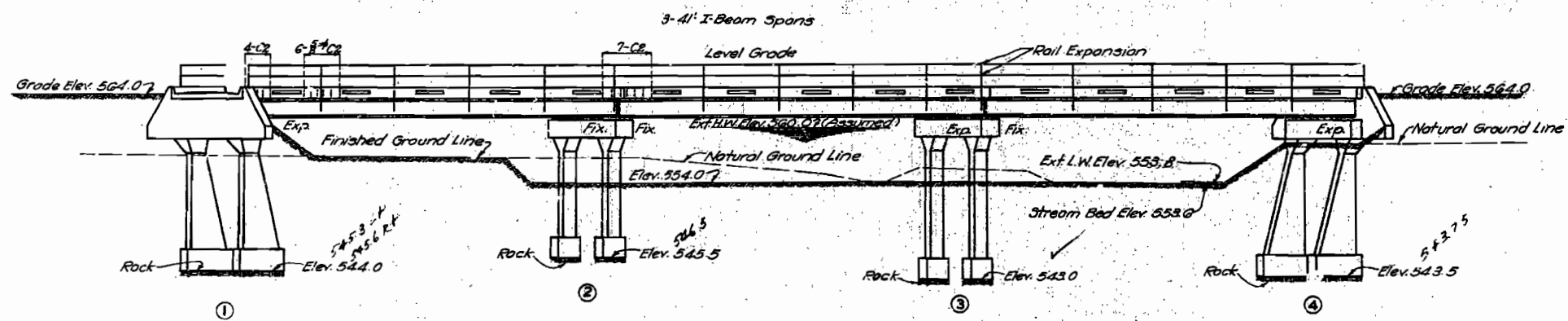


MISSOURI STATE HIGHWAY DEPARTMENT

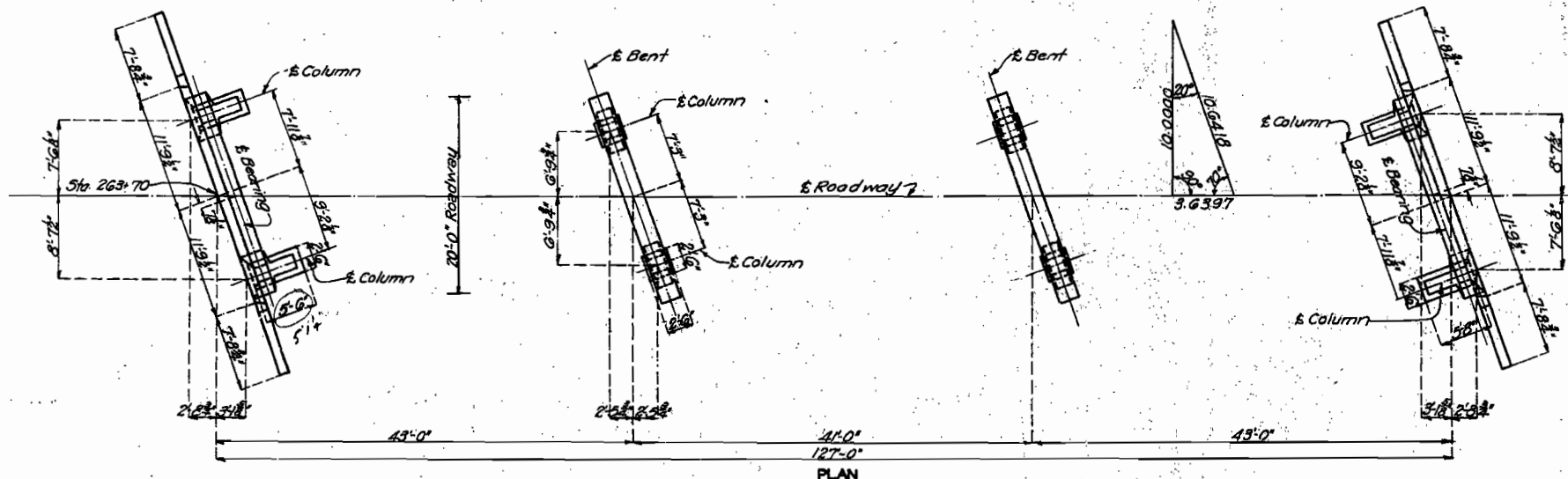
FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
5	MO.	100-1-1	19		

Note: Outlets in curbs to be centered between rail posts.



Note: All loose, shelly, or disintegrated rock shall be removed and the footings placed on hard, solid undisturbed rock. If soft rock or shale is encountered, the footings shall be carried at least 18" into the same.

GENERAL ELEVATION



PLAN

NO.	SIZE	LENGTH	MARK	LOCATION	Bending Sketch and Cutting Diagram
32	3/8"	22'-6"	C1	Curb	
188	3/8"	2'-0"	C2	"	
16	3/8"	21'-6"	C3	"	
464	3/8"	21'-0"	S1	Slab	
104	3/8"	22'-3"	S2	"	
90	3/8"	22'-3"	S3	"	
12	3/8"	22'-3"	S4	"	
52	3/8"	21'-3"	S5	"	

Note: Dimensions given are along centerline of bars and are for computed lengths.

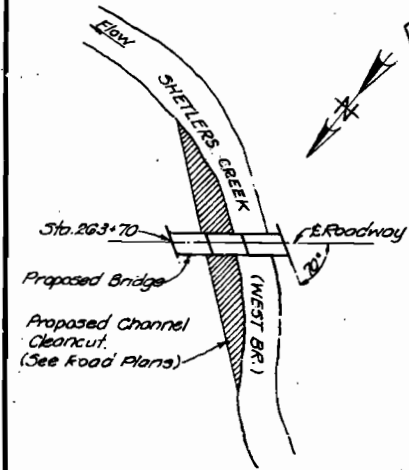
GENERAL NOTES:
 Concrete in slab and curbs to be 1:2:3 1/2 mix, Class "X". All other concrete to be 1:2:4 mix, Class "B".
 Exposed edges to be beveled 3" where no other bevel is noted.
 Bridge excavation in accordance with Section I of Standard Specifications issued April 1, 1930, except that quantities paid for will be computed from extreme low water Elev. 553.8, where existing ground line is below this elevation.

I-Beams with fastenings, spacers, handrail, handrail posts with fastenings will be paid for as structural steel.
 Cost of metallic edge moulding will be included in price bid for concrete.
 Detail shop drawings shall be submitted to the State Highway Department in duplicate and shall be approved before steel is fabricated. Where rubber compound is specified on plans for use in partition and expansion joints, the pre-moulded joint shall be securely stitched to one face of concrete with copper wire.
 Paint: Shop, none; Field, contact surfaces of bolted field connections one coat red lead and surfaces inaccessible after erection three coats of red lead. No other paint to be applied by contractor. Red lead required shall be furnished by the contractor. See Special Provisions.
 See Special Provisions in regard to permissible beam substitutions and basis of payment.

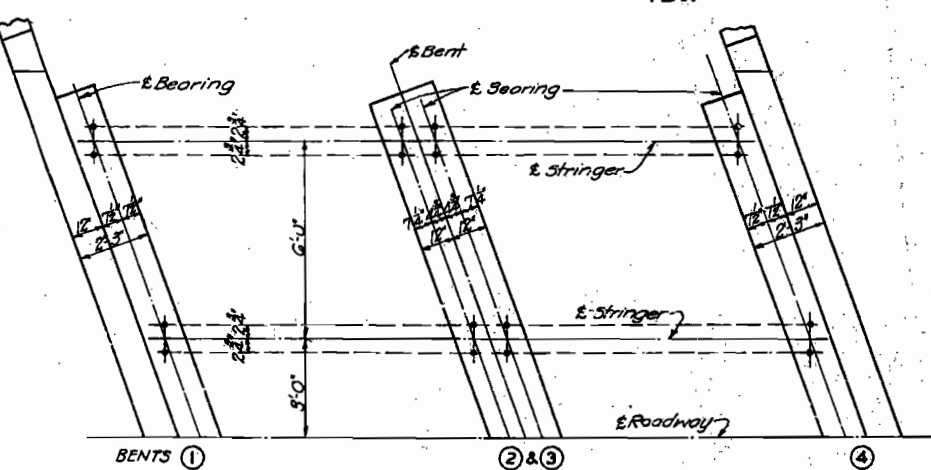
Rivets 3/8", holes 1/2", except in handrail where rivets shall be 1/2", holes 5/8". Field connections for handrail channels shall be 3/8" button head bolts and for connection of rail to rail posts shall be 3/8" bolts, holes 1/2". All other field connections riveted except as noted.

FINAL QUAN.	ITEM	SUPERSTR.	SUBSTR.	TOTAL
24.5	Excavation Class 1 Cu. Yds		25	25
124.0	Excavation Class 2 Cu. Yds		126	126
43.9	Concrete 1:2:4 mix, Class "B" Cu. Yds		43.3	43.3
63.3	Concrete 1:2:4 mix, Class "X" Cu. Yds	63.3		63.3
55610	Fabricated Structural Steel Lbs.	56250		56250
21980	Reinforcing Steel Lbs.	16000	5980	21980
7.5	Test Holes Lin Ft.			

Note: Bridge excavation above Elev. 535.0 will be paid for as Class 1 Bridge Excavation.
 Bridge excavation below Elev. 555.0 will be paid for as Class 2 Bridge Excavation.



LOCATION SKETCH



HALF ANCHOR BOLT PLAN

BRIDGE OVER SHETLERS CREEK (WEST BR.)
 STATE ROAD FROM MARQUAND TO BUCKHORN
 ABOUT 5.25 MILES S.W. OF MARQUAND
 PROJECT NO. REF. NO. 6(SB) S1 STA. 263+70

MADISON COUNTY
 SUBMITTED BY: *N.R. Lacy* DATE: 2/16/34
 APPROVED BY: *T.H. Cutler* DATE: 2/16/34

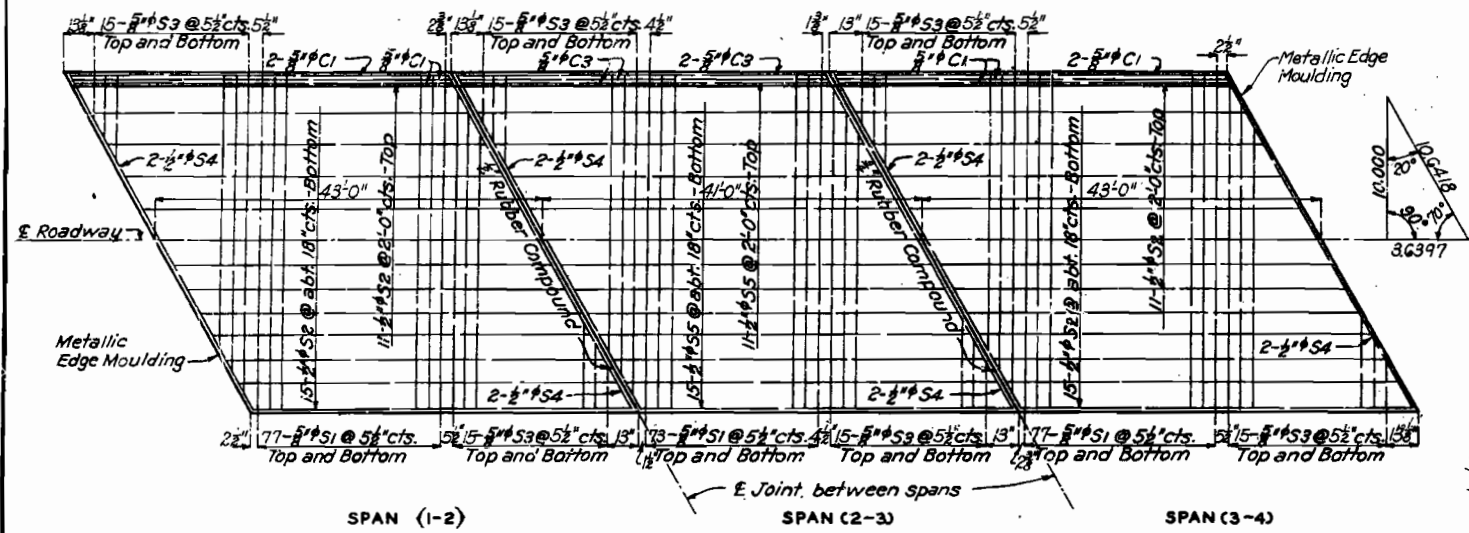
Drawn Aug. 1933 By R.J.G.
 Traced Aug. 1933 By R.J.G. Assembled Feb. 1934 By R.J.G.-G.W.
 Checked Aug. 1933 By M.W.R. Checked Feb. 1934 By M.W.R.

Note: This drawing is not to scale. Follow dimensions. Sheet No. 1 of 3.

MISSOURI STATE HIGHWAY DEPARTMENT

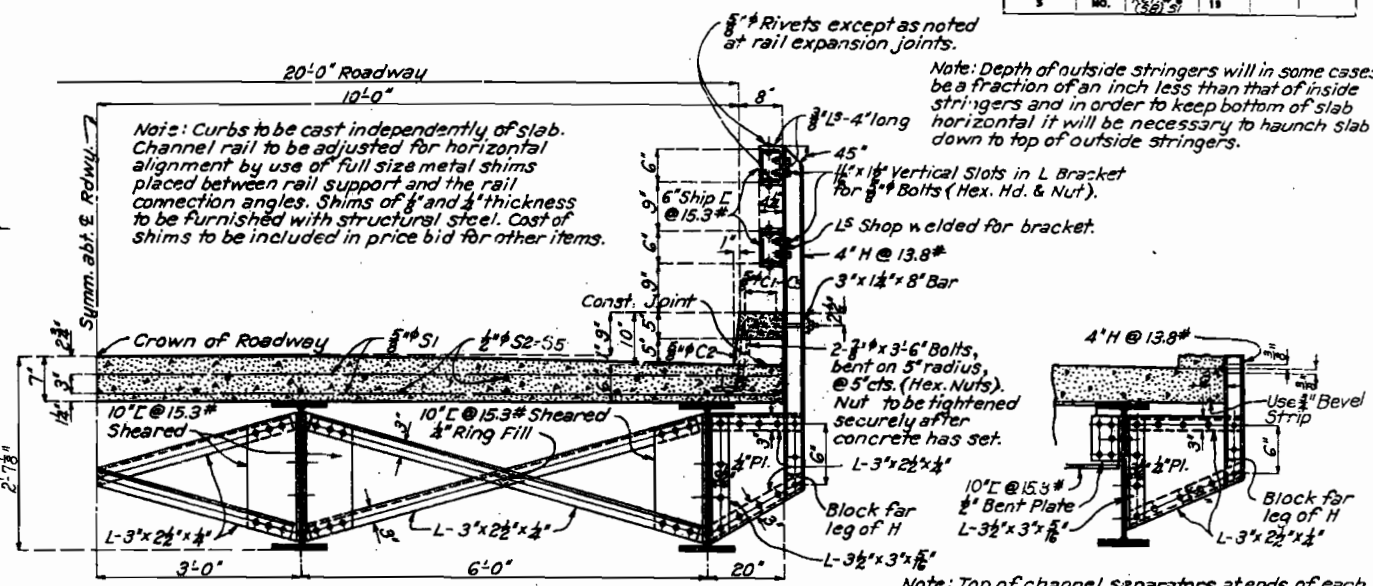
FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
5	MO.	1561	19		

Note: Space dowel bars "C2" at approximately 12" cts. in curbs between outlets and at ends.

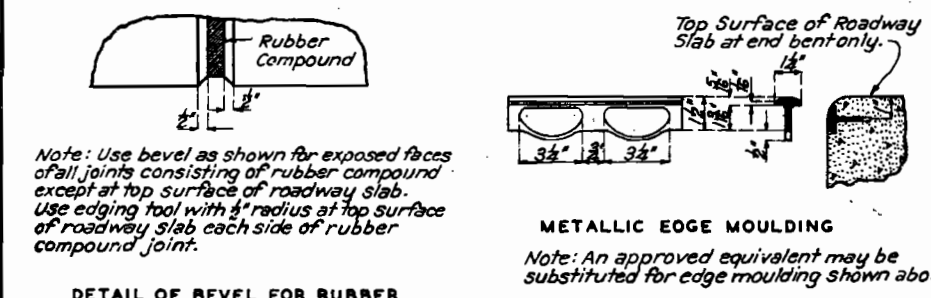


PLAN OF SLAB SHOWING REINFORCING

Note: Modify curbs as shown in "Location of Posts at End Bents" on this sheet.



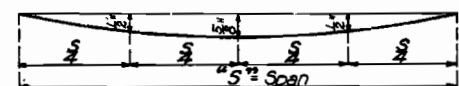
HALF SECTION THRU SPAN



DETAIL OF BEVEL FOR RUBBER COMPOUND JOINTS

METALLIC EDGE MOLDING

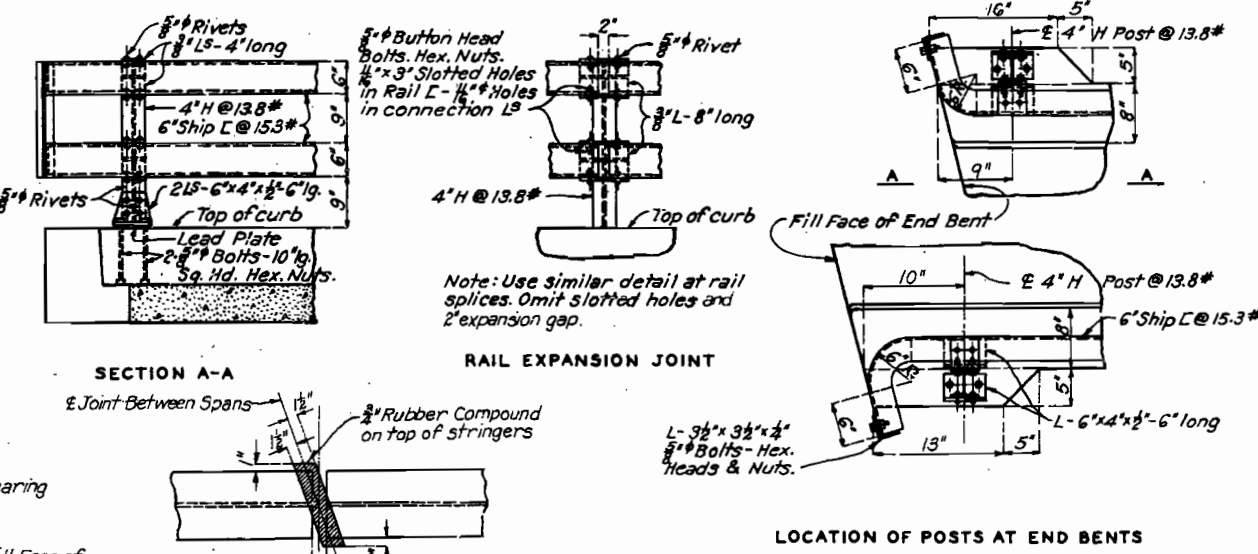
Note: Top of curbs under end posts shall be finished to a smooth surface parallel to grade. Not less than one nor more than four soft lead plates of 1/8" thickness shall be used under angles of each end rail post for aligning rail to correct elevation. Plates shall be 3/8" x 6" and shall be punched 3/8" on same gauge as the angles. No grouting permitted. Cost of lead plates to be included in price bid for other items.



Note: Floor slab to be brought to grade and dead load deflection taken care of by increasing slab thickness. Depth of slab at outside face of curb to be kept uniform and bottom surface of slab warped between curb and outside beam to obtain required thickness at beam. Payment will be allowed for additional concrete required for thickening slab. This additional concrete is included in "Estimated Quantities".

PERMISSIBLE BEAM SUBSTITUTIONS.

Std. I Beams	Beth. Beams
Inside 24" @ 85"	Outside 24" @ 79"
Outside 24" @ 81"	Inside 24" @ 74"



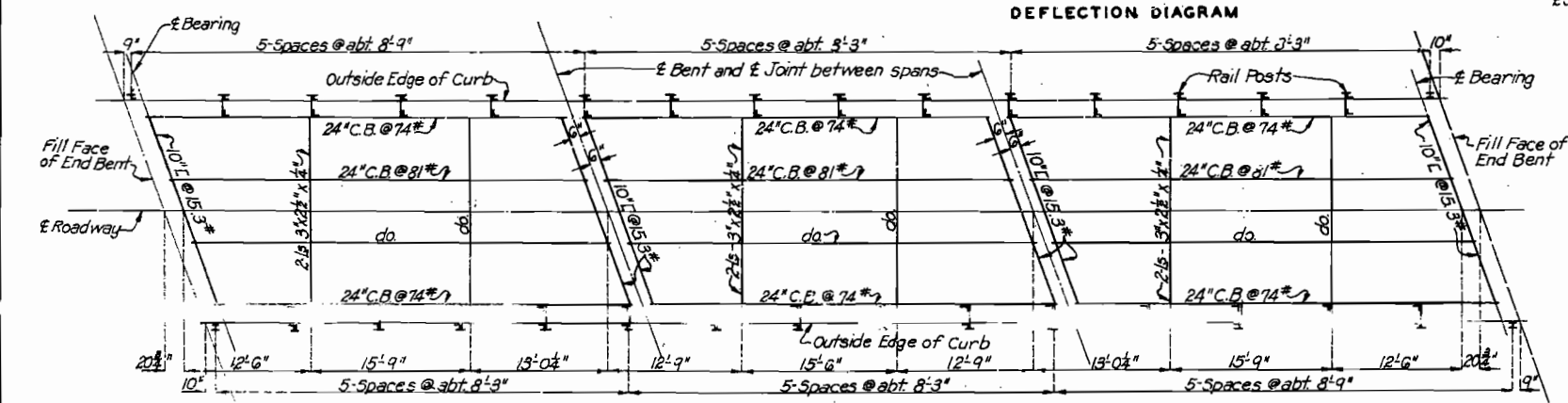
DETAIL OF RAIL BRACKETS AT ENDS OF SPAN

RAIL EXPANSION JOINT

SECTION A-A

DETAIL OF RUBBER COMPOUND OVER STRINGERS AT EXP. JOINT-BENT NO. 3

LOCATION OF POSTS AT END BENTS



STRUCTURAL STEEL LAYOUT



TYPICAL PART ELEVATION SHOWING CURB DETAILS

BRIDGE OVER SHETLERS CREEK-WEST BRANCH

STATE ROAD FROM MARQUAND TO BUCKHORN
ABOUT 5.25 MILES S.W. OF MARQUAND
PROJECT NO. REF. NO. 6 (SB) SI STA. 263+70
MADISON COUNTY

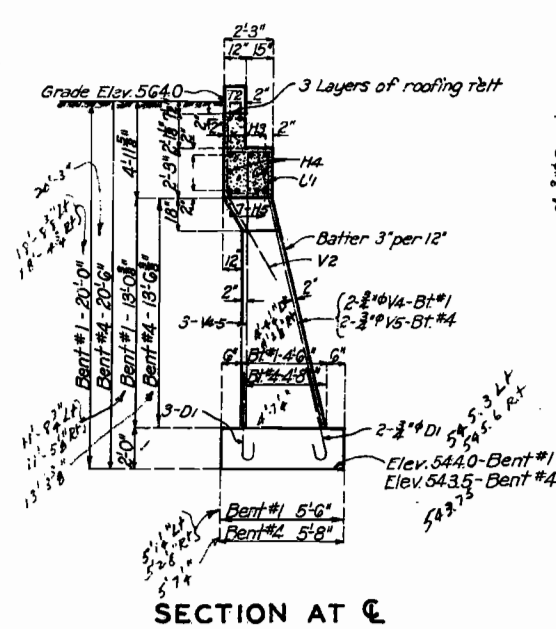
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Traced Aug. 1933 By R.J.G. Assembled Feb. 1934 By P.15-H.W.H.
Checked Aug. 1933 By N.M.R. Checked Feb. 1934 By N.M.R.

Note: This drawing is not to scale. Follow dimensions.

