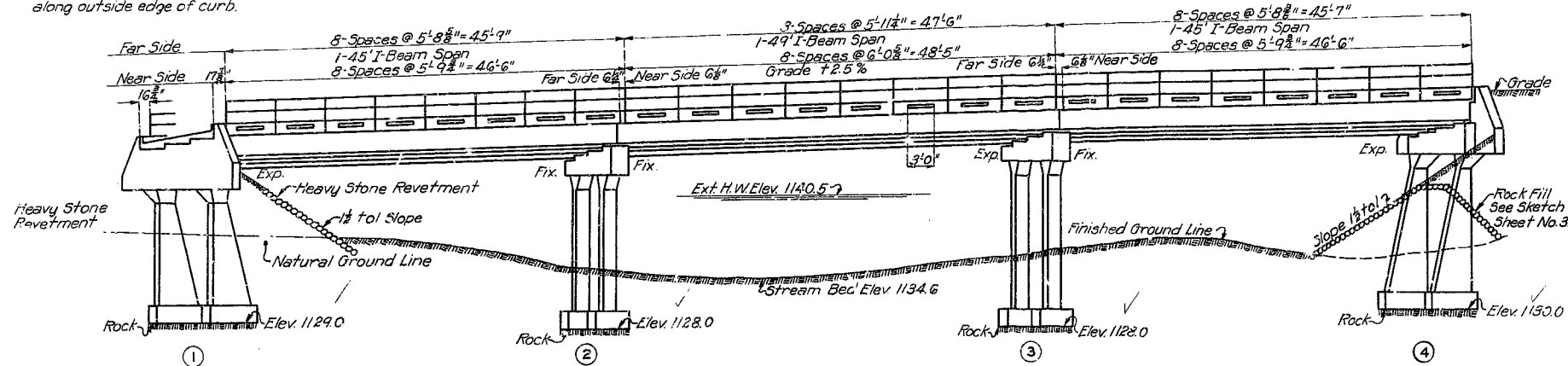


MISSOURI STATE HIGHWAY DEPARTMENT

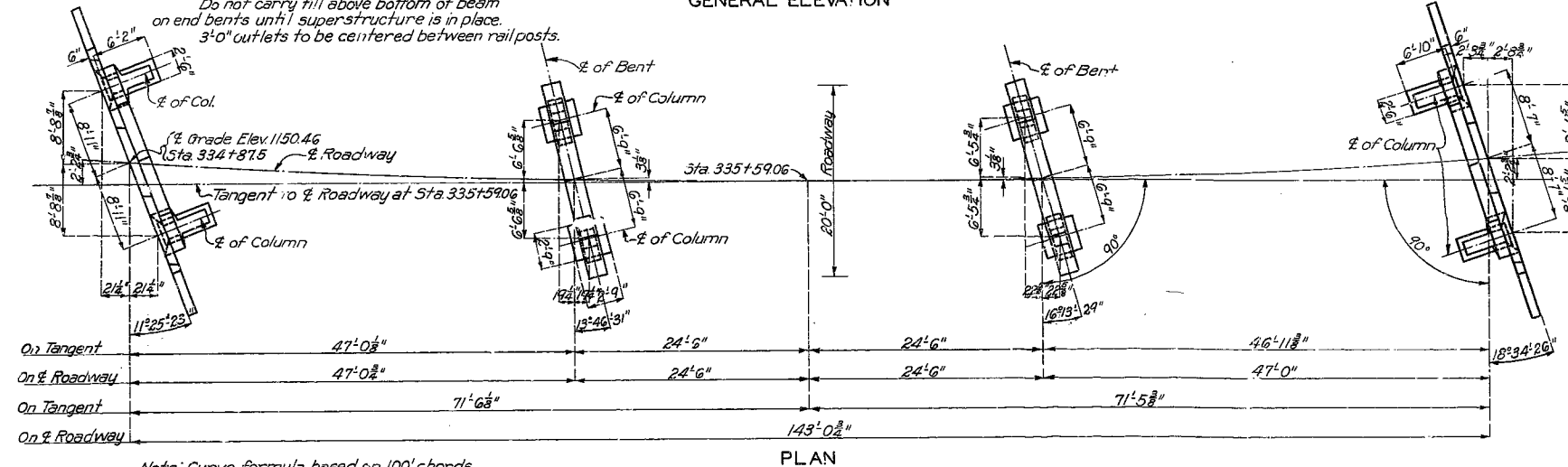
FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
5	MO.	S.J.-53	19		

Note: Dimensions of rail are given along outside edge of curb.



