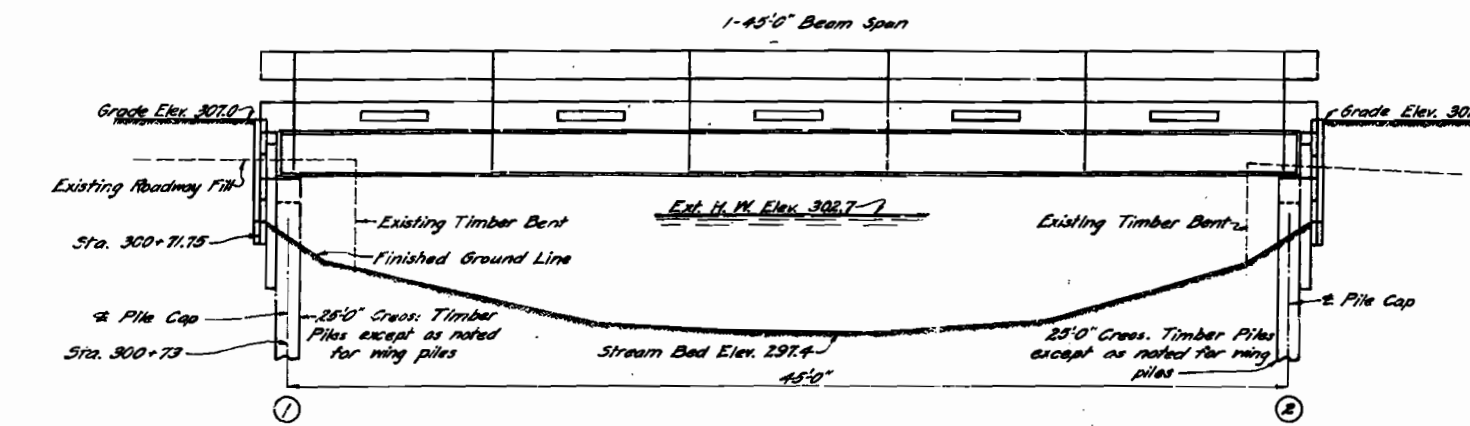


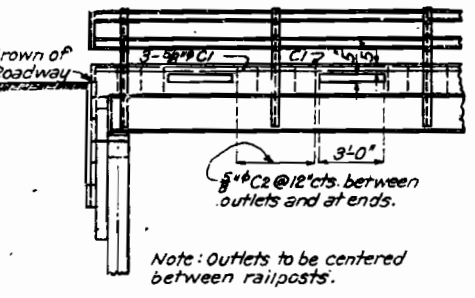
# MISSOURI STATE HIGHWAY DEPARTMENT

FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	FISCAL YEAR	LA. NO.	SHEET
3	MO.	W.P.S.S. 560A	1934	10	10



Note: See Special Provisions in regard to ordering piling. All piling to be crosscut and ordered in the following lengths: 4 @ 15'-0" and 8 @ 25'-0". These lengths include 12" cut-offs. Wing piles to be driven to full penetration. All other piles to be driven to full penetration unless a bearing of 14 tons is obtained at a lesser depth, in which case pile shall be driven to Elev. 281.0.

GENERAL ELEVATION



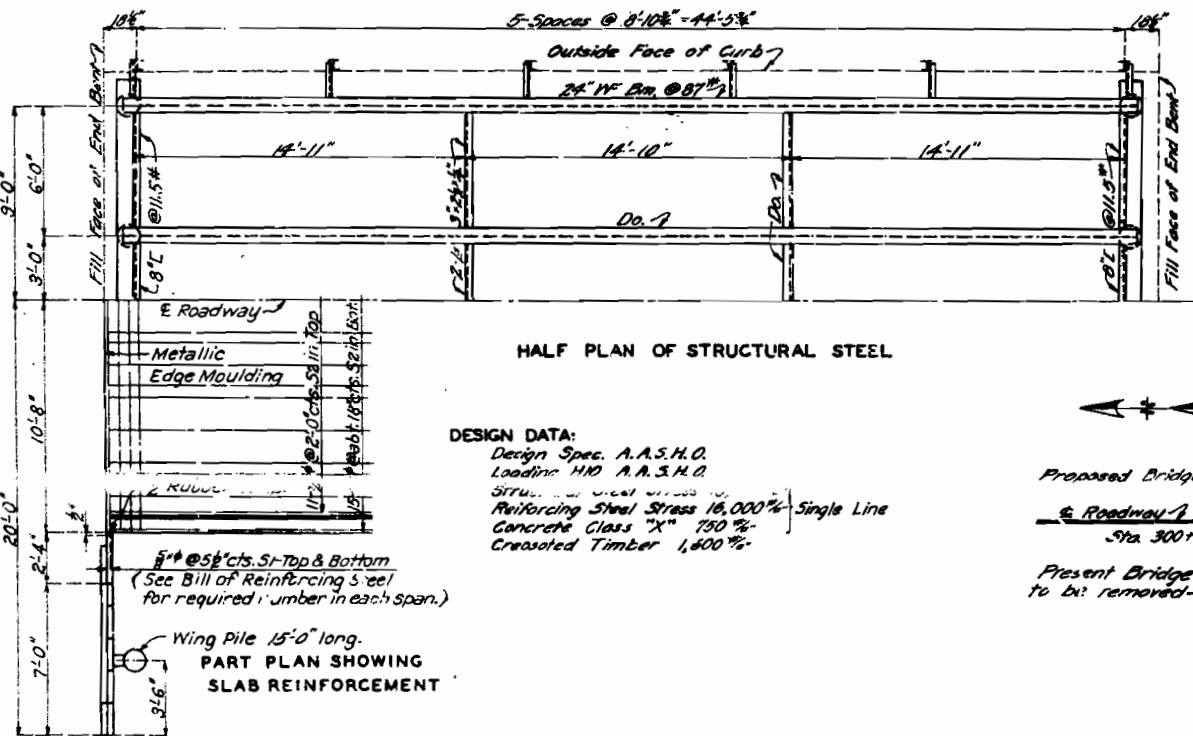
PART ELEVATION



DEFLECTION DIAGRAM

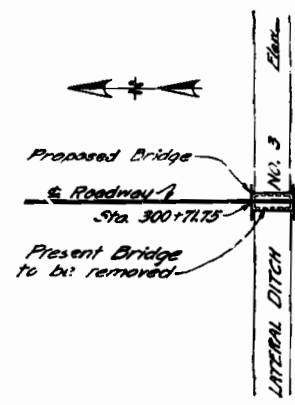
Note: Floor slab to be brought to grade and dead load deflection taken care of by increasing slab thickness. Depth of slab at outside face of curb to be kept uniform and bottom surface of slab warped between curb and outside beam to obtain required thickness at beam. Payment will be allowed for additional concrete required for thickening slab. This additional concrete is included in "Estimated Quantities."