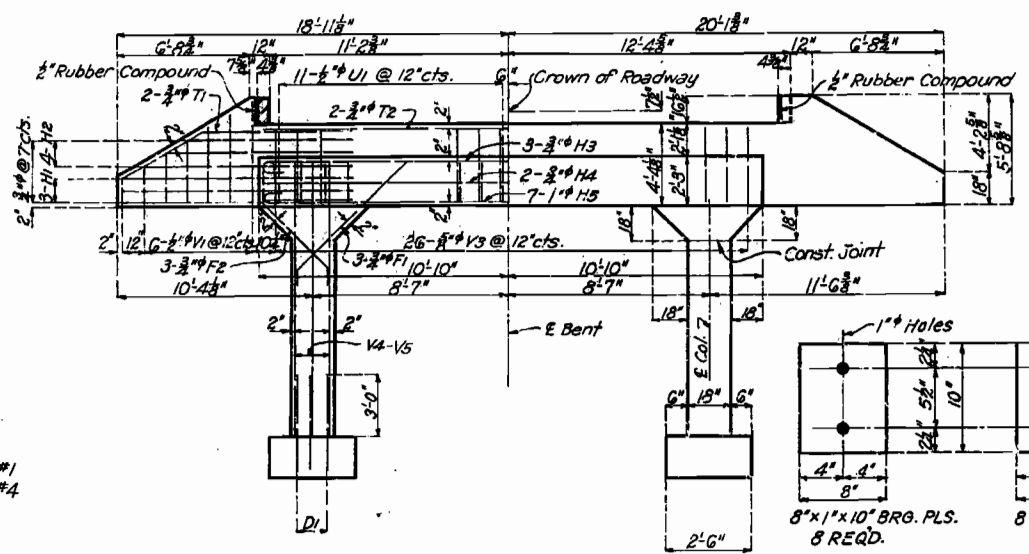
Sheet No. 2 of 3

MISSOURI STATE HIGHWAY DEPARTMENT

FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	FISCAL YEAR	TOTAL SHEETS
5	MO.	2-27	19	

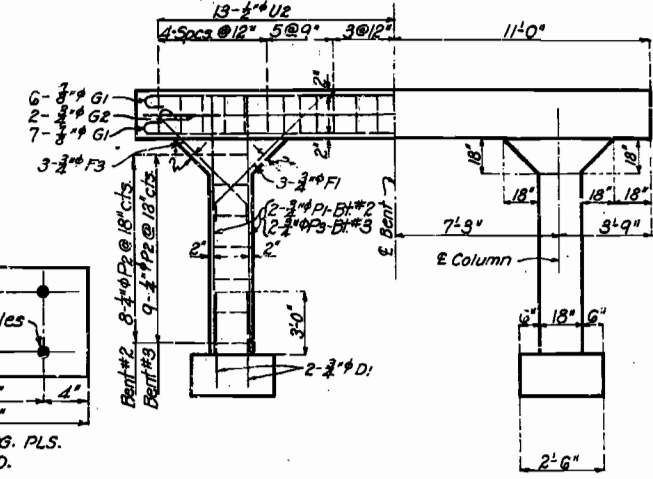


SECTION AT C

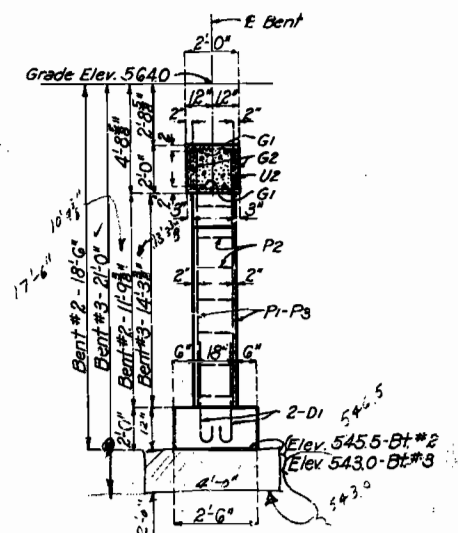


ELEVATION

DETAILS OF BEARING PLATES



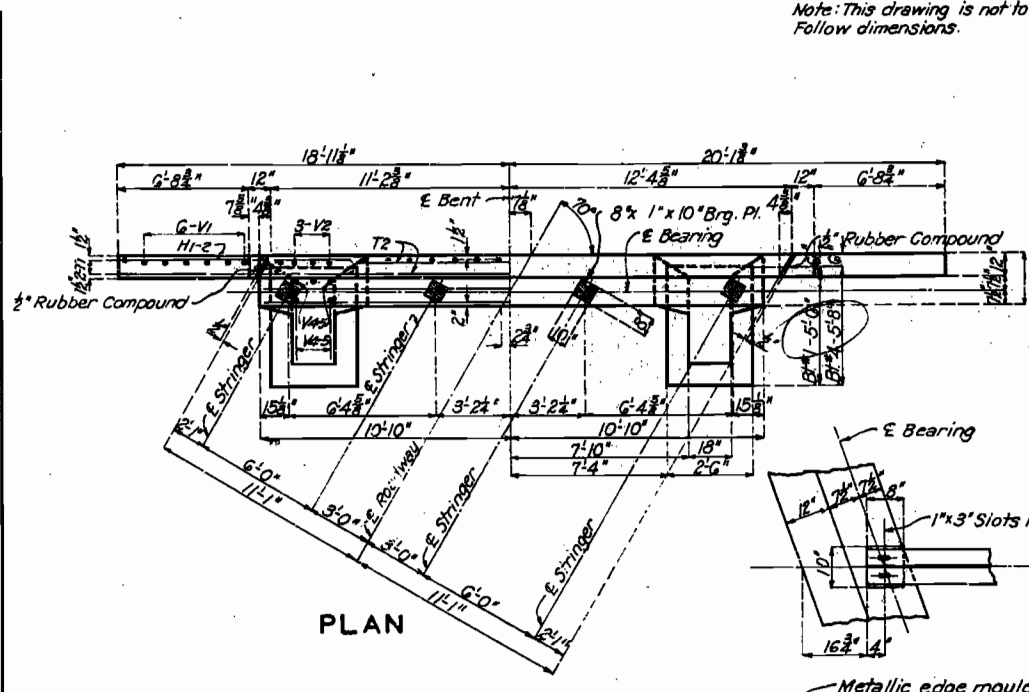
ELEVATION



SECTION AT E

BILL OF REINFORCING STEEL FOR SUBSTRUCTURE				
No.	Size	Length	Mark	Location
END BENTS NO. 1 & 4				
20	3/4"	5'-3"	D1	Footings
12	3/4"	7'-3"	F1	Haunch
12	3/4"	7'-9"	F2	"
12	3/4"	12'-3"	H1	Wings
8	3/4"	13'-9"	H2	"
6	3/4"	23'-6"	H3	Beam
4	3/4"	21'-3"	H4	"
14	1/2"	23'-6"	H5	"
8	3/4"	14'-3"	T1	Wings
4	3/4"	21'-9"	T2	Bkwall.
44	3/4"	9'-0"	U1	Beam
12	3/4"	6'-9"	V1	Wing
12	3/4"	7'-6"	V2	Column
52	3/4"	4'-0"	V3	Bkwall.
10	3/4"	15'-0"	V4	Col. Bt.#1
10	3/4"	15'-6"	V5	Col. Bt.#4
INT. BENTS NO. 2 & 3				
16	3/4"	5'-3"	D1	Footings
12	3/4"	7'-3"	F1	Haunch
12	3/4"	8'-0"	F3	"
26	3/4"	23'-9"	G1	Beam
4	3/4"	21'-9"	G2	"
8	3/4"	13'-6"	P1	Col. Bt.#2
34	3/4"	6'-0"	P2	Cols.
8	3/4"	16'-0"	P3	Col. Bt.#3
50	3/4"	8'-0"	U2	Beam

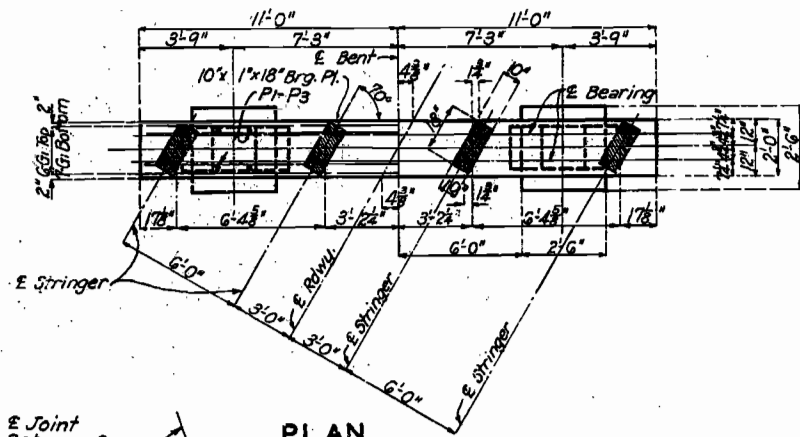
Note: Dimensions given are along centerline of bars and are for computed lengths. Reinforcing bars 3/4" or over in diameter, which are bent to an angle greater than 90°, shall be of structural grade. For bill of reinforcing in superstructure see Sheet No. 1 of 3.



PLAN

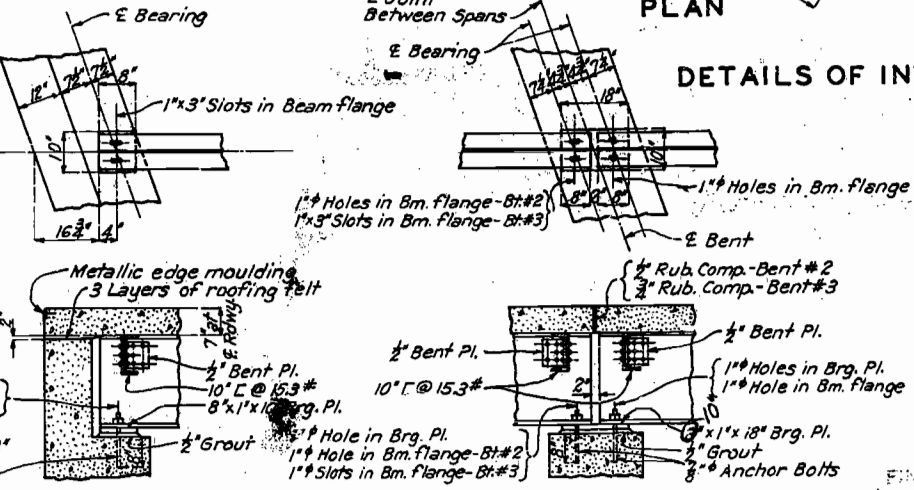
DETAILS OF END BENTS NO. 1 & 4

Note: This drawing is not to scale. Follow dimensions.



PLAN

DETAILS OF INT. BENTS NO. 2 & 3



AT END BENTS NO. 1 & 4

AT INT. BENTS NO. 2 & 3

DETAILS OF BEARINGS

BRIDGE OVER SHETLERS CREEK-WEST BRANCH
 STATE ROAD FROM MARQUAND TO BUCKHORN
 ABOUT 5.25 MILES S.W. OF MARQUAND
 PROJECT NO. REF. NO. 6 (S) ST. STA 263 + 70
MADISON COUNTY

Assembled Feb. 1934 By P.H.S.-H.W.H.-R.J.G.
 Checked Feb. 1934 By M.M.R.
 Drawn April 1932 By R.J.G.
 Checked Aug. 1932 By P.H.S.

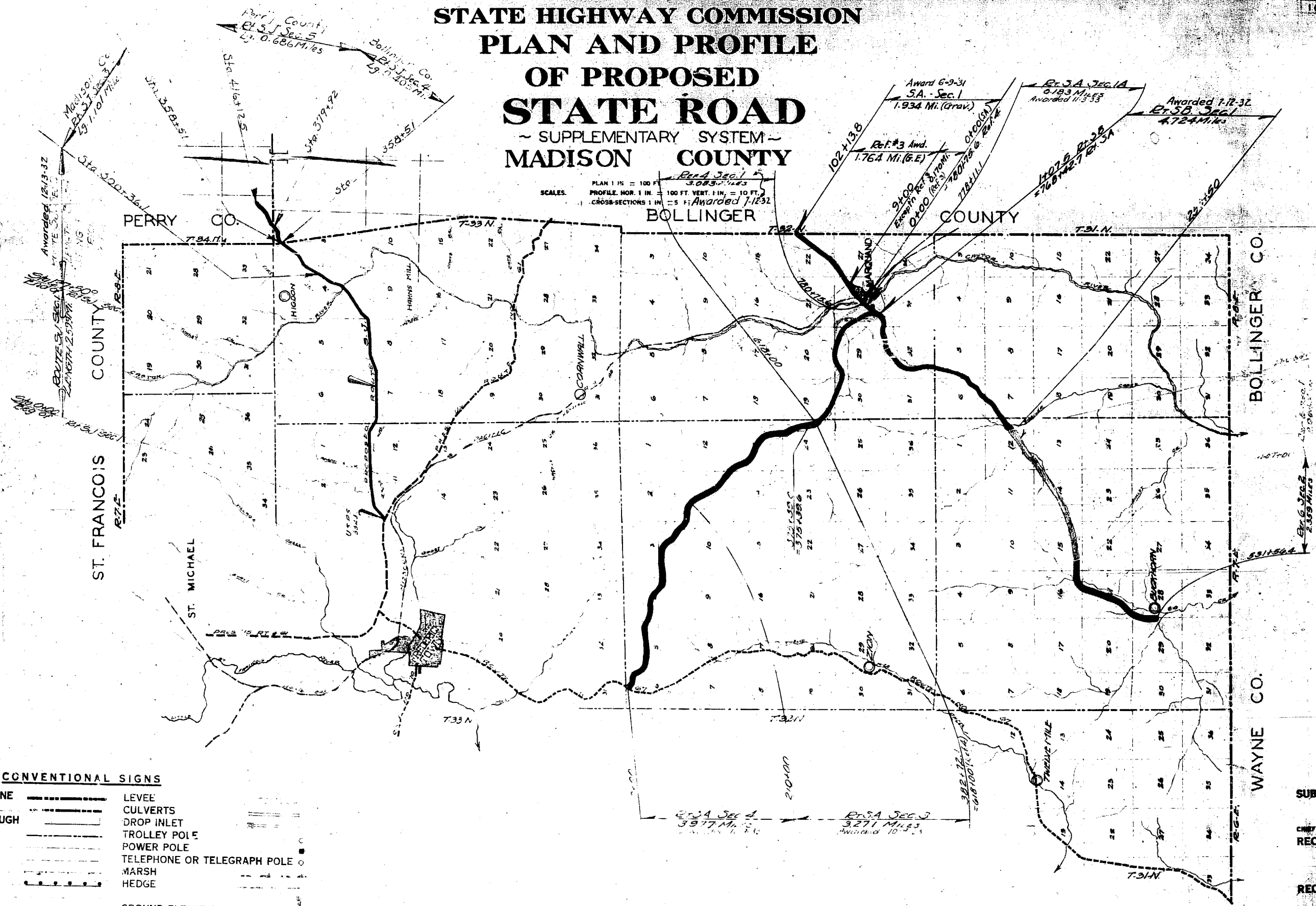
374

MISSOURI STATE HIGHWAY COMMISSION PLAN AND PROFILE OF PROPOSED STATE ROAD ~ SUPPLEMENTARY SYSTEM ~ MADISON COUNTY

ROAD NO. 10
 COUNTY MADISON
 DISTRICT NO. 5
 SECTION 1
 S.A. 1234
 REF. #3
 REF. #4
 S.A. 1A
 REF. #5, 12

E-3-3

PLAN 1 IN. = 100 FT.
 PROFILE, HOR. 1 IN. = 100 FT. VERT. 1 IN. = 10 FT.
 CROSS-SECTIONS 1 IN. = 5 FT.



CONVENTIONAL SIGNS

STATE AND NATIONAL LINE	-----	LEVEE	-----
CITY LINE	-----	CULVERTS	-----
CITY VILLAGE OR BOROUGH	-----	DROP INLET	-----
TOWNSHIP LINE	-----	TROLLEY POLE	-----
SECTION LINE	-----	POWER POLE	-----
GRANT LINE	-----	TELEPHONE OR TELEGRAPH POLE	-----
FENCE LINE	-----	MARSH	-----
GUARD RAIL	-----	HEDGE	-----
UNFENCED PROPERTY	-----		
RIGHT OF WAY LINE	-----	GROUND ELEVATION	-----
TRAVELED WAY	-----	GRADE ELEVATION	-----
RAILROADS	-----	SURFACE LINE	-----
RETAINING WALL	-----	GRADE LINE	-----
BASE OR SURVEY LINE	-----		

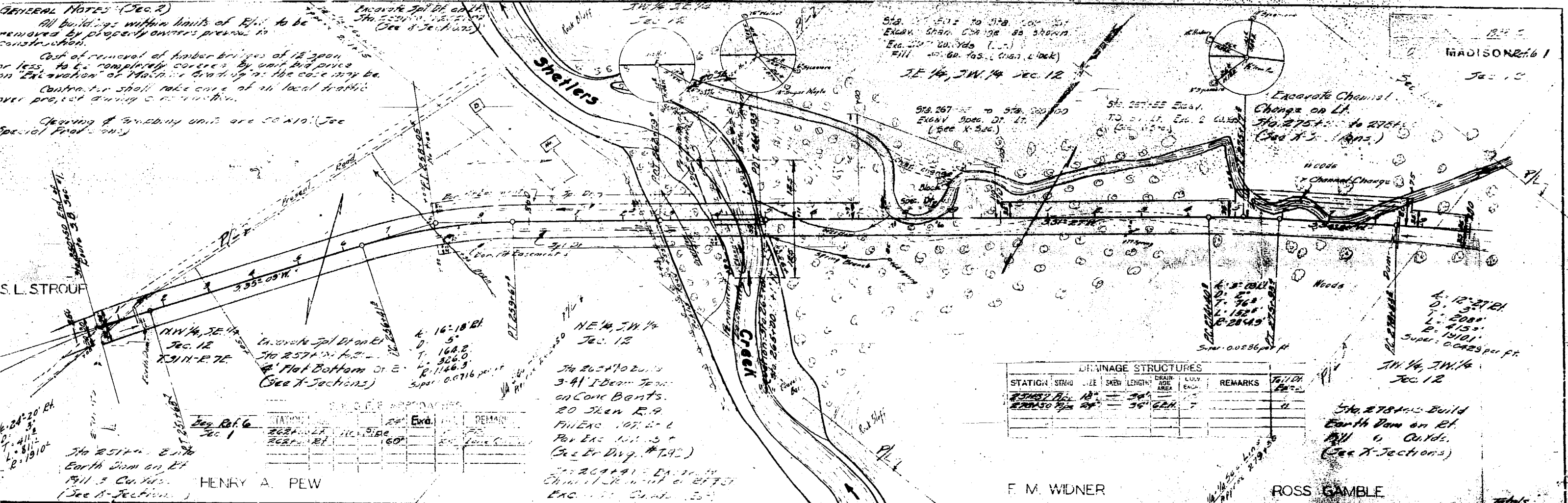
SUBMITTED
 [Redacted]
 CHIEF ENGINEER NO. STATE HIGHWAY COMMISSION
 RECOMMENDED FOR APPROVAL
 [Redacted]
 DISTRICT NO. 5
 RECOMMENDED FOR APPROVAL
 [Redacted]
 CHIEF ENGINEER BUREAU OF PUBLIC ROADS
 APPROVED
 [Redacted]
 DIRECTOR BUREAU OF PUBLIC ROADS

GENERAL NOTES (Sec. 2)

All buildings within limits of E.P. to be removed by property owners previous to construction.
 Cost of removal of timber bridges of 12 span or less to be completely covered by unit bid price in "Excavation" of Madison's Contract; the case may be.
 Contractor shall take care of all local traffic over project during construction.
 Clearing of scrubby units are 50' x 10'. (See Special Provisions)

S. L. STROUP

MADISON #16
 Sec. 12

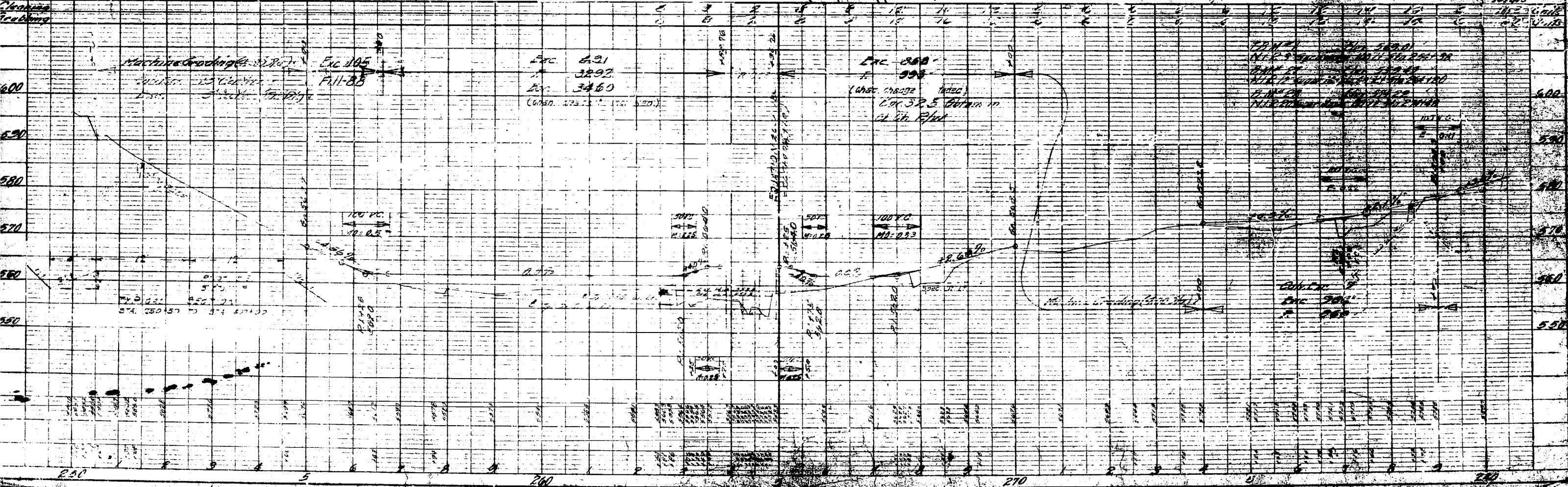


STATION	STRUT	SIZE	SPREW	LENGTH	DRAIN HOE AREA	LUVY EACH	REMARKS	TAIL CH. FEET
275+00	18"	36"	24"	24'	624	7		
275+50	18"	36"	24"	24'	624	7		

HENRY A. PEW

F. M. WIDNER

ROSS GAMBLE



TOTALS
 1123
 22' 11"

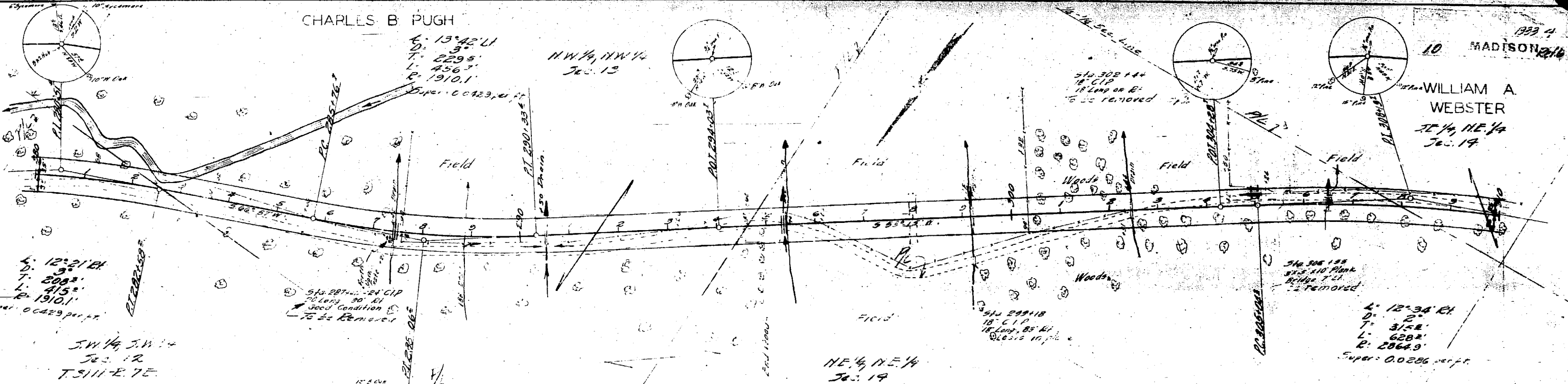
CHARLES B PUGH

E. 13°42' Lt
D. 5'
T. 2295'
L. 456'
R. 1910.1

NW 1/4, NW 1/4
Sec. 12

10 MADISON 1894

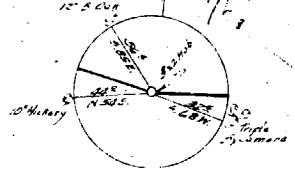
WILLIAM A WEBSTER
SE 1/4, NE 1/4
Sec. 19



E. 12°21' Lt
D. 3'
T. 2082'
L. 415'
R. 1910.1

SW 1/4, SW 1/4
Sec. 12
T. 9111 E. 7 E.