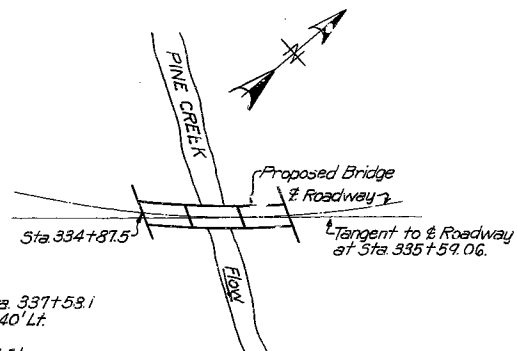
Note: Carry all footings at least 18" into shale and other soft rock or 6" into solid hard rock. Do not carry fill above bottom of beam on end bents until superstructure is in place. 3'-0" outlets to be centered between rail posts.

GENERAL ELEVATION

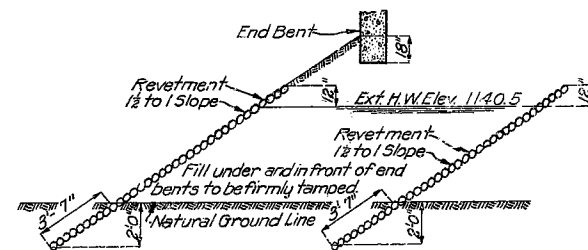


Note: Curve formula based on 100' chords cannot be used in locating bents.

PLAN



LOCATION SKETCH



FRONT OF END BENTS SIDE SLOPES OF FILL. Heavy stone revetment shall be placed on fill at end of bridge as shown in sketches. Approximately 100 sq. yds. of heavy stone revetment work included in road contract.

HEAVY STONE REVETMENT

GENERAL NOTES:

Concrete in superstructure to be 1:2.3 1/2 mix, Class "X". All other concrete to be 1:2.4 mix, Class "B". Exposed edges to be beveled 1/4" where no other bevel is noted. Detail shop drawings for structural steel shall be submitted to the State Highway Department in duplicate and shall be approved before steel is fabricated. Rivets 3/4" and holes 1 1/8", except in handrail; 5/8" button head bolts at expansion and 3/4" rivets at other connections in handrail. Paint: Shop, None. Field: Surfaces inaccessible after erection, three coats of red lead; contact surfaces with bolted field connections, one coat of red lead. No other paint to be applied by contractor. All paint required will be furnished by the Missouri State Highway Department. Cost of metallic edge moulding will be included in unit bid price for concrete. Where rubber compound is specified on plans for use in partition and expansion joints, the pre-moulded joint shall be securely stitched to one face of concrete with copper wire. Bridge excavation in accordance with Section I of Standard Specifications issued April 1, 1930.

