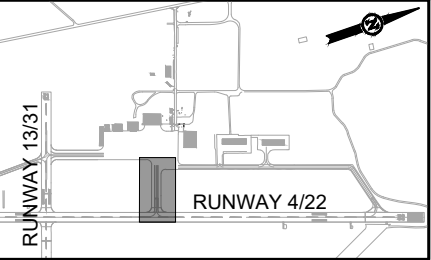
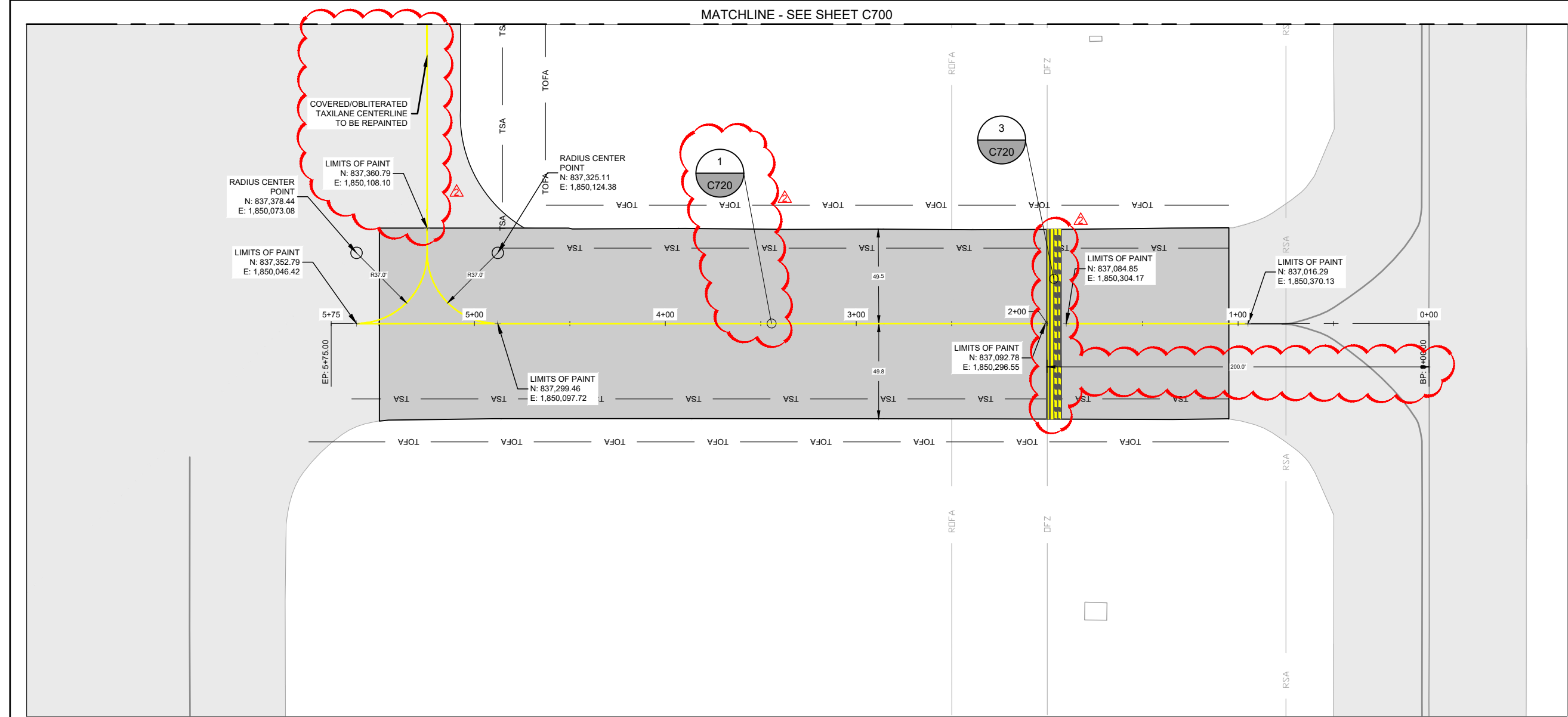
TAXIWAY A RECONSTRUCTION AND
TAXIWAY CONNECTOR REHABILITATION

PAVEMENT MARKING PLAN
TAXIWAY A

MoDOT PROJ. NO. 23-056A-1	WOOLPERT PROJ. NO. 10016991.00
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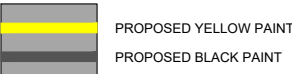
SHEET NAME
C701