Sta. 287+00 - 24' CIP
18' Long, 30' Ht
Good Condition
To be removed

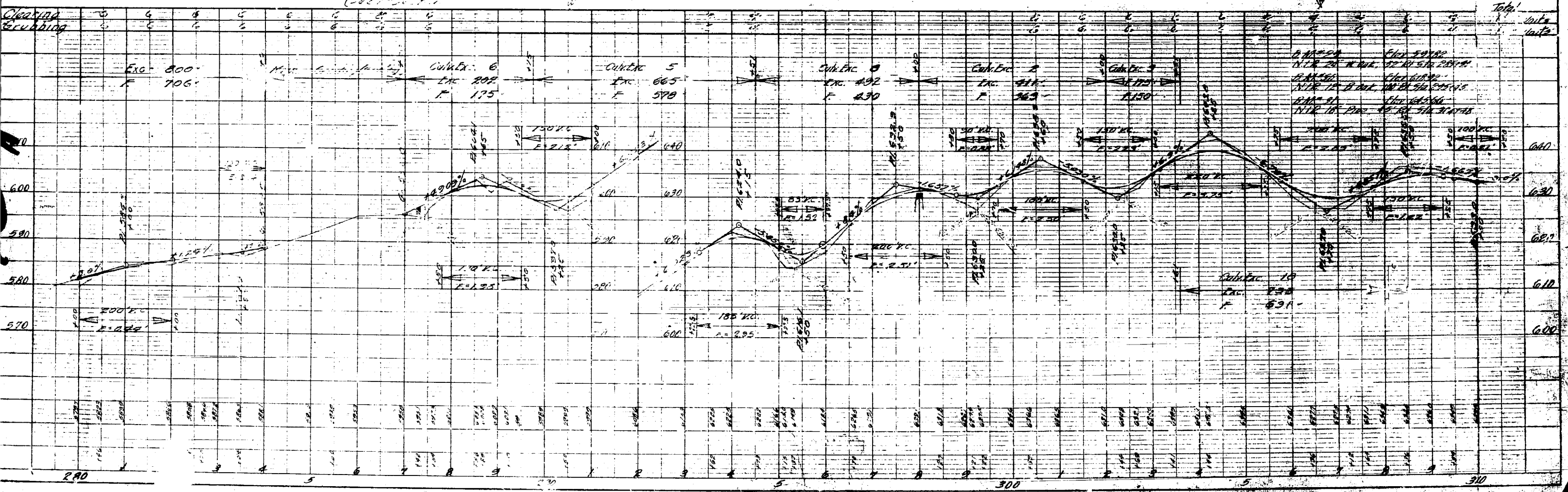


Sta. 287+00
Earth Dam
To be removed

STATION	15'	Exc.	MARKS
305+00 Lt		10	10
298+00 Lt		10	10

DRAINAGE STRUCTURES						
STATION	SIZE	NEW LENGTH	DRAIN AREA	CURV. TO CAV.	REMARKS	TO BE REMOVED
287+00	24"	60'	1550'	5'	Double	
287+00	24"	60'	1550'	5'	Double	
290+50	24"	24'	74'	5'	Double	
295+27	24"	60'	184'	5'	Double	
295+27	18"	30'	347'	15'		
302+42	18"	34'	314'	15'		
306+50	24"	28'	251'	10'		

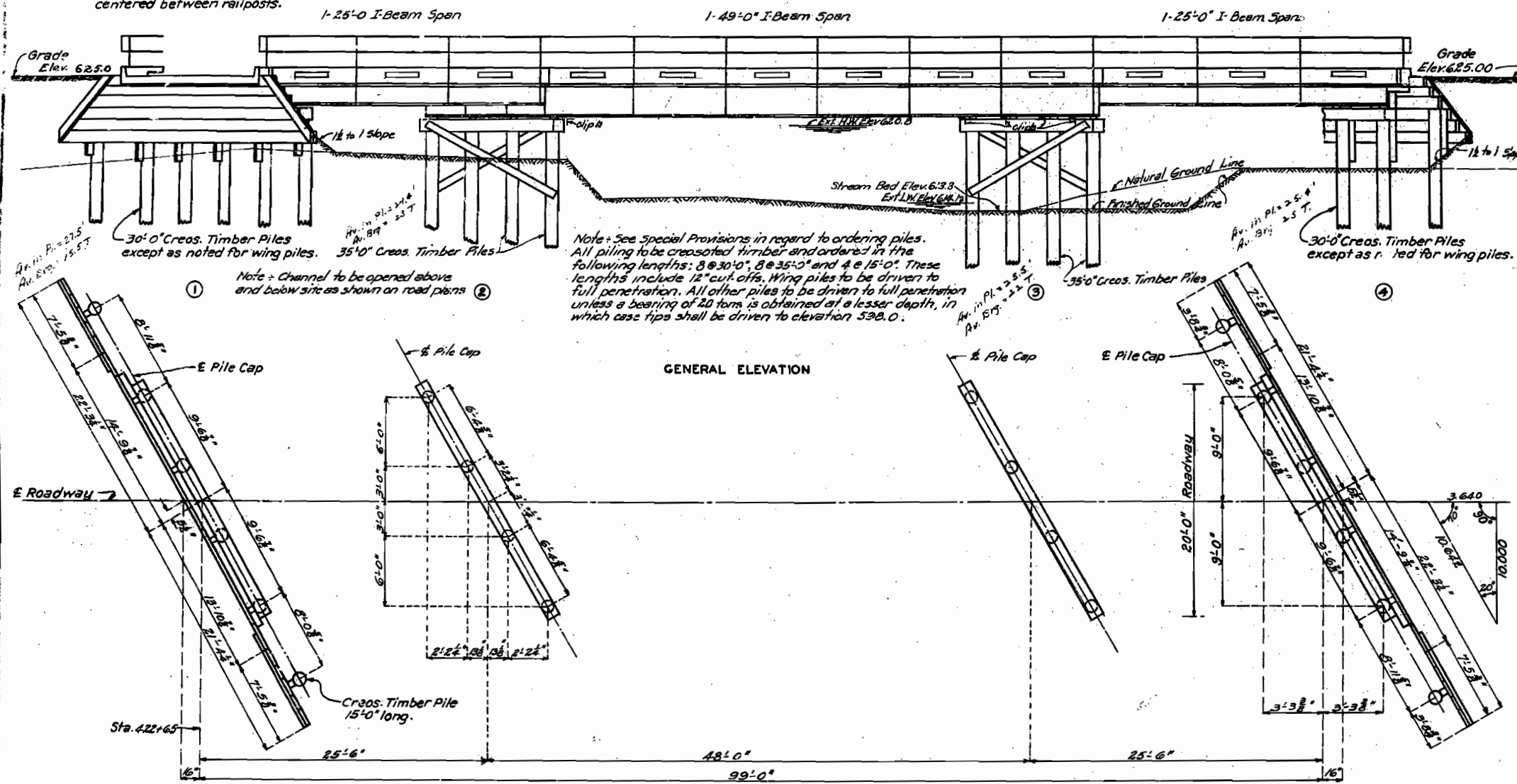
ROSS GAMBLE



MISSOURI STATE HIGHWAY DEPARTMENT

FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
5	MO.	22-23	19	1	3

Note: Outlets in curbs to be centered between railposts.



BILL OF REINFORCING STEEL FOR SUPERSTRUCTURE					
No.	Size	Length	Location	Bending Sketch and Cutting Diagram	
16	5/8"	26'-0"	C1		
158	5/8"	2'-0"	C2		
16	5/8"	25'-6"	C3		
344	5/8"	21'-0"	S1		
52	5/8"	26'-0"	S2		
90	5/8"	22'-3"	S3		
12	5/8"	22'-3"	S4		
52	5/8"	25'-3"	S5		

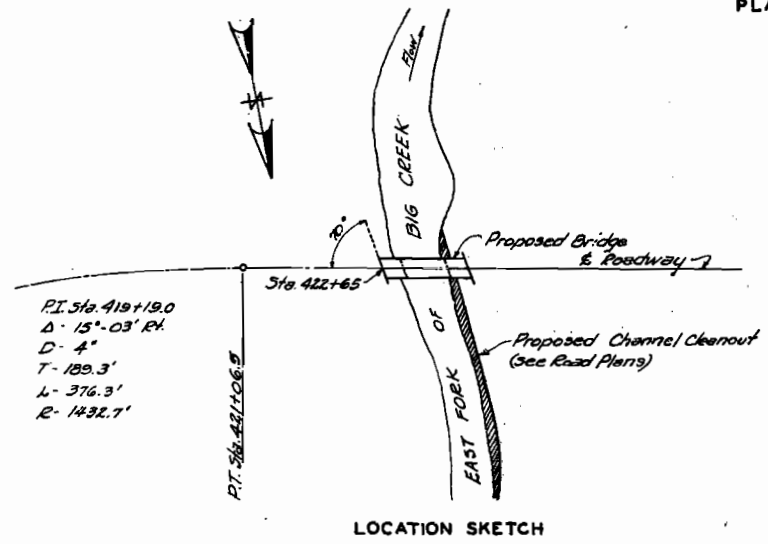
GENERAL NOTES:

All concrete to be 1:2:3 1/2 mix, Class "X". Exposed edges to be beveled 3/4" where no other bevel is noted. All timber to be creosoted Close Grained Structural Douglas Fir of the West Coast Region; Structural Square Edge and Sound Long Leaf Southern Yellow Pine; Dense Structural Square Edge and Sound Short Leaf Southern Yellow Pine; or untreated Prime Structural California Redwood. All timber rough full-sawn except as noted in timber bill for pile caps. Slight variations in sawing to be in accordance with grading rules. All treated timber to be cut to lengths, shaped and bored as shown before treating. Backing plank are all billed 6' long and are to be fitted and cut in the field. Payment will be based on quantities of material in finished structure. Field holes for drift pins shall be field bored 3/4". Unless otherwise noted, all other field holes in timber shall be field bored 3/4". When bolts with countersunk heads are indicated on plans, cut washers shall be used under heads. O.G. washers shall be used under heads of all other bolts and under nuts of all bolts. Cost of substructure hardware to be included in price bid for timber in place. I-Beams with fastenings, spacers, handrail, handrail posts with fastenings, clip angles, and cap plate on end bents with fastenings, will be paid for as structural steel. Cost of metallic edge moulding will be included in price bid for concrete. Detail shop drawings shall be submitted to the State Highway Department in duplicate and shall be approved before steel is fabricated. Where rubber compound is specified on plans for use in partition and expansion joints, the pre-moulded joint shall be securely stitched to one face of concrete with copper wire. Paint: Shop, none; Field, contact surfaces of bolted field connections one coat red lead and surfaces inaccessible after erection three coats of red lead. No other paint to be applied by contractor. Red lead required shall be furnished by the contractor. See Special Provisions. See Special Provisions in regard to permissible beam substitutions and basis of payment. Protection caps to be placed on heads of all piles of pile bents in accordance with Section 23-6 of Standard Specifications issued April 1, 1930. Rivets 3/4" holes 1 1/2", except in handrail where rivets shall be 5/8" holes 1 1/2". Field connections for handrail channels shall be 5/8" button head bolts and for connection of rail to railposts shall be 5/8" bolts, holes 1 1/2". All other field connections riveted except as noted.

Bridge excavation will be allowed for all bents within horizontal limits shown and noted on these design plans, sheet #2. This excavation will be computed from existing ground line to bottom end of 6"x6" backing supports for end bents and to bottom of sway bracing for interior bents.

FINAL QUAN.	ESTIMATED QUANTITIES		
	ITEM	SUPERSTR.	SUBSTR. TOTAL
15.5	Excavation Class 1 Cu. Yds.		10
	Excavation Class 2 Cu. Yds.		10
	Concrete 1:2:4 mix, Class "B" Cu. Yds.		
50.6	Concrete 1:2:3 1/2 mix, Class "X" Cu. Yds.	50.6	50.6
43020	Fabricated Structural Steel Lbs.	43000	43000
12770	Reinforcing Steel Lbs.	12770	12770
551	Creosoted Timber Piles Lin. Ft.	622	622
96	Creos. Timber Pile Cut-offs Lin. Ft.	20	20
2500	Timber (See Special Provisions) FBM	2500	2500

Note: All bridge excavation will be paid for as Class I Bridge Excavation. Estimated quantities for creosoted timber piles, except wing piles, include 4 lineal feet per pile as allowance for metal shoes in accordance with specifications.



P.I. Sta. 419+19.0
 Δ - 15'-03" RH
 D - 4"
 T - 189.3'
 L - 376.3'
 R - 1432.7'

BRIDGE OVER EAST FORK OF BIG CREEK

STATE ROAD FROM MARQUAND TO BUCKHORN
 ABOUT 8.5 MILES S.W. OF MARQUAND
 PROJECT NO. REF. 66SB-52 STA. 422 + 65
 MADISON COUNTY
 SUBMITTED BY: *N.R. Fox* DATE: 2/16/34
 APPROVED BY: *T.H. Cutler* DATE: 2/16/34

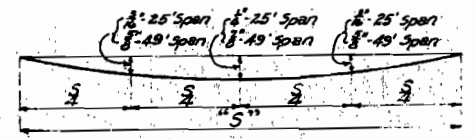
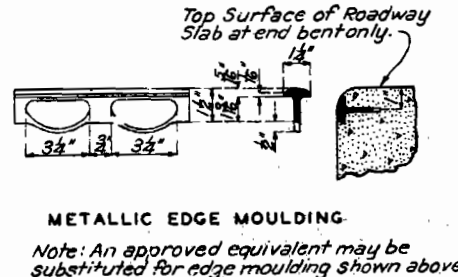
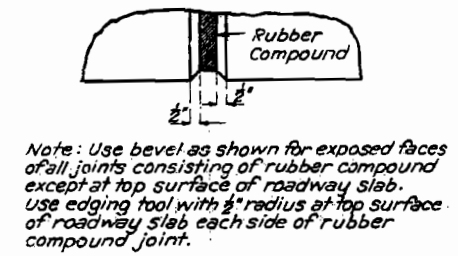
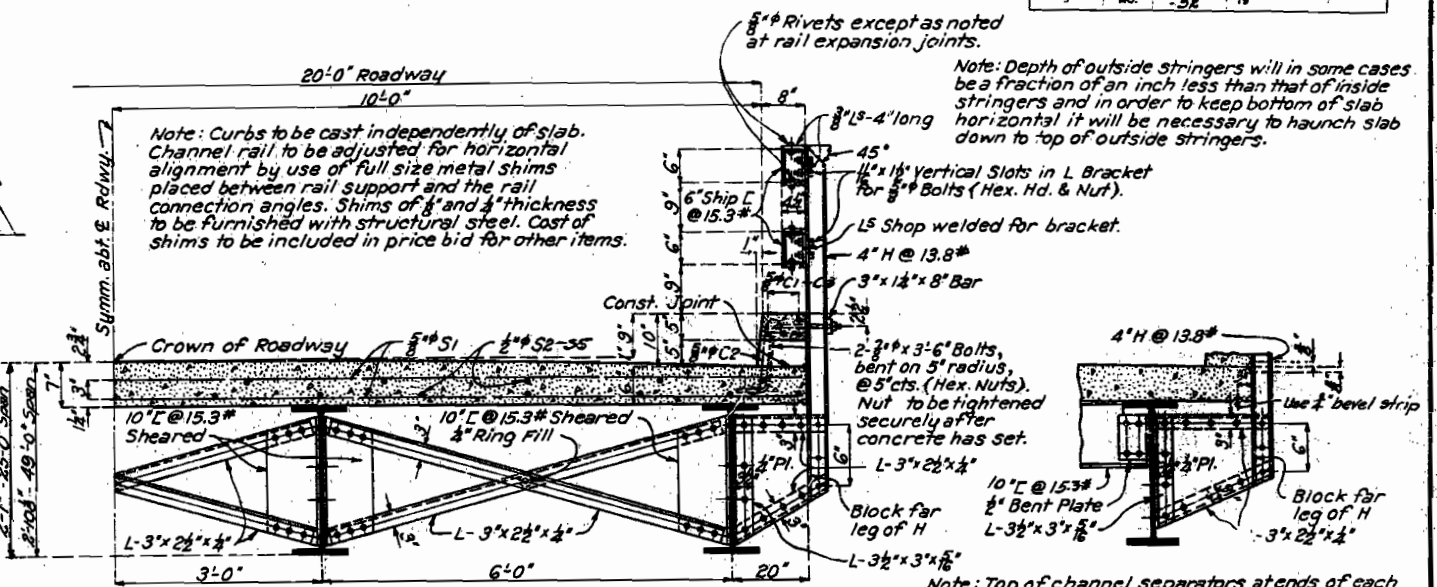
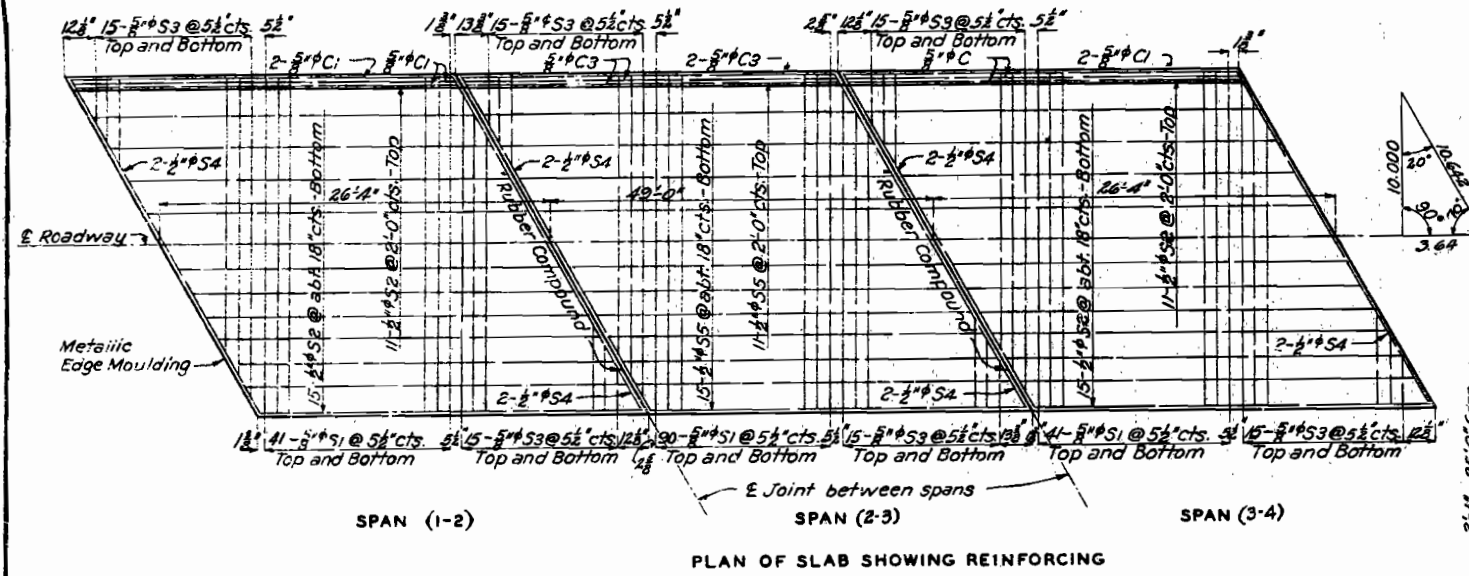
Drawn Aug. 1933 By R.J.G.
 Traced Aug. 1933 By R.J.G. Assembled Feb. 1934 By H.A. & J.H.M.
 Checked Aug. 1935 By M.M.R. Checked Feb. 1934 By L.K.S.

Note: This drawing is not to scale. Follow dimensions. Sheet No. 1 of 3.

MISSOURI STATE HIGHWAY DEPARTMENT

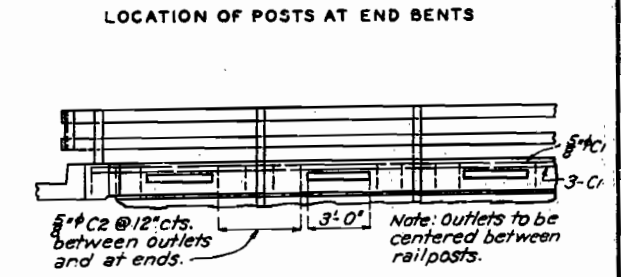
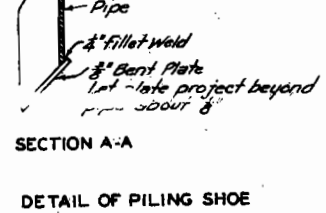
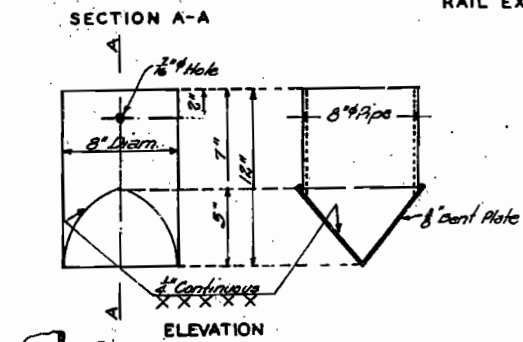
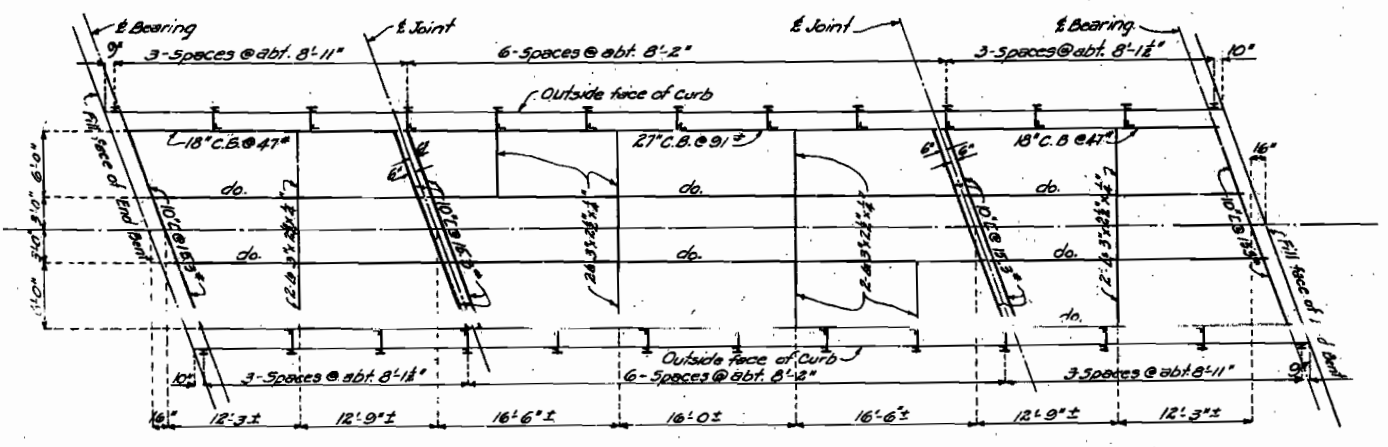
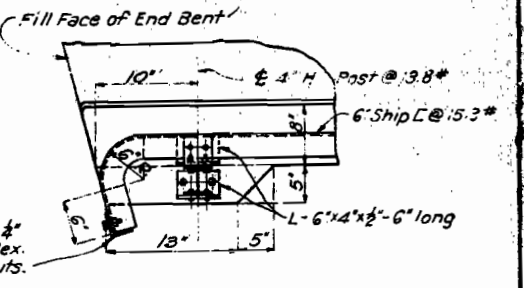
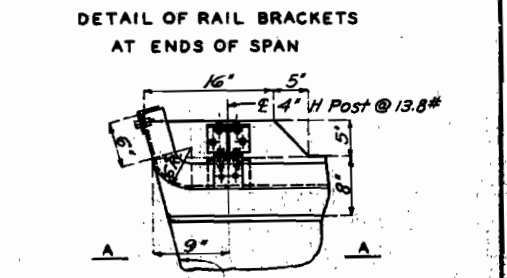
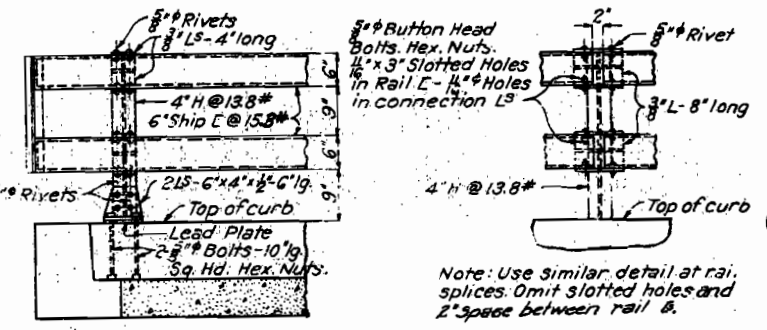
FED. ROAD DIST. NO.	STATE NO.	FED. AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
5	MO.	-52	19	3	3

Note: Space dowel bars "C2" at approximately 12" cts. in curbs between outlets and at ends.



Note: Floor slab to be brought to grade and dead load deflection taken care of by increasing slab thickness. Depth of slab at outside face of curb to be kept uniform and bottom surface of slab warped between curb and outside beam to obtain required thickness at beam. Payment will be allowed for additional concrete required for thickening slab. This additional concrete is included in "Estimated Quantities".

