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|--|-----------|--|-----|---|----------------|--|------|-----------|----------|-----|---------------------|----------------|--------|
|  | | Missouri Department of Transportation | | | | March 13, 2025 | | | | | | | |
| | | State Bridge Inspection Report | | | | 4:45:52PM | | | | | | | |
| COUNTY: MISSISSIPPI | | DISTRICT: SE | | CLASS: STATBR | | FED-ID: 4509 | | | | | | | |
| | | | | BRIDGE: F0788 | | | | | | | | | |
| ***GENERAL STRUCTURE INFORMATION*** | | | | | | ***BRIDGE INSPECTION INFORMATION*** | | | | | | | |
| ROUTE: MO80E FEATURE: DRAIN DTCH #29 STATUS: A-OPEN LOG MILE: 20.784 DETOUR: 12.00 MILES NHS: NO BUILT: 1922 REHAB: 1954 LOCATION: S 28 T 25 R 17 E LATITUDE: 36 46 35.54 (DMS) LONGITUDE: 89 13 13.85 (DMS) | | # SPANS: 1 LANES ON: 2 LANES UNDER: 0 COMPASS DIRECTION: WEST to EAST DIRECTION OF TRAFFIC: 2-WAY TRAF FUNCTIONAL CLASS: RL-MAJOR COLLECTOR NBI OWNER: MODOT NBI MAINTAINED: MODOT MAINTENANCE DISTRICT: SE MAINTENANCE COUNTY: MISSISSIPPI SUB AREA: 7H09 | | PLACE CODE: 48926 MISSISSIPPI LENGTH: 66 FT 0 IN MAXIMUM SPAN: 60 FT 0 IN APPROACH ROADWAY: 22 FT 0 IN CURB TO CURB: 20 FT 0 IN OUT TO OUT: 21 FT 0 IN AADT: 406 AADT YEAR: 2024 AADT TRUCK: 2.5% FUTURE AADT: 568 FUTURE AADT YEAR: 2044 | | DATE: 11/29/2023 RESPONSIBILITY: BRIDGEDIV | | | | | | | |
| | | | | | | FREQUENCY: 24 CALCULATED INTERVAL**: 23 | | | | | | | |
| | | | | | | TEAM LEADER: CURT RICKERSON ELEMENT: NO | | | | | | | |
| | | | | INSPECTOR 2: RANDY WEAVER INSPECTOR 4: | | | | | | | | | |
| | | | | INSPECTOR 3: CHRISTOPHER BYRD (NTL) | | | | | | | | | |
| | | | | ** When calculated interval exceeds the frequency, a justification comment per BIRM is required. | | | | | | | | | |
| ***FRACTURE CRITICAL INSPECTION INFORMATION*** | | | | | | ***INDEPTH INSPECTION INFORMATION*** | | | | | | | |
| DATE: 11/29/2023 RESPONSIBILITY: BRIDGEDIV CATEGORY: PONY TRUSS | | | | DATE: RESPONSIBILITY: CATEGORY: | | | | | | | | | |
| FREQUENCY: 24 CALCULATED INTERVAL**: 23 NBI: YES | | | | FREQUENCY: CALCULATED INTERVAL**: NBI: | | | | | | | | | |
| TEAM LEADER: CURT RICKERSON INSPECTOR 3: CHRISTOPHER BYRD (NTL) METHOD: CLIMBER | | | | TEAM LEADER: INSPECTOR 3: METHOD: | | | | | | | | | |
| INSPECTOR 2: RANDY WEAVER INSPECTOR 4: | | | | INSPECTOR 2: INSPECTOR 4: | | | | | | | | | |
| ** When calculated interval exceeds the frequency, a justification comment per BIRM is required. | | | | ** When calculated interval exceeds the frequency, a justification comment per BIRM is required. | | | | | | | | | |
| FRACTURE CRITICAL INSPECTION COMMENTS | | | | INDEPTH INSPECTION COMMENTS | | | | | | | | | |
| | | | | | | | | | | | | | |
| ***SPECIAL INSPECTION INFORMATION*** | | | | | | ***UNDERWATER INSPECTION INFORMATION*** | | | | | | | |
| DATE: 12/13/2021 RESPONSIBILITY: BRIDGEDIV CATEGORY: CHANNEL CROSS SEC | | | | DATE: 12/14/2021 RESPONSIBILITY: BRIDGEDIV CATEGORY: DRY | | | | | | | | | |
| FREQUENCY: 120 CALCULATED INTERVAL**: 98 NBI: NO | | | | FREQUENCY: 60 CALCULATED INTERVAL**: 47 NBI: NO | | | | | | | | | |
| TEAM LEADER: RANDY WEAVER INSPECTOR 3: JASE SHELTON (NTLQ) METHOD: WT TAPE | | | | TEAM LEADER: RANDY WEAVER INSPECTOR 3: KEVIN WEGENER METHOD: VISUAL | | | | | | | | | |
| INSPECTOR 2: KEVIN WEGENER INSPECTOR 4: | | | | INSPECTOR 2: JASE SHELTON (NTLQ) INSPECTOR 4: | | | | | | | | | |
| ** When calculated interval exceeds the frequency, a justification comment per BIRM is required. | | | | ** When calculated interval exceeds the frequency, a justification comment per BIRM is required. | | | | | | | | | |
| SPECIAL INSPECTION COMMENTS | | | | UNDERWATER INSPECTION COMMENTS | | | | | | | | | |
| | | | | | | | | | | | | | |
| OTHER SPECIAL INSPECTIONS | | | | OTHER UNDERWATER INSPECTIONS | | | | | | | | | |
| DATE | FREQUENCY | CATEGORY | NBI | CALCULATED INTERVAL | RESPONSIBILITY | METHOD | DATE | FREQUENCY | CATEGORY | NBI | CALCULATED INTERVAL | RESPONSIBILITY | METHOD |
| 08/25/2009 | 999 | GUSSET PLATES | NO | | BRIDGEDIV | CLIMBER | | | | | | | |