SHEET NO.
43 of 54



KEYMAP

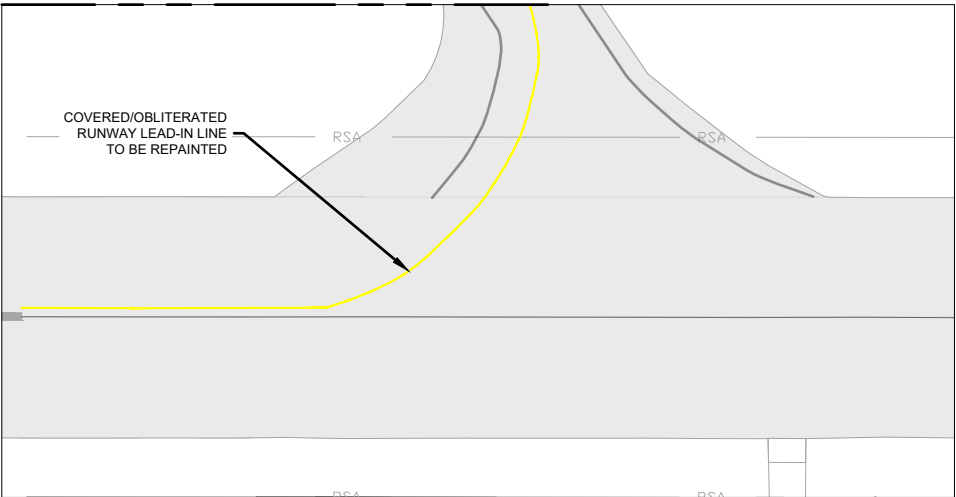
PAINT MARKINGS LEGEND



GENERAL MARKING NOTES

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2. CONTRACTOR WILL BE REQUIRED TO REPAINT ANY MARKINGS THAT ARE OUTSIDE THE PROJECT WORK LIMITS WHICH ARE DAMAGED BY THE CONTRACTORS OPERATIONS. REPAINTING OF THESE DAMAGED AREAS WILL BE AT THE CONTRACTORS EXPENSE.
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9. THE CONTRACTOR SHALL MATCH THE EXISTING PAINT LAYOUT. EXISTING MARKINGS SHALL BE SURVEY VERIFIED PRIOR TO PAVEMENT REMOVAL.
10. ALL CURING COMPOUND SHALL BE REMOVED, AND ALL AREAS THOROUGHLY CLEANED PRIOR TO PLACING PAINT MARKINGS.
11. PERMANENT STRIPING TO BE DONE BY OTHERS.

MATCHLINE - SEE SHEET C701



ISSUED FOR BID

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LAURA K. KOONCE	2022012014	08/05/2025
NAME	REG. NO.	DATE
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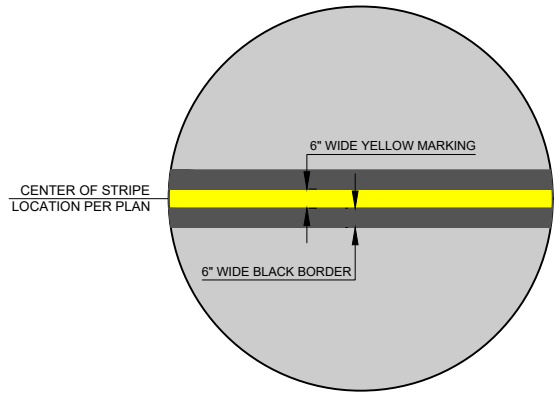
DES: F.Z.D.	ISSUE RECORD			
	NO.	BY	DATE	DESCRIPTION
DR: N.B.B.	1	L.K.K.	08/05/2025	ISSUED FOR BID
CH: C.L.G.	2	L.K.K.	08/29/2025	ADDENDUM NO. 1
APP: L.K.K.				