DEFLECTION					
SPAN	C	D	SPAN	C	D
15'-0"	16"	33'-0"	25'-0"	16"	43'-0"
17'-0"	16"	35'-0"	27'-0"	16"	45'-0"
19'-0"	16"	37'-0"	29'-0"	16"	47'-0"
21'-0"	16"	39'-0"	31'-0"	16"	49'-0"
23'-0"	16"	41'-0"			
25'-0"	16"	43'-0"			
27'-0"	16"	45'-0"			
29'-0"	16"	47'-0"			
31'-0"	16"	49'-0"			



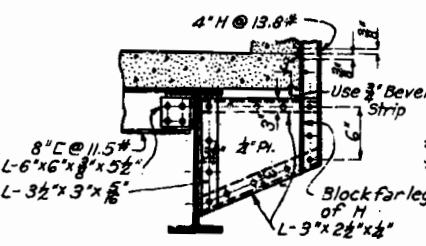
DESIGN DATA:  
Design Spec. A.A.S.H.O.  
Loading H10 A.A.S.H.O.  
Reinforcing Steel Stress 16,000 psi - Single Line  
Concrete Class "X" 750 psi  
Creosoted Timber 1,600 psi

PART PLAN SHOWING SLAB REINFORCEMENT



LOCATION SKETCH

DIMENSION "B"			
SPAN	"B"	SPAN	"B"
15'-0"	20'-0"	33'-0"	2'-4"
17'-0"	21'-0"	35'-0"	2'-4"
19'-0"	22'-0"	37'-0"	2'-4"
21'-0"	23'-0"	39'-0"	2'-6"
23'-0"	24'-0"	41'-0"	2'-7"
25'-0"	25'-0"	43'-0"	2'-7"
27'-0"	26'-0"	45'-0"	2'-7"
29'-0"	27'-0"	47'-0"	2'-9"
31'-0"	28'-0"	49'-0"	2'-9"



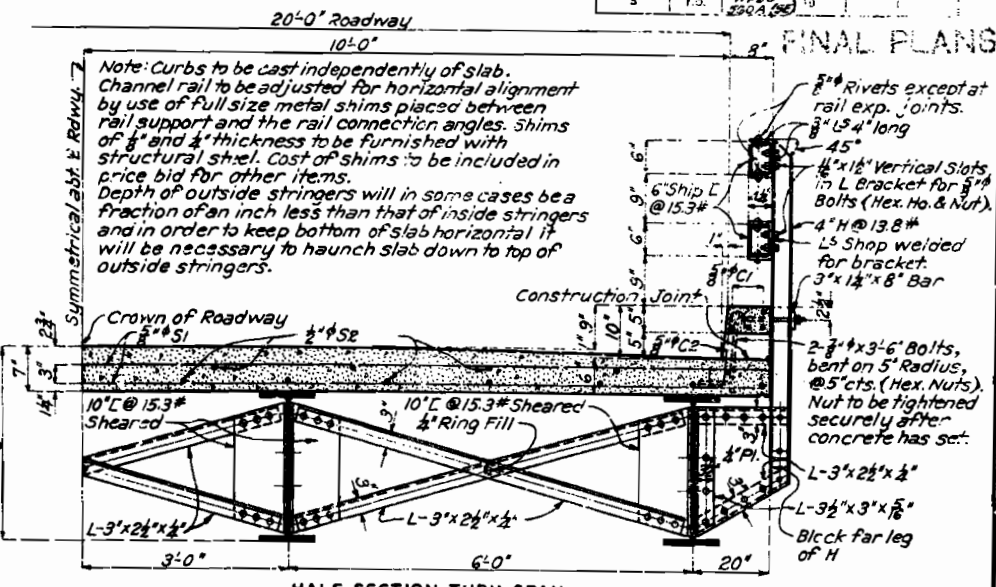
DETAIL OF RAIL BRACKET AT ENDS OF SPAN

Note: Top of channel separators at ends of I-beam span to be flush with bottom of floor slab as shown in Section Thru End Bent At E.

SPAN	TABLE OF STRINGERS			
	PER PLANS		PERMISSIBLE SUBSTITUTIONS	
	CARNEGIE BEAMS	STANDARD I-BEAMS	BETHLEHEM BEAMS	
	Inside	Outside	Inside	Outside
15'-0"	14" @ 30"	14" @ 30"	12" @ 35"	14" @ 30"
17'-0"	14" @ 30"	14" @ 30"	12" @ 35"	14" @ 30"
19'-0"	14" @ 30"	14" @ 30"	12" @ 35"	14" @ 30"
21'-0"	14" @ 30"	14" @ 30"	12" @ 35"	14" @ 30"
23'-0"	14" @ 30"	14" @ 30"	12" @ 35"	14" @ 30"
25'-0"	14" @ 30"	14" @ 30"	12" @ 35"	14" @ 30"
27'-0"	14" @ 30"	14" @ 30"	12" @ 35"	14" @ 30"
29'-0"	14" @ 30"	14" @ 30"	12" @ 35"	14" @ 30"
31'-0"	14" @ 30"	14" @ 30"	12" @ 35"	14" @ 30"

FINAL QUANTITIES	ESTIMATED QUANTITIES			
	ITEM	SUPERSTR.	SUBSTR.	TOTAL
46.5	Bridge Excavation Class 1 Cu. Yds.		60	60
	Bridge Excavation Class 2 Cu. Yds.			
23.7	Concrete Class "X" Cu. Yds.	23.7		23.7
22,840	Fabricated Structural Steel Lbs.	22,840		22,840
5,930	Reinforcing Steel Lbs.	5,930		5,930
247	Creosoted Timber Piles Lin. Ft.		247	247
22	Creosoted Timber Pile Cut-offs Lin. Ft.		12	12
	Timber (Creosoted) F.B.M.	1,980		1,980
	Removal of existing bridge Lump Sum			

Note: Bridge excavation will be allowed for all bents within horizontal limits shown and noted on these design plans, sheet #2. This excavation will be computed from existing ground line to bottom end of 6"x6" backing supports for end bents.  
Old roadway fill under ends of bridge shall be excavated to a depth of 2'-0" below bottom of beams within the maximum horizontal limits of 4'-0" outside of curb lines and will be paid for at unit price bid for roadway excavation.



HALF SECTION THRU SPAN

## GENERAL NOTES:

All concrete to be Class "X".  
Exposed edges to be beveled 3" where no other bevel is noted.  
All timber shall be crosscut and shall be either "Dense Langleaf or Shortleaf Structural Square Edge & Sound Southern Yellow Pine" or "Close Grained Select Structural Douglas Fir of the West Coast Region".  
All timber rough full-sawn except as noted in timber bill for pile caps. Slight variations in sawing to be in accordance with grading rules. All treated timber to be cut to lengths, shaped and bored as shown before treating. Backing plank are all billed 6" long and are to be fitted and cut in the field. Payment will be based on quantities of material in finished structure.  
Field holes for drift pins shall be field bored 3/4". Unless otherwise noted, all other field holes in timber shall be field bored 3/4".  
When bolts with countersunk heads are indicated on plans, cut washers shall be used under heads. O.G. washers shall be used under heads of all other bolts and under nuts of all bolts.  
Cost of substructure hardware to be included in price bid for timber in place. I-beams with fasteners, spacers, handrail, handrail posts with fasteners, and cap plate on end bents with fasteners, will be paid for as structural steel.  
Cost of metallic edge moulding will be included in price bid for concrete.  
Detail shop drawings shall be submitted to the State Highway Department in duplicate and shall be approved before steel is fabricated.  
Where rubber compound is specified on plans for use in partition and expansion joints, the pre-moulded joint shall be securely stitched to one face of concrete with copper wire.  
Paint: Shop, none: Field, contact surfaces of bolted field connections one coat red lead and surfaces inaccessible after erection three coats of red lead. No other paint to be applied by contractor. Red lead required shall be furnished by the contractor. See Special Provisions.  
Bar supports and spacers will be required for reinforcing steel in superstructure. See Standard G10R and Special Provisions.  
Protection caps to be placed on heads of all piles of pile bents in accordance with Section 23-6 of Standard Specifications issued April 1, 1930.  
Rivets 3/4" holes 1/2", except in handrail where rivets shall be 3/4" holes 1/2". Field connections for handrail channels shall be 3/4" button head bolts and for connection of rail to railposts shall be 3/4" bolts, holes 1/2". All other field connections riveted except as noted.  
All concrete shall be proportioned by the weight proportioning method. See Special Provisions.  
B.M. # 20B Elev. 307.75 - N.E. Corn. V. Curb N. End New Bridge Sta. 300+71.75

## BRIDGE OVER LATERAL DITCH NO. 3

STATE ROAD FROM WYATT TO ROUTE 55  
ABOUT 1.0 MILE NORTH OF DEVENTER  
PROJECT NO. W.P.S.S. 560A (SE) STA. 300+71.75

MISSISSIPPI COUNTY  
SUBMITTED BY: *T.R. Luck* DATE: 10/18/35  
APPROVED BY: *T.H. Cutler* DATE: 10/18/35  
BRIDGE ENGINEER  
CHIEF ENGINEER

STD. C-110R  
T-418

NO CONSTRUCTION CHANGES

T.6 FINAL PLANS

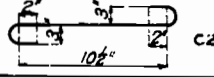
Note: This drawing is not to scale. Follow dimensions.