COMPLETE BILL OF REINFORCING STEEL

No.	Size	Length	Mark	Location	Bending Sketches & Cutting Diagrams	No.	Size	Length	Mark	Location
Bents No. 1 & 2						Bents No. 2 & 3 (continued)				
20	3/4"	5'-9"	D1	Footings		30	3/4"	8'-0"	U1	Beams
12	3/4"	7'-6"	F1	Haunches		18	3/4"	10'-0"	U2	"
12	3/4"	7'-6"	F2	"						
18	3/4"	12'-9"	H1	Wings						
4	3/4"	21'-6"	H2	"						
18	3/4"	23'-3"	H3	Beams						
4	3/4"	21'-3"	H4	"						
6	3/4"	22'-9"	H5	"						
6	3/4"	10'-0"	H6	"						
2	3/4"	24'-6"	H7	Wings						
4	3/4"	12'-7"	T1	"						
4	3/4"	21'-6"	T2	Buckwalls						
4	3/4"	18'-6"	T3	Wings						
6	3/4"	7'-0"	V1	"						
2	3/4"	5'-3"	V2	"						
14	3/4"	4'-0"	V3	Backwalls						
12	3/4"	4'-6"	V4	"						
16	3/4"	5'-0"	V5	"						
2	3/4"	7'-0"	V6	Wings						
6	3/4"	10'-3"	V7	"						
16	3/4"	8'-6"	V8	Br-Bent #2						
9	3/4"	10'-6"	V9	"						
12	3/4"	8'-0"	V10	Haunches						
10	3/4"	15'-6"	V11	Columns						
10	3/4"	18'-0"	V12	"						
16	3/4"	9'-0"	V13	Br-Bent #1						
9	3/4"	11'-0"	V14	"						
Bents No. 2 & 3						Superstructure				
16	3/4"	5'-9"	D1	Footings		16	3/4"	24'-6"	C1	Curb
12	3/4"	8'-3"	F3	Haunches		16	3/4"	25'-0"	C2	"
12	3/4"	8'-3"	F4	"		8	3/4"	25'-6"	C3	"
12	3/4"	23'-3"	G1	Beams		8	3/4"	26'-0"	C4	"
4	3/4"	21'-0"	G2	"		168	3/4"	3'-0"	C5	"
18	3/4"	9'-6"	G3	"		550	3/4"	20'-9"	S1	Slab
4	3/4"	22'-0"	G4	"		104	3/4"	24'-6"	S2	"
8	3/4"	17'-6"	P1	Columns		40	3/4"	23'-9"	S3	"
8	3/4"	18'-9"	P2	"		20	3/4"	22'-3"	S4	"
42	3/4"	7'-0"	P3	"		52	3/4"	25'-6"	S5	"
						12	3/4"	21'-3"	S6	"

Note: Dimensions are given along centerline of bars and are for computed lengths. Reinforcing bars 3/4" or over in diameter, which are bent to an angle greater than 90°, shall be of structural grade.

ESTIMATED QUANTITIES

Item	Superstruct	Substructure	Total	FINAL QUAN.
Excavation Class I	Cu Yds.	25	25	30
Excavation Class II	Cu Yds.	76	76	76
Concrete 1:2.4 mix "B"	Cu Yds.	537	537	537
Concrete 1:2.3 1/2 mix "X"	Cu Yds.	684	684	684
Fabricated Structural Steel	Lbs.	58400	58400	58400
Reinforcing Steel	Lbs.	17720	23970	23970
Heating Concrete	Cu Yds.			45.8
Drilling Test Holes	Cu Yds.			40

Note: Bridge excavation above Elev. 1135.0 will be paid for as Class I Bridge Excavation. Bridge excavation below Elev. 1135.0 will be paid for as Class II Bridge Excavation.

B.M. Elev. 1137.33 - Nail in side 16" Sycamore 130' Rt Sta. 336+35.

BRIDGE OVER PINE CREEK

STATE ROAD FROM WILLOW SPRINGS TO ROUTE 17

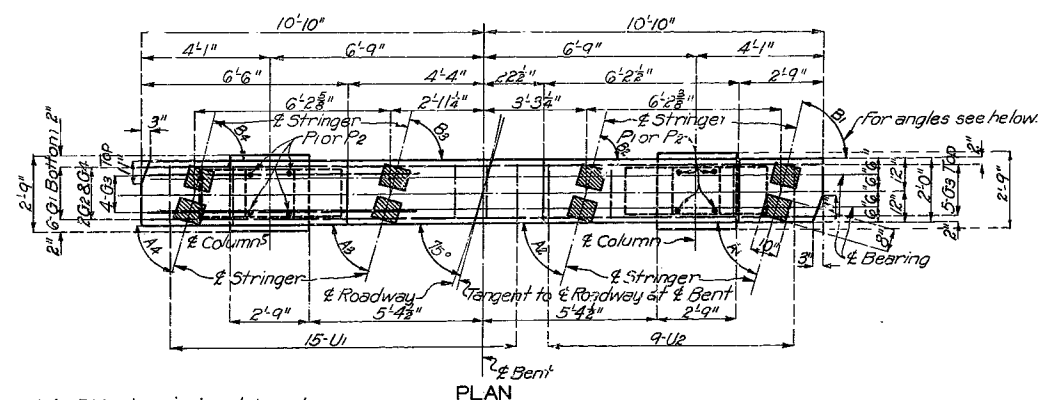