TAXIWAY A RECONSTRUCTION AND
TAXIWAY CONNECTOR REHABILITATION

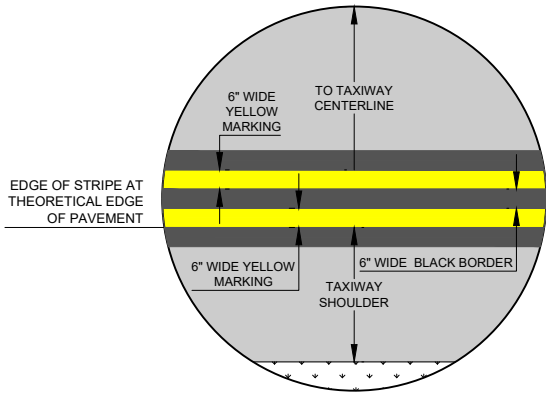
PAVEMENT MARKING PLAN
TAXIWAY B CONNECTOR

MoDOT PROJ. NO. 23-056A-1	WOOLPERT PROJ. NO. 10016991.00
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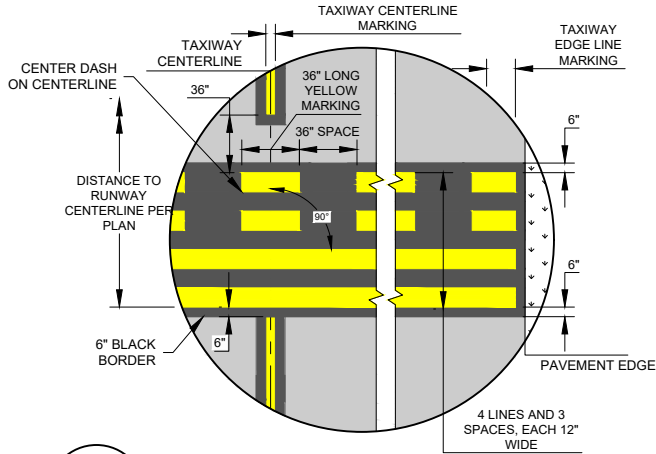
SHEET NAME C702
SHEET NO. 44 of 54



1 TAXIWAY CENTERLINE DETAIL
NOT TO SCALE
NOTE: BLACK BORDERS ARE NOT REQUIRED ON NEW ASPHALT PAVEMENT.



2 TAXIWAY SOLID EDGE LINE DETAIL
NOT TO SCALE



3 RUNWAY HOLDING POSITION DETAIL
NOT TO SCALE
NOTE: BLACK BORDERS ARE NOT REQUIRED ON NEW ASPHALT PAVEMENT.

ISSUED FOR BID

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LAURA K. KOONCE	2022012014	08/05/2025
NAME	REG. NO.	DATE
FOR AND ON BEHALF OF WOOLPERT, INC.		

ENTIRE SHEET HAS BEEN REVISED AS PART OF ADDENDUM NO. 1



DES: F.Z.D.	ISSUE RECORD			
	NO.	BY	DATE	DESCRIPTION
DR: N.B.B.	1	L.K.K.	08/05/2025	ISSUED FOR BID
	2	L.K.K.	08/29/2025	ADDENDUM NO. 1
CH: C.L.G.				
APP: L.K.K.				

TAXIWAY A RECONSTRUCTION AND
TAXIWAY CONNECTOR REHABILITATION

PAVEMENT MARKING
DETAILS AND NOTES

MoDOT PROJ. NO. 23-056A-1	WOOLPERT PROJ. NO. 10016991.00
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SHEET NAME
C720
SHEET NO.
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Meeting Agenda: Pre-Bid Conference – Meeting Minutes

Rolla National Airport (VIH)

MoDOT Project No. 23-056A-1

Date: Tuesday, August 19, 2025, 9:00 A.M. (CST)

Location: 631 Airport Drive, Vichy, MO 65580, Airport Terminal Building Conference Room

Attendees: Refer to Sign-In Sheet

Agenda:

1. RECORDING OF ATTENDEES

- A. Recording of attendees, firm represented, address, phone number and email.
- B. Attendance & plan holder's list will be sent to all attendees.

2. PROJECT DESCRIPTION & INTRODUCTIONS

- A. Project Scope of Work.
 - I. Taxiway A Reconstruction and Taxiway Connector Rehabilitation
- B. Airport Sponsor.
 - I. Darrin Bacon, Airport Manager
 - II. Darin Pryor, Director of Public Works
- C. MoDOT Aviation.
 - I. Kimberly Masters, Operations Specialist
- D. Airport Engineering. (Woolpert, Inc.)
 - I. Laura Koonce, Project Manager
 - II. Joe Pestka, Project Director
- E. Project Schedules (Total 57 Calendar Days)
 - I. Schedule I: Taxiway A Reconstruction
 - II. Schedule II: Taxiway B Rehabilitation
 - III. Schedule III: Taxiway A Underdrains