Sheet 1 of 2

Drawn July 1933 By R.J.G.  
Traced July 1933 By R.J.G.  
Checked Aug. 1933 By N.W.R.  
Assembled Dec. 1934 By N.W.R.  
Checked Dec. 1934 By H.H.M.

# MISSOURI STATE HIGHWAY DEPARTMENT

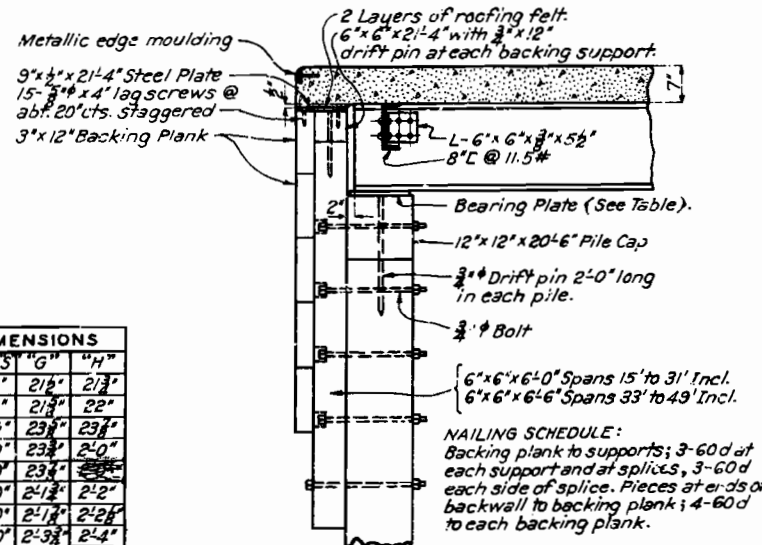
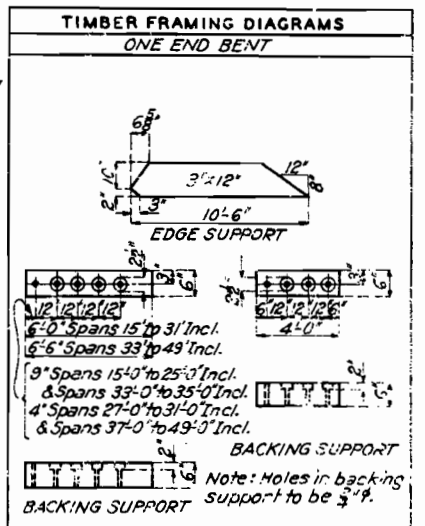
FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
5	MO.	WPS55 580A (26)	1934	2	2

BILL OF REINFORCING STEEL															
NO.	SIZE	LENGTH	MARK	LOCATION	BENDING SKETCH										
"A"	3"φ	"B"	C1	Curb											
"C"	3"φ	2'-0"	C2	"											
"D"	3"φ	2'-0"	S1	Slab											
"E"	3"φ	"F"	S2	"											
SPAN		SINGLE SPAN				ONE END SPAN				ONE INT. SPAN					
"A"	"B"	"C"	"D"	"E"	"A"	"B"	"C"	"D"	"E"	"A"	"B"	"C"	"D"	"E"	"F"
15'-0"	8	16'-9"	76	26	16'-9"	8	15'-9"	70	26	15'-9"	8	14'-9"	64	26	14'-9"
17'-0"	8	18'-9"	86	26	18'-9"	8	17'-9"	80	26	17'-9"	8	16'-9"	74	26	16'-9"
19'-0"	8	20'-9"	92	26	20'-9"	8	19'-9"	88	26	19'-9"	8	18'-9"	84	26	18'-9"
21'-0"	8	22'-9"	100	26	22'-9"	8	21'-9"	96	26	21'-9"	8	20'-9"	92	26	20'-9"
23'-0"	8	24'-9"	112	26	24'-9"	8	23'-9"	106	26	23'-9"	8	22'-9"	100	26	22'-9"
25'-0"	8	26'-9"	118	26	26'-9"	8	25'-9"	114	26	25'-9"	8	24'-9"	110	26	24'-9"
27'-0"	8	28'-9"	130	26	28'-9"	8	27'-9"	124	26	27'-9"	8	26'-9"	118	26	26'-9"
29'-0"	8	30'-9"	138	26	30'-9"	8	29'-9"	132	26	29'-9"	8	28'-9"	126	26	28'-9"
31'-0"	8	32'-9"	146	26	32'-9"	8	31'-9"	140	26	31'-9"	8	30'-9"	134	26	30'-9"
33'-0"	15	18'-6"	154	52	18'-6"	16	18'-0"	150	52	18'-0"	16	17'-6"	146	52	17'-6"
35'-0"	16	19'-6"	162	52	19'-6"	16	19'-0"	158	52	19'-0"	16	18'-6"	154	52	18'-6"
37'-0"	16	20'-6"	172	52	20'-6"	16	20'-0"	168	52	20'-0"	16	19'-6"	164	52	19'-6"
39'-0"	16	21'-6"	180	52	21'-6"	16	21'-0"	176	52	21'-0"	16	20'-6"	172	52	20'-6"
41'-0"	16	22'-6"	188	52	22'-6"	16	22'-0"	184	52	22'-0"	16	21'-6"	180	52	21'-6"
43'-0"	16	23'-6"	198	52	23'-6"	16	23'-0"	194	52	23'-0"	16	22'-6"	190	52	22'-6"
45'-0"	16	24'-6"	206	52	24'-6"	16	24'-0"	202	52	24'-0"	16	23'-6"	200	52	23'-6"
47'-0"	16	25'-6"	216	52	25'-6"	16	25'-0"	210	52	25'-0"	16	24'-6"	204	52	24'-6"
49'-0"	16	26'-6"	224	52	26'-6"	16	26'-0"	220	52	26'-0"	16	25'-6"	216	52	25'-6"

Note: Reinforcing bars in each span to be billed and tagged separately.