Design_No = F0788

Page 1

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|---|---------------------|---|---------------------|--|----------------|--------------------|
|  | | Missouri Department of Transportation | | | March 13, 2025 | |
| | | State Bridge Inspection Report | | | 4:45:52PM | |
| COUNTY: MISSISSIPPI | | DISTRICT: SE | CLASS: STATBR | FED-ID: 4509 | BRIDGE: F0788 | |
| ***STRUCTURE POSTING*** | | | | | | |
| APPROVED CATEGORY: S-1 | | NO POSTING REQUIRED | | | | |
| Ton 1: | | Ton 2: | | Ton 3: | | |
| COMMENTS: | | | | | | |
| FIELD CATEGORY: S-1 | | NO POSTING REQUIRED | | | | |
| Ton 1: | | Ton 2: | | Ton 3: | PROBLEM: | PROBLEM DIRECTION: |
| COMMENTS: | | | | | | |
| ***GENERAL COMMENTS/MAJOR RATED ITEMS*** | | | | | | |
| GENERAL COMMENTS: (MADSEJ, 01/10/2020)--(60') PONY TRUSS SPAN | | | | | | |
| [ITEM 58] DECK: | | COMMENTS: (MADSEJ, 01/22/2018)--EXCESSIVE CRACKING WITH MEDIUM TO HEAVY EFFLORESCENCE THROUGHOUT THE DECK | | | | |
| 6-SATISFACTORY CONDITION | | | | | | |
| RATING : 01/18/2018 | | | | | | |
| [ITEM 59] SUPER: | | COMMENTS: (MADSEJ, 01/10/2022)--MEDIUM TO HEAVY PACKRUST THOUGHOUT THE FLOORBEAM AND EXTERIOR STRINGER TOP FLANGES CAUSING THE DECK | | | | |
| 5-FAIR CONDITION | | TO LIFT UP TO 3/4" | | | | |
| RATING : 05/18/2001 | | | | | | |
| [ITEM 60] SUB: | | COMMENTS: (MADSEJ, 01/10/2022)--MINOR CONCRETE PILE EXPOSURE AT THE EAST ABUTMENT | | | | |
| 6-SATISFACTORY CONDITION | | | | | | |
| RATING : 05/18/2001 | | | | | | |
| [ITEM 61] BANK/CHANNEL: | | COMMENTS: (MADSEJ, 01/22/2018)--STEEP ERODING AND SLOUGHING BANKS THROUGHOUT THE CHANNEL. | | | | |
| 6-WIDESPREAD MINOR DAMAGE | | | | | | |
| RATING : 05/18/2001 | | | | | | |
| [ITEM 113] SCOUR: | | COMMENTS: | | | | |
| 8-STABLE FOR CALCULATED | | | | | | |
| RATING : 05/18/2001 | | | | | | |
| EVALUATION TYPE : | | | | | | |
| [ITEM 71] WATERWAY ADEQUACY: | | COMMENTS: | | | | |
| DECK/APPRCH OVERTOP SLIGT | | | | | | |
| RATING : 05/18/2001 | | | | | | |
| [ITEM 72] APPRRDWY ALIGNMENT: | | COMMENTS: | | | | |
| 8-VERYGOOD | | | | | | |
| RATING : 05/18/2001 | | | | | | |
| ***RAILING AND APPROACH PAVEMENT COMPONENTS AND RATINGS*** | | | | | | |
| [ITEM 36A] BRIDGE RAILING RATING: | | DOESNT MEET CURRNT STND-0 | RATING : 02/24/2004 | COMMENTS: | | |
| <u>MATERIAL</u> | <u>CONSTRUCTION</u> | <u>DIRECTION</u> | <u>COMMENTS</u> | | | |
| REINFORCED CONCRETE | CURB | BOTH | | | | |
| <u>CONDITION</u> | <u>LOCATION 1</u> | <u>LOCATION 2</u> | <u>SEVERITY</u> | <u>COMMENT</u> | | |
| SCALING | THROUGHOUT | | MODERATE | | | |
| STEEL | CHANNEL-DOUBLE | BOTH | | | | |
| <u>CONDITION</u> | <u>LOCATION 1</u> | <u>LOCATION 2</u> | <u>SEVERITY</u> | <u>COMMENT</u> | | |
| COLLISION DAMAGE | RANDOM | | MINOR | (BYRDC2, 11/29/2023)--SW CORNER & N.E CORNER | | |
| [ITEM 36B] TRANSITION RAILING RATING: | | NOT PROVIDED-0 | RATING : 05/18/2001 | COMMENTS: | | |
| [ITEM 36C] APPROACH RAILING RATING: | | NOT PROVIDED-0 | RATING : 05/18/2001 | COMMENTS: | | |
| Design_No = F0788 | | | | | | |
| Page 2 | | | | | | |
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|---|--|---------------------------------------|--|---|--|----------------------------|--|
|  | | Missouri Department of Transportation | | | | March 13, 2025 | |
| | | State Bridge Inspection Report | | | | 4:45:52PM | |
| COUNTY: MISSISSIPPI | | DISTRICT: SE | | CLASS: STATBR | | FED-ID: 4509 | |
| | | | | BRIDGE: F0788 | | | |
| [ITEM 36D] RAIL END TREATMENT RATING: NOT PROVIDED-0 | | | | RATING : 05/18/2001 | | COMMENTS: | |
| APPROACH PAVEMENT: *Overall condition assigned for each approach pavemenet component is shown below. | | | | | | | |
| <u>MATERIAL</u> | | <u>CONSTRUCTION</u> | | <u>DIRECTION</u> | | <u>CONDITION*</u> | |
| ASPHALT | | BITUMINOUS MAT | | BOTH | | GOOD | |
| ***DRAINAGE, EXPANSION DEVICES, BANK/SLOPE, AND DECK PROTECTIVE COMPONENTS*** | | | | | | | |
| <u>DECK PROTECTIVE COMPONENTS:</u> | | | | | | | |
| <u>SERIES TYPE-#</u> | | <u>COMPONENT</u> | | <u>MATERIAL</u> | | <u>CONSTRUCTION</u> | |
| MAIN SERIES-1 | | WEARING SURFACE | | ASPHALT | | BITUMINOUS SEAL COAT | |
| <u>THICKNESS</u> | | <u>YEAR APPLIED</u> | | <u>MANUFACTURE</u> | | <u>OVERALL CONDITION</u> | |
| .4 IN | | 2003 | | | | GOOD | |
| <u>COMMENT:</u> | | | | | | | |
| | | DECK PROTECTION | | NOTAPPLICABLE | | NONE | |
| <u>COMMENT:</u> | | | | | | | |
| | | MEMBRANE | | NOTAPPLICABLE | | NONE | |
| <u>COMMENT:</u> | | | | | | | |
| <u>DRAINAGE COMPONENTS:</u> | | | | | | | |
| <u>COMPONENT</u> | | <u>MATERIAL</u> | | <u>CONSTRUCTION</u> | | <u>DIRECTION</u> | |
| DRAINAGE | | REINFORCED CONCRETE | | CURB OUTLET | | | |
| <u>EXPANSION DEVICE COMPONENTS:</u> | | | | | | | |
| <u>SUB UNIT-#</u> | | <u>SUB LABEL</u> | | <u>COMPONENT</u> | | <u>MATERIAL</u> | |
| | | | | | | | |
| <u>CONSTRUCTION</u> | | <u>GAP</u> | | <u>YEAR APPLIED</u> | | <u>MANUFACTURE</u> | |
| | | | | | | | |
| <u>OVERALL CONDITION</u> | | | | | | | |
| <u>COMMENT:</u> | | | | | | | |
| <u>BANK/SLOPE PROTECTION COMPONENTS:</u> | | | | | | | |
| <u>COMPONENT</u> | | <u>MATERIAL</u> | | <u>CONSTRUCTION</u> | | <u>DIRECTION</u> | |
| | | | | | | | |
| <u>COMMENTS</u> | | | | | | | |
| | | | | | | | |
| ***DECK COMPONENTS*** | | | | | | | |
| <u>SPAN TYPE-#</u> | | <u>COMPONENT</u> | | <u>MATERIAL</u> | | <u>CONSTRUCTION</u> | |
| MAIN SPANS-1 | | DECK | | REINFORCED CONCRETE | | CAST-IN-PLACE | |
| <u>COMMENTS</u> | | <u>LOCATION 1</u> | | <u>LOCATION 2</u> | | <u>SEVERITY</u> | |
| (PARKEL, 06/30/2004)--SEALED W/BACKER ROD & HOT POUR | | THROUGHOUT | | THROUGHOUT | | FEW | |
| 4-1-2004 | | THROUGHOUT | | THROUGHOUT | | MEDIUM | |
| | | THROUGHOUT | | THROUGHOUT | | MANY | |
| | | THROUGHOUT | | THROUGHOUT | | MANY | |
| ***SUPERSTRUCTURE COMPONENTS*** | | | | | | | |
| <u>SERIES TYPE-#</u> | | <u>SPAN TYPE</u> | | <u>MATERIAL</u> | | <u>CONSTRUCTION</u> | |
| MAIN SERIES-1 | | SIMPLE SPAN | | STEEL | | PONY TRUSS | |
| <u>LABEL</u> | | <u>COMMENTS</u> | | <u>SPAN</u> | | <u>COMPOSITE INDICATOR</u> | |
| | | | | MAIN SPANS-1 | | NON-COMPOSITE | |
| <u>LENGTH</u> | | <u>WEATHERING STEEL</u> | | <u>COMMENTS</u> | | <u>LOCATION 1</u> | |
| 60 FT 0 IN | | NO | | (MADSEJ, 01/10/2020)--SEE THE FRACTURE CRITICAL ELEMENT TABLE FOR COMPONENT CONDITIONS. | | LOCATION 2 | |
| <u>SEVERITY</u> | | <u>MEASUREMENT</u> | | <u>COMMENT</u> | | | |
| | | | | | | | |
| Design_No = F0788 | | | | | | | |
| Page 3 | | | | | | | |
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Missouri Department of Transportation State Bridge Inspection Report

March 13, 2025
4:45:52PM

COUNTY: MISSISSIPPI

DISTRICT: SE

CLASS: STATBR

FED-ID: 4509

BRIDGE: F0788

SUBSTRUCTURE COMPONENTS

| <u>SUBSTRUCTURE</u> | <u>SKEW</u> | <u>LENGTH</u> | <u>MATERIAL</u> | <u>CONSTRUCTION</u> | <u>LABEL</u> | <u>COMMENTS</u> | |
|-----------------------------|------------------|---------------|---------------------|---------------------|--------------|-----------------|-----------------------------------|
| ABUTMENT-1 | RA-42 DEGREES | 43 FT 10 IN | REINFORCED CONCRETE | NON-INTEGRAL | | | |
| | <u>CONDITION</u> | | <u>LOCATION 1</u> | <u>LOCATION 2</u> | | <u>SEVERITY</u> | <u>MEASUREMENT</u> <u>COMMENT</u> |
| <u>ASSOCIATED COMPONENT</u> | | | <u>MATERIAL</u> | <u>CONSTRUCTION</u> | | | |
| BEAM CAP | | | REINFORCED CONCRETE | CAST-IN-PLACE | | | |
| | <u>CONDITION</u> | | <u>LOCATION 1</u> | <u>LOCATION 2</u> | | <u>SEVERITY</u> | <u>MEASUREMENT</u> <u>COMMENT</u> |
| | VERTICAL CRACKS | | THROUGHOUT | | | FEW | |
| PILING | | | REINFORCED CONCRETE | PRE-CAST | | | |
| | <u>CONDITION</u> | | <u>LOCATION 1</u> | <u>LOCATION 2</u> | | <u>SEVERITY</u> | <u>MEASUREMENT</u> <u>COMMENT</u> |
| STRAIGHT WINGS | | | REINFORCED CONCRETE | CAST-IN-PLACE | | | |
| | <u>CONDITION</u> | | <u>LOCATION 1</u> | <u>LOCATION 2</u> | | <u>SEVERITY</u> | <u>MEASUREMENT</u> <u>COMMENT</u> |
| BACKWALL | | | REINFORCED CONCRETE | CAST-IN-PLACE | | | |
| | <u>CONDITION</u> | | <u>LOCATION 1</u> | <u>LOCATION 2</u> | | <u>SEVERITY</u> | <u>MEASUREMENT</u> <u>COMMENT</u> |
| | VERTICAL CRACKS | | THROUGHOUT | | | MINOR | |
| FIXED BEARING | | | STEEL | FLAT PLATE | | | |
| | <u>CONDITION</u> | | <u>LOCATION 1</u> | <u>LOCATION 2</u> | | <u>SEVERITY</u> | <u>MEASUREMENT</u> <u>COMMENT</u> |
| | PACK RUST | | THROUGHOUT | | | HEAVY | |
| ABUTMENT-2 | RA-42 DEGREES | 43 FT 10 IN | REINFORCED CONCRETE | NON-INTEGRAL | | | |
| | <u>CONDITION</u> | | <u>LOCATION 1</u> | <u>LOCATION 2</u> | | <u>SEVERITY</u> | <u>MEASUREMENT</u> <u>COMMENT</u> |
| <u>ASSOCIATED COMPONENT</u> | | | <u>MATERIAL</u> | <u>CONSTRUCTION</u> | | | |
| BEAM CAP | | | REINFORCED CONCRETE | CAST-IN-PLACE | | | |
| | <u>CONDITION</u> | | <u>LOCATION 1</u> | <u>LOCATION 2</u> | | <u>SEVERITY</u> | <u>MEASUREMENT</u> <u>COMMENT</u> |
| | EROSION | | GROUND LINE | | | PILE EXPOSED | |
| | VERTICAL CRACKS | | THROUGHOUT | | | FEW | |
| PILING | | | REINFORCED CONCRETE | PRE-CAST | | | |
| | <u>CONDITION</u> | | <u>LOCATION 1</u> | <u>LOCATION 2</u> | | <u>SEVERITY</u> | <u>MEASUREMENT</u> <u>COMMENT</u> |
| | EXPOSED | | AT BEAM CAP | | | MINOR | |
| STRAIGHT WINGS | | | REINFORCED CONCRETE | CAST-IN-PLACE | | | |
| | <u>CONDITION</u> | | <u>LOCATION 1</u> | <u>LOCATION 2</u> | | <u>SEVERITY</u> | <u>MEASUREMENT</u> <u>COMMENT</u> |
| BACKWALL | | | REINFORCED CONCRETE | CAST-IN-PLACE | | | |
| | <u>CONDITION</u> | | <u>LOCATION 1</u> | <u>LOCATION 2</u> | | <u>SEVERITY</u> | <u>MEASUREMENT</u> <u>COMMENT</u> |
| | VERTICAL CRACKS | | THROUGHOUT | | | MINOR | |
| FIXED BEARING | | | STEEL | FLAT PLATE | | | |
| | <u>CONDITION</u> | | <u>LOCATION 1</u> | <u>LOCATION 2</u> | | <u>SEVERITY</u> | <u>MEASUREMENT</u> <u>COMMENT</u> |
| | PACK RUST | | THROUGHOUT | | | HEAVY | |