Woolpert, Inc.
931 Wildwood Drive, Suite 101
Jefferson City, MO 65109
+1 573.636.3200

F. Major Work Items.

Schedule I			
Item No.	Description	Units	Quantity
C-100a	Contractor's Quality Control Program	LS	1
C-102a	Temporary Erosion Control	LS	1
C-105a	Mobilization	LS	1
P-101a	Pavement Removal – Full Depth	SY	11,580
P-101b	Pavement Removal – Partial Depth	SY	280
P-152a	Unclassified Excavation	CY	910
P-156a	Cement Treated Subgrade, 18 Inches	SY	8,430
P-156b	Cement	TON	280
P-208a	Aggregate Base Course, 8 Inches	CY	2,190
P-208b	Geotextile Fabric	SY	9,830
P-401a	Asphalt Surface Course	TON	2,060
P-603a	Bituminous Tack Coat	GAL	1,310
P-620a	Temporary Airport Pavement Marking	SF	3,480
P-620b	Permanent Airport Pavement Marking, Yellow	SF	2,040
P-620c	Permanent Airport Pavement Marking, Black	SF	1,440
T-901a	Seeding with Hydromulch	AC	2.50
L-108a	Install #8 AWG, L-824C, 5000 Volt Wire	LF	5,100
L-108b	Install #6 AWG Bare Copper Counterpoise, Including Ground Rods	LF	4,100
L-110a	Install 1-2" SCH 40 PVC Conduit, Direct Earth Buried (DEB)	LF	3,900
L-110b	Install 1-2" SCH 40 HDPE Conduit, Directionally Bored	LF	190
L-125a	Remove Stake Mounted Taxiway Edge Light, Complete	EA	33
L-125b	Install L-861T LED Taxiway Edge Light, Complete	EA	55
Schedule II			
Item No.	Description	Units	Quantity
C-100a	Contractor's Quality Control Program	LS	1
C-105a	Mobilization	LS	1
P-101b	Pavement Removal – Partial Depth	SY	5,180
P-401a	Asphalt Surface Course	TON	800
P-603a	Bituminous Tack Coat	GAL	760
P-620a	Temporary Airport Pavement Marking	SF	640
P-620b	Permanent Airport Pavement Marking, Yellow	SF	640
P-620d	Pavement Marking Obliteration	SF	580
Schedule III			
Item No.	Description	Units	Quantity
C-100a	Contractor's Quality Control Program	LS	1
C-105a	Mobilization	LS	1
P-152a	Unclassified Excavation	CY	80
D-705a	Install 6-Inch Perforated Polyethylene Pipe	LF	3,900
D-705b	Install 6-Inch Non-Perforated Polyethylene Pipe	LF	840
D-751a	Install 6-Inch Underdrain Cleanout	EA	24
D-751b	Install 18x18 Inch Inspection Pit	EA	8

*Will award schedules based on funding availability. May not award Sch II or III depending on funding.

3. BID OPENING DATE & TIME

- A. Date: September 17, 2025, 11:00 A.M. (CST)
- B. Opened at: Rolla City Hall
901 North Elm Street, 3rd Floor Conference Room
Rolla, MO 65401
- C. Bid security: 5% of bid amount – Section 3: Instructions to Bidders, Paragraph 10 Bid Security and Section 20-10 Bid guarantee.
- D. Bid proposal: Part 2: Bidding Documents (complete and submit all Part 2 pages).
- E. [Bid Hold](#): [120 Days](#)

4. DBE GOALS

- A. 3.00% of Contract Amount.
- B. Acceptable DBE firms are not limited to firms based within the state in which the project is being constructed, however an out-of-state DBE firm must be certified in that state where the project is located before execution of the contract. Only DBE certified firms, in the project state, count toward DBE participation/goals.
- C. DBE certification documents will need to be submitted at the time of bid opening. Failure to include supporting DBE documentation with the bid proposal may result in bid non-responsiveness.
- D. Contractor to ensure that the DBE company is registered on the MRCC website and is certified to perform the type of work specified in the bid.
- E. The DBE information submitted with the bid proposal cannot change during construction unless approved by MoDOT.
- F. The DBE goal will be calculated based on the full bid. Needs to meet 3% prior to bid.