Span	STANDARD I-BEAMS		BETHLEHEM BEAMS	
	Inside	Outside	Inside	Outside
25'-0"	13" x 5.7"	13" x 6.47"	13" x 4.7"	13" x 4.7"
49'-0"	24" x 15"	24" x 10.59"	26" x 9.1"	26" x 9.1"



BRIDGE OVER EAST FORK OF BIG CREEK

STATE ROAD FROM MARQUAND TO BUCKHORN
ABOUT 8.5 MILES S.W. OF MARQUAND
PROJECT NO. REF#6(SB)-S2 STA. 422 +65

MADISON COUNTY

376

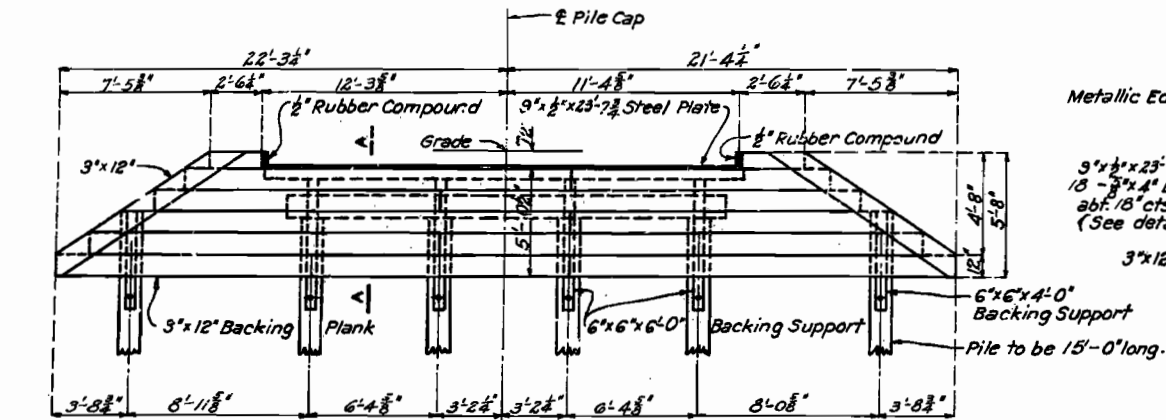
Drawn Aug. 1933 By R.J.G.
Traced Aug. 1933 By R.J.G. Assembled Feb. 1934 By H.A.A.-J.H.M.
Checked Aug. 1933 By N.M.R. Checked Feb. 1934 By L.A.S.

Note: This drawing is not to scale. Follow dimensions.

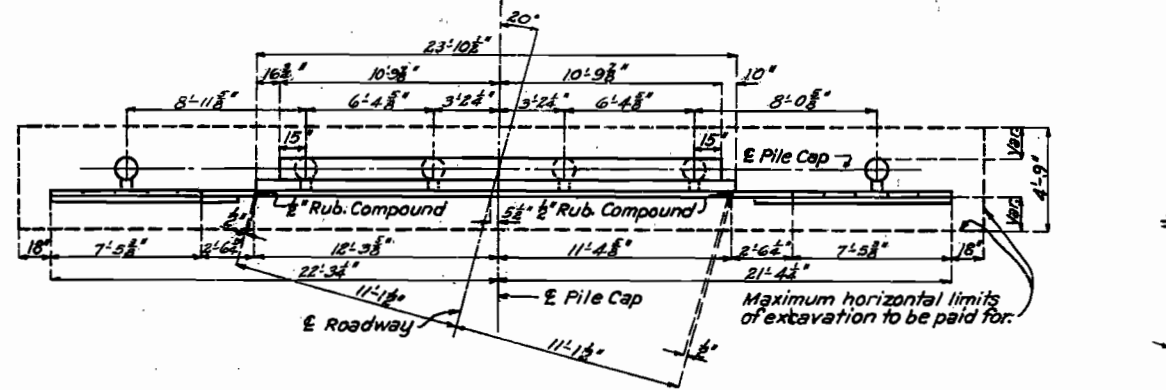
Sheet No. 2 of 3.

MISSOURI STATE HIGHWAY DEPARTMENT

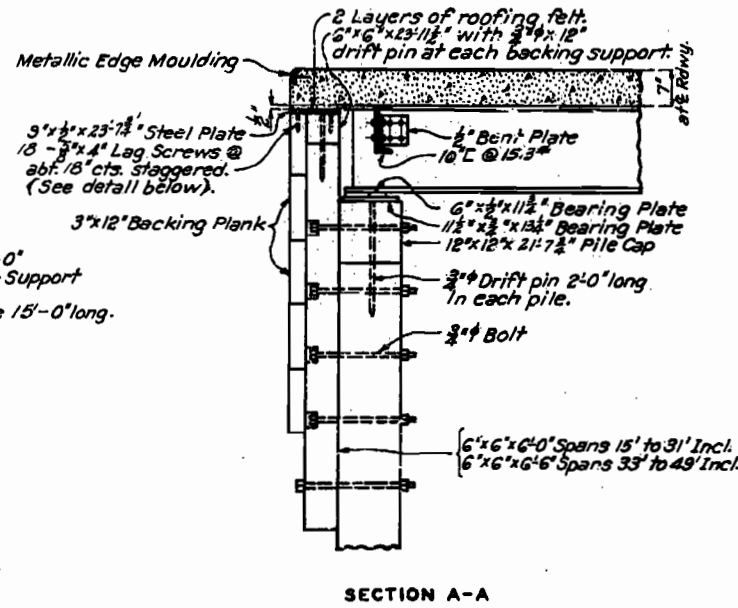
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1	MO.	227615B	19		



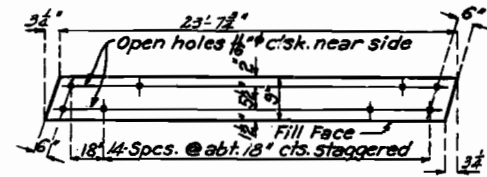
ELEVATION OF END BENT



PLAN OF END BENT



SECTION A-A

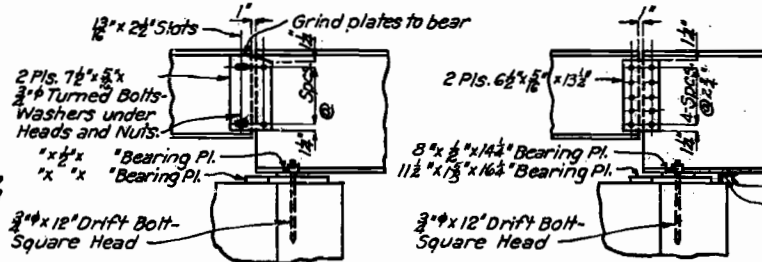


END BENT CAP PLATE

Note: Any irregularity in alignment of piling in end bents to be corrected by facing one surface of the 6"x6" backing support so as to place the surface of the backing in a true plane and eliminate any strain in the backing plank. Splice in backing plank to be made at center of 6"x8" backing support and to be alternated on the intermediate supports.

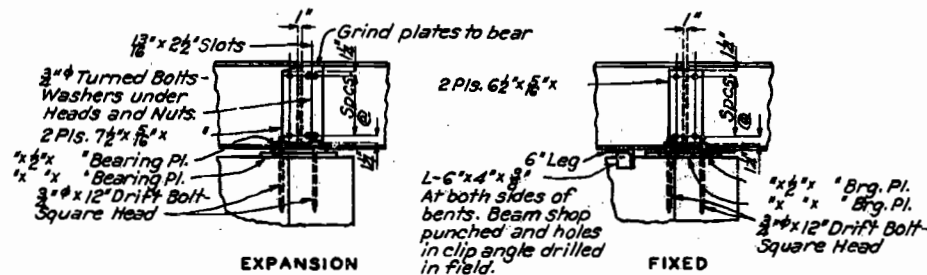
NAILING SCHEDULE:

Backing plank to supports; 3-60d at each support and at splices, 3-60d each side of splice. Pieces at ends of backwall to backing plank; 4-60d to each backing plank.

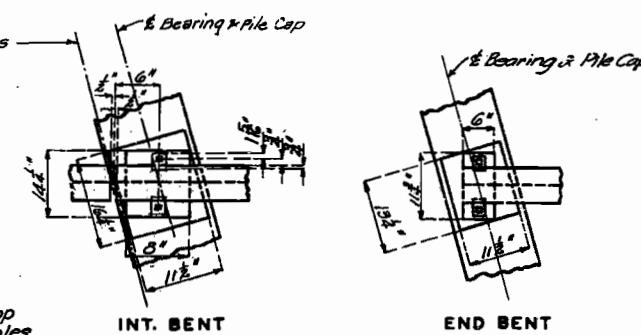


EXPANSION

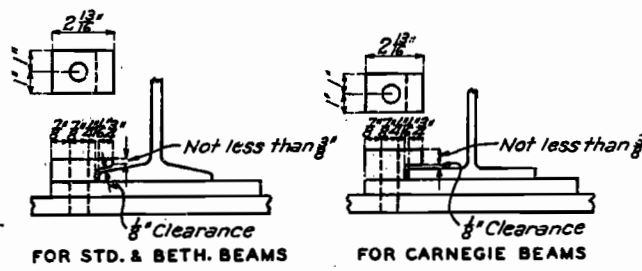
FIXED



TYPICAL JOINTS OVER INT. BENTS

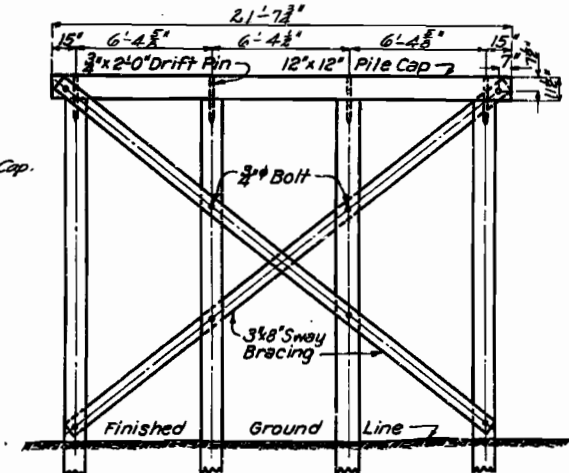


PLAN OF BEARING PLATES



DETAILS OF FLANGE CLAMPS

Note: Cast iron clamps used on bearing plates to have 1/2" clearance at flange of beam to allow for expansion. All clamps to have 3/8" bored holes. Use two clamps only on each I-Beam at pile caps on end bents.



DETAIL OF INTERIOR BENT

Note: Omit sway bracing when distance from bottom of pile cap to ground is less than 5'-0". Carry sway bracing down to approximately Elev. 64.30. Excavation will be allowed for interior bents within the maximum horizontal limits of 4'-9" in width and 21'-7 1/2" in length.

SUBSTRUCTURE TIMBER BILL					
PIECE	NO. PCS	SIZE	LENGTH	REMARKS	SHAPING & BORING SKETCHES
Backing Plank	2	3"x12"	18'-8"	Cut to length.	
"	2	3"x12"	25'-11 1/2"	"	
"	2	3"x12"	23'-5 1/2"	"	
"	2	3"x12"	17'-11 1/2"	"	
"	2	3"x12"	15'-5 1/2"	"	
"	2	3"x12"	22'-9 1/2"	"	
"	2	3"x12"	20'-2 1/2"	"	
"	2	3"x12"	14'-9 1/2"	"	
"	2	3"x12"	12'-3 1/2"	"	
"	2	3"x12"	19'-7"	"	
Edge Support	4	3"x12"	10'-11"	Cut to length & shape.	
Shoulder Plank	2	3"x8"	2'-7 1/2"	"	
"	2	3"x8"	2'-6 1/2"	"	
Backing Support	8	6"x6"	6'-0"	"	
"	4	6"x6"	4'-0"	"	
Back. Support Cap	2	6"x6"	23'-10 1/2"	Cut to length.	
Pile Cap	4	12"x12"	21'-7 1/2"	"	
Sway Bracing	4	3"x8"	4'-0"	"	

* S2S to exactly 1 1/4" depth. Note: Pile caps to be classified as "Beams and Stringers". All other timber to be classified as "Joists and Plank".

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Drawn Aug. 1933 By R.J.G. Assembled Feb. 1934 By H.B.A. - J.M.M.
Traced Aug. 1933 By R.J.G. Checked Feb. 1934 By L.K.S.
Checked Aug. 1933 By N.M.R.

Note: This drawing is not to scale. Follow dimensions.

Sheet No. 3 of 3.

BRIDGE OVER EAST FORK OF BIG CREEK

STATE ROAD FROM MARQUAND TO BUCKHORN
ABOUT 8.5 MILES S.W. OF MARQUAND

PROJECT NO. REF. 6(SB)-52 STA. 422 + 65

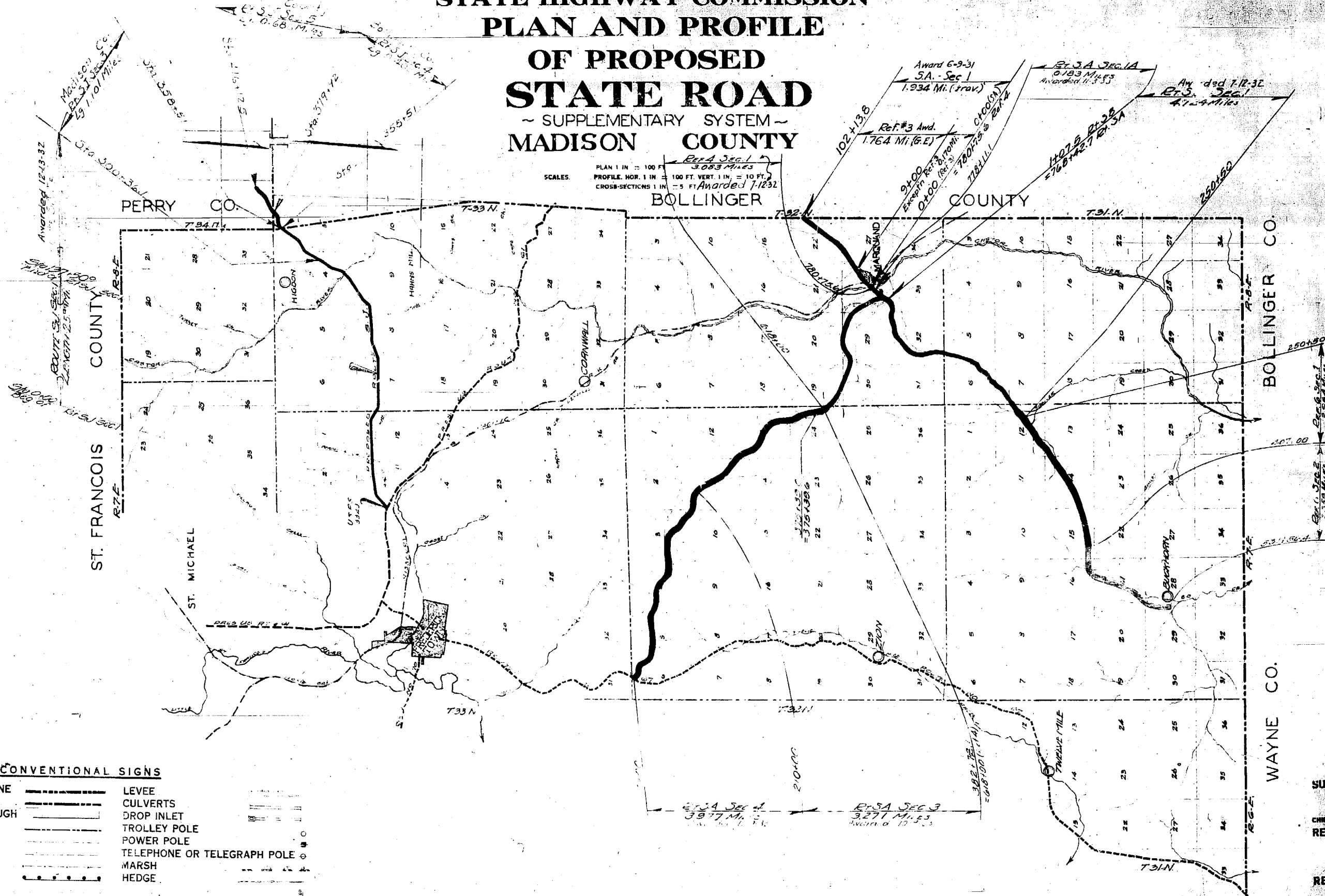
MADISON COUNTY

T-93

MISSOURI STATE HIGHWAY COMMISSION PLAN AND PROFILE OF PROPOSED STATE ROAD ~ SUPPLEMENTARY SYSTEM ~ MADISON COUNTY

E-3-3
S.A. #34
Ref #3
Ref #4
Ref #5/2

PLAN 1 IN = 100 FT
PROFILE HOR. 1 IN = 100 FT. VERT. 1 IN = 10 FT.
CROSS-SECTIONS 1 IN = 5 FT



135

CONVENTIONAL SIGNS

STATE AND NATIONAL LINE		LEVEE	
COUNTY LINE		CULVERTS	
CITY, VILLAGE OR BOROUGH		DROP INLET	
TOWNSHIP LINE		TROLLEY POLE	
SECTION LINE		POWER POLE	
GRANT LINE		TELEPHONE OR TELEGRAPH POLE	
FENCE LINE		MARSH	
GUARD RAIL		HEDGE	
UNFENCED PROPERTY		GROUND ELEVATION	
RIGHT OF WAY LINE		GRADE ELEVATION	
TRAVELED WAY		SURFACE LINE	
RAILROADS		GRADE LINE	
RETAINING WALL			
BASE OR SURVEY LINE			

SUBMITTED _____
 CHIEF ENGINEER NO. STATE HIGHWAY COMMISSION
 RECOMMENDED FOR APPROVAL _____
 DISTRICT ENGINEER - DISTRICT NO. 5
 RECOMMENDED FOR APPROVAL _____
 CHIEF ENGINEER BUREAU PUBLIC ROADS
 APPROVED _____
 DIRECTOR BUREAU OF PUBLIC ROAD

ELI TRIPP ET JX

SE 1/4, SW 1/4
Sec. 15

SW 1/4, SW 1/4
Sec. 15
T. 31 N. - R. 7 E.

FEDERAL ROAD DIST. NO. 5 NO. 1935 E7
10 MILITARY DIST. 1-2

Field

Field

SE 1/4, SE 1/4
Sec. 16

Field
7/11

J. L. HOVIS

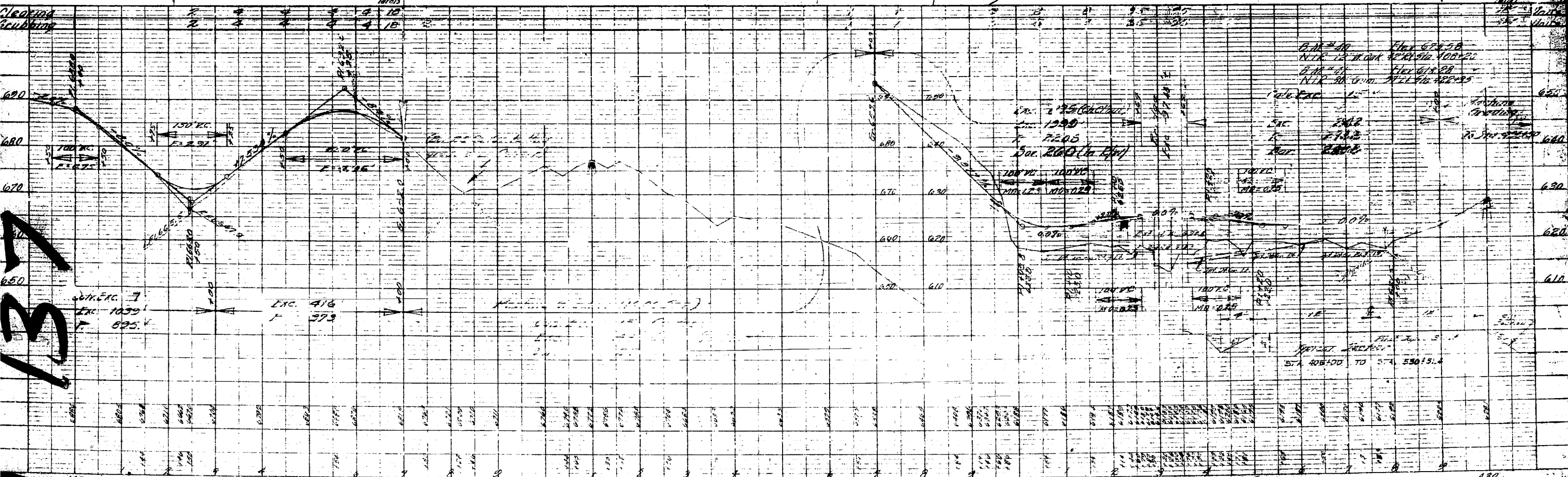
GENERAL NOTES - (Sec. 3)
All buildings within limits of R/W to be removed by property owners previous to construction.
Contractor shall take care of all law & traffic over project during construction.

STATION	STA.	15'	24'	Exc.	FILL	P. MARKS
412+50	41	20	20	50	50	15
414+50	42	20	20	50	50	15
416+50	43	20	20	50	50	15
418+50	44	20	20	50	50	15
420+50	45	20	20	50	50	15

STATION	STA.	15'	24'	Exc.	FILL	P. MARKS
422+50	46	20	20	50	50	15
424+50	47	20	20	50	50	15
426+50	48	20	20	50	50	15
428+50	49	20	20	50	50	15
430+50	50	20	20	50	50	15

End Ref. 6
Sec. 1

Top Ref. 6
Sec. 2 - W R HOVIS



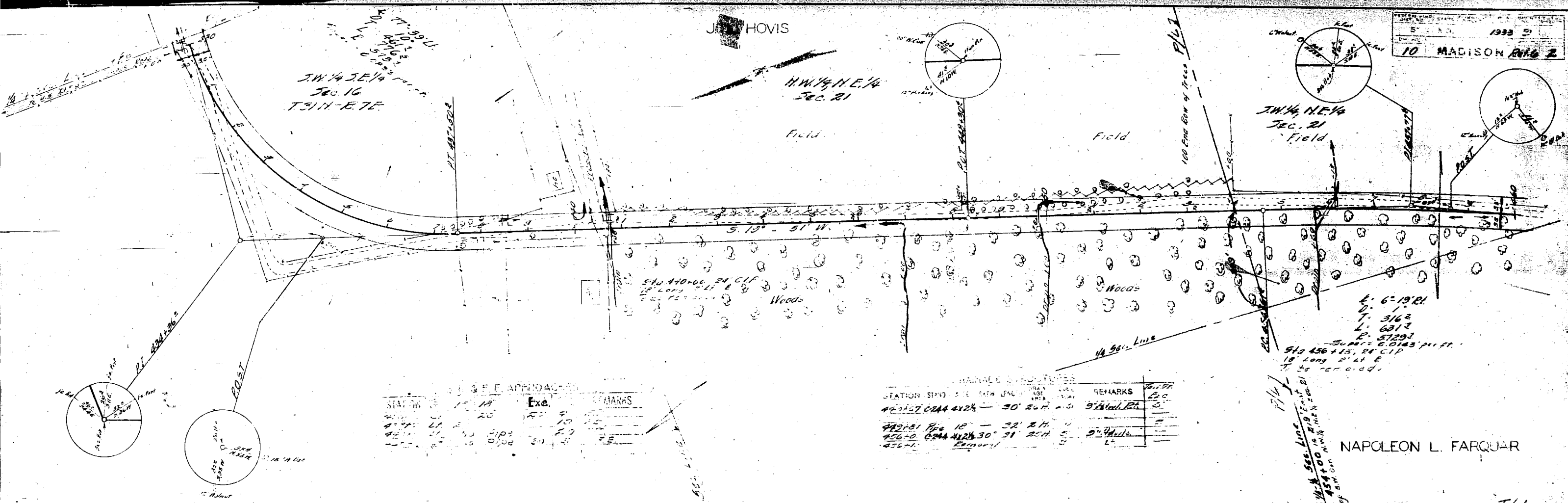
B

EAST FORK OF BIG CREEK

Sta 422+65 Build
2-25' x 1-49' I-Beam Span
on Creo. P. & Bents.
20' Span E.P.
Fill Exc. 97'48"±
Rev. Exc. 97'48"±
(See Br. Div. #E93)

Excavate 7/11 on Lt.
Sta. 423+70 to 424+15
Sta. 427+00 to 428+25
(See X-Sections.)
Exc. 3.75 Cu. Yds. (Est.)