OVER/UNDER ROUTES CLEARANCE INFORMATION

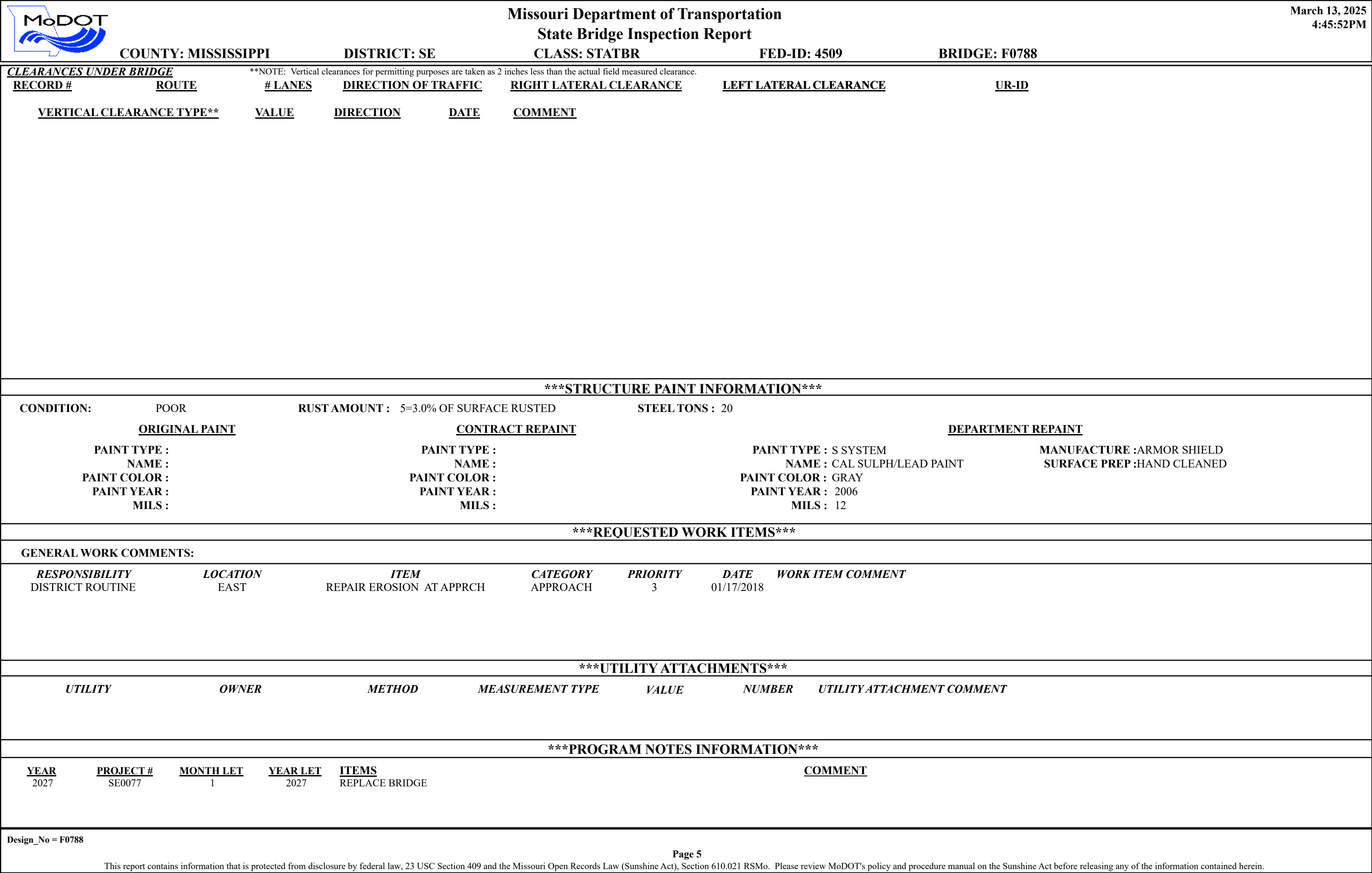
CLEARANCES OVER DECK


****NOTE:** Vertical clearances for permitting purposes are taken as 2 inches less than the actual field measured clearance.

| <u>VERTICAL CLEARANCE TYPE**</u> | <u>VALUE</u> | <u>DIRECTION</u> | <u>DATE</u> | <u>COMMENT</u> |
|----------------------------------|--------------|------------------|-------------|----------------|
|----------------------------------|--------------|------------------|-------------|----------------|

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|  | | | Missouri Department of Transportation | | | March 13, 2025 | | | | | |
| | | | State Bridge Inspection Report | | | 4:45:52PM | | | | | |
| COUNTY: MISSISSIPPI | | | DISTRICT: SE | | CLASS: STATBR | | FED-ID: 4509 | | | | |
| | | | | | BRIDGE: F0788 | | | | | | |
| ***COMPUTER GENERATED RATINGS AND DEFICIENCY ITEMS*** | | | | | | ***ADVANCED SIGN INFORMATION*** | | | | | |
| NOTE: The items listed in this section are updated whenever computer edits are ran on a structure after the inspection updates have been entered in to TMS. | | | | | | SIGN # | | | | | |
| | | | | | | SIGN TYPE | | | | | |
| | | | | | | PROBLEM | | | | | |
| | | | | | | PROBLEM DIRECTION | | | | | |
| <u>Rated Item</u> | | | | | | | | | | | |
| <u>Rating</u> | | | | | | | | | | | |
| <u>Rating Date</u> | | | | | | | | | | | |
| [Item 67] Structure Evaluation Rating: 4-MEETS MINIMUM TOLERABLE 5/18/2001 | | | | | | | | | | | |
| [Item 68] Deck Geometry Rating: 3-BASICALLY INTOL CORRECT 3/6/2024 | | | | | | | | | | | |
| [Item 69] Underclearance: N-NOT APPLICABLE 5/18/2001 | | | | | | | | | | | |
| Sufficiency Rating: 43.1% 3/6/2024 | | | | | | | | | | | |
| Deficiency: FUNCTIONAL 3/6/2024 | | | | | | | | | | | |
| Funding Eligibility: ---- | | | | | | | | | | | |
| Estimated New Structure Length: ---- | | | | | | | | | | | |
| Estimated Structure Cost: ---- | | | | | | | | | | | |
| Estimated Total Project Cost: ---- | | | | | | | | | | | |
| Year of Cost Estimate: ---- | | | | | | | | | | | |
| NOTE: The above structure length and cost estimates are computer generated using algorithms in the TMS system. These algorithms are generalized to use NBI items to come up with a new structure length and width to calculate a new area which is taken times a representative cost per square foot. The actual structure size and cost may vary significantly from these numbers once site specific engineering is done. | | | | | | | | | | | |
| | | | | | | ***OUTFALL INSPECTION INFORMATION*** | | | | | |
| | | | | | | # OUTFALLS: | | | | | |
| | | | | | | INSPECTOR: | | | | | |
| | | | | | | STATUS: | | | | | |
| | | | | | | DATE: | | | | | |
| | | | | | | NOTES: | | | | | |



Missouri Department of Transportation
Bridge Inventory and Inspection System
Structural Inventory & Appraisal Sheet

March 13, 2025
5:03:40pm

COUNTY : MISSISSIPPI BRIDGE : F0788 R1 REVIEW STATUS : CONVERTED NBI STATUS : T
RECORD TYPE : ROUTE CARRIED 'ON' STRUCT RUN DATE : 3/7/2025 SUBMITTAL YEAR : 2025

| GENERAL STRUCTURE INFORMATION | | | ROUTE DESIGNATION INFORMATION | | |
|--------------------------------|-------------------------|---------------------------|---------------------------------|-----------------------------|---------------------------|
| 1 | State | MISSOURI | 5A | Record Type | ROUTE CARRIED 'ON' STRUCT |
| 2 | District | SE | 5B | Route Signing Prefix | MO |
| 3 | County | MISSISSIPPI | 5C | Designated Level of Service | MAINLINE |
| 8 | Federal ID No. | 4509 | 5D | Route Number | 00080 |
| 27 | Year Built | 1922 | 5E | Directional Suffix | NOT APPLICABLE |
| 106 | Year Reconstructed | 1954 | 7 | Facility Carried | MO 80 E |
| 42A | Type of Service On | HIGHWAY | 12 | Base Hwy. Network | NO |
| 21 | Structure Maintenance | STATE HIGHWAY AGENCY | 13A | LRS Inventory Route No. | |
| 22 | Structure Owner | STATE HIGHWAY AGENCY | 13B | Subroute No. | |
| 33 | Br. Median Code | NO MEDIAN | 20 | Toll Status | ON FREE ROAD |
| 37 | Historical Significance | NOT ELIGIBLE FOR NR OF HP | 26 | Functional Classification | 07-RURAL MAJOR COLLECTOR |
| 101 | Parallel Struc Desg | NONE EXISTS | 28A | Lanes on Structure | 02 |
| 103 | Temporary Structure | NOT TEMPORARY | 100 | STRAHNET Designation | RTE NOT A DEFENSE HWY |
| 112 | NBIS Bridge Length | YES | 104 | National Highway System | NOT ON NHS |
| | | | 105 | Federal Lands Highway | NOT APPLICABLE |
| | | | 110 | Designated Nat. Network | NO |
| STRUCTURE LOCATION INFORMATION | | | STRUCTURE TRAFFIC INFORMATION | | |
| 4 | Place | MISSISSIPPI | 29 | AADT | 406 |
| | Code | 48926 | 30 | AADT Year | 2024 |
| 9 | Location | S 28 T 25 N R 17 E | 102 | Direction of Traffic | 2-WAY TRAFFIC |
| 11 | Milepoint | 20.91 miles | 109 | AADT Truck Percent | 3% |
| 16 | Latitude | 36 D 46 M 36 S | 114 | Future AADT | 568 |
| 17 | Longitude | 89 D 13 M 14 S | 115 | Future AADT Year | 2044 |
| UNDERRECORD INFORMATION | | | STRUCTURE GEOMETRIC INFORMATION | | |
| 6 | Features Intersected | DRAIN DTCH #29 | 10 | Inventory Rte. Vert. Clear | 99 Ft. 99 In. |
| 42B | Type of Service Under | WATERWAY | 19 | By pass Detour Length | 11.88 miles |
| 28B | Lanes Under Structure | 00 | 32 | Approach Roadway Width | 21 Ft. 12 In. |
| 54A | Vert. Clearance Ref. | N/A | 34 | Skew | 42.00 Degrees |
| 54B | Vert. Clearance | 0 Ft. 0 In. | 35 | Struct. Flared | NO |
| 55A | Rt. Lat Clear Ref. | N/A | 47 | Total Horiz. Clear | 20 Ft. 0 In. |
| 55B | Rt. Lat Clearance | 0 Ft. 0 In. | 48 | Maximum Span Length | 60 Ft. 0 In. |
| 56 | Left Lat Clearance | 0 Ft. 0 In. | 49 | Structure Length | 65 Ft. 11 In. |
| 38 | Navigation Control | PERMIT NOT REQ | 50A | Left Curb/Sidewalk Width | 0 Ft. 0 In. |
| 39 | Nav Vertical Clear | 0 Ft. 0 In. | 50B | Right Curb/Sidewalk Width | 0 Ft. 0 In. |
| 40 | Nav Horizontal Clear | 0 Ft. 0 In. | 51 | Curb to Curb Br. Width | 20 Ft. 0 In. |
| 111 | Nav. Pier Protection | | 52 | Deck Width (Out-Out) | 20 Ft. 12 In. |
| 116 | Nav. Cl. Vert. Clear | | 53 | Vert. Clearance Over Deck | 99 Ft. 99 In. |