SUBSTRUCTURE TIMBER BILLS				
PIECE	NO.	SIZE	LENGTH	REMARKS
Backing Plank	1	3"x12"	6'-6"	Cut to length.
"	1	3"x12"	17'-6"	"
"	1	3"x12"	22'-0"	"
"	1	3"x12"	16'-0"	"
"	1	3"x12"	20'-6"	"
"	1	3"x12"	14'-6"	"
"	1	3"x12"	19'-0"	"
"	1	3"x12"	7'-0"	"
"	1	3"x12"	17'-6"	"
"	2	3"x12"	11'-6"	"
Shoulder Plank	2	3"x8"	2'-9"	"
Edge Support	2	3"x12"	10'-6"	Cut to length & shape.
Backing Support	4	6"x6"	6'-0"	"
Spans 15' to 31' Incl.				
Backing Support	4	6"x6"	6'-6"	"
Spans 33' to 49' Incl.				
Backing Support	2	6"x6"	4'-0"	"
Back Support Cap	1	6"x6"	2'-4"	Cut to length.
Pile Cap	1	12"x12"	20'-6"	"

Note: Pile caps to be classified as beams and stringers. All other timber to be classified as joist and plank. \* S2S to exact depth of 11 1/2".

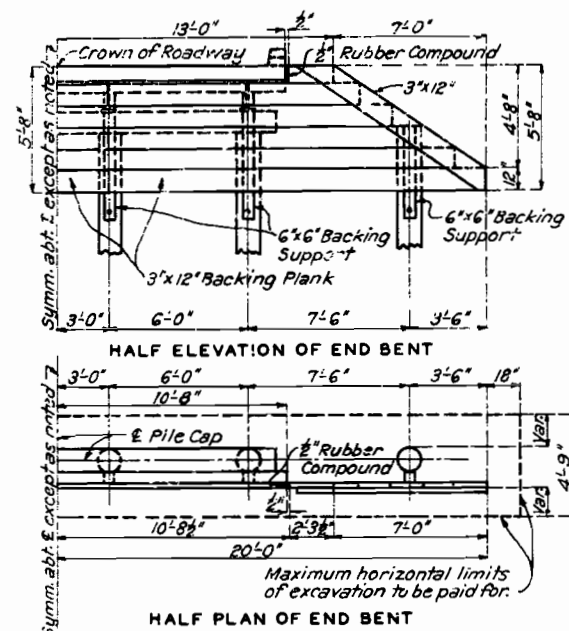


SECTION THRU END BENT AT E

DIMENSIONS			
SPANS	"G"	"H"	"I"
15'-0"	2 1/2"	2 1/2"	
17'-0"	2 1/2"	2 1/2"	
19'-0"	2 1/2"	2 1/2"	
21'-0"	2 1/2"	2 1/2"	
23'-0"	2 1/2"	2 1/2"	
25'-0"	2 1/2"	2 1/2"	
27'-0"	2 1/2"	2 1/2"	
29'-0"	2 1/2"	2 1/2"	
31'-0"	2 1/2"	2 1/2"	
33'-0"	2 1/2"	2 1/2"	
35'-0"	2 1/2"	2 1/2"	
37'-0"	2 1/2"	2 1/2"	
39'-0"	2 1/2"	2 1/2"	
41'-0"	2 1/2"	2 1/2"	
43'-0"	2 1/2"	2 1/2"	
45'-0"	2 1/2"	2 1/2"	
47'-0"	2 1/2"	2 1/2"	
49'-0"	2 1/2"	2 1/2"	

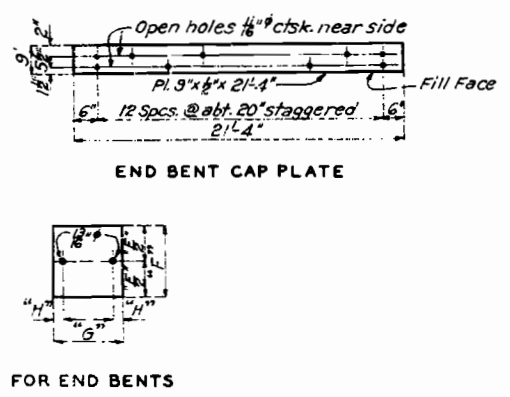
NAILING SCHEDULE:  
Backing plank to supports; 3-60d at each support and at splices, 3-60d each side of splice. Pieces at ends of backwall to backing plank; 4-60d to each backing plank.

Note: Any irregularity in alignment of piling in end bents to be corrected by facing one surface of the 6"x6" backing support so as to place the surface of the backing in a true plane; end limit to any strain on the backing plank. Splice in backing plank to be made at center of 6"x6" backing support and to be aligned on the two intermediate supports.



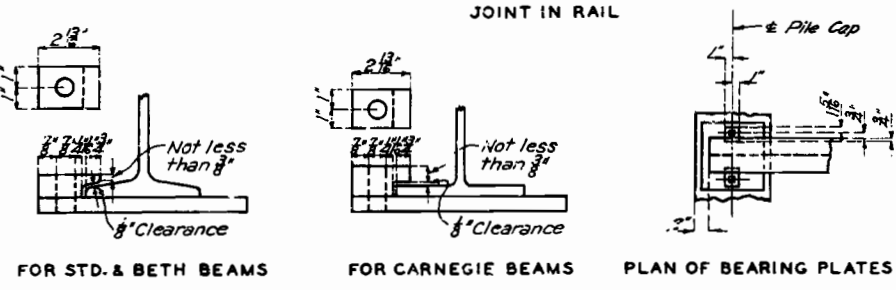
HALF PLAN OF END BENT

TABLE OF BEARING PLATES									
SPAN	NO. PLATES	STRINGER	AT END BENTS				AT INT. BENTS		
"S"			THICK	"E"	"G"	"H"	THICK	"E"	"G"
15'-0"	4	All	3"	10"	11"	12"	3"	10"	11"
17'-0"	4	"	3"	10"	11"	12"	3"	10"	11"
19'-0"	4	"	3"	10"	11"	12"	3"	10"	11"
21'-0"	4	"	3"	10"	11"	12"	3"	10"	11"
23'-0"	4	"	3"	10"	11"	12"	3"	10"	11"
25'-0"	4	"	3"	10"	11"	12"	3"	10"	11"
27'-0"	4	"	3"	10"	11"	12"	3"	10"	11"
29'-0"	4	"	3"	10"	11"	12"	3"	10"	11"
31'-0"	4	"	3"	10"	11"	12"	3"	10"	11"
33'-0"	4	"	3"	11"	13"	14"	3"	11"	13"
35'-0"	4	"	3"	11"	13"	14"	3"	11"	13"
37'-0"	4	"	3"	11"	13"	14"	3"	11"	13"
39'-0"	4	"	3"	11"	13"	14"	3"	11"	13"
41'-0"	4	"	3"	11"	13"	14"	3"	11"	13"
43'-0"	2	Outside	3"	11"	14"	15"	3"	11"	14"
45'-0"	2	Inside	3"	11"	14"	15"	3"	11"	14"
47'-0"	4	All	3"	11"	14"	15"	3"	11"	14"
49'-0"	4	"	3"	11"	14"	15"	3"	11"	14"



END BENT CAP PLATE

FOR END BENTS



FOR STD. & BETH BEAMS

FOR CARNEGIE BEAMS

PLAN OF BEARING PLATES

DETAILS OF FLANGE CLAMPS  
Note: Cast iron clamps used on bearing plates to have 1/16" clearance at flange of beam to allow for expansion. All clamps to have 3/4" cored holes. Use two clamps only on each I-Beam at pile caps on end bents.

## BRIDGE OVER LATERAL DITCH NO. 3

STATE ROAD FROM WYATT TO ROUTE 55  
ABOUT 1.0 MILE NORTH OF DEVENTER  
PROJECT NO WPS55 580A (SE) STA. 300 + 71.75  
MISSISSIPPI COUNTY

Drawn Aug. 1933 By R.J.G.  
Traced Aug. 1933 By R.J.G.  
Checked Aug. 1933 By N.W.R.  
Assembled Dec. 1934 By N.W.R.  
Checked Dec. 1934 By H.H.M.