Total
Exc. 1039
Fill 895



STATION APPROACH

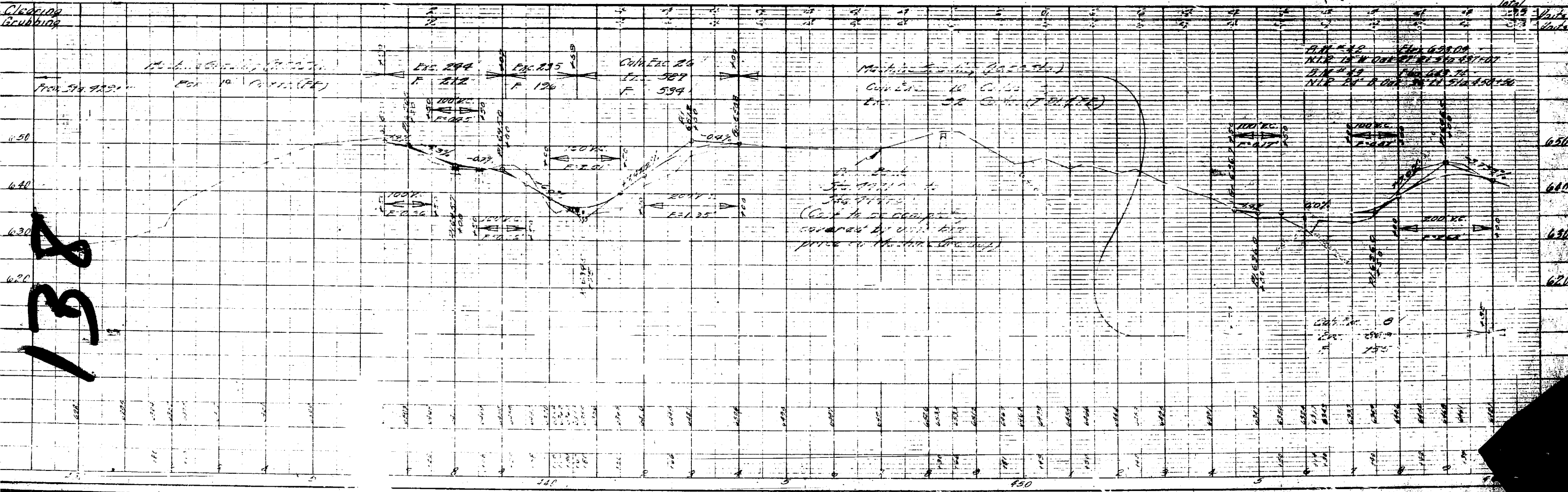
STATION	APPROACH	REMARKS
1	18	Exe.
2	20	10
3	20	20
4	20	30

STATION SPWD

STATION	SPWD	REMARKS
42257	0244 4225	50' 20" 20' 5' 9' 10" 20'
42258	100 10	32' 20" 10'
42259	0244 4225 30 31	20' 10' 5'
42260	0244 4225 30 31	20' 10' 5'

L. 6:19' 21"
T. 3162
P. 6312
R. 57293
Superc. 0.0143 per ft.
Sta. 436 + 15, 24' CIP
10' Long 2' 4" E
7' 50" recorded.

NAPOLEON L. FARQUAR



138

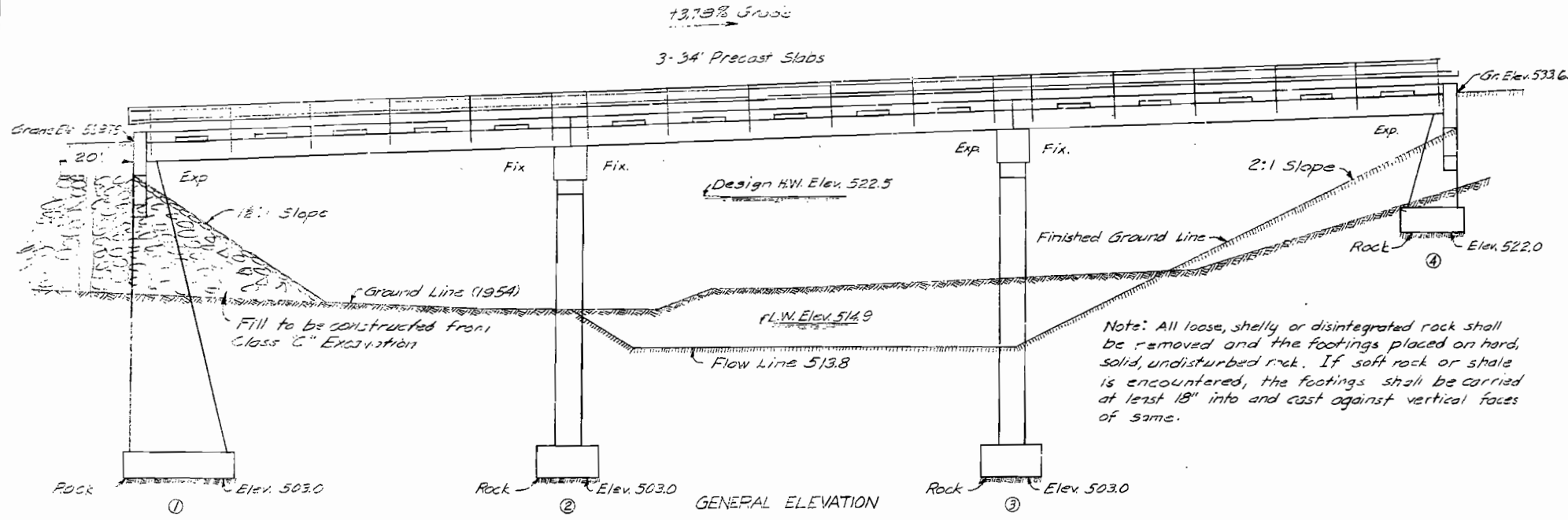
Total
3522
333

Exc. 294
Exc. 295
Culvert 20
Exc. 296

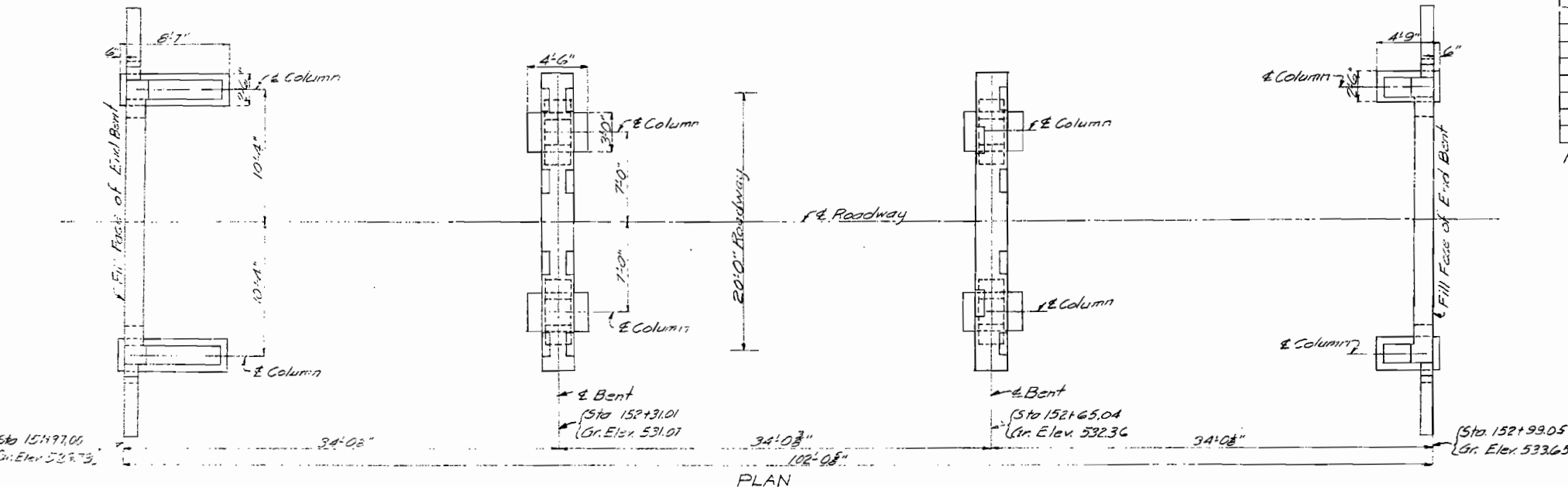
Exc. 297
Exc. 298

MISSOURI STATE HIGHWAY DEPARTMENT

FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
5	MO.	5-171(2)B	19		



Note: All loose, shelly or disintegrated rock shall be removed and the footings placed on hard, solid, undisturbed rock. If soft rock or shale is encountered, the footings shall be carried at least 18" into and cast against vertical faces of same.



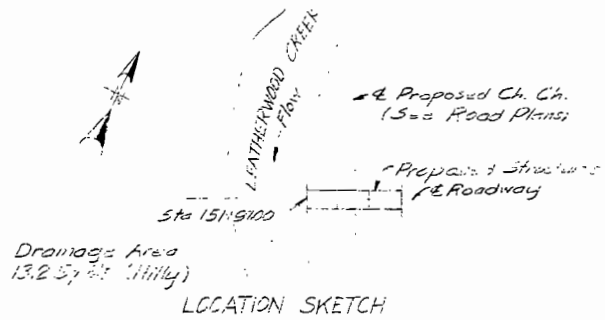
BILL OF REINFORCING STEEL - SUBSTRUCTURE				Bending Sketches and Cutting Diagrams	
No.	Size	Length	Mark	Location	
End Bents No. 1 and 4					
14	#6	5'3"	D1	Footing B1	
16	#6	7'3"	F1	Haunch	
10	#6	21'9"	H1	Beam	
8	#11	24'3"	H2	"	
4	#5	21'9"	H3	"	
4	#4	21'9"	H4	"	
16	#3	11'6"	H5	Wing	
4	#5	11'0"	H6	"	
8	#6	11'6"	T1	"	
12	#6	10'9"	T2	"	
44	#4	9'6"	U1	Beam	
10	#4	7'9"	V1	Wing	
8	#6	22'6"	V2	Col. Bt. #1	
6	#6	23'6"	V3	"	
12	#4	19'9"	V4	"	
38	#3	14'9"	V5	"	
8	#6	10'0"	V6	Col. Bt. #4	
6	#6	10'6"	V7	"	
4	#4	4'6"	V8	"	
6	#3	10'9"	V9	"	
Int. Bents No. 2 and 3					
32	#6	5'9"	D2	Footing	
32	#6	8'0"	F2	Haunch	
20	#7	25'0"	G1	Beam	
4	#6	22'9"	G2	"	
82	#3	7'9"	P1	Col. Bt. #2	
16	#6	23'3"	V10	Col. Bt. #2	
16	#6	24'0"	V11	Col. Bt. #3	
46	#4	9'9"	U2	Beam	

Note: For Bill of Reinforcing Steel-Superstructure see Sheet No. 4 of A.

ESTIMATED QUANTITIES			
Item	Substr.	Superstr.	Total
Class 1 Excavation for Structures	Cu. Yds.	30	30
Class 2 Excavation for Structures	Cu. Yds.	149	149
*Class "A" or Lightweight Concrete	Cu. Yds.		639
Class "B" Concrete	Cu. Yds.	60.3	636
Fabricated Structural Steel	Lbs.	1800	1800
Reinforcing Steel	Lbs.	7300	30,550
Asphalt Wearing Surface	Sq. Yds.		227
Metal Guard Rail	Lin. Ft.	206	206

Note: Excavation for bridge made above Elev. 5160 will be paid for as Class 1 Excavation for Structures.
Excavation for bridge made below Elev. 5160 will be paid for as Class 2 Excavation for Structures.
* See Special Provisions.

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GENERAL NOTES:
 Design Specifications A.A.S.H.O. 1953
 Loading H10-44
 Reinforcing Steel Stress 18,000%
 Class "B" Concrete Stress 1000%
 Class "A" Concrete Stress 1500%
 All substructure concrete and curbs to be Class "B".
 All precast superstructure units shall be Class "A" or lightweight concrete. See Special Provisions.
 Where joint filler is specified on the plans it shall conform with the requirements for Prepacked Material for Filler as given in Section 713 A(1)(h) of the Standard Specifications.
 Fabricated Structural Steel Superstructure in side guard rail posts, bolts fastening these posts to concrete and bolts and washers fastening precast sections together.
 Cost of cement marker used in assembling precast units to be included in unit price bid for precast units in place.
 Paint Shop men; Field contact surfaces of bolted field connections (steel to steel) on coat of red lead and surface of rail posts in contact with concrete three coats of red lead. No other paint to be applied by the contractor. Payment for cleaning and painting such surfaces will be included in unit price bid for Structural Steel.

Note: This drawing is not to scale. Follow dimensions.

B.M. Elev. 523.20 Nail in root of 18" Walnut 95' Lt.
Sta. 147+27

BRIDGE OVER LEATHERWOOD CREEK
 STATE ROAD WEST OF JEWETT
 ABOUT 11.5 MILES E. OF ANNAPOLIS
 PROJECT NO. 5-171(2) (C) STA. 151+97
 MADISON COUNTY FINISHED

DESIGNED BY: J.A. Williams DATE: 12/17/1954
 APPROVED BY: [Signature] DATE: 12/17/1954

Drawn Dec. 1954 by J.E.L.
 Checked Dec. 1954 by J.D.W.

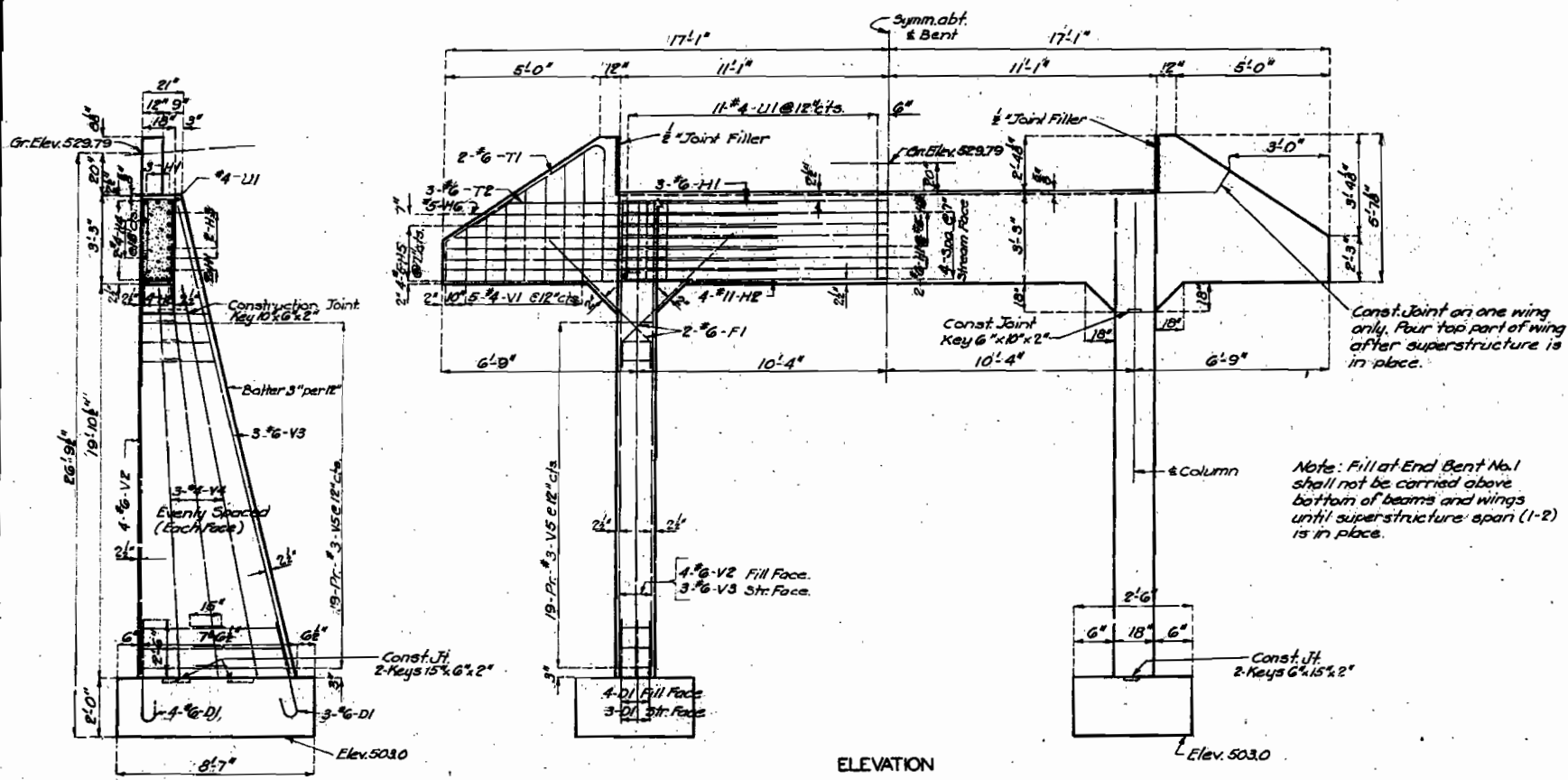
Sheet No. 1 of 4

STD 21A-6
 STD 510-3
 FINISHED

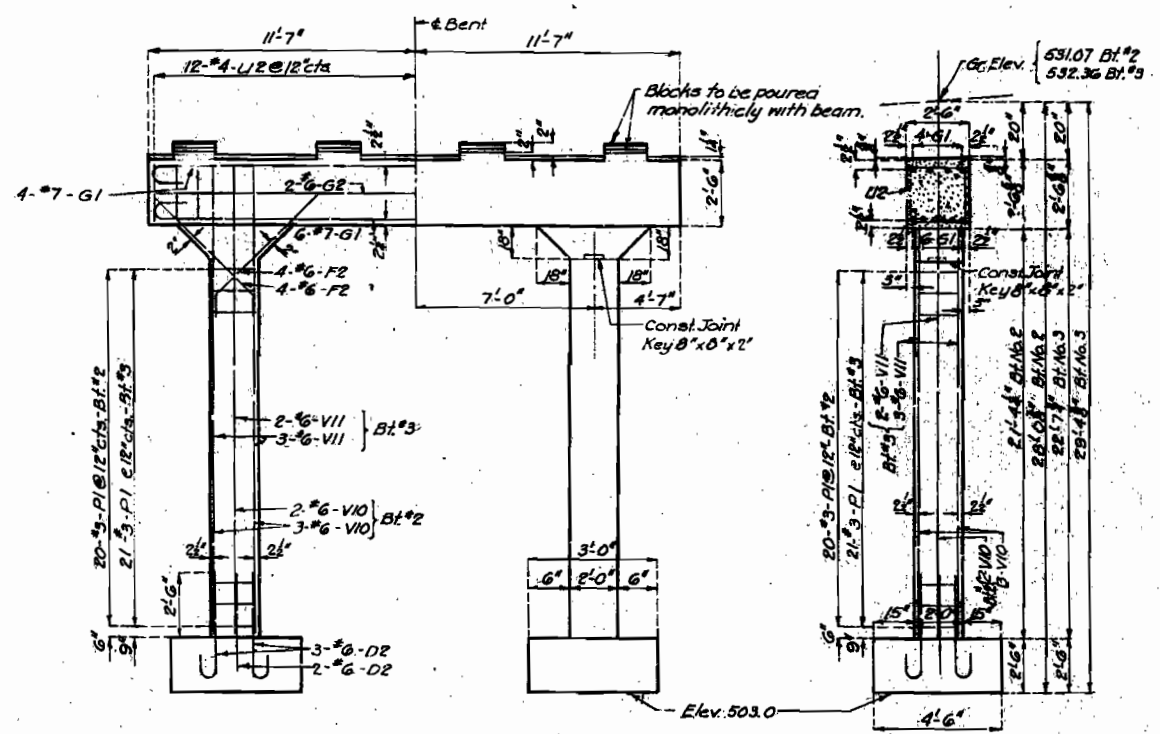
SEE FINAL PLANS BROWN LINES

MISSOURI STATE HIGHWAY DEPARTMENT

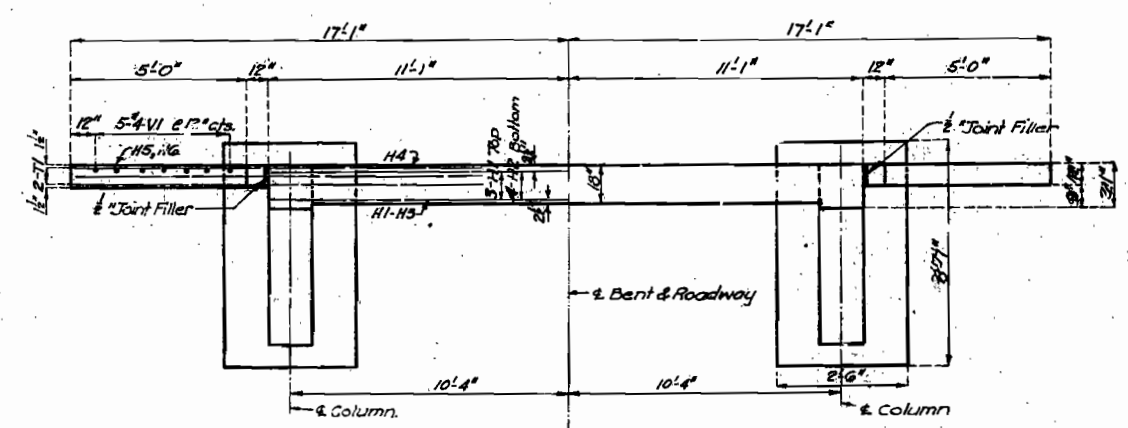
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5	NO.	5-171(2)	19		



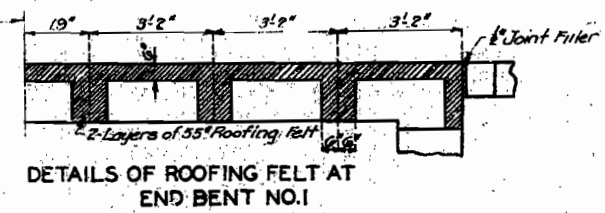
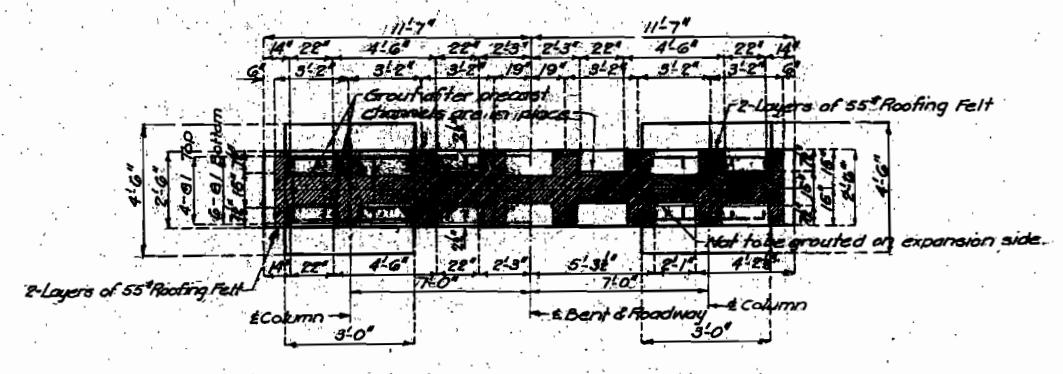
SECTION AT E



SECTION AT E



DETAILS OF END BENT NO. 1



DETAILS OF INTERMEDIATE BENTS NO. 2 & 3

BRIDGE OVER LEATHERWOOD CREEK
 STATE ROAD WEST OF JEWETT
 ABOUT 11.5 MILES E. OF ANNAPOLIS
 PROJECT NO. 5-171(2) (SCISTA. 151 +97)
MADISON COUNTY

462

Assembled Dec. 1954 by J.L.L. & B.R.G. + J.N.K.
 Checked Dec. 1954 by J.D.M.

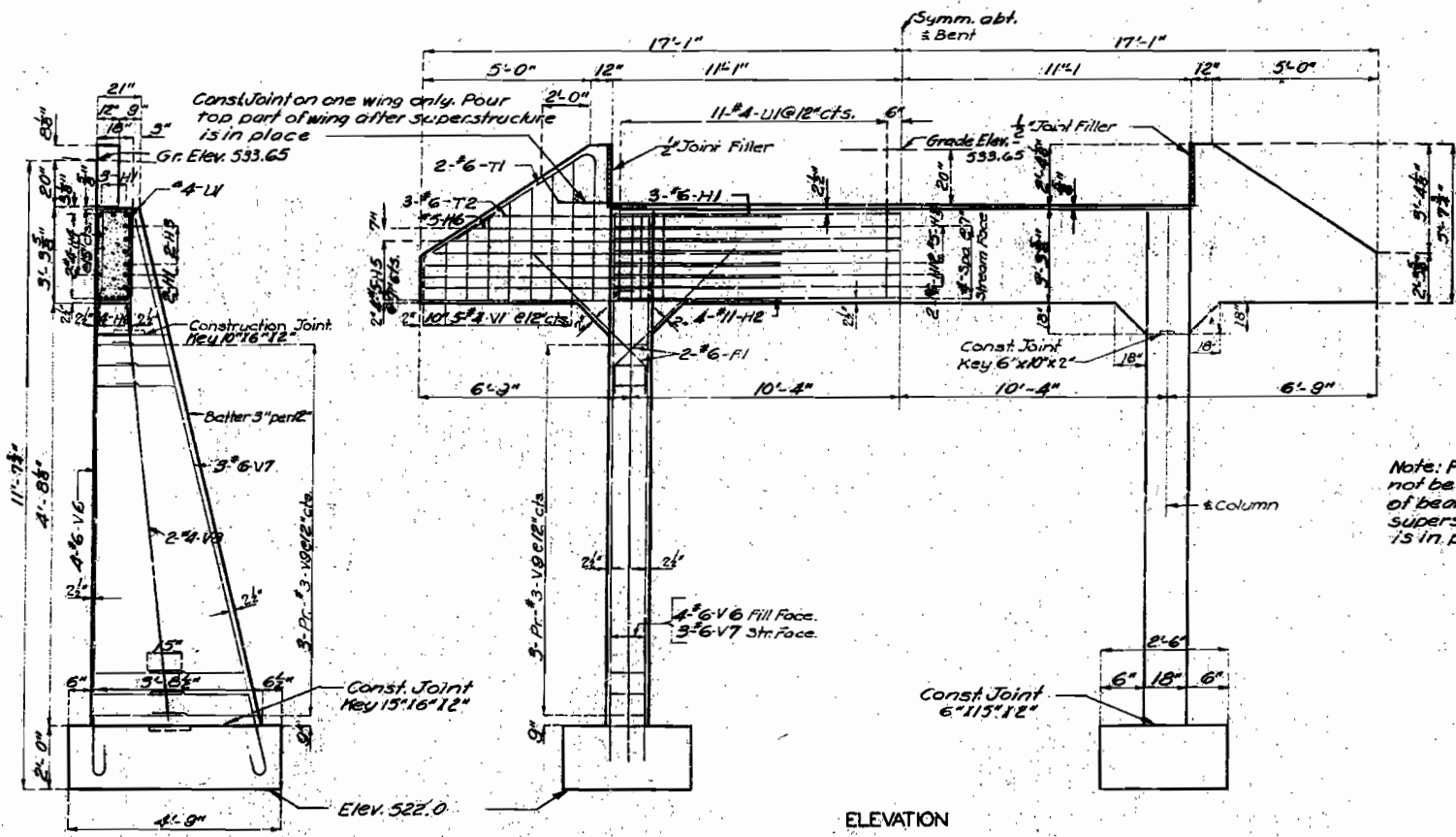
Note: This drawing is not to scale. Follow dimensions.