Design_No = F0788 and Inventory_Appraisal_Submittal_Year = 2025



Missouri Department of Transportation
Bridge Inventory and Inspection System
Structural Inventory & Appraisal Sheet

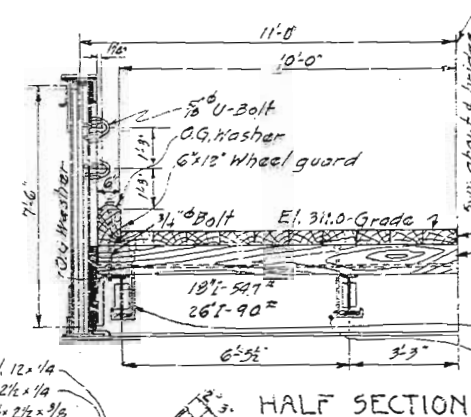
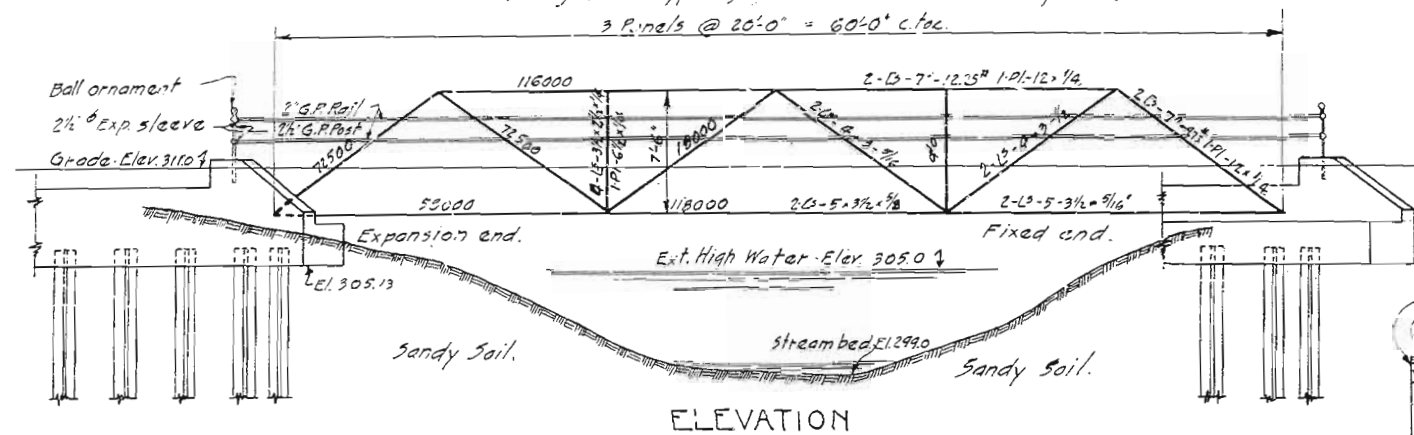
March 13, 2025
5:03:40pm

COUNTY : MISSISSIPPI BRIDGE : F0788 R1 REVIEW STATUS : CONVERTED NBI STATUS : T
RECORD TYPE : ROUTE CARRIED 'ON' STRUCT RUN DATE : 3/7/2025 SUBMITTAL YEAR : 2025

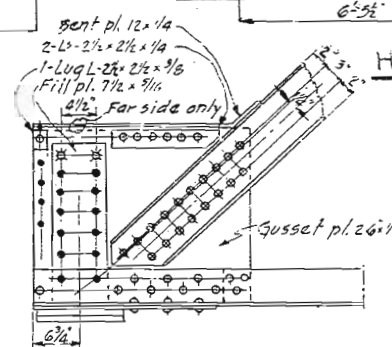
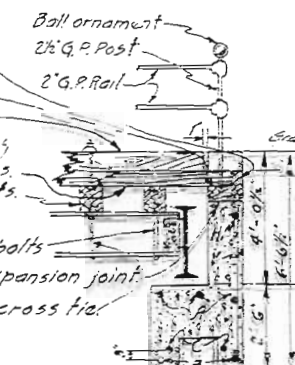
| LOAD RATING AND POSTING INFORMATION | | | MATERIAL/CONSTRUCTION INFORMATION | | |
|-------------------------------------|-----------------------------|---------------------------|-----------------------------------|--|----------------|
| 31 | Design Load | H 15 | 43A | Main Struc. Mat type | STEEL |
| 41 | Structure Status | OPEN NO RESTRICTIONS | 43B | Main struc Constr. Type | TRUSS - THRU |
| 63 | Oper. Rating Meth. | LOAD FACTOR | 45 | # of Main Spans | 1 |
| 64 | Operating Rating | 27 Tons. | 44A | Appr Struc. Mat type | 000 |
| 65 | Inventory Rating Meth | LOAD FACTOR | 44B | Appr Struc. Cnstr. type | 000 |
| 66 | Inventory Rating | 16 Tons. | 46 | # of Approach Span | 0 |
| 70 | Bridge Posting Code | =>LEGAL LOADS | 107 | Deck Mat/Constr. | 1 CONCRETE CIP |
| PROPOSED IMPROVEMENT INFORMATION | | | 108A | Wear Surf Mat/Constr. | 6 BITUMINOUS |
| Sufficiency Rating 43.1 Percent | | | 108B | Membrane Mat/Constr. | 0 NONE |
| Deficiency Rating FUNCTIONAL | | | 108C | Deck Protect Mat/Constr. | 0 NONE |
| Funding Eligibility FULL | | | CONDITION RATING INFORMATION | | |
| 75A | Proposed Work | REPLACEMENT SUBSTND LOAD | 58 | Deck Cond. Rating | 6 |
| 75B | Work Done By | Contract | 59 | Superstructure Cond. Rating | 5 |
| 76 | New Struc Length | 91 Ft. 10 In. | 60 | Substructure Cond. Rating | 6 |
| 94 | Struc Improve Cost | \$ 534,000 | 61 | Channel /Channel Protection Cond. Rating | 6 |
| 95 | Roadway Improve Cost | \$ 53,000 | 62 | Culvert Cond. Rating | N |
| 96 | Total Project Cost | \$ 800,000 | INSPECTION INFORMATION | | |
| 97 | Year of Cost Estimates | 2025 | 90 | Gen. Insp Date | 11 / 23 |
| APPRAISAL RATING INFORMATION | | | 91 | Gen. Insp. Frequency | 24 Months |
| 36A | Br. Rail App. Rating | DOES NOT MEET ACCEPT STND | 92A | Frac. Critical Inspection | Y Months 24 |
| 36B | Transition Rail App. Rating | DOES NOT MEET ACCEPT STND | 93A | Frac. Critical Insp. Date | 11 / 23 |
| 36C | Approach Rail App. Rating | DOES NOT MEET ACCEPT STND | 92B | Underwater Inspection | N Months |
| 36D | Rail End Treat. App. Rating | DOES NOT MEET ACCEPT STND | 93B | Underwater Insp. Date | |
| 67 | Struc Eval App. Rating | 4 | 92C | Special Inspection | N Months |
| 68 | Deck Geometry App. Rating | 3 | 93C | Special Inspection Date | |
| 69 | Underclearance App. Rating | N | BORDER BRIDGE INFORMATION | | |
| 71 | Waterway Adeq. App. Rating | 6 | 98 | Neighboring State Code | |
| 72 | Approach Road App. Rating | 8 | 98B | Neighboring State % Respon | |
| 113 | Scour Assess App. Rating | 8 | 99 | Neighboring State Struc. No. | |
| APPROVED POSTING INFORMATION | | | FIELD POSTING INFORMATION | | |
| Approved Posting Category S-1 | | | Field Posting Category S-1 | | |
| Ton1 Ton2 Ton3 | | | Ton1 Ton2 Ton3 | | |
| Tonnage Values for Posting Sign | | | Tonnage Values for Posting Sign | | |
| General Text for Posting Sign | | | General Text for Posting Sign | | |
| NO POSTING REQUIRED | | | NO POSTING REQUIRED | | |

Design_No = F0788 and Inventory_Appraisal_Submittal_Year = 2025

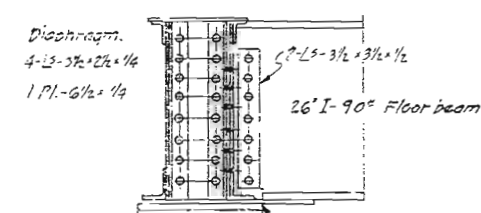
For general type of details see Std. Drawing 5260.
3 Panels @ 20'-0" = 60'-0" c.to.c.