Note: This drawing is not to scale. Follow dimensions.


Sheet No. 2 of 2


FA.

NO CONSTRUCTION CHANGES

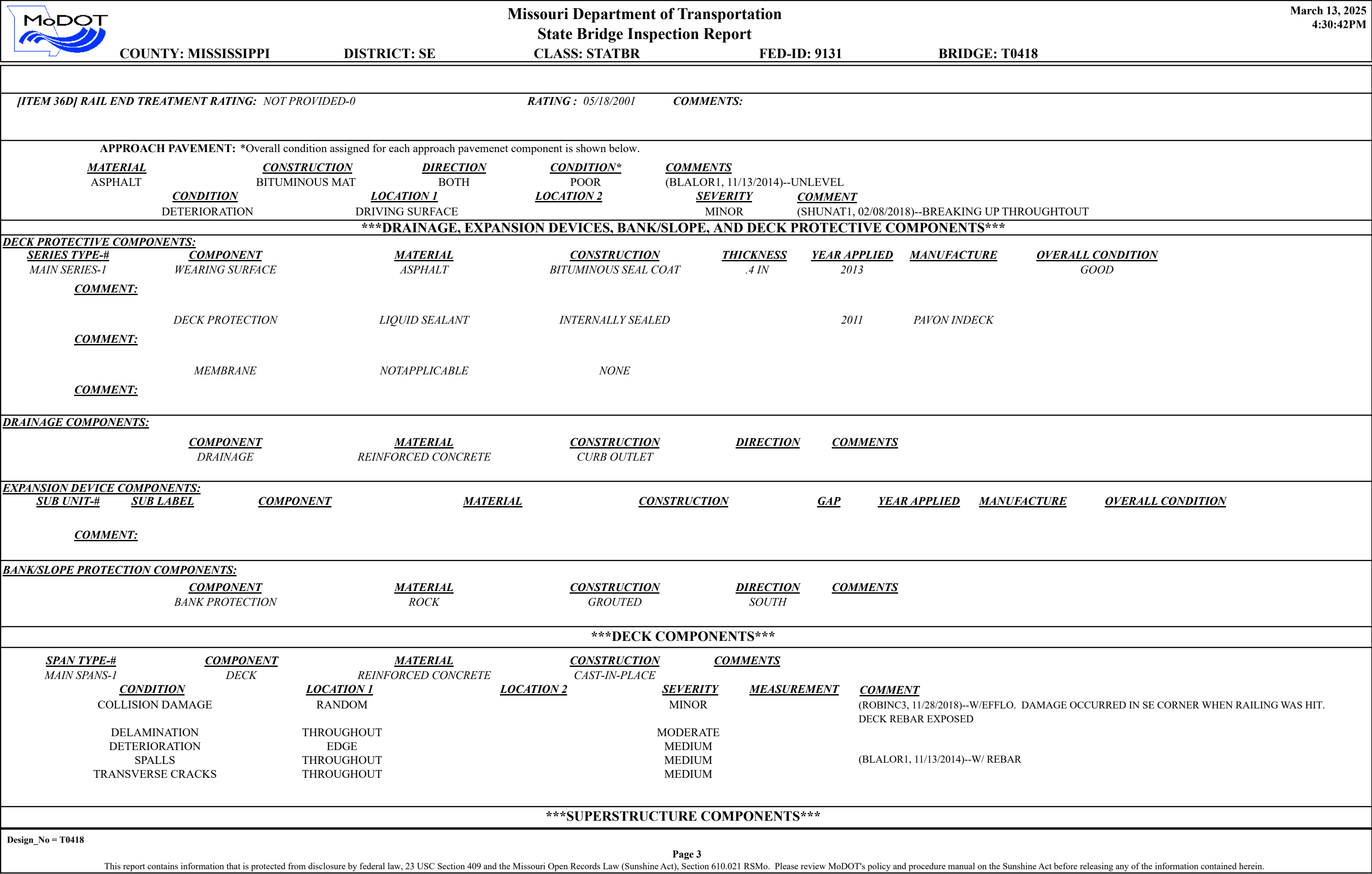
T-418


T6 FINAL PLANS

		<div>Missouri Department of Transportation</div> <div>State Bridge Inspection Report</div>				<div>March 13, 2025</div> <div>4:30:42PM</div>			
COUNTY: MISSISSIPPI		DISTRICT: SE		CLASS: STATBR		FED-ID: 9131		BRIDGE: T0418	
***GENERAL STRUCTURE INFORMATION***							***BRIDGE INSPECTION INFORMATION***		
<div>ROUTE: MO77S</div> <div>FEATURE: LATERAL DTCH #3</div> <div>STATUS: P-POSTLOAD</div> <div>LOG MILE: 51.658</div> <div>DETOUR: 21.00 MILES</div> <div>NHS: NO</div> <div>BUILT: 1935</div> <div>REHAB:</div> <div>LOCATION: S 3 T 25 R 17 E</div> <div>LATITUDE: 36 50 6.47 (DMS)</div> <div>LONGITUDE: 89 12 42.24 (DMS)</div>		<div># SPANS: 1</div> <div>LANES ON: 1</div> <div>LANES UNDER: 0</div> <div>COMPASS DIRECTION: NORTH to SOUTH</div> <div>DIRECTION OF TRAFFIC: 1-LN/2-WAY</div> <div>FUNCTIONAL CLASS: RL-MAJOR COLLECTOR</div> <div>NBI OWNER: MODOT</div> <div>NBI MAINTAINED: MODOT</div> <div>MAINTENANCE DISTRICT: SE</div> <div>MAINTENANCE COUNTY: MISSISSIPPI</div> <div>SUB AREA: 7H09</div>		<div>PLACE CODE: 48926 MISSISSIPPI</div> <div>LENGTH: 47 FT 0 IN</div> <div>MAXIMUM SPAN: 45 FT 0 IN</div> <div>APPROACH ROADWAY: 20 FT 0 IN</div> <div>CURB TO CURB: 20 FT 0 IN</div> <div>OUT TO OUT: 21 FT 4 IN</div> <div>AADT: 270</div> <div>AADT YEAR: 2024</div> <div>AADT TRUCK: 25.0%</div> <div>FUTURE AADT: 378</div> <div>FUTURE AADT YEAR: 2044</div>		<div>DATE: 11/04/2024</div> <div>RESPONSIBILITY: DISTRICT</div> <div>FREQUENCY: 12</div> <div>CALCULATED INTERVAL**: 12</div> <div>TEAM LEADER: STEVE RIGHTNOWAR</div> <div>ELEMENT: NO</div> <div>INSPECTOR 2:</div> <div>INSPECTOR 4:</div> <div>INSPECTOR 3:</div> <div>** When calculated interval exceeds the frequency, a justification comment per BIRM is required.</div>			
						GENERAL INSPECTION COMMENTS			
***FRACTURE CRITICAL INSPECTION INFORMATION***					***INDEPTH INSPECTION INFORMATION***				
<div>DATE:</div> <div>FREQUENCY:</div> <div>TEAM LEADER:</div> <div>INSPECTOR 2:</div> <div>** When calculated interval exceeds the frequency, a justification comment per BIRM is required.</div>					<div>RESPONSIBILITY:</div> <div>CALCULATED INTERVAL**:</div> <div>INSPECTOR 3:</div> <div>INSPECTOR 4:</div> <div>CATEGORY:</div> <div>NBI:</div> <div>METHOD:</div> <div>** When calculated interval exceeds the frequency, a justification comment per BIRM is required.</div>				
FRACTURE CRITICAL INSPECTION COMMENTS					INDEPTH INSPECTION COMMENTS				
***SPECIAL INSPECTION INFORMATION***					***UNDERWATER INSPECTION INFORMATION***				
<div>DATE: 06/13/2023</div> <div>FREQUENCY: 120</div> <div>TEAM LEADER:</div> <div>INSPECTOR 2: JERROD JERNIGAN</div> <div>** When calculated interval exceeds the frequency, a justification comment per BIRM is required.</div>					<div>RESPONSIBILITY: DISTRICT</div> <div>CALCULATED INTERVAL**: 116</div> <div>INSPECTOR 3:</div> <div>INSPECTOR 4:</div> <div>CATEGORY: CHANNEL CROSS SEC</div> <div>NBI: NO</div> <div>METHOD: WT TAPE</div> <div>** When calculated interval exceeds the frequency, a justification comment per BIRM is required.</div>				
SPECIAL INSPECTION COMMENTS					UNDERWATER INSPECTION COMMENTS				
OTHER SPECIAL INSPECTIONS					OTHER UNDERWATER INSPECTIONS				
<div>DATE</div> <div>FREQUENCY</div> <div>CATEGORY</div> <div>NBI</div> <div>CALCULATED INTERVAL</div> <div>RESPONSIBILITY</div> <div>METHOD</div> <div>01/30/2018</div> <div>999</div> <div>QUALITY ASSURANCE</div> <div>NO</div> <div>BRIDGEDIV</div>					<div>DATE</div> <div>FREQUENCY</div> <div>CATEGORY</div> <div>NBI</div> <div>CALCULATED INTERVAL</div> <div>RESPONSIBILITY</div> <div>METHOD</div>				
Design_No = T0418									
<div>Page 1</div> <div>This report contains information that is protected from disclosure by federal law, 23 USC Section 409 and the Missouri Open Records Law (Sunshine Act), Section 610.021 RSMo. Please review MoDOT's policy and procedure manual on the Sunshine Act before releasing any of the information contained herein.</div>									