Sheet No. 2 of 4

Deck Under - 2 Column End Bent - 2 Column Int. Bent -

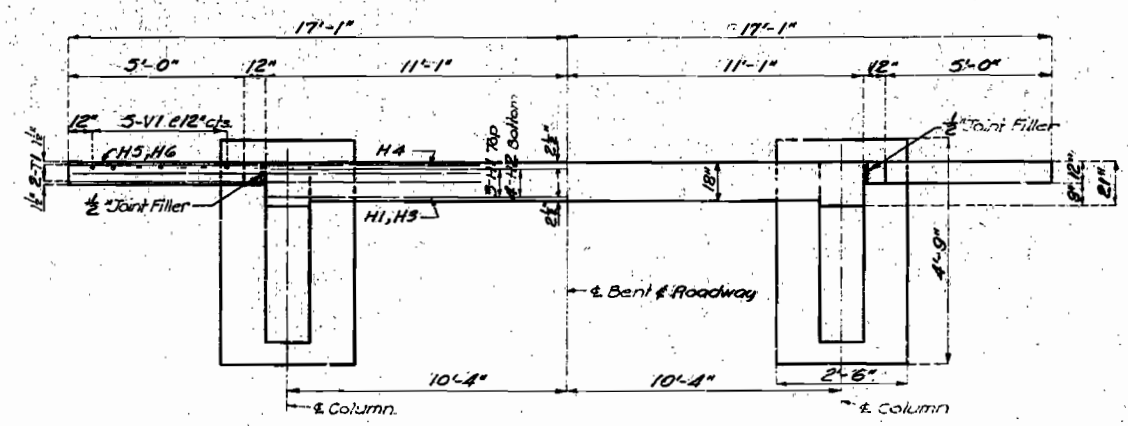
MISSOURI STATE HIGHWAY DEPARTMENT

FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
5	MO	S-17(2)(3C)	19		

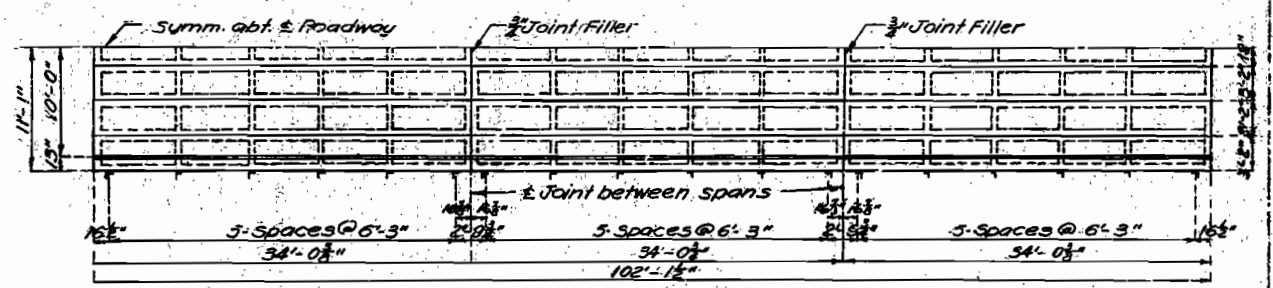
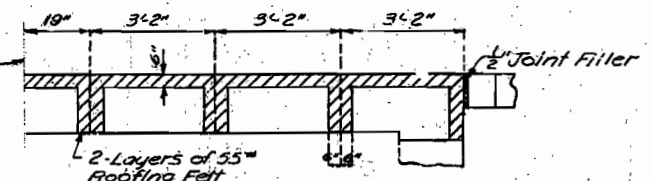


Note: Fill at End Bent No. 4 shall not be carried above bottom of beam and wings until superstructure span (3-4) is in place.

SECTION AT E



DETAILS OF END BENT NO. 4



Note: Dimensions shown above are parallel to grade.

BRIDGE OVER LEATHERWOOD CREEK
 STATE ROAD WEST OF JEWETT
 ABOUT 11.5 MILES E. OF ANNAPOLIS
 PROJECT NO. S-17(2) (SC) STA. 151+97
MADISON COUNTY

FORWARD

FINISHED

FINISHED

Assembled Dec. 1954 by J.E.L. & J.H.K.
Checked Dec. 1954 by J.D.M.

Note: This drawing is not to scale. Follow dimensions.

Sheet No. 3 of 4

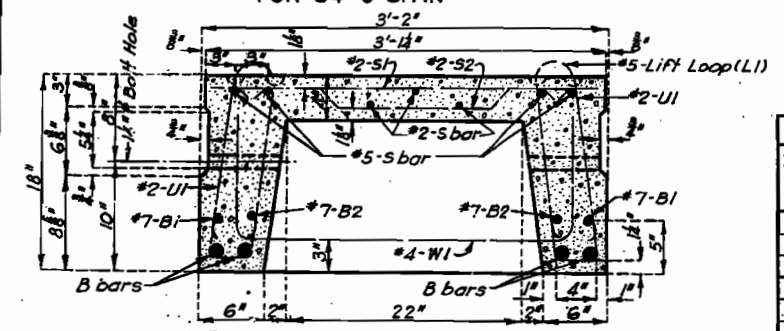
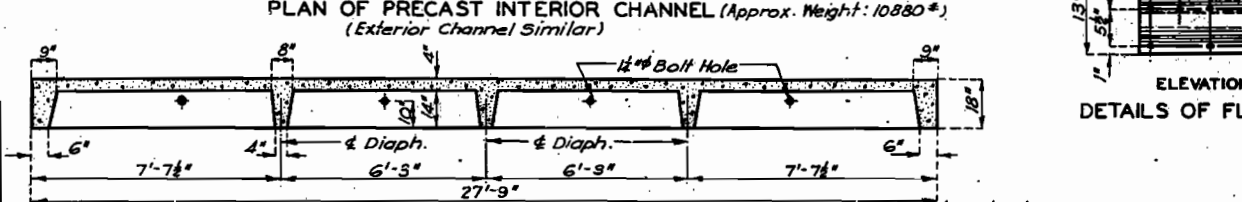
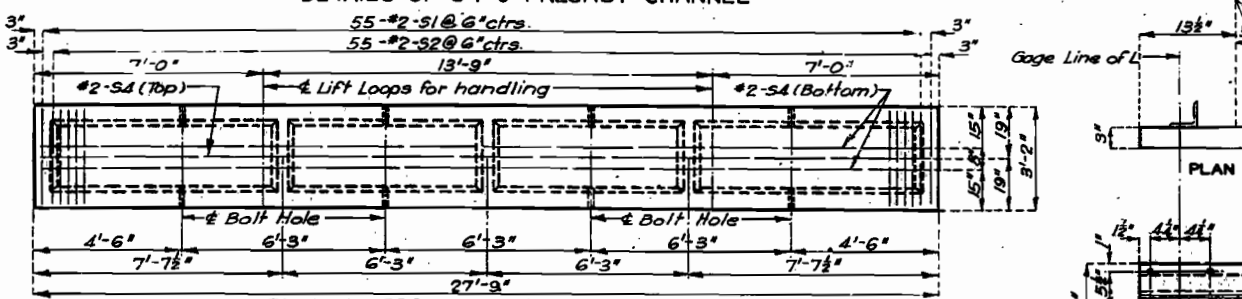
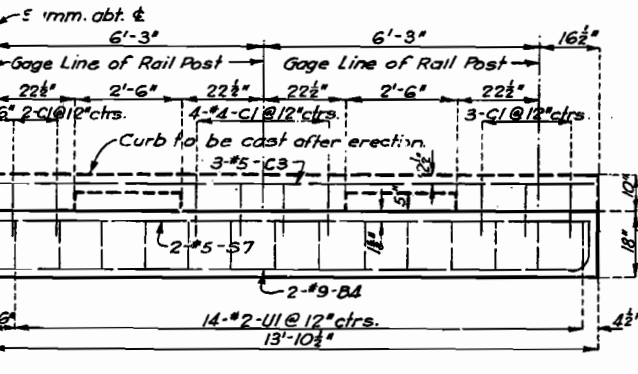
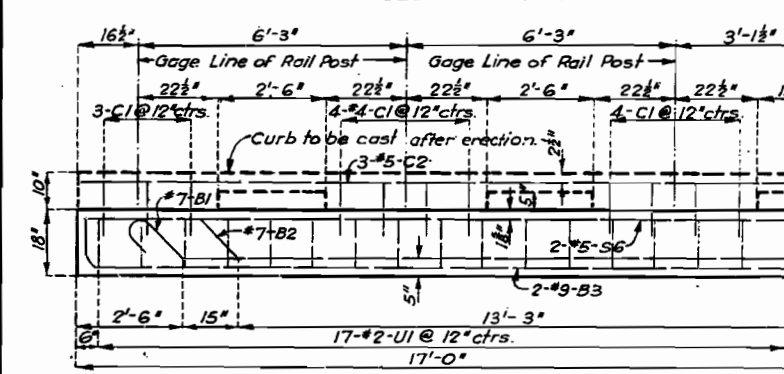
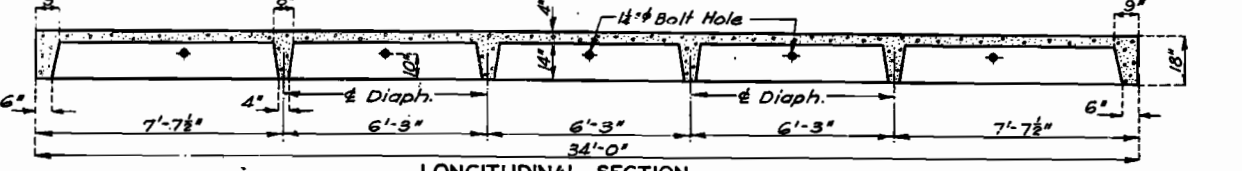
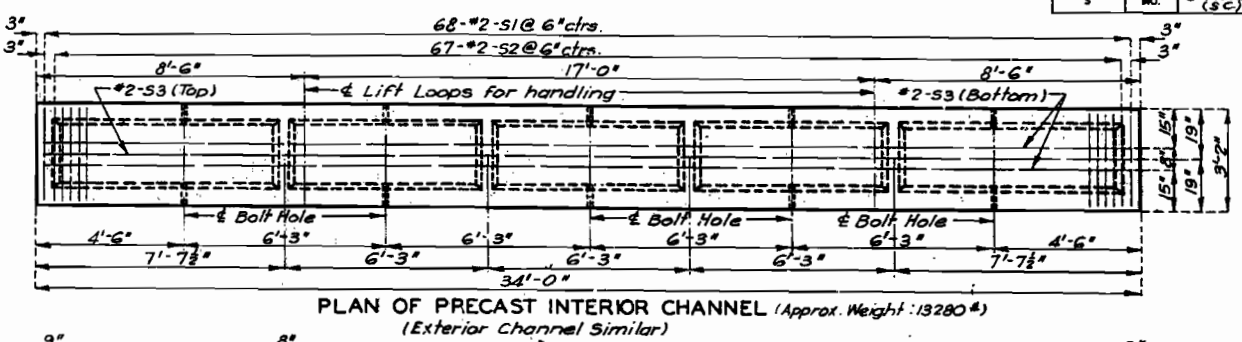
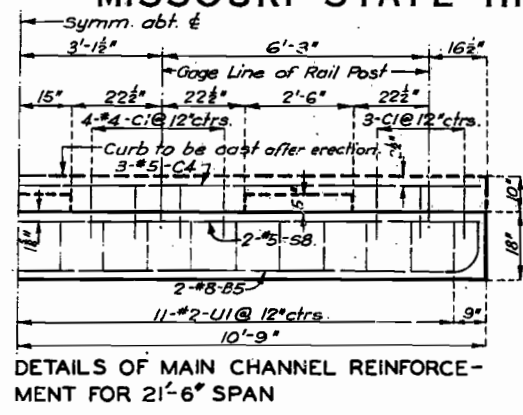
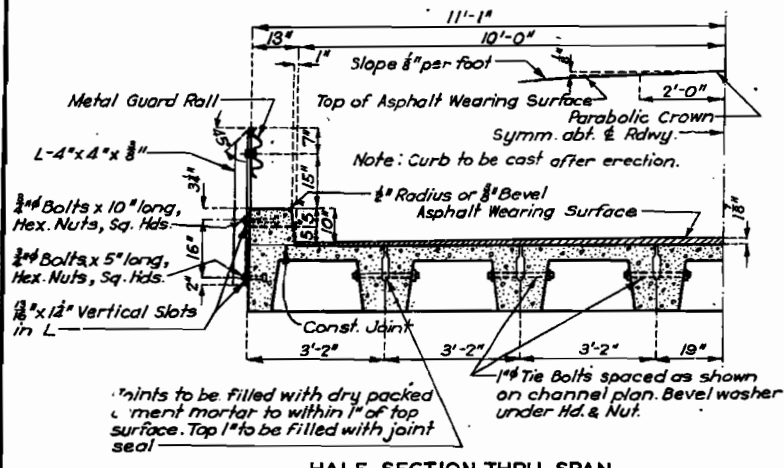
Deck Girder - 2 Column End Bent

T-763

403

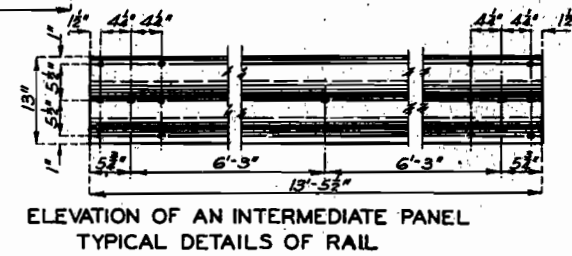
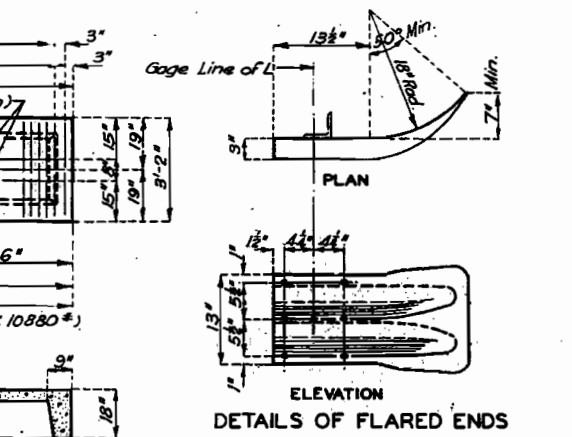
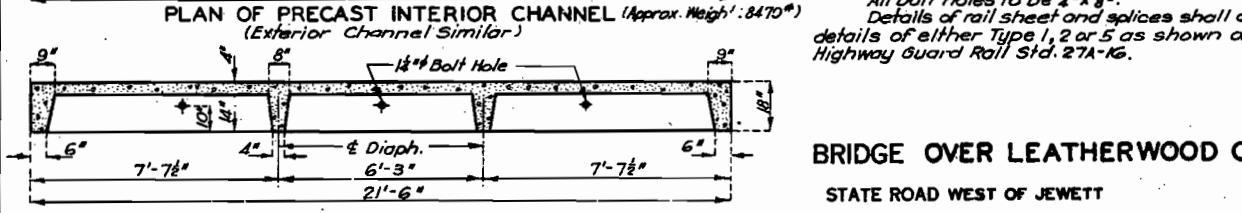
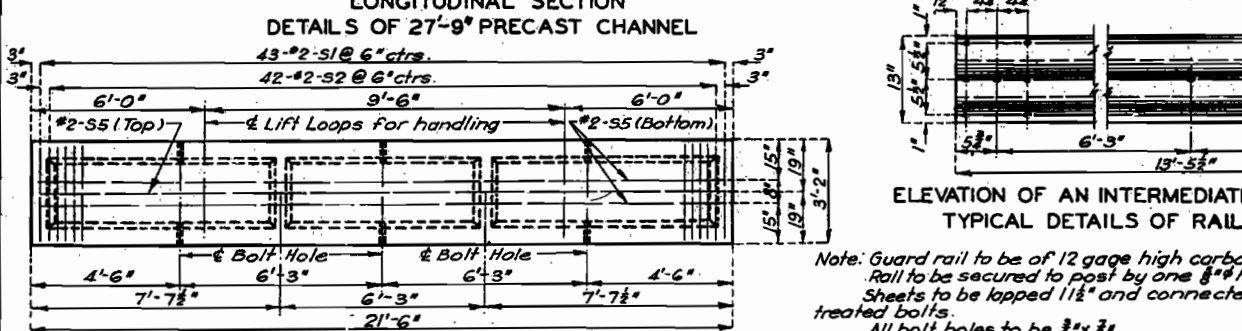
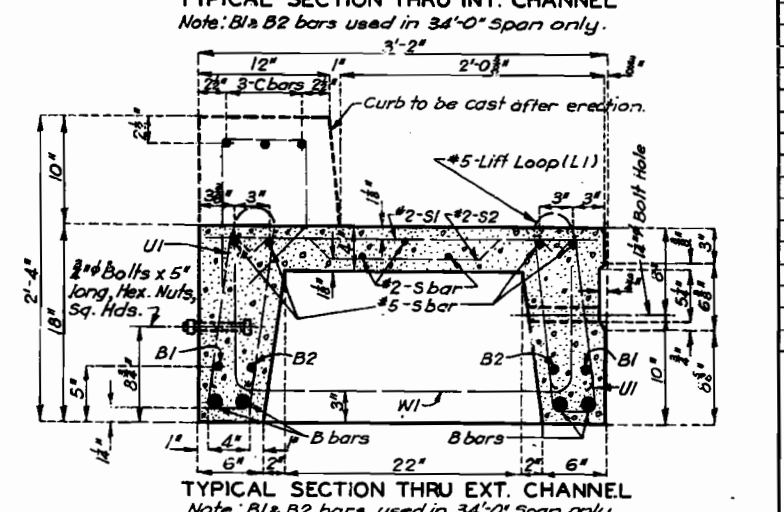
MISSOURI STATE HIGHWAY DEPARTMENT

FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
5	MO.	S-171(2) (SC)	19		



Note: #5 Lift Loops to be cut off in field after units have been set in final position at bridge site and walls to be filled with grout containing iron oxide (Embeco or an approved equivalent) by Contractor, except lift loops under curbs.

BILL OF REINFORCING STEEL FOR SUPERSTRUCTURE							
Total No.	One Span			Size	Length	Mark Location	Bending Sketches
	No.	No.	No.				
42	14	7	7	#7	34'-9"	B1 Channel	Symm. abt. &
42	14	7	7	#7	34'-0"	B2 "	
84	28	14	14	#9	35'-6"	B3 "	4 1/2"
				#9	29'-3"	B4 "	
				#8	23'-0"	B5 "	4 1/2"
132	28	14	14	#4	3'-6"	C1 Curb	
18	6	3	3	#5	33'-9"	C2 "	4 1/2"
				#5	27'-6"	C3 "	
				#5	21'-3"	C4 "	4 1/2"
1,428	301	385	476	#2	3'-0"	S1 Slab	
1,407	294	385	469	#2	3'-0"	S2 "	4 1/2"
63	21	21	21	#2	33'-9"	S3 "	
				#2	27'-6"	S4 "	4 1/2"
84	21	21	21	#2	21'-3"	S5 "	
				#5	33'-9"	S6 "	4 1/2"
28	28	28	28	#5	27'-6"	S7 "	
				#5	21'-3"	S8 "	4 1/2"
1,428	294	392	476	#2	3'-9"	U1 Channel	
176	28	35	42	#4	4'-6"	W1 Diaph.	4 1/2"
84	28	28	28	#5	3'-0"	L1 Channel	



Note: Guard rail to be of 12 gage high carbon steel sheet. Rail to be secured to post by one #8 heat treated bolt. Sheets to be lapped 11 1/2" and connected by 6-#8 heat treated bolts. All bolt holes to be 3/4". Details of rail sheet and splices shall conform with the details of either Type 1, 2 or 5 as shown on Missouri State Highway Guard Rail Std. 27A-16.

BRIDGE OVER LEATHERWOOD CREEK
 STATE ROAD WEST OF JEWETT
 ABOUT 11.5 MILES E. OF ANNAPOLIS
 PROJECT NO. S-171(2) (SC) STA. 151+97
MADISON COUNTY

Assembled Dec. 1954 by B.R.G. & J.M.K.
 Checked Dec. 1954 by J.D.M.

Note: This drawing is not to scale. Follow dimensions.

Sheet No. 4 of 4

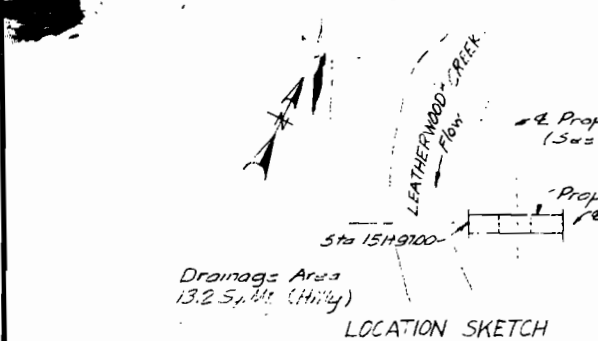
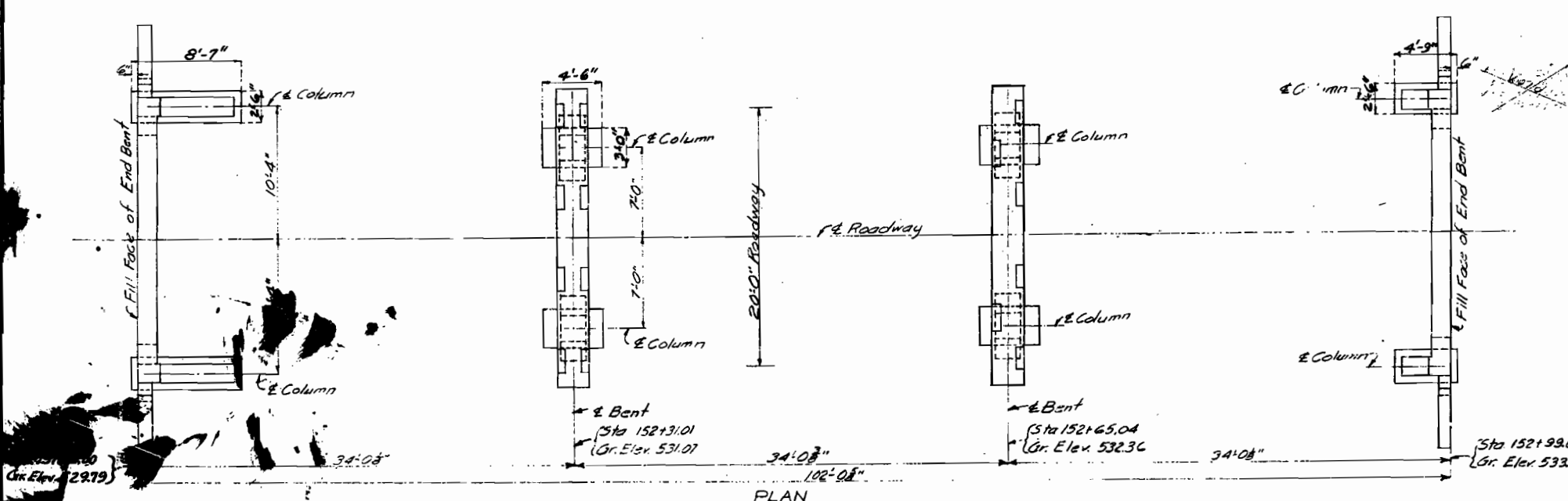
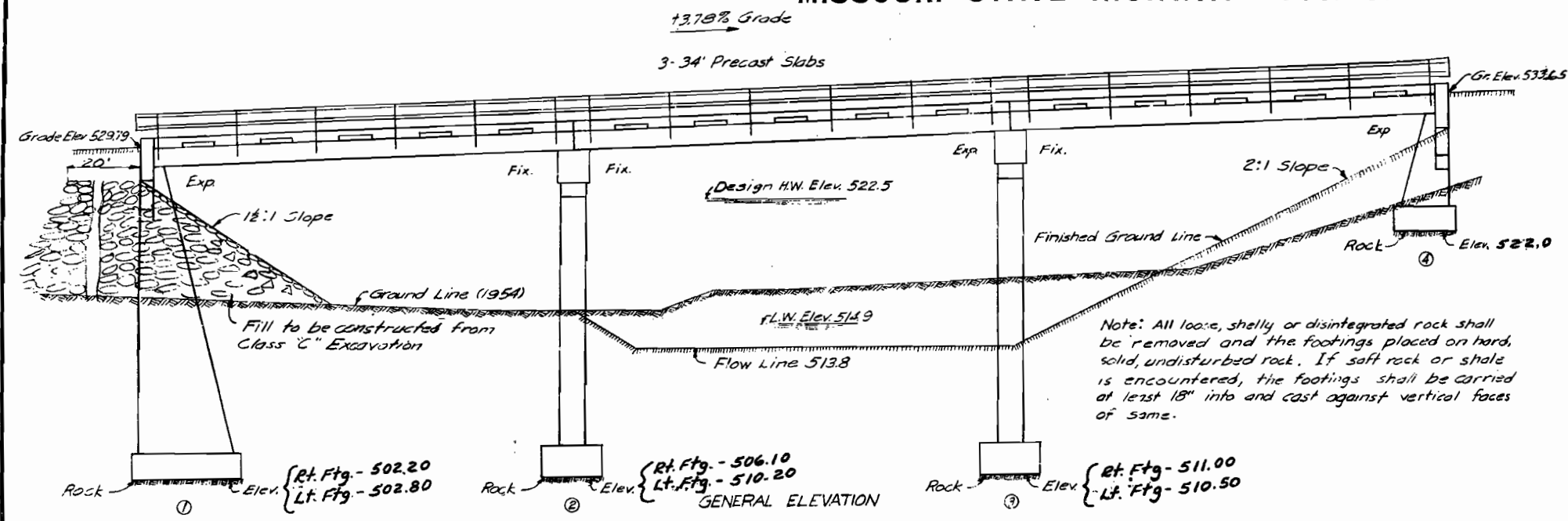
T-763

404

MISSOURI STATE HIGHWAY DEPARTMENT

FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
5	MO.	5-171(2)B4	19		

FINAL PLANS



Drawn Dec. 1954 by J.E.L.
Checked Dec. 1954 by J.D.M.