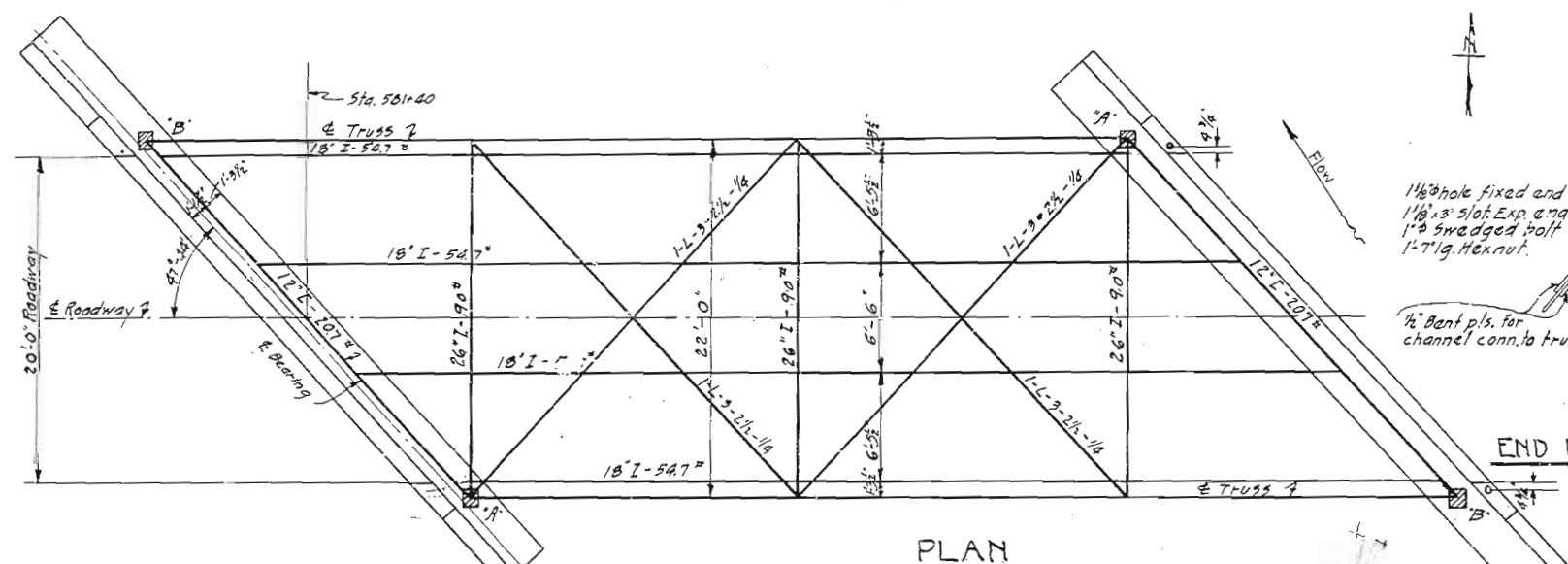
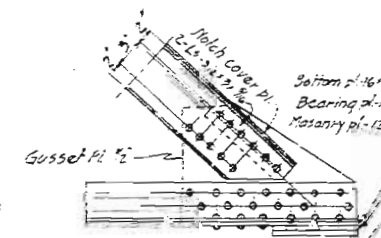
4" x 10" End Plank
6" x 8" sill securely spiked
to floor and end plank
6" x 12" Guard rail
4" x 12" plank spiked to each
cross tie with 2-8" spikes.
6" x 10" Cross ties @ 2'0" cts.
3/4" hook bolts c/w heads thru each
alternate cross tie staggered with bolts
thru guard rail.
3/4" hook bolts thru each alternate cross tie



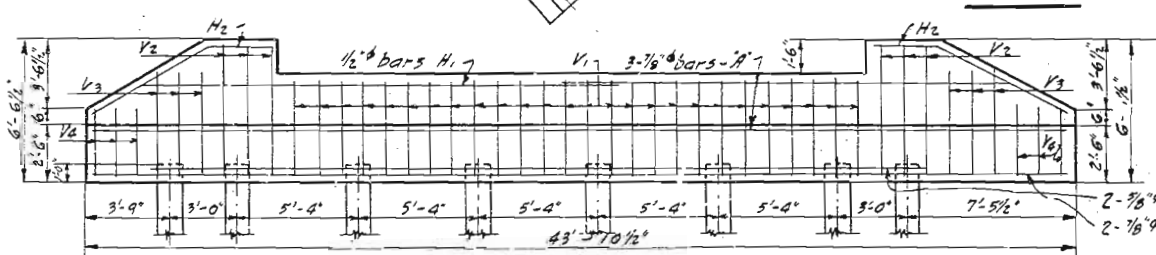
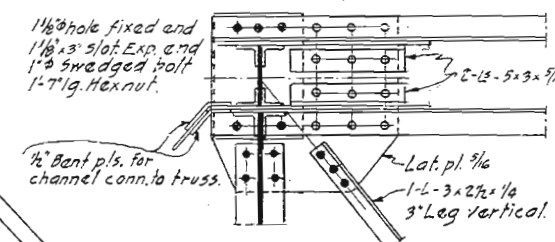
HALF SECTION



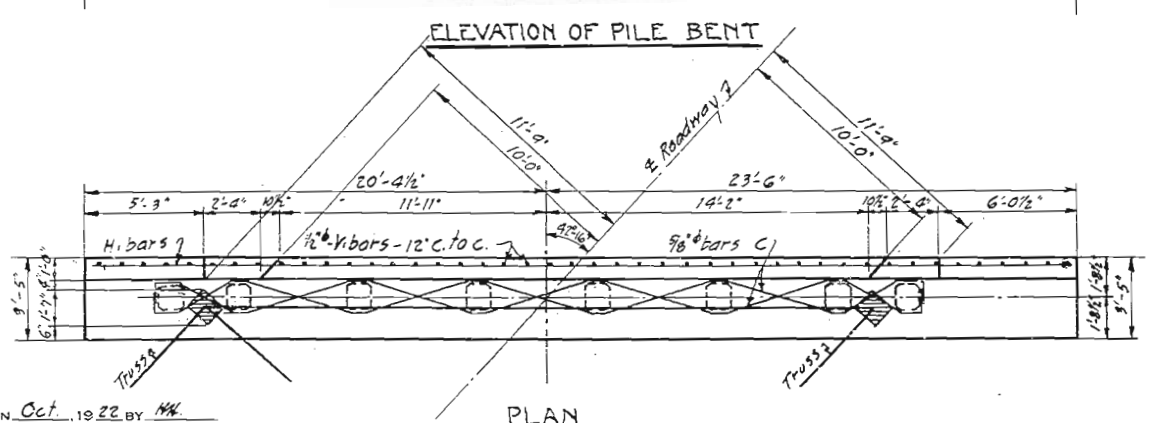
SECTION THRU PILE CAP



END POST CONNECTION AT 'A'

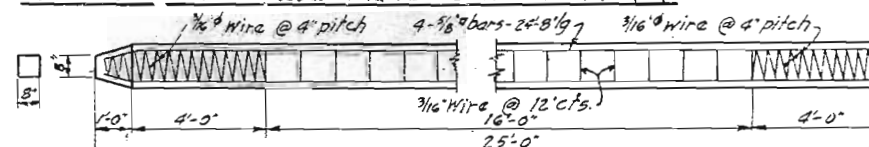


ELEVATION OF PILE BENT

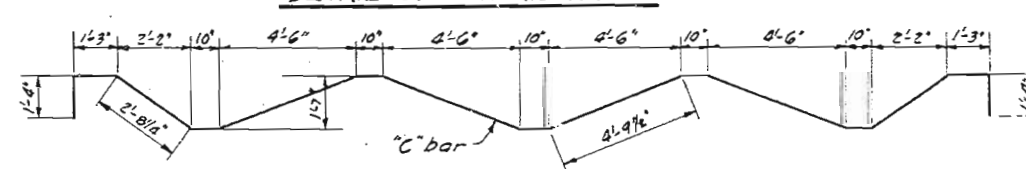


PLAN

CHANNEL CONNECTIONS & BEARING PLATES AT END STRINGERS



DETAIL OF CONCRETE PILE



| BILL OF REINFORCING STEEL | | | | |
|---------------------------|------|----------------|---------|--------------|
| no. | size | Mark | Length | Remark |
| 12 | 7/8" | A | 24'-0" | Straight |
| 8 | 7/8" | B | 24'-0" | . |
| 4 | 3/4" | C | 33'-10" | see sketch |
| 52 | 7/8" | V | 4'-6" | Straight |
| 12 | 7/8" | V ₂ | 6'-0" | . |
| 12 | 7/8" | V ₃ | 4'-8" | . |
| 12 | 7/8" | V ₄ | 3'-3" | . |
| 4 | 7/8" | H ₁ | 21'-0" | . |
| 4 | 7/8" | H ₂ | 9'-0" | See in field |

GENERAL NOTES

GENERAL NOTES
Live Load: as per specifications of 1922.
River: $\frac{3}{4}$ " dia. Open holes $\frac{1}{4}$ " dia.
Detail shop drawings shall be submitted to the State Highway Dept. in quadruplicate and shall be approved before steel work is fabricated.
All metal shall be painted one shop coat of red lead and two field coats of an approved brand as per specifications. Field coats to be of different colors.
All lumber to be dense yellow pine creosoted. Floor planks to be dressed to a uniform width and thickness 5 1/2".
Piles to be driven to sustain a load of 20 tons per pile.
Concrete to be 1:2:4 mix.
Field connections to be turned bolts.