5. QUALIFICATION OF BIDDERS

- A. Qualifications shall be furnished per Section 20-02. Each bidder shall furnish the Owner satisfactory evidence of his or her competency and financial responsibility to perform the proposed work. This includes resumes of key personnel, equipment list, and a list detailing experience on similar construction projects.
- B. Prime contractor is to ensure that the SAM registration is current.
- C. Prime contractor is to have good standing with the secretary of state at least seven (7) days prior to bid opening.

6. CRITICAL CONTRACT DATES

- A. Notice of Award: TBD
- B. Notice to Proceed: TBD
- C. Construction Start: Estimated Spring 2026
- D. Project Time: Refer to Section 80-07 and Section 80-08 for more information.
 - I. Total Project: 57 Calendar Days.
 - II. Schedule I: 45 Calendar Days.
 - III. Schedule II: 4 Calendar Days.
 - IV. Schedule III: 8 Calendar Days.

7. BONDING

- A. Payment Bond: Part 1: General, Notice to Bidders and Part 3: Contract Documents, 100% of Bid Amount.
- B. Performance Bond: Part 1: General, Notice to Bidders and Part 3: Contract Documents, 100% of Bid Amount.

8. INSURANCE REQUIREMENTS

- A. Refer to Part 5: Special Provisions, Local Provisions and Section 70-21.

9. FEDERAL WAGE RATES (DAVIS BACON ACT)

- A. Federal wage rates are required for this project. Refer to Part 6: Wage Rates.
- B. Contractor and all Subcontractors are required to submit certified payrolls.

10. ENGINEER/RESIDENT PROJECT REPRESENTATIVE (RPR) FIELD OFFICE

- A. Not required. Refer to Section 60-05 for more information.

11. CONSTRUCTION SAFETY AND PHASING PLAN (CSPP)

- A. Construction Safety and Phasing Plan (CSPP) – Refer to Part 7: Safety Documents, Construction Safety and Phasing Plan (CSPP).
- B. The Contractor shall review and adhere to the CSPP prepared by the Engineer.
- C. The Contractor shall submit a Safety Plan Compliance Document (SPCD) [Refer to Part 7: Safety Documents, Construction Safety and Phasing Compliance Document (SPCD)] to the engineer and airport operator for approval PRIOR to the issuance of Notice to Proceed as required by FAA Advisory Circular (AC) 150/5370-2G “Operational Safety on Airports During Construction”.
- D. [Runway Closure Marker is to be lighted; Taxiway Closure Marker may be tarp or other approved material.](#)

12. ENVIRONMENTAL REQUIREMENTS

- A. Discuss all project specific requirements for environmentally sensitive areas. Discuss protocols for working in and around these sensitive areas. Adherence to these requirements will be strictly enforced.
- B. All changes to haul routes, staging areas, material storage areas, borrow/waste areas, and limits of disturbance will require approval by FAA Environmental.
- C. [Woolpert will submit the Stormwater Pollution Prevention Plan and Land Disturbance Permit.](#)

13. LIQUIDATED DAMAGES

- A. Refer to Part 5: Special Provisions, Local Provisions.
- B. As compensation for non-use, the Contractor shall be assessed a liquidated damage of \$750/Calendar Days for each day that the work remains uncompleted beyond the contract period.
- C. As compensation for expenses incurred for unscheduled employment of the Engineer, up to \$750/ Calendar Days for the construction manager plus up to \$750/ Calendar Days for each additional resident engineer plus any incurred expenses (per diem, lodging, etc.) will be charged to the

Contractor for that time which exceeds the number of Calendar Days allowed in this paragraph. Further, each phase of work under the project has additional liquidated damage clauses, as outlined in Section 80-08 FAILURE TO COMPLETE ON TIME.

14. MATERIALS/SPECIFICATIONS

- A. Contractor to be familiar with FAA Specifications
- B. Local or state specifications will not be allowed.
- C. Contractor to comply with P-156 specification.
- D. Contractors should check the gradations for P-208. Make sure quarries can meet the gradations and all other specification requirements.
- E. P-610 concrete will require ASR testing on the mix design.
- F. P-401 Asphalt Mix – Contractor to ensure that the asphalt plant will be able to produce a mix design that meets the specification.