GENERAL NOTES:
 Design Specifications A.A.S.H.O. 1953
 Loading 410-44
 Reinforcing Steel Stress 18,000%
 Class "B" Concrete Stress 1000%
 Class "A" Concrete Stress 1500%
 All substructure concrete and curbs to be Class "B".
 All precast superstructure units shall be Class "A" or lightweight concrete. See Special Provisions.
 Where joint filler is specified on the plans it shall conform with the requirements for Prepacked Material for Filler as given in section 3313 A(1) of the Standard Specifications.
 Fabricated structural steel substructure includes guard rail posts, bolts retaining these posts to concrete and bolts and washers fastening precast sections together.
 Cost of cement mortar used in assemblies precast units to be included in unit prices bid for precast units in place.
 Paint: Shop prime; Field coat four surfaces of bolted field connectors (slats, slats, slats and red lead and surfaces of rail posts in contact with concrete three coats of red lead. All other paint to be applied by the contractor. Payment for cleaning and painting such surfaces will be included in unit prices bid for structural steel.

BILL OF REINFORCING STEEL - SUBSTRUCTURE				Bending Sketches and Cutting Diagrams	
No.	Size	Length	Mark	Location	
End Bents No. 1 and 3					
14	#6	5'3"	D1	Footing B1	8' 2'6"
16	#6	7'3"	F1	Haunch	3' 5'11"
10	#6	21'9"	H1	Beam	3' 5'11"
8	#11	24'3"	H2	"	5'11" 9'7"
4	#5	21'9"	H3	"	5'11" 5'7"
4	#4	21'9"	H4	"	10'9"
16	#3	11'6"	H5	Wing	5-VI CUT 10
4	#5	11'0"	H6	"	19-V5 CUT 38 & BEND AS SHOWN
8	#6	11'6"	T1	"	3-V9 CUT 6 & BEND AS SHOWN
12	#6	10'9"	T2	"	
44	#4	9'6"	U1	Beam	
10	#4	7'9"	V1	Wing	
8	#6	22'6"	V2	Col. Bt. #1	
6	#6	23'6"	V3	"	
12	#4	19'9"	V4	"	
38	#3	14'9"	V5	"	
8	#6	10'0"	V6	Col. Bt. #4	
6	#6	10'6"	V7	"	
4	#4	4'6"	V8	"	
6	#3	10'9"	V9	"	
Int. Bents No. 2 and 3					
32	#6	5'9"	D2	Footing	
32	#6	8'0"	F2	Haunch	
20	#7	25'0"	G1	Beam	
4	#6	22'9"	G2	"	
182	#3	7'9"	P1	Col. Bt. #3	
16	#6	23'3"	V10	Col. Bt. #2	
16	#6	24'6"	V11	Col. Bt. #3	
46	#4	9'9"	U2	Beam	

Note: For Bill of Reinforcing Steel - Superstructure see Sheet No. 4 of 4.

FINAL QUANTITIES			
Item	Substr.	Superstr.	Total
Class 1 Excavation for Structures	Cu. Yds.	22.5	22.5
Class 2 Excavation for Structures	Cu. Yds.	100.5	100.5
Class "A" or Lightweight Concrete	Cu. Yds.		73.7
Class "B" Concrete	Cu. Yds.	57.3	62.6
Fabricated Structural Steel	Lbs.	1820	1820
Reinforcing Steel	Lbs.	7300	23250
Asphalt Wearing Surface	Sq. Yds.		0
Metal Guard Rail	Lin. Ft.		206
Class 2 Excavation (below plan)	Cu. Yd.	1.0	1.0
Drilling Test Holes		48	48

Note: Excavation for bridge made above Elev. 516.0 will be paid for as Class 1 Excavation for Structures.
 Excavation for bridge made below Elev. 516.0 will be paid for as Class 2 Excavation for Structures.
 * See Special Provisions.

B.M. Elev. 523.20 Nail in root of 18" Walnut 95' Lt.
 Sta 147+27

BRIDGE OVER LEATHERWOOD CREEK
 STATE ROAD WEST OF JEWETT
 ABOUT 11.5 MILES E. OF ANNAPOLIS
 PROJECT NO. 5-171(2) (SCSTA 151+97)
 MADISON COUNTY

SUBMITTED BY: J.A. Williams DATE: 12/17/1954
 APPROVED BY: Rex M. Hutton DATE: 12/17/1954

STD 27A-16
 STR. C-110B3
 T-763

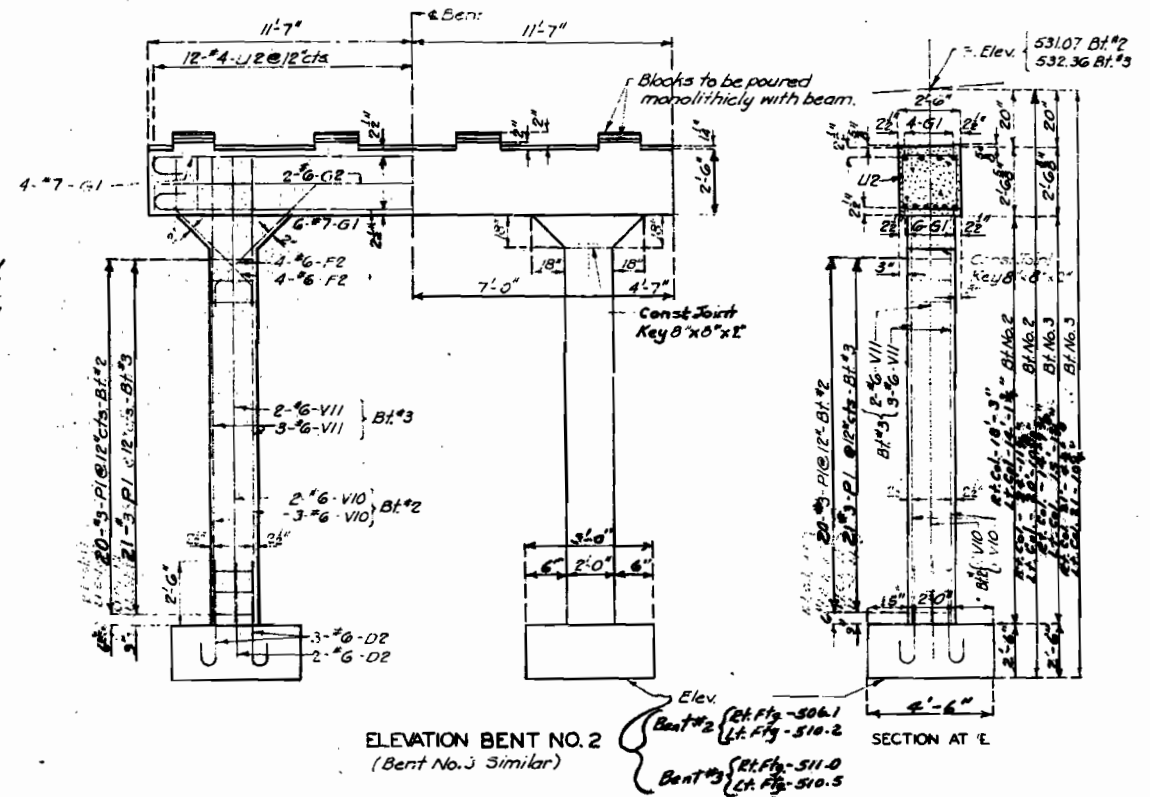
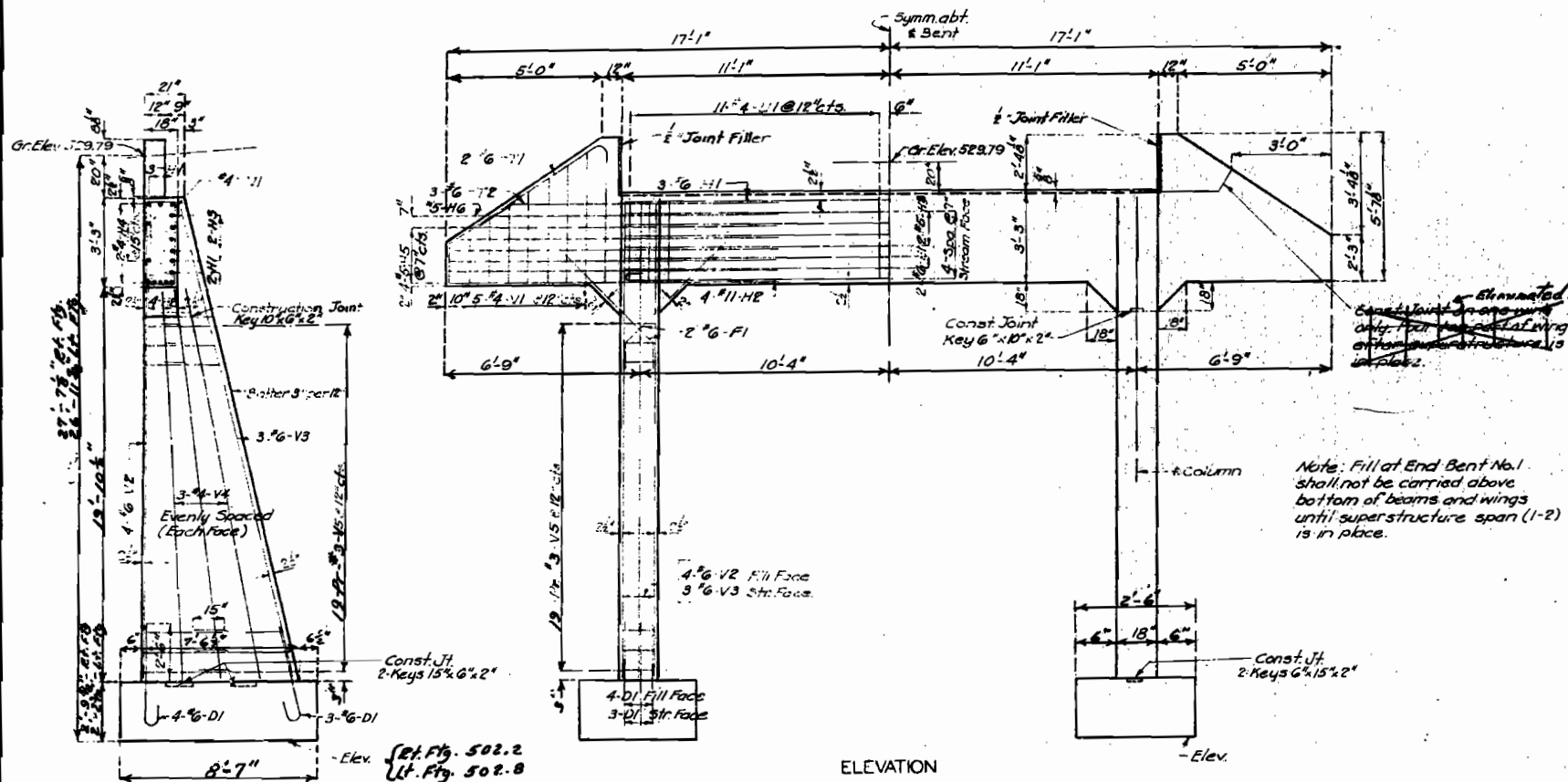
FINAL PLANS

405

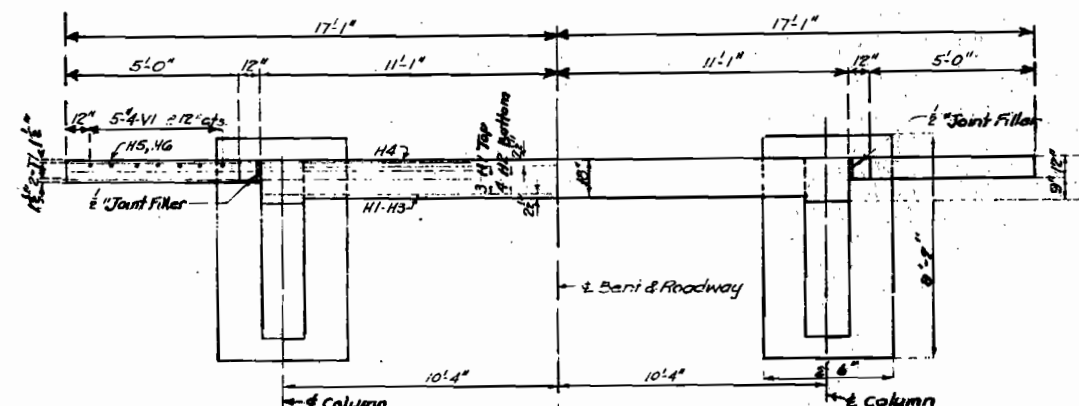
MISSOURI STATE HIGHWAY DEPARTMENT

FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
5	MO	S-1711(2)	10	11	15

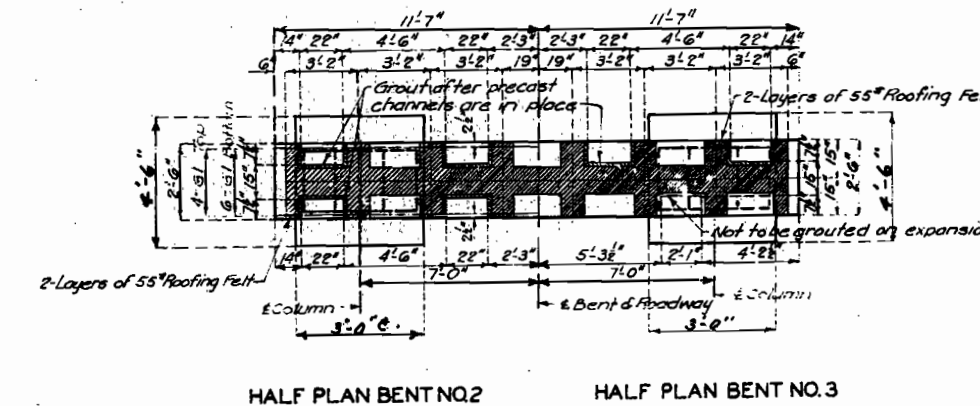
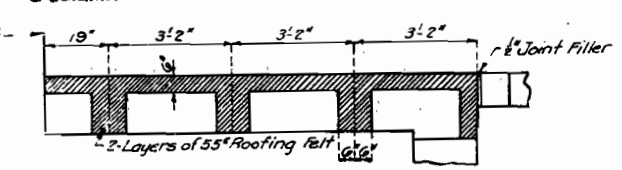
FINAL PLANS



SECTION AT E



DETAILS OF END BENT NO. 1



DETAILS OF INTERMEDIATE BENTS NO. 2 & 3

BRIDGE OVER LEATHERWOOD CREEK

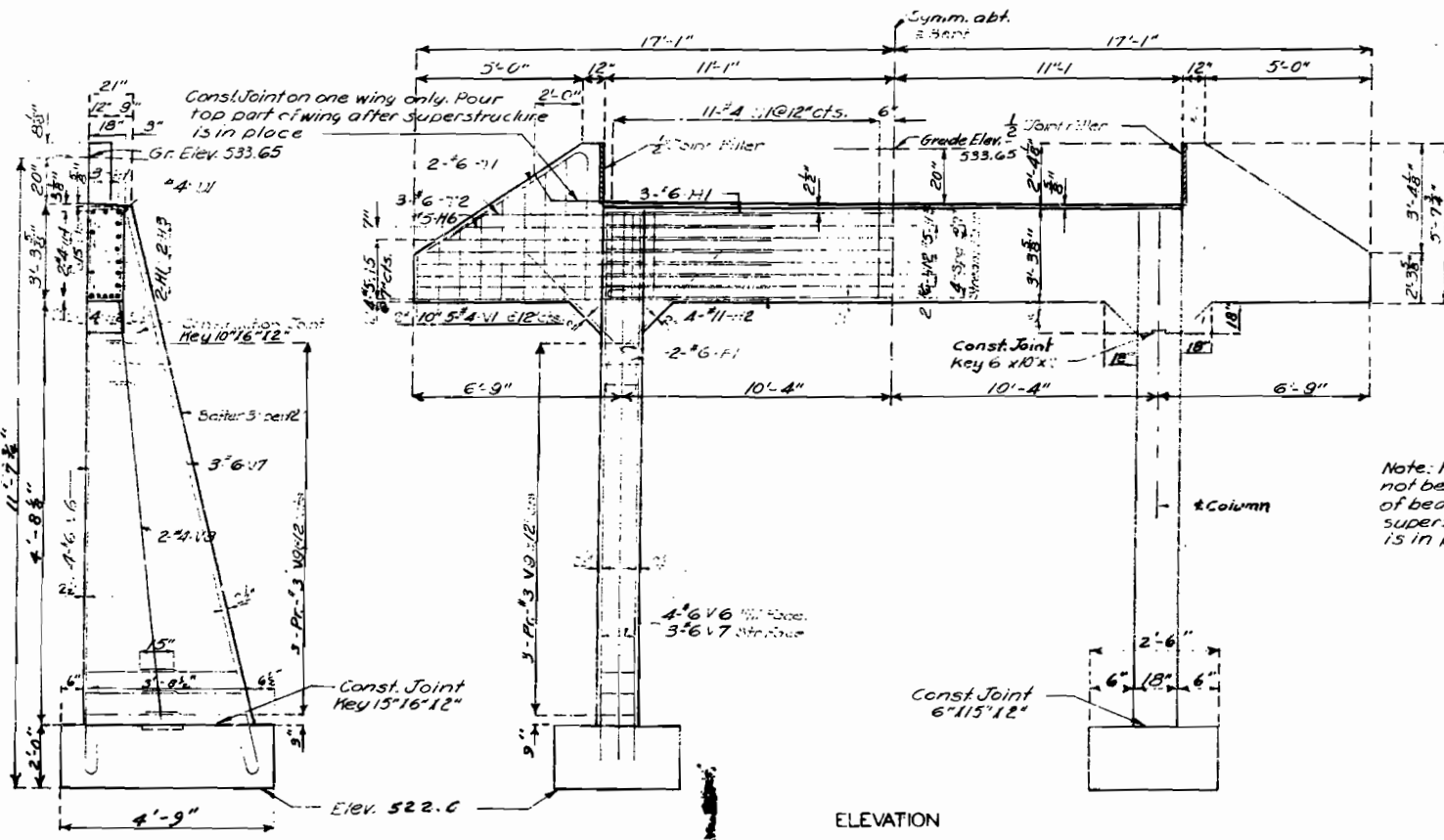
STATE ROAD WEST OF JEWETT
ABOUT 11.5 MILES E. OF ANNAPOLIS
PROJECT NO. S-1711(2) (SC) STA. 131+97

MADISON COUNTY

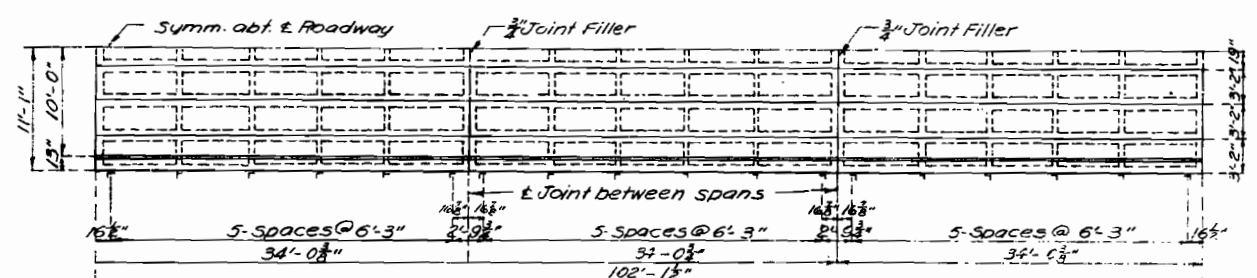
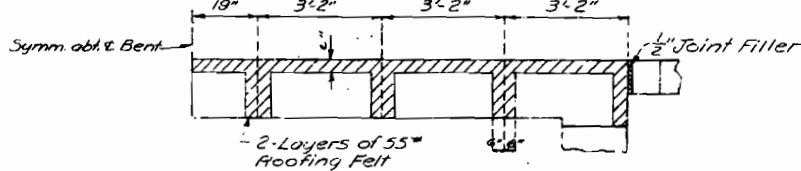
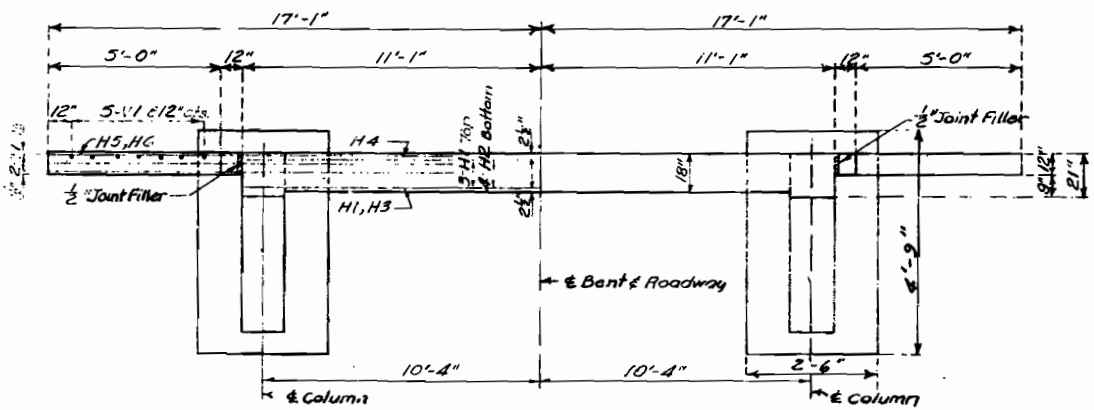
406

MISSOURI STATE HIGHWAY DEPARTMENT

STATE ROAD WEST OF JEWETT
 PROJECT NO. S-17(12) (SC) STA 151+97
 DRAWING NO. BRIDGE 2A OF #3



Note: Fill at End Bent No. 4 shall not be carried above bottom of beam and wings, until superstructure, span (3-4) is in place.