See F-788R1

MISSOURI STATE HIGHWAY DEPARTMENT
BRIDGE OVER SUB-LATERAL OF DRAINAGE DISTRICT NO. 29
STATE ROAD FROM CHARLESTON TO WOLF ISLAND
ABOUT 4 MILES FROM DEVENTER
PROJECT NO. 198 B STA. 581+40

MISSISSIPPI COUNTY

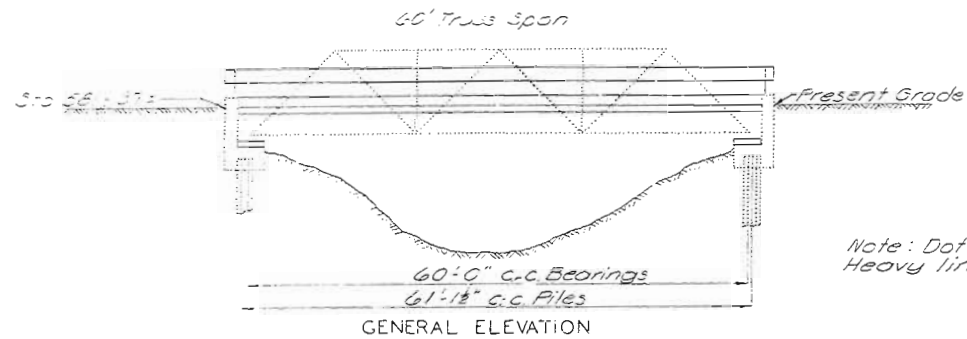
SUBMITTED BY: Cheryl D. Miller
APPROVED BY: [Signature]

DRAWN Oct., 1922 BY H.H.
CH'K'D Oct 1922 BY E.C.

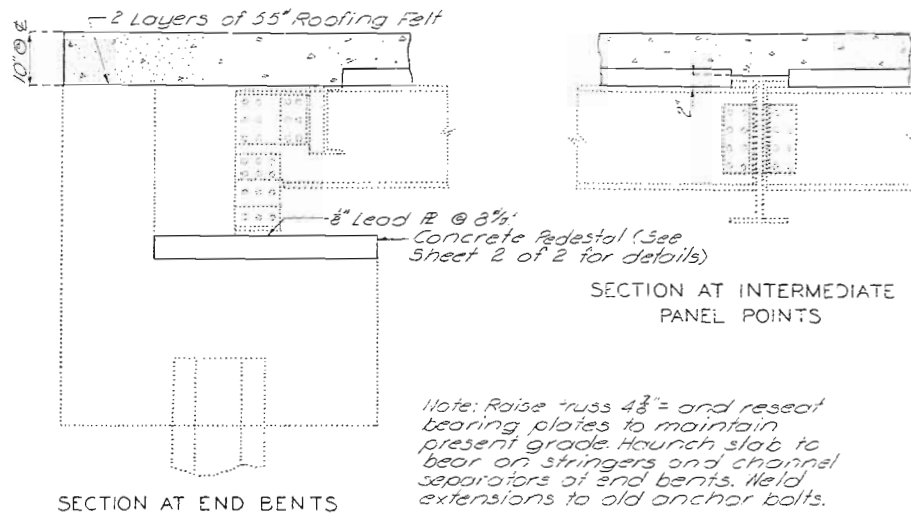
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| 51 |
| 5260 |
| F788R |

MISSOURI STATE HIGHWAY DEPARTMENT

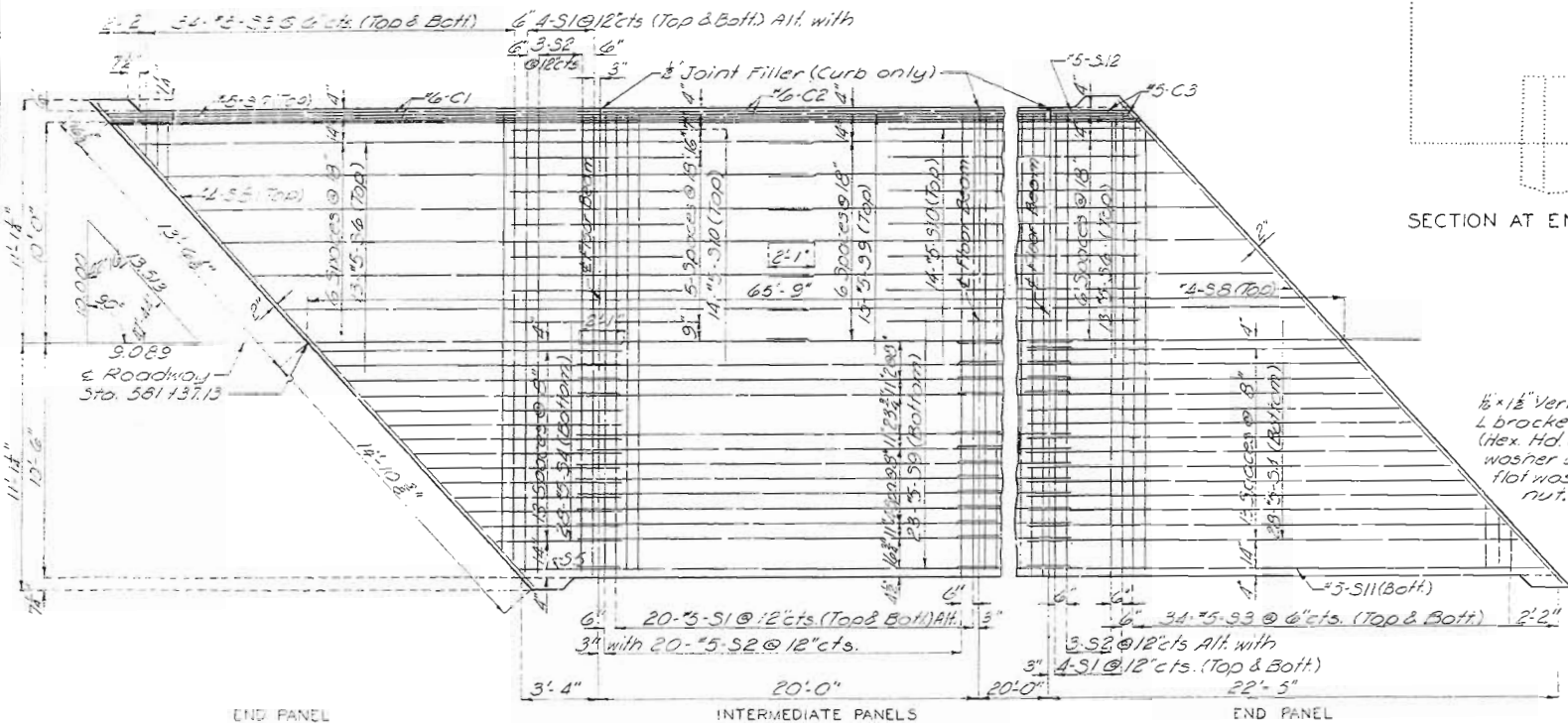
| FED. ROAD DIST. NO. | STATE | FED. AID PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
|---------------------|-------|--------------------|-------------|-----------|--------------|
| 5 | MO. | | 19 | | |



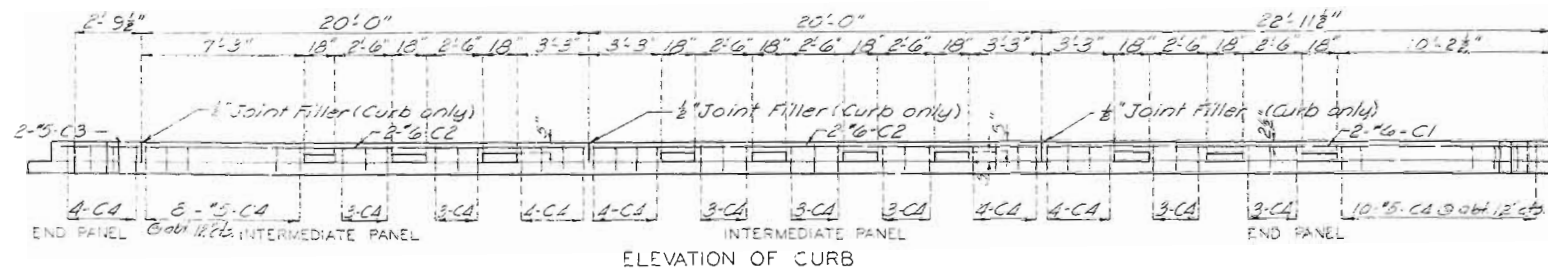
Note: Dotted lines indicate old work. Heavy lines indicate new work.



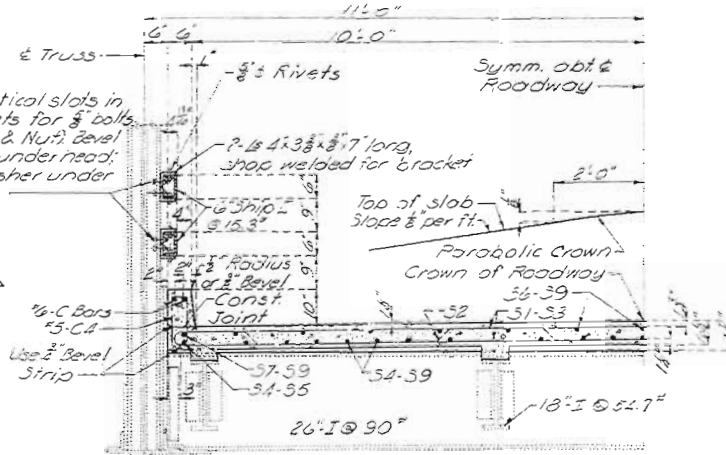
Note: Raise truss 4 3/4" and reset bearing plates to maintain present grade. Haunch slab to bear on stringers and channel separators at end bents. Weld extensions to old anchor bolts.



PLAN OF SLAB SHOWING REINFORCING STEEL



ELEVATION OF CURB



HALF SECTION THRU SLAB

Note: Curbs to be cast independently of slab. Slab to be constructed to a uniform depth of not less than 6 1/2" or if desired the bottom of slab may be built on chords between toes of haunches at stringers.