		<div>Missouri Department of Transportation</div> <div>State Bridge Inspection Report</div>			<div>March 13, 2025</div> <div>4:30:42PM</div>	
COUNTY: MISSISSIPPI		DISTRICT: SE	CLASS: STATBR	FED-ID: 9131	BRIDGE: T0418	
***STRUCTURE POSTING***						
APPROVED CATEGORY: S-17		CL OF BR AND TRK OVR 15 T 15MPH ON BR EXCPT SNGL UNTRKS WT LMT 19 T OTHR TRKS WT LMT 35 T.				
Ton 1: 15		Ton 2: 19	Ton 3: 35			
COMMENTS:						
FIELD CATEGORY: S-17		CL OF BR AND TRK OVR 15 T 15MPH ON BR EXCPT SNGL UNTRKS WT LMT 19 T OTHR TRKS				
Ton 1: 15		Ton 2: 19	Ton 3: 35	PROBLEM:		PROBLEM DIRECTION:
COMMENTS: (WIESEJ1, 08/08/2011)--S APPR NOT POSTED						
***GENERAL COMMENTS/MAJOR RATED ITEMS***						
GENERAL COMMENTS: (BOWDEJ1, 09/05/2008)--(45') SMP WF GDR SPAN						
[ITEM 58] DECK: 5-FAIR CONDITION			COMMENTS: (SHUNAT1, 02/08/2018)--SPALLS AND DELAMS THROUGHTOUT			
RATING : 05/18/2001						
[ITEM 59] SUPER: 5-FAIR CONDITION			COMMENTS: (LAMBEJ1, 11/04/2011)--TOP FLANGE RUSTING ON EXT. GIRDERS			
RATING : 11/03/2016			(DENNIB1, 11/03/2016)--INITIAL SECTION LOSS BOTTOM FLANGE			
[ITEM 60] SUB: 6-SATISFACTORY CONDITION			COMMENTS: (LAMBEJ1, 11/04/2011)--MINOR TIMBER DECAY			
RATING : 05/18/2001						
[ITEM 61] BANK/CHANNEL: 6-WIDESPREAD MINOR DAMAGE			COMMENTS: (SHUNAT1, 02/08/2018)--MINOR BANK EROSION SOUTH BANK			
RATING : 11/13/2017						
[ITEM 113] SCOUR: 8-STABLE FOR CALCULATED			COMMENTS:			
RATING : 05/18/2001						
EVALUATION TYPE :						
[ITEM 71] WATERWAY ADEQUACY: DECK ABOVE FLOOD ELEV			COMMENTS:			
RATING : 05/18/2001						
[ITEM 72] APPRRDWY ALIGNMENT: 8-VERYGOOD			COMMENTS:			
RATING : 05/18/2001						
***RAILING AND APPROACH PAVEMENT COMPONENTS AND RATINGS***						
[ITEM 36A] BRIDGE RAILING RATING: DOESNT MEET CURRNT STND-0			RATING : 02/24/2004	COMMENTS:		
<u>MATERIAL</u>	<u>CONSTRUCTION</u>	<u>DIRECTION</u>	<u>COMMENTS</u>			
REINFORCED CONCRETE	CURB	BOTH				
<u>CONDITION</u>	<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>COMMENT</u>		
COLLISION DAMAGE	RANDOM		MINOR	(BLALOR1, 11/13/2014)--SE CORNER BROKE OFF		
SPALLS	RANDOM		MODERATE	(BLALOR1, 11/13/2014)--W/ REBAR		
STEEL	CHANNEL-DOUBLE	BOTH				
<u>CONDITION</u>	<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>COMMENT</u>		
RUSTING	THROUGHOUT		MINOR			
[ITEM 36B] TRANSITION RAILING RATING: NOT PROVIDED-0			RATING : 05/18/2001	COMMENTS:		
[ITEM 36C] APPROACH RAILING RATING: NOT PROVIDED-0			RATING : 05/18/2001	COMMENTS:		
Design_No = T0418						
Page 2						
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		Missouri Department of Transportation					March 13, 2025		
		State Bridge Inspection Report					4:30:42PM		
COUNTY: MISSISSIPPI		DISTRICT: SE		CLASS: STATBR		FED-ID: 9131		BRIDGE: T0418	
<u>SERIES TYPE-#</u>	<u>SPAN TYPE</u>	<u>MATERIAL</u>	<u>CONSTRUCTION</u>	<u>LABEL</u>	<u>COMMENTS</u>				
MAIN SERIES-1	SIMPLE SPAN	STEEL	WIDE FLANGE GIRDERS						
<u>SPAN</u>	<u>COMPOSITE INDICATOR</u>	<u>LENGTH</u>	<u>WEATHERING STEEL</u>	<u>COMMENTS</u>					
MAIN SPANS-1	NON-COMPOSITE	45 FT 0 IN	NO	(ROBINC3, 12/21/2018)--DEAD LOAD DEFLECTION ON ALL BEAMS MINOR 1/2"					
<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>			
DECK LIFTING		THROUGHOUT		MINOR					
PACK RUST		THROUGHOUT		MINOR		(LAMBEJ1, 11/04/2011)--EXT. GIRDERS			
RUSTING		THROUGHOUT		MODERATE					
SECTION LOSS		BOTTOM FLANGE		INITIAL					
SECTION LOSS		TOP FLANGE		INITIAL					
***SUBSTRUCTURE COMPONENTS***									
<u>SUBSTRUCTURE</u>	<u>SKEW</u>	<u>LENGTH</u>	<u>MATERIAL</u>	<u>CONSTRUCTION</u>	<u>LABEL</u>	<u>COMMENTS</u>			
ABUTMENT-1		20 FT 6 IN	TIMBER	NON-INTEGRAL					
	<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>		
<u>ASSOCIATED COMPONENT</u>		<u>MATERIAL</u>		<u>CONSTRUCTION</u>					
BEAM CAP		TIMBER		BEAM					
	<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>		
	DECAYING		THROUGHOUT		MINOR				
PILING		TIMBER		OTHER					
	<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>		
	SPLITTING		THROUGHOUT		MINOR				
STRAIGHT WINGS		TIMBER		PLANKS					
	<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>		
	OTHER		BACKWALL		NOT APPLICABLE		(SHUNAT1, 02/08/2018)--MINOR DECAY		
WING PILES		TIMBER		OTHER					
	<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>		
	SPLITTING		THROUGHOUT		MODERATE		(SHUNAT1, 02/08/2018)--MODERATE DECAY THROUGHOUT		
BACKWALL		TIMBER		PLANKS					
	<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>		
	OTHER		RANDOM		NOT APPLICABLE		(SHUNAT1, 02/08/2018)--MINOR DECAY		
FIXED BEARING		STEEL		FLAT PLATE					
	<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>		
	PACK RUST		THROUGHOUT		MINOR				
	RUSTING		THROUGHOUT		MINOR				
ABUTMENT-2		20 FT 6 IN	REINFORCED CONCRETE	NON-INTEGRAL					
	<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>		
<u>ASSOCIATED COMPONENT</u>		<u>MATERIAL</u>		<u>CONSTRUCTION</u>					
BEAM CAP		TIMBER		BEAM					
	<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>		
	DECAYING		THROUGHOUT		MINOR				
PILING		TIMBER		OTHER					
	<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>		
	DECAYING		GROUND LINE		MINOR				
	SPLITTING		THROUGHOUT		MINOR				
STRAIGHT WINGS		TIMBER		PLANKS					
	<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>		
WING PILES		TIMBER		OTHER					
	<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>		
	DECAYING		THROUGHOUT		MODERATE				
BACKWALL		TIMBER		PLANKS					