15. MISCELLANEOUS

- A. Airport Security – Badging is not required for this project.
- B. Airport Driving – The airport will not require driver safety training class for supervising personnel working in the aircraft movement area.
- C. Review Construction Safety Overall Phasing Plan drawing G050 – note the Staging Area & Airport Access gate.
- D. Review Construction Safety Individual Phasing Plan sheets G052-G058 – the contractor shall install flasher barricades around the perimeter of the construction site bordering the airfield to isolate the contractor from aircraft.
- E. Contractor to provide runway and taxiway closure markers.
- F. Contractor shall stay within project boundaries.
- G. Radios for the project to be provided by Contractor. See Part 7: Safety Documents, Construction Safety and Phasing Plan (CSPP).
- H. State Sales & Use Taxes – See Part 1: General, Instruction to Bidders Paragraph 27 and Part 5: Special Provisions, Sales and Use Taxes for clarification – will be provided after award.
- I. Quality Assurance testing will be completed by TSi Geotechnical, Inc. under the direction of the Engineer. The Quality Control testing and Quality Assurance testing shall be completed by separate firms.
- J. Quality Control by Contractor – Part 4: FAA AC 150/5370-10H, Section 60 - Control of Materials.
- K. Contractor to submit a Contractor Quality Control Program (CQCP). See Section C-100.
- L. Survey requirements. Refer to Section 50-07.
- M. Water availability – [The Contractor is to provide connections to water. Airport will provide access to water.](#)
- N. Questions will only be taken via written format to the Project Manager until September 5, 2025, 4:00 P.M. (CST).

16. ADDENDUM UPDATE

- A. Addendum No. 1 to be issued: TBD

17. QUESTIONS / ANSWERS

Q. Is the CTS mixed with Asphalt Millings?

A. No, the Cement Treated Subgrade is to be mixed with in-situ subgrade material.

Q. Can the Contractor use a MoDOT specification for concrete instead of P-610 if it is under a certain quantity?

A. The P-610 specification is required for quantities over 20 CY. It is anticipated that this project will use more than 20 CY of material. Therefore, it is required that the P-610 specification is used for this project.

Q. Can the edge lights be trenched?

A. The Contractor may use whatever means and methods that they determine will meet the specification requirements.

Q. Where is RPR's testing facility?

A. The QA's AASHTO certified lab is located in Springfield, MO.

Q. Will topsoil be hauled in from offsite?

A. Topsoil will be stripped 4 inches and placed back at approximately 3 inches. Contractor to ensure grass growth after placement per specification.

Q. Will the Contractor be required to take spot elevation on each lift of material?

A. Yes, the contractor is required to meet grade at the top of each material per associated specification.

Q. Will the excess material be hauled onsite or offsite?

A. All excess material will be hauled offsite.

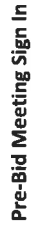
Q. Can the start date be pushed to July?

A. The Contractor may start construction that best fits their schedule. It is recommended that they have all of the materials approved prior to construction to prevent any delays during construction. Please note that there is funding that might potentially expire by the end of July, and a portion of the project will need to be completed prior to that time.

Q. Will there be a cure time if the CTS is placed into two different lifts?

A. The Contractor may use whatever means and methods that they determine will meet the specification requirements.

18. PROJECT SITE TOUR



Rolla National Airport (VIH)
Taxiway A Reconstruction and Taxiway
Connector Rehabilitation
Schedule I: Taxiway A Reconstruction
Schedule II: Taxiway B Rehabilitation
Schedule III: Taxiway A Underdrains

MoDOT Project #: 23-056A-1

[illegible]

VIH Rolla National Airport Taxiway A Reconstruction and Taxiway Connector Rehabilitation

Quest eBidDoc™ Number: 9801915

Closing Date: Wed, 09/17/2025 11:00 AM CDT Posting Type: Construction Project Owner Name: City of Rolla Solicitor Name: Woolpert Contact:

Laura Koonce Phone: 573.658.9477 Email: Laura.Koonce@woolpert.com

Company Name & Address	Contact Name/Email Address	Phone/Fax	Bus. Cert	Bus. Desig	Entry Date	Doc Type	Comments
ConstructConnect 3825 Edwards Rd, Cincinnati, OH-45209	Eric Mills content@constructconnect.com	800-364-2059 866-570-8187		Plan Room	08/07/2025	eBidDoc	
ePlan 1400 Forum Blvd Site 7B, Columbia, MO-65203	Eplan Reporter eplan@eplanbidding.com	5734477130 573-355-5404		Plan Room	08/07/2025	eBidDoc	
Capital Paving 1369 Business Park Rd, Linn Creek, MO-65052	Jeffrey Creamer jcreamer@capitalpavingmo.com	573-317-3700		Prime Bidder	08/11/2025	eBidDoc	