Channel rail to be adjusted for horizontal alignment by use of full size metal shims placed between rail support and the rail connection angles. Shims of 3/4" and 1/2" thickness to be furnished with structural steel. Cost of shims to be included in price bid for other items.

| ESTIMATED QUANTITIES | | | | |
|----------------------------------|----------|--------|----------|-------|
| Item | Quantity | Unit | Subtotal | Total |
| Class B Concrete | 1.9 | cu yds | 315 | 331 |
| Reinforcing Steel | 220 | Lbs. | 9,080 | 9,300 |
| Prod. Structural Steel | | Lbs. | 4850 | 4850 |
| Class I Excavation For Structure | 10 | cu yds | | 10 |

| COMPLETE BILL OF REINFORCING STEEL | | | |
|------------------------------------|------|--------|---------------|
| No. | Size | Length | Mark Location |
| Superstructure | | | |
| 1 | #6 | 22'-5" | C1 Curb |
| 2 | #5 | 31'-9" | C2 |
| 3 | #5 | 31'-0" | C3 |
| 4 | #5 | 21'-9" | C4 |
| 5 | #5 | 14'-3" | S12 Slab |
| 6 | #5 | 20'-8" | S1 Slab |
| 7 | #5 | 23'-0" | S2 |
| 8 | #5 | 26'-0" | S3 |
| 9 | #5 | 27'-6" | S4 |
| 10 | #5 | 41'-3" | S5 |
| 11 | #5 | 47'-6" | S6 |
| 12 | #5 | 33'-3" | S7 |
| 13 | #5 | 28'-0" | S8 |
| 14 | #5 | 22'-0" | S9 |
| 15 | #5 | 8'-0" | S10 |
| 16 | #5 | 23'-3" | S11 |
| Substructure | | | |
| 17 | #5 | 41'-9" | P1 Pedestal |
| 18 | #5 | 21'-3" | P2 |
| 19 | #5 | 6'-9" | P3 |

| Bending Sketches and Cutting Diagrams | |
|---------------------------------------|--|
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GENERAL NOTES

Design Specifications A.A.S.H.O. 1953
Loading H15-44
Structural Steel Stress: 18,000 psi
Reinforcing Steel Stress: 18,000 psi
Concrete, Class B, Stress: 1,000 psi
All concrete shall be Class B.
Rivets 3/4" holes 3/8"
Point: Shop, none; Field, contact surfaces of bolted rail connections one coat of red lead and surfaces inaccessible after erection three coats of red lead. No other paint to be applied by Contractor. Red lead required shall be furnished by Contractor. Payment for cleaning and painting such surfaces will be included in unit price bid for fabricated structural steel.
Where joint filler is specified on the plans it shall conform with the requirements for Premium Joint Filler as given in Section 38-19.111 of the Standard Specifications.

BRIDGE OVER SUB-LATERAL (D.D. NO. 29)

STATE ROAD FROM RTE. 103 THRU ANNISON TO MO. SLAND
ABOUT 9.5 MILES S.E. OF ANNISON
PROJECT NO. RT. 55-SEC. 7 STA. 38+43.7
MISSISSIPPI COUNTY

DRAWN BY J. A. Williams DATE 1/15/1954
APPROVED BY R. M. Whitton DATE 1/15/1954

FINISHED

5-76881

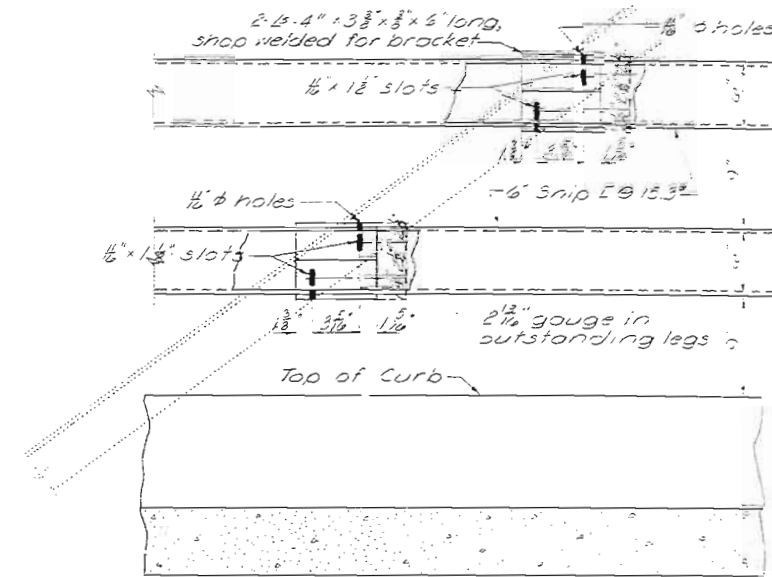
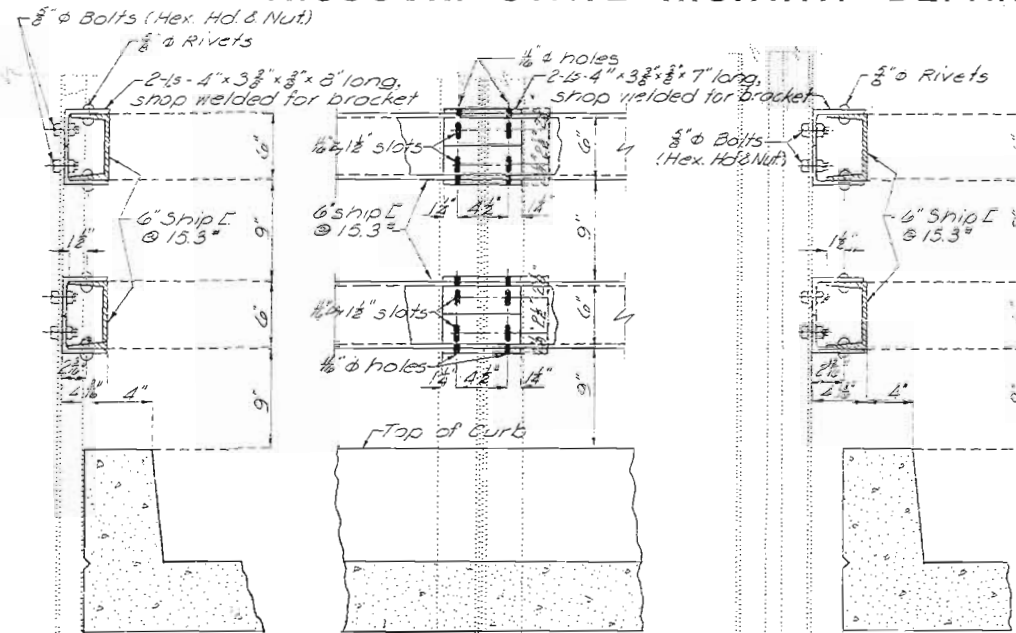
Drawn Dec. 1953 by W.M.C.
Checked Jan. 1954 by J.F.K.

Note: This drawing is not to scale. Follow dimensions.

Sheet No. 1 of 2

SEE FINAL PLANS DRAWN LINES

| FED. ROAD DIST. NO. | STATE | FED. AID PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
|---------------------|-------|--------------------|-------------|-----------|--------------|
| 5 | MO | | 19 | | |



RAIL BRACKETS AT TRUSS DIAGONAL

Required: $\begin{cases} \$ \text{ for Rt.} \\ \$ \text{ for Lt.} \end{cases}$

2-1/2-3/8" x 4" x 5/8" x 7" long, shop welded for bracket

5/8" φ Rivets

5/8" φ Rivets

L-6" x 4" x 5/8" x 21"

2-1/2-3/8" x 4" x 5/8" x 4"

4" H x 13.0"

2-1/2-6" x 4" x 5/8" x 6"

1/2" x 6" long

Top of curb

Lead plates

2-5/8" φ Bolts 10" long; Sq. Hds., Hex. Nuts.

5/8" φ Button Hd Bolts Hex. Nuts. 1/2" φ Holes in rail and bracket

2-1/2"

2-1/2"

Truss Vertical

RAIL AT END BENTS

RAIL SPLICE AT TRUSS VERTICAL

RAIL SPLICE AT TRUSS VERTICAL

PART ELEVATION

SECTION A-A

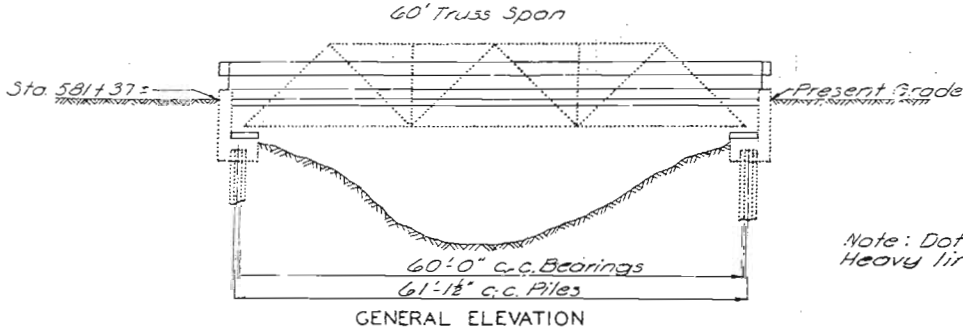
F-738R:

NO CONSTRUCTION CHARGES

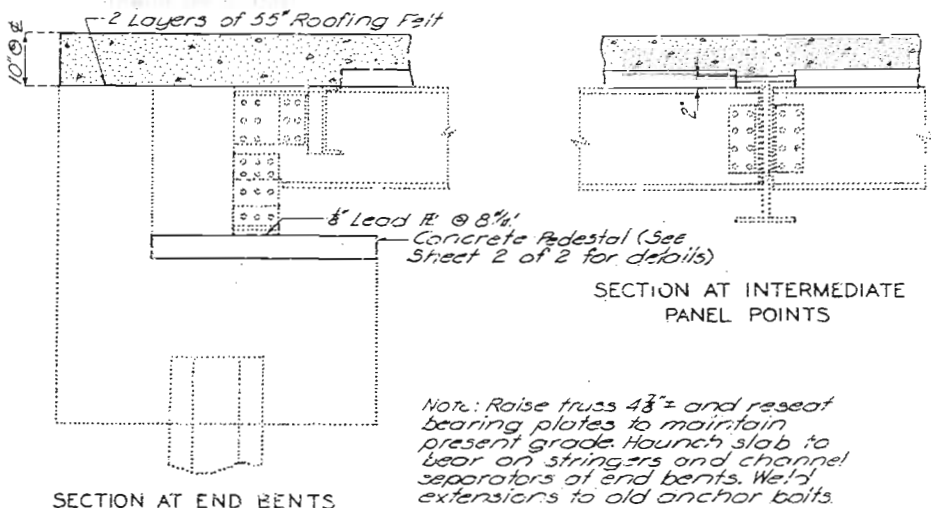
MISSOURI STATE HIGHWAY DEPARTMENT

| FED. ROAD DIST. NO. | STATE | FED. AID PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
|---------------------|-------|--------------------|-------------|-----------|--------------|
| 5 | MO. | 55-7 | 19 | 6 | 22 |