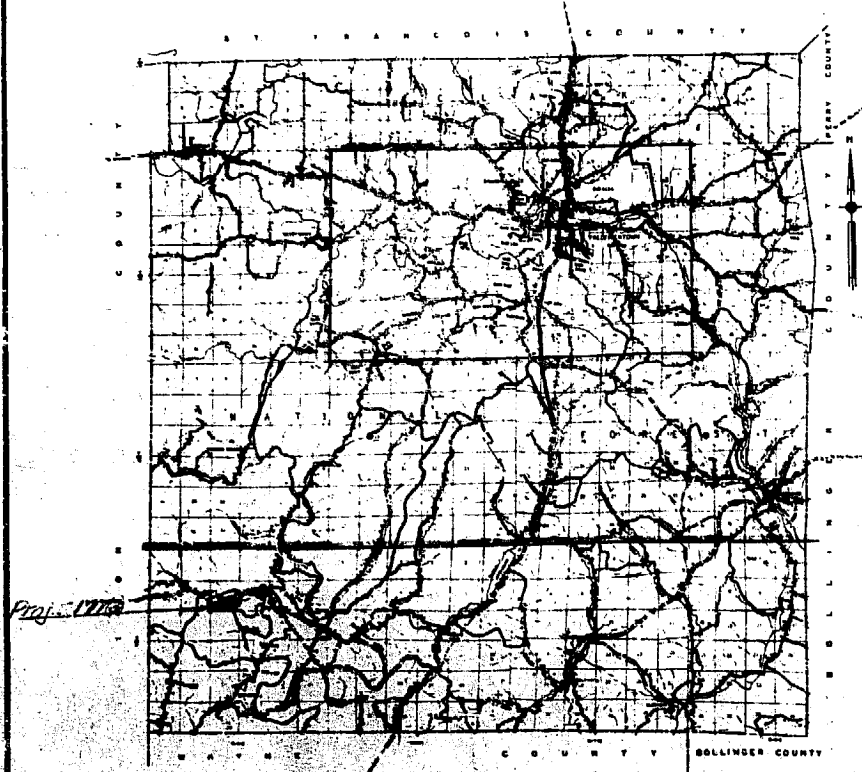


Note: Dimensions shown above are parallel to grade.

BRIDGE OVER LEATHERWOOD CREEK
 STATE ROAD WEST OF JEWETT
 ABOUT 11.5 MILES E. OF ANNAPOLIS
 PROJECT NO. S-17(12) (SC) STA 151+97
 MADISON COUNTY

407

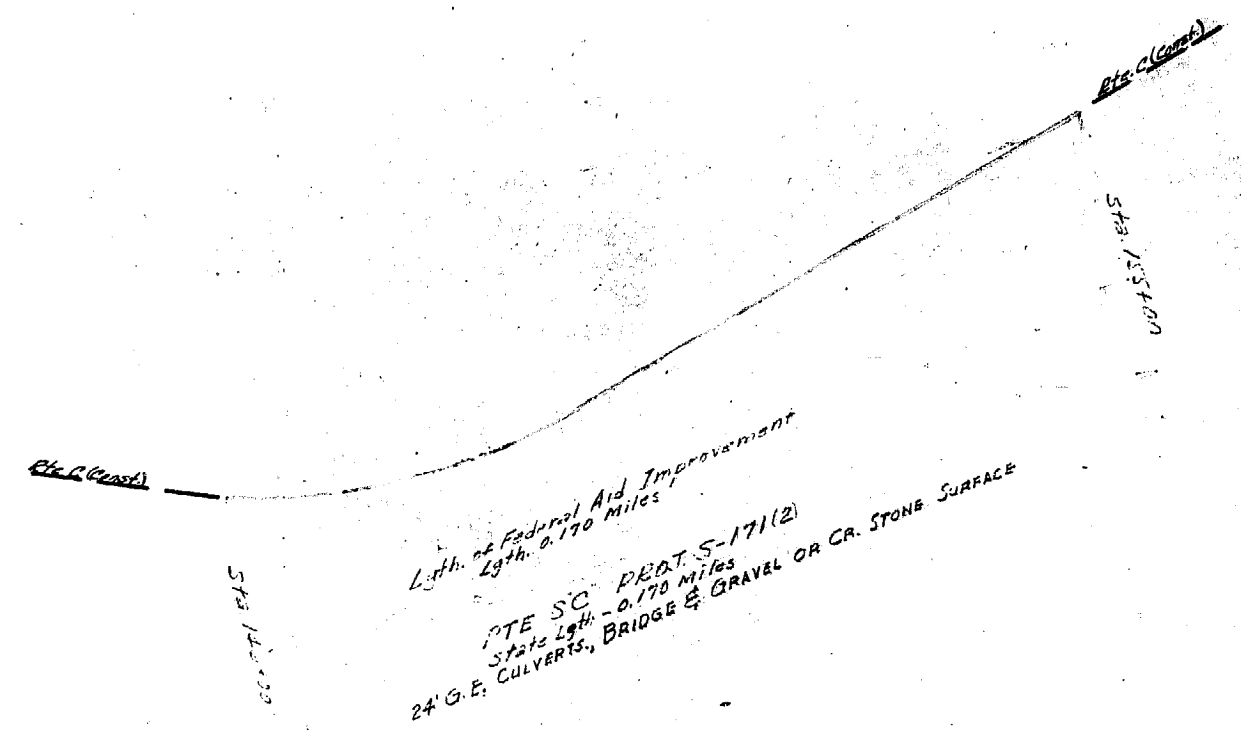
LOCATION MAP



MISSOURI
STATE HIGHWAY COMMISSION
PLAN AND PROFILE
OF PROPOSED
STATE ROAD

FEDERAL AID PROJECT
MADISON COUNTY

COUNTY
MADISON
STATE ROUTE NO.
C
PROJECT NO.
S-171(2)



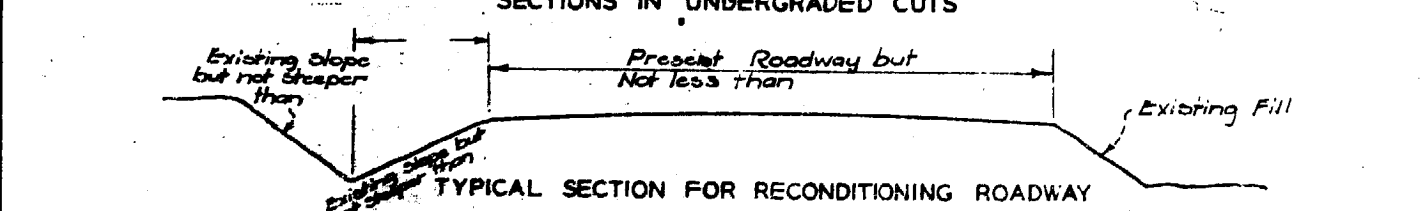
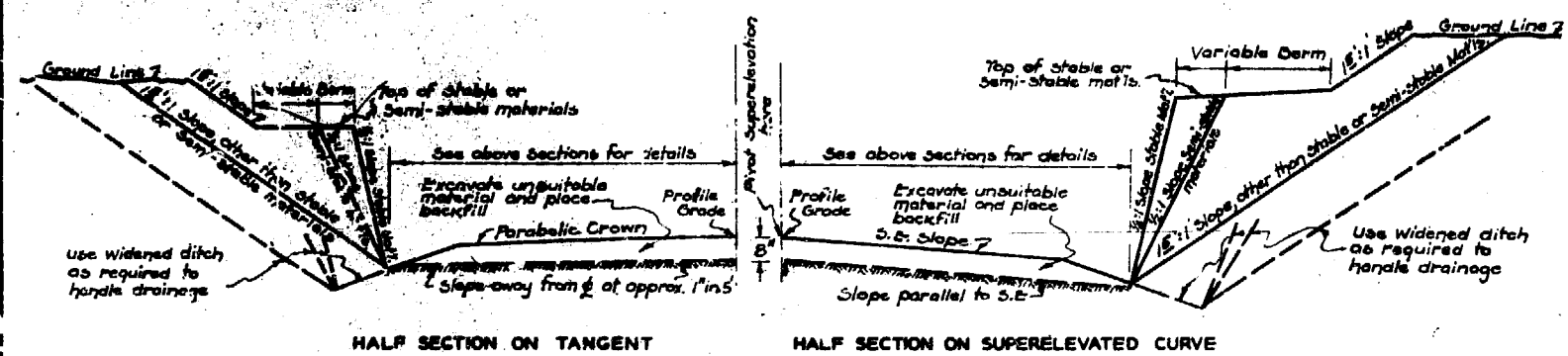
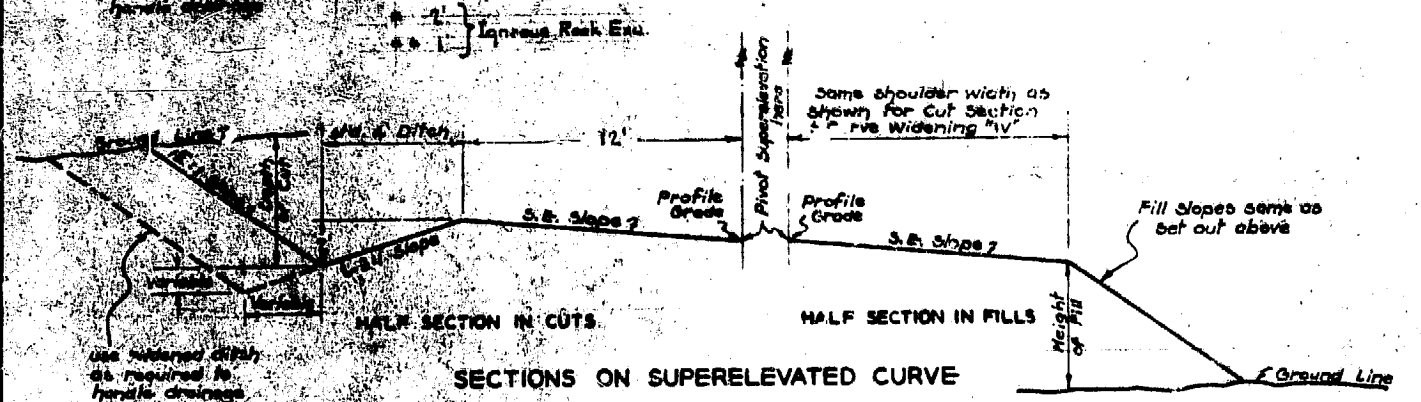
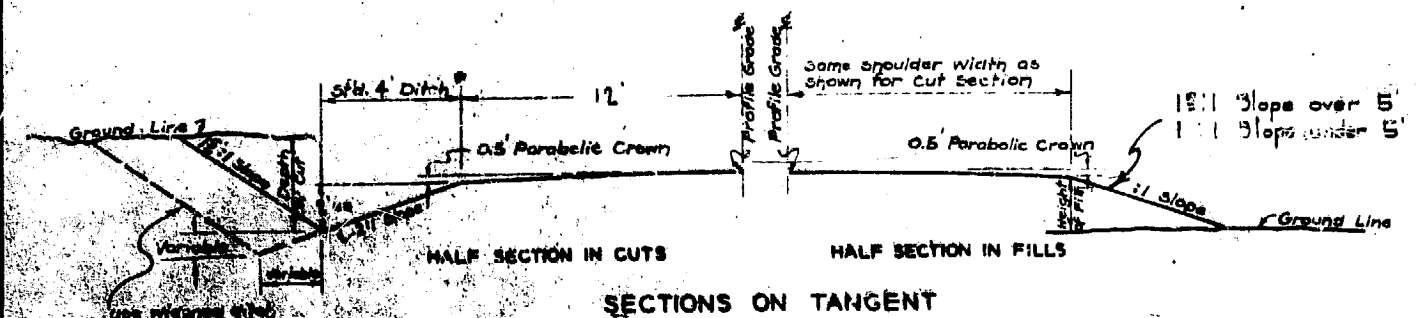
CONVENTIONAL SIGNS

STATE AND FEDERAL LINE	LEVEL	
COUNTY LINE	CULVERTS	
CITY, VILLAGE OR BOROUGH	TRAIL	
SCHOOL LINE	POWERS POLE	
SECTION LINE	TELEPHONE OR TELEGRAPH POLE	
GRANT LINE	MARSH	
FENCE LINE	EDGE	
GUARD RAIL		
IMPERCED PROPERTY		
RIGHT OF WAY LINE	GROUND ELEVATION	
TRAVELLED WAY	GRADE ELEVATION	
RAIL ROAD	SURFACE LINE	
RETAINING WALL	GRADE LINE	
BASELINE SURVEY LINE		



SUBMITTED BY: _____ DATE: _____

MISSOURI STATE HIGHWAY COMMISSION



GENERAL NOTES:-

Machine Grading and Reconditioning Roadway shall be completed in accordance with these Typical Sections using the standard depth of ditch except as otherwise noted on Plan Sheets and as required to provide proper drainage. Covering of at least 1 foot will be required over all drainage structures located within machine and Reconditioning Roadway sections.

In transitioning from one slope to another, use a 25 Foot length of transition.

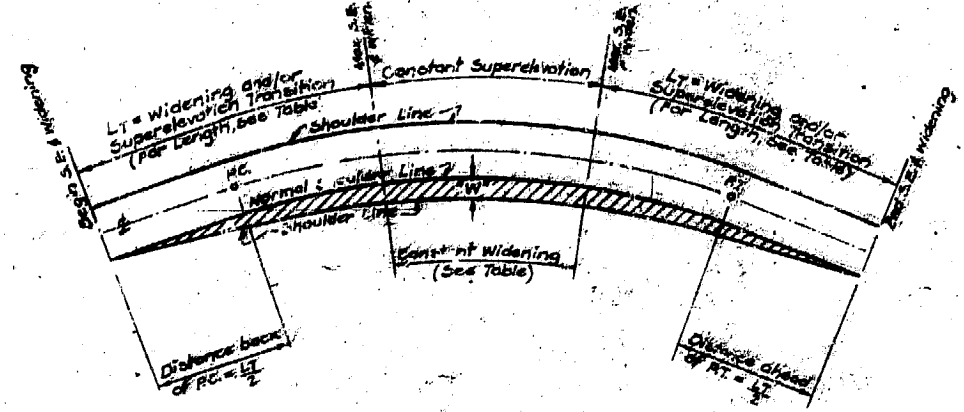
All information shown on these TYPICAL SECTIONS is for the purpose of indicating the required parabolic crown in tangent section, and general design and construction details. Actual construction of roadbed widths, slopes, depth and width of ditches, undergraded cuts and other features shall conform to the details shown on CROSS-SECTIONS and PLAN & PROFILE Sheets or as directed by the Engineer.

Hand finishing of side slopes of cuts and fills will not be required. Machine finishing to a smooth plane will be considered satisfactory.

Unless otherwise shown on the plans, the roadway ditch at all cross road culverts shall be widened to five (5) feet at the inlet with a fifty (50) foot transition to the standard roadway ditch. In "Machine Grading" limits the cost of this excavation is to be included in the contract unit price for Machine Grading.

The cost of constructing Ditch Blocks in "Machine Grading" limits is to be included in the contract unit price for "Machine Grading".

SCHEME OF WIDENING AND SUPERELEVATION TRANSITION



SUPERELEVATION AND WIDENING DATA

Degree of Curve	Design Speeds																		
	30 M.P.H.			35 M.P.H.			40 M.P.H.			45 M.P.H.			50 M.P.H.			55 M.P.H.			
	S	W	LT	S	W	LT	S	W	LT	S	W	LT	S	W	LT	S	W	LT	
1°00' to 1°30'	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
1°30'	0	0	150	0	0	150	0	0	150	0	0	150	0	0	150	0	0	150	0
2°00'	0	0	150	0	0	150	0	0	150	0	0	150	0	0	150	0	0	150	0
2°30'	0	0	150	0	0	150	0	0	150	0	0	150	0	0	150	0	0	150	0
3°00'	0	0	150	0	0	150	0	0	150	0	0	150	0	0	150	0	0	150	0
3°30'	0	0	150	0	0	150	0	0	150	0	0	150	0	0	150	0	0	150	0
4°	0	0	150	0	0	150	0	0	150	0	0	150	0	0	150	0	0	150	0
5°	0	0	150	0	0	150	0	0	150	0	0	150	0	0	150	0	0	150	0
6°	0	0	150	0	0	150	0	0	150	0	0	150	0	0	150	0	0	150	0
7°	0	0	150	0	0	150	0	0	150	0	0	150	0	0	150	0	0	150	0
8°	0	0	150	0	0	150	0	0	150	0	0	150	0	0	150	0	0	150	0
9°	0	0	150	0	0	150	0	0	150	0	0	150	0	0	150	0	0	150	0
10°	0	0	150	0	0	150	0	0	150	0	0	150	0	0	150	0	0	150	0
11°	0	0	150	0	0	150	0	0	150	0	0	150	0	0	150	0	0	150	0
12°	0	0	150	0	0	150	0	0	150	0	0	150	0	0	150	0	0	150	0
13°	0	0	150	0	0	150	0	0	150	0	0	150	0	0	150	0	0	150	0
14°	0	0	150	0	0	150	0	0	150	0	0	150	0	0	150	0	0	150	0
15°	0	0	150	0	0	150	0	0	150	0	0	150	0	0	150	0	0	150	0
15°00' to 22°59'	0	0	150	0	0	150	0	0	150	0	0	150	0	0	150	0	0	150	0
23°00' to 29°59'	0	0	150	0	0	150	0	0	150	0	0	150	0	0	150	0	0	150	0
30°00' & above	0	0	150	0	0	150	0	0	150	0	0	150	0	0	150	0	0	150	0

Notes:-

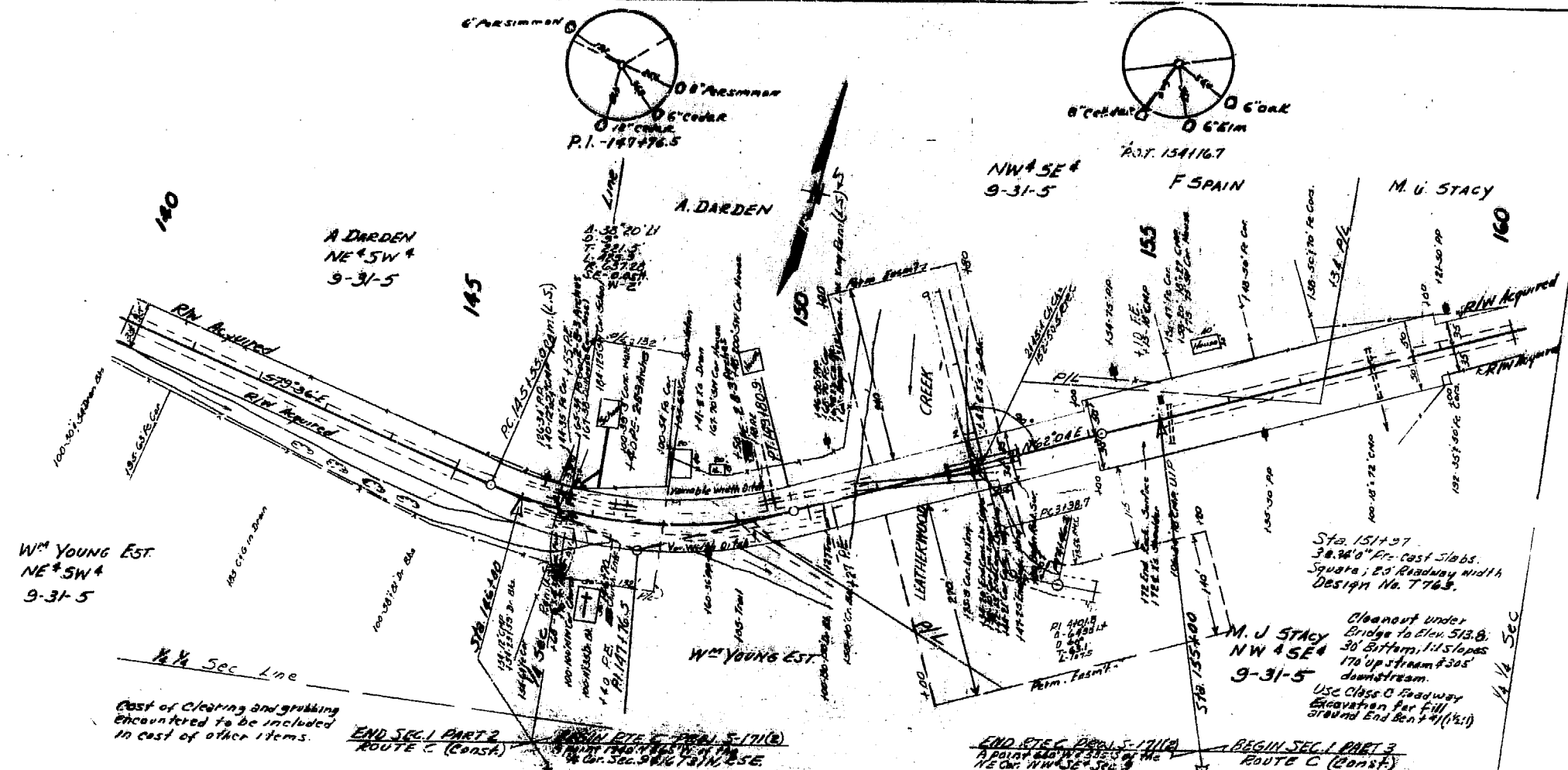
- S - denotes Superlevation in feet per foot
- W - denotes Widening of Surfacing and inside shoulder in feet
- LT - denotes length of Superlevation and/or widening transition in feet
- Crown is to be eliminated on all Superelevated Curves.
- Values for degree of Curve not shown in above table shall be identical with those for the nearest tabulated Curve. In case of tie, use values for next higher degree Curve.

MISSOURI STATE HIGHWAY COMMISSION
TYPICAL SECTIONS
 FOR
24 FT. GRADED EARTH
 FOR
SUPPLEMENTARY ROADS
 (DESIGN SPEED 30 M.P.H.)
 ROUTE 50 COUNTY MADISON
 PROJ. OR SEC. 5-171(2)

APPROVED *[Signature]* ENGR. SURVEYS AND PLANS
 APPROVED *[Signature]* CHIEF ENGINEER

STATE/FEDERAL PROJ. NO.	MO 5-17(2)	SHEET NO.	3	TOTAL SHEETS	1
ROUTE NO.	10	COUNTY	MADISON	ROUTE	C

FINAL PLANS

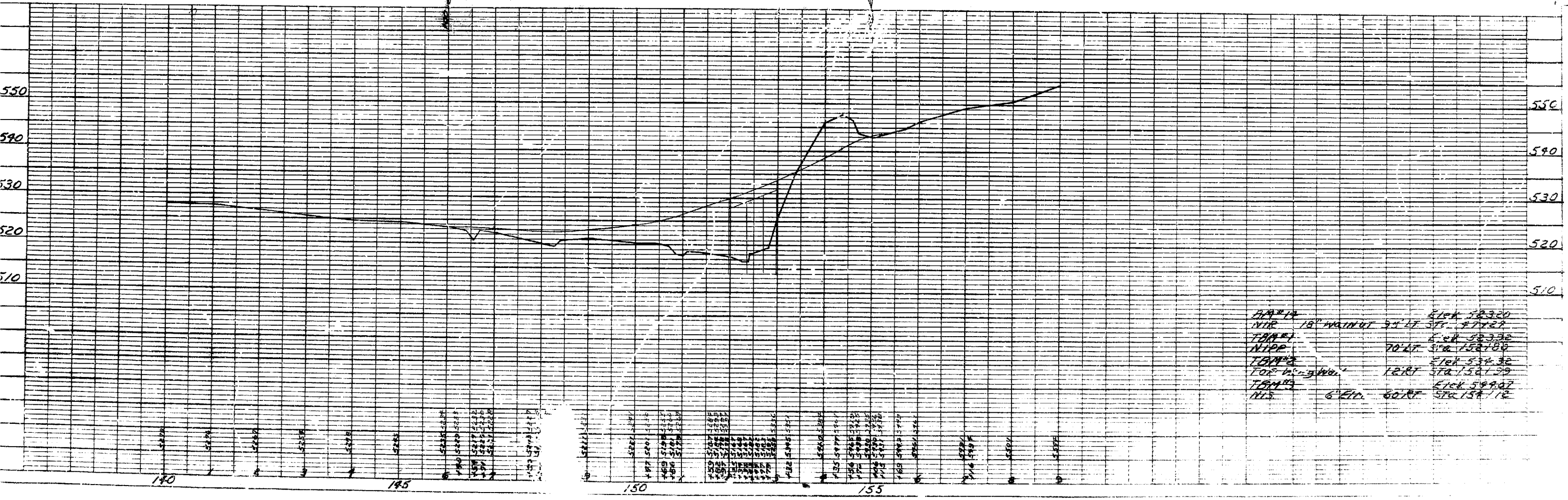


Cost of clearing and grubbing encountered to be included in cost of other items.

END SEC. 1 PART 2
ROUTE C (CONST.)

END SEC. 2 PART 1
ROUTE C (CONST.)

BEGIN SEC. 1 PART 3
ROUTE C (CONST.)



144' 19" Elev 523.20
 145' 18" main cut 95' RT Elev 523.22
 146' 18" Elev 523.22
 147' 18" Elev 523.22
 148' 18" Elev 523.22
 149' 18" Elev 523.22
 150' 18" Elev 523.22
 151' 18" Elev 523.22
 152' 18" Elev 523.22
 153' 18" Elev 523.22
 154' 18" Elev 523.22
 155' 18" Elev 523.22
 156' 18" Elev 523.22
 157' 18" Elev 523.22
 158' 18" Elev 523.22
 159' 18" Elev 523.22
 160' 18" Elev 523.22
 161' 18" Elev 523.22
 162' 18" Elev 523.22
 163' 18" Elev 523.22
 164' 18" Elev 523.22
 165' 18" Elev 523.22
 166' 18" Elev 523.22
 167' 18" Elev 523.22
 168' 18" Elev 523.22
 169' 18" Elev 523.22
 170' 18" Elev 523.22