Design\_No = T0418

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		Missouri Department of Transportation			March 13, 2025	
		State Bridge Inspection Report			4:30:42PM	
COUNTY: MISSISSIPPI		DISTRICT: SE	CLASS: STATBR	FED-ID: 9131	BRIDGE: T0418	
<div><div><div><div>CONDITION</div><div>DECAYING</div><div>SPLITTING</div></div><div>FIXED BEARING</div></div><div><div>LOCATION 1</div><div>THROUGHOUT</div><div>THROUGHOUT</div></div></div> <div><div>STEEL</div><div>LOCATION 1</div><div>THROUGHOUT</div><div>THROUGHOUT</div></div>						

LOCATION 2

FLAT PLATE

SEVERITY

MODERATE


MODERATE

MEASUREMENT

COMMENT

(ROBINC3, 11/28/2018)--INITIAL

(ROBINC3, 11/28/2018)--NO LOSS OF BEARING

		Missouri Department of Transportation				March 13, 2025																																		
		State Bridge Inspection Report				4:30:42PM																																		
COUNTY: MISSISSIPPI		DISTRICT: SE		CLASS: STATBR		FED-ID: 9131																																		
						BRIDGE: T0418																																		
<div>RESPONSIBILITYLOCATIONITEMCATEGORYPRIORITYDATEWORK ITEM COMMENT</div>																																								
***UTILITY ATTACHMENTS***																																								
<div>UTILITYOWNERMETHODMEASUREMENT TYPEVALUENUMBERUTILITY ATTACHMENT COMMENT</div>																																								
***PROGRAM NOTES INFORMATION***																																								
<div>YEARPROJECT #MONTH LETYEAR LETITEMSCOMMENT</div> <div>2027SE006212027REPLACE BRIDGE</div>																																								
***COMPUTER GENERATED RATINGS AND DEFICIENCY ITEMS***					***ADVANCED SIGN INFORMATION***																																			
<div>NOTE: The items listed in this section are updated whenever computer edits are ran on a structure after the inspection updates have been entered in to TMS.</div> <table><tr><td><u>Rated Item</u></td><td><u>Rating</u></td><td><u>Rating Date</u></td></tr><tr><td>[Item 67] Structure Evaluation Rating:</td><td>2-BASICALLY INTOLRBLE REQ</td><td>1/3/2024</td></tr><tr><td>[Item 68] Deck Geometry Rating:</td><td>4-MEETS MINIMUM TOLERABLE</td><td>3/25/2002</td></tr><tr><td>[Item 69] Underclearance:</td><td>N-NOT APPLICABLE</td><td>5/18/2001</td></tr><tr><td>Sufficiency Rating:</td><td>47.2%</td><td>3/6/2024</td></tr><tr><td>Deficiency:</td><td>STRUCTURAL</td><td>1/3/2024</td></tr><tr><td>Funding Eligibility:</td><td>FULL</td><td>----</td></tr><tr><td>Estimated New Structure Length:</td><td>69 FT.</td><td>----</td></tr><tr><td>Estimated Structure Cost:</td><td>\$358,731</td><td>----</td></tr><tr><td>Estimated Total Project Cost:</td><td>\$538,097</td><td>----</td></tr><tr><td>Year of Cost Estimate:</td><td>2025</td><td>----</td></tr></table> <div>NOTE: The above structure length and cost estimates are computer generated using algorithms in the TMS system. These algorithms are generalized to use NBI items to come up with a new structure length and width to calculate a new area which is taken times a representative cost per square foot. The actual structure size and cost may vary significantly from these numbers once site specific engineering is done.</div>					<u>Rated Item</u>	<u>Rating</u>	<u>Rating Date</u>	[Item 67] Structure Evaluation Rating:	2-BASICALLY INTOLRBLE REQ	1/3/2024	[Item 68] Deck Geometry Rating:	4-MEETS MINIMUM TOLERABLE	3/25/2002	[Item 69] Underclearance:	N-NOT APPLICABLE	5/18/2001	Sufficiency Rating:	47.2%	3/6/2024	Deficiency:	STRUCTURAL	1/3/2024	Funding Eligibility:	FULL	----	Estimated New Structure Length:	69 FT.	----	Estimated Structure Cost:	\$358,731	----	Estimated Total Project Cost:	\$538,097	----	Year of Cost Estimate:	2025	----	<div>SIGN #SIGN TYPEPROBLEMPROBLEM DIRECTION</div> <div>1YIELD TO ONCOMING TRAFFIC</div> <div>2B - ONE LANE BRIDGE</div>		
					<u>Rated Item</u>	<u>Rating</u>	<u>Rating Date</u>																																	
					[Item 67] Structure Evaluation Rating:	2-BASICALLY INTOLRBLE REQ	1/3/2024																																	
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***OUTFALL INSPECTION INFORMATION***																																								
<div># OUTFALLS:INSPECTOR:</div> <div>STATUS:DATE:</div> <div>NOTES:</div>																																								





Missouri Department of Transportation  
Bridge Inventory and Inspection System  
Structural Inventory & Appraisal Sheet

March 13, 2025  
4:57:21pm

COUNTY : MISSISSIPPI BRIDGE : T0418 REVIEW STATUS : CONVERTED NBI STATUS : T  
RECORD TYPE : ROUTE CARRIED 'ON' STRUCT RUN DATE : 3/7/2025 SUBMITTAL YEAR : 2025

GENERAL STRUCTURE INFORMATION

1	State	MISSOURI
2	District	SE
3	County	MISSISSIPPI
8	Federal ID No.	9131
27	Year Built	1935
106	Year Reconstructed	0
42A	Type of Service On	HIGHWAY
21	Structure Maintenance	STATE HIGHWAY AGENCY
22	Structure Owner	STATE HIGHWAY AGENCY
33	Br. Median Code	NO MEDIAN
37	Historical Significance	NOT ELIGIBLE FOR NR OF HP
101	Parallel Struc Desg	NONE EXISTS
103	Temporary Structure	NOT TEMPORARY
112	NBIS Bridge Length	YES

ROUTE DESIGNATION INFORMATION

5A	Record Type	ROUTE CARRIED 'ON' STRUCT
5B	Route Signing Prefix	MO
5C	Designated Level of Service	MAINLINE
5D	Route Number	00077
5E	Directional Suffix	NOT APPLICABLE
7	Facility Carried	MO 77 S
12	Base Hwy. Network	NO
13A	LRS Inventory Route No.	
13B	Subroute No.	
20	Toll Status	ON FREE ROAD
26	Functional Classification	07-RURAL MAJOR COLLECTOR
28A	Lanes on Structure	01
100	STRAHNET Designation	RTE NOT A DEFENSE HWY
104	National Highway System	NOT ON NHS
105	Federal Lands Highway	NOT APPLICABLE
110	Designated Nat. Network	NO

STRUCTURE LOCATION INFORMATION

4	Place	MISSISSIPPI
	Code	48926
9	Location	S 3 T 25 N R 17 E
11	Milepoint	51.96 miles
16	Latitude	36 D 50 M 6 S
17	Longitude	89 D 12 M 42 S

STRUCTURE TRAFFIC INFORMATION

29	AADT	270
30	AADT Year	2024
102	Direction of Traffic	ONE LANE BRIDGE FOR 2-WAY
109	AADT Truck Percent	25%
114	Future AADT	378
115	Future AADT Year	2044

UNDERRECORD INFORMATION

6	Features Intersected	LATERAL DTCH #3
42B	Type of Service Under	WATERWAY
28B	Lanes Under Structure	00
54A	Vert. Clearance Ref.	N/A
54B	Vert. Clearance	0 Ft. 0 In.
55A	Rt. Lat Clear Ref.	N/A
55B	Rt. Lat Clearance	0 Ft. 0 In.
56	Left Lat Clearance	0 Ft. 0 In.
38	Navigation Control	PERMIT NOT REQ
39	Nav Vertical Clear	0 Ft. 0 In.
40	Nav Horizontal Clear	0 Ft. 0 In.
111	Nav. Pier Protection	
116	Nav. Cl. Vert. Clear	

STRUCTURE GEOMETRIC INFORMATION

10	Inventory Rte. Vert. Clear	99 Ft. 99 In.
19	By pass Detour Length	21.25 miles
32	Approach Roadway Width	20 Ft. 0 In.
34	Skew	0.00 Degrees
35	Struct. Flared	NO
47	Total Horiz. Clear	20 Ft. 0 In.
48	Maximum Span Length	44 Ft. 11 In.
49	Structure Length	46 Ft. 11 In.
50A	Left Curb/Sidewalk Width	0 Ft. 0 In.
50B	Right Curb/Sidewalk Width	0 Ft. 0 In.
51	Curb to Curb Br. Width	20 Ft. 0 In.
52	Deck Width (Out-Out)	21 Ft. 4 In.
53	Vert. Clearance Over Deck	99 Ft. 99 In.

Design\_No = T0418 and Inventory\_Appraisal\_Submittal\_Year = 2025



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Bridge Inventory and Inspection System  
Structural Inventory & Appraisal Sheet

March 13, 2025  
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COUNTY : MISSISSIPPI BRIDGE : T0418 REVIEW STATUS : CONVERTED NBI STATUS : T  
RECORD TYPE : ROUTE CARRIED 'ON' STRUCT RUN DATE : 3/7/2025 SUBMITTAL YEAR : 2025

LOAD RATING AND POSTING INFORMATION				MATERIAL/CONSTRUCTION INFORMATION					
31	Design Load	H 10		43A	Main Struc. Mat type	STEEL			
41	Structure Status	POSTED FOR LOAD		43B	Main struc Constr. Type	STRINGER/MULTIBEAM - GRD			
63	Oper. Rating Meth.	LOAD FACTOR		45	# of Main Spans	1			
64	Operating Rating	18 Tons.		44A	Appr Struc. Mat type	000			
65	Inventory Rating Meth	LOAD FACTOR		44B	Appr Struc. Cnstr. type	000			
66	Inventory Rating	11 Tons.		46	# of Approach Span	0			
70	Bridge Posting Code	30.0-39.9% BELOW		107	Deck Mat/Constr.	1 CONCRETE CIP			
PROPOSED IMPROVEMENT INFORMATION				108A	Wear Surf Mat/Constr.	6 BITUMINOUS			
	Sufficiency Rating	47.2	Percent	108B	Membrane Mat/Constr.	0 NONE			
	Deficiency Rating	STRUCTURAL		108C	Deck Protect Mat/Constr.	7 INTERNALLY SEALED			
	Funding Eligibility	FULL		CONDITION RATING INFORMATION					
75A	Proposed Work	REPLACEMENT SUBSTND LOAD		58	Deck Cond. Rating	5			
75B	Work Done By	Contract		59	Superstructure Cond. Rating	5			
76	New Struc Length	68 Ft. 11 In.		60	Substructure Cond. Rating	6			
94	Struc Improve Cost	\$ 359,000		61	Channel /Channel Protection Cond. Rating	6			
95	Roadway Improve Cost	\$ 36,000		62	Culvert Cond. Rating	N			
96	Total Project Cost	\$ 538,000		INSPECTION INFORMATION					
97	Year of Cost Estimates	2025		90	Gen. Insp Date	11 / 24			
APPRAISAL RATING INFORMATION				91	Gen. Insp. Frequency	12	Months		
36A	Br. Rail App. Rating	DOES NOT MEET ACCEPT STND		92A	Frac. Critical Inspection	N	Months		
36B	Transition Rail App. Rating	DOES NOT MEET ACCEPT STND		93A	Frac. Critical Insp. Date				
36C	Approach Rail App. Rating	DOES NOT MEET ACCEPT STND		92B	Underwater Inspection	N	Months		
36D	Rail End Treat. App. Rating	DOES NOT MEET ACCEPT STND		93B	Underwater Insp. Date				
67	Struc Eval App. Rating	2		92C	Special Inspection	N	Months		
68	Deck Geometry App. Rating	4		93C	Special Inspection Date				
69	Underclearance App. Rating	N		BORDER BRIDGE INFORMATION					
71	Waterway Adeq. App. Rating	8		98	Neighboring State Code				
72	Approach Road App. Rating	8		98B	Neighboring State % Respon				
113	Scour Assess App. Rating	8		99	Neighboring State Struc. No.				
APPROVED POSTING INFORMATION				FIELD POSTING INFORMATION					
Approved Posting Category		S-17		Field Posting Category		S-17			
		Ton1	Ton2	Ton3		Ton1	Ton2	Ton3	
Tonnage Values for Posting Sign		15	19	35	Tonnage Values for Posting Sign		15	19	35
General Text for Posting Sign					General Text for Posting Sign				
CL OF BR AND TRK OVR 15 T 15MPH ON BR EXCPT SNGL E UNT					CL OF BR AND TRK OVR 15 T 15MPH ON BR EXCPT SNGL E UNT				
TRKS WT LMT 19 T OTHR TRKS WT LMT 35 T.					TRKS WT LMT 19 T OTHR TRKS WT LMT 35 T.				

Design\_No = T0418 and Inventory\_Appraisal\_Submittal\_Year = 2025