FINAL PLANS



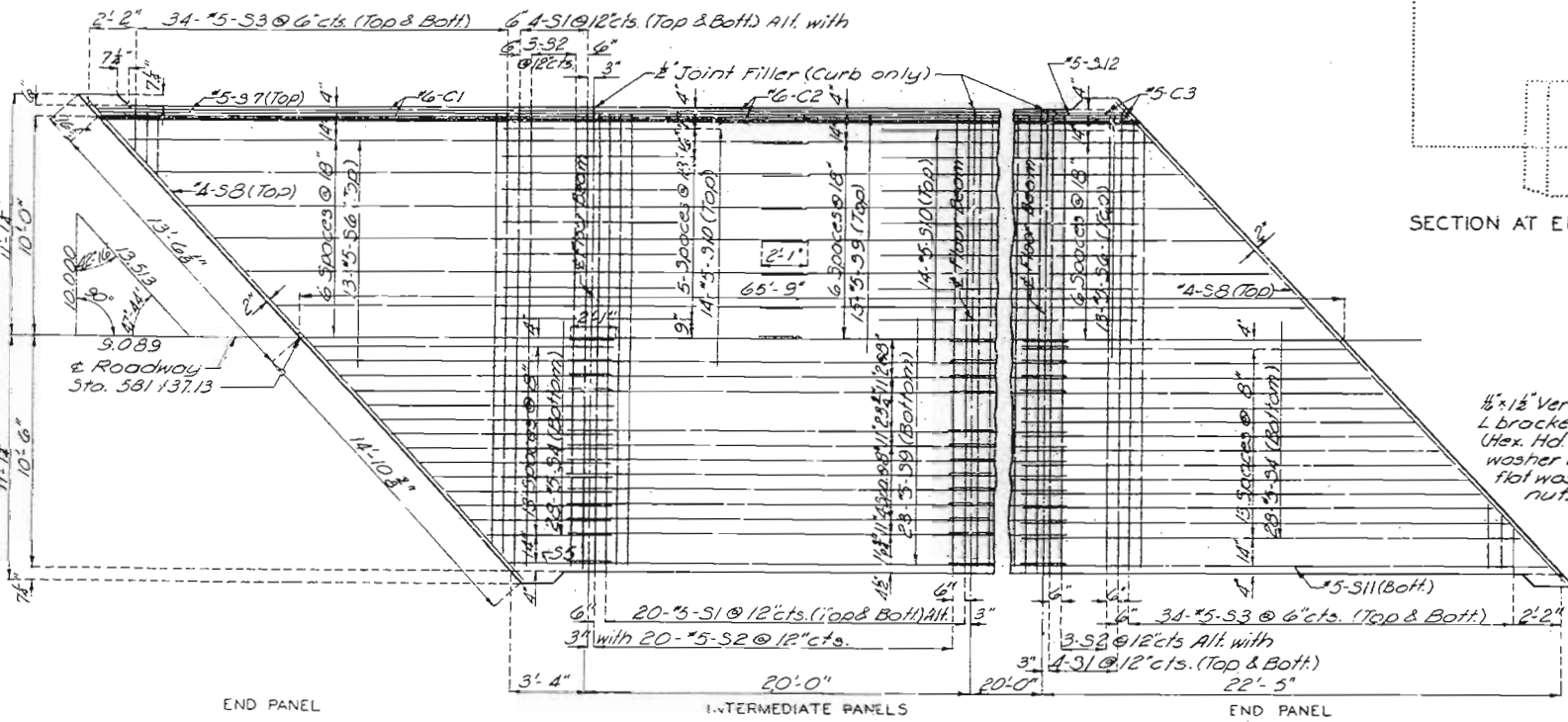
GENERAL ELEVATION



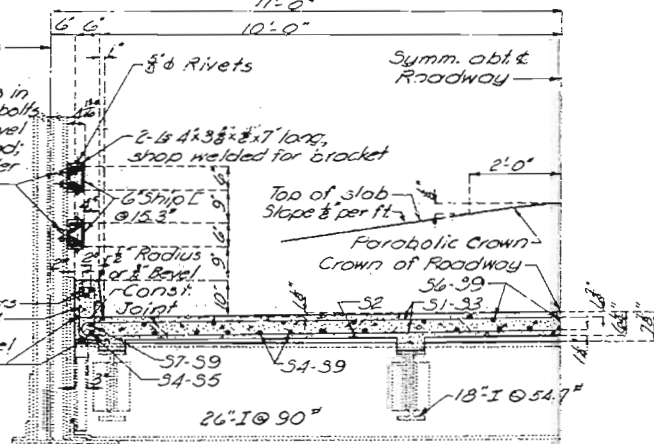
SECTION AT INTERMEDIATE PANEL POINTS

SECTION AT END BENTS

| COMPLETE BILL OF REINFORCING STEEL. | | | | | | |
|-------------------------------------|------|--------|---------------|----------|---------------------------------------|--------------|
| No. | Size | Length | Work Location | | Bending Sketches and Cutting Diagrams | |
| Superstructure | | | | | | |
| 4 | #6 | 22'-0" | C1 | Curb | 15'-6 1/2" | 16'-3" |
| 8 | #6 | 19'-9" | C2 | " | 3'-7 1/2" | 2'-4" |
| 4 | #5 | 3'-0" | C3 | " | | |
| 118 | #3 | 2'-3" | C4 | " | | |
| 2 | #5 | 14'-3" | S12 | Slab | 31'-11 1/2" | 15'-2 1/2" |
| 96 | #3 | 20'-9" | S1 | Slab | 47'-6" | 27'-6" |
| 46 | #3 | 23'-0" | S2 | " | | |
| 68 | #3 | 26'-0" | S3 | " | | |
| 28 | #5 | 27'-6" | S4 | " | 3-SE CUT 13 | 23-54 CUT 28 |
| 2 | #5 | 4'-3" | S5 | " | 4'-0 3/4" | 6'-6" |
| 13 | #5 | 47'-6" | S6 | " | | 5" C4 |
| 2 | #5 | 33'-3" | S7 | " | | |
| 2 | #4 | 28'-0" | S8 | " | 21'-11 1/2" | 4'-0 3/4" |
| 61 | #5 | 22'-0" | S9 | " | 26'-0" | |
| 42 | #5 | 8'-0" | S10 | " | 34-S3 CUT 68 & BEND AS SHOWN | |
| 2 | #5 | 23'-3" | S11 | " | | 4'-0 3/4" |
| Substructure | | | | | | |
| 18 | #5 | 4'-9" | P1 | Pedestal | | |
| 36 | #5 | 2'-3" | P2 | " | | |
| 6 | #5 | 6'-9" | P3 | " | | |
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PLAN OF SLAB SHOWING REINFORCING STEEL



HALF SECTION THRU SLAB

Note: Curbs to be cast independently of slab. Slab to be constructed to a uniform depth up not less than 6 1/2" or if desired the bottom of slab may be built on chords between tops of haunches at stringers.

Channel rail to be adjusted for horizontal alignment by use of full size metal shims placed between rail support and the rail connection angles. Shims of 3/4" and 1/2" thickness to be furnished with structural steel. Cost of shims to be included in price bid for other items.

GENERAL NOTES

Design Specifications A.A.S.H.O. 1953

Loading H15-44

Structural Steel Stress: 18,000 psi

Reinforcing Steel Stress: 18,000 psi

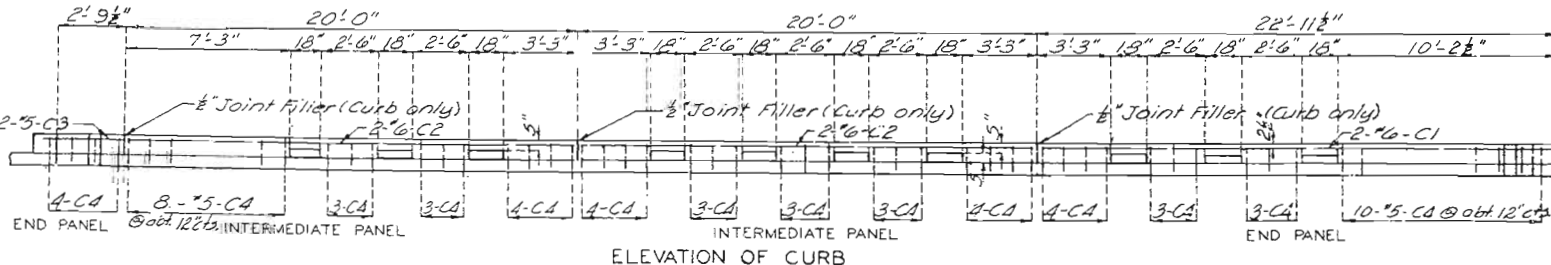
Concrete, Class 'B' Stress: 1,000 psi

All concrete shall be Class 'B'

Rivets 3/4" & holes 1/2"

Paint: Shop, none; field, contact surfaces of bolted rail connections one coat of red lead and surfaces inaccessible after erection three coats of red lead. No other paint to be applied by Contractor. Red lead required shall be furnished by Contractor. Payment for cleaning and painting such surfaces will be included in unit price bid for fabricated Structural Steel.

Where joint filler is specified on the plans it shall conform with the requirements for Premolded Material for Filler as given in Section 38-19A(1) of the Standard Specifications.



ELEVATION OF CURB

| FINAL QUANTITIES | | | | |
|--|----------|-----------|-------|------|
| Item | Substr. | Superstr. | Total | |
| Class 'B' Concrete | Cu. Yds. | 36 | 9.5 | 45.5 |
| Reinforcing Steel | Lbs. | 220 | 920 | 1140 |
| Fab. Structural Steel | Lbs. | | 4800 | 4800 |
| Class I Excavation For Structures Curb | | 11.5 | | 11.5 |

BRIDGE OVER SUB-LATERAL (D.D. NO. 29)

STATE ROAD FROM RTE. 105 THRU ANNISTON TO WOLF ISLAND ABOUT 9.5 MILES S.E. OF ANNISTON

PROJECT NO. RT. 55-SEC. 7 STA. 581+37.2

MISSISSIPPI COUNTY

SUBMITTED BY J.A. Williams DATE 1/15/1954

APPROVED BY Red McWhitt DATE 1/15/1954

STD C-1033

F-788RI

Note: This drawing is not to scale. Follow dimensions.

Sheet No. 1A of 1

FINAL PLANS

347

Drawn Dec. 1953 by W.M.C.

Checked Jan. 1954 by C.F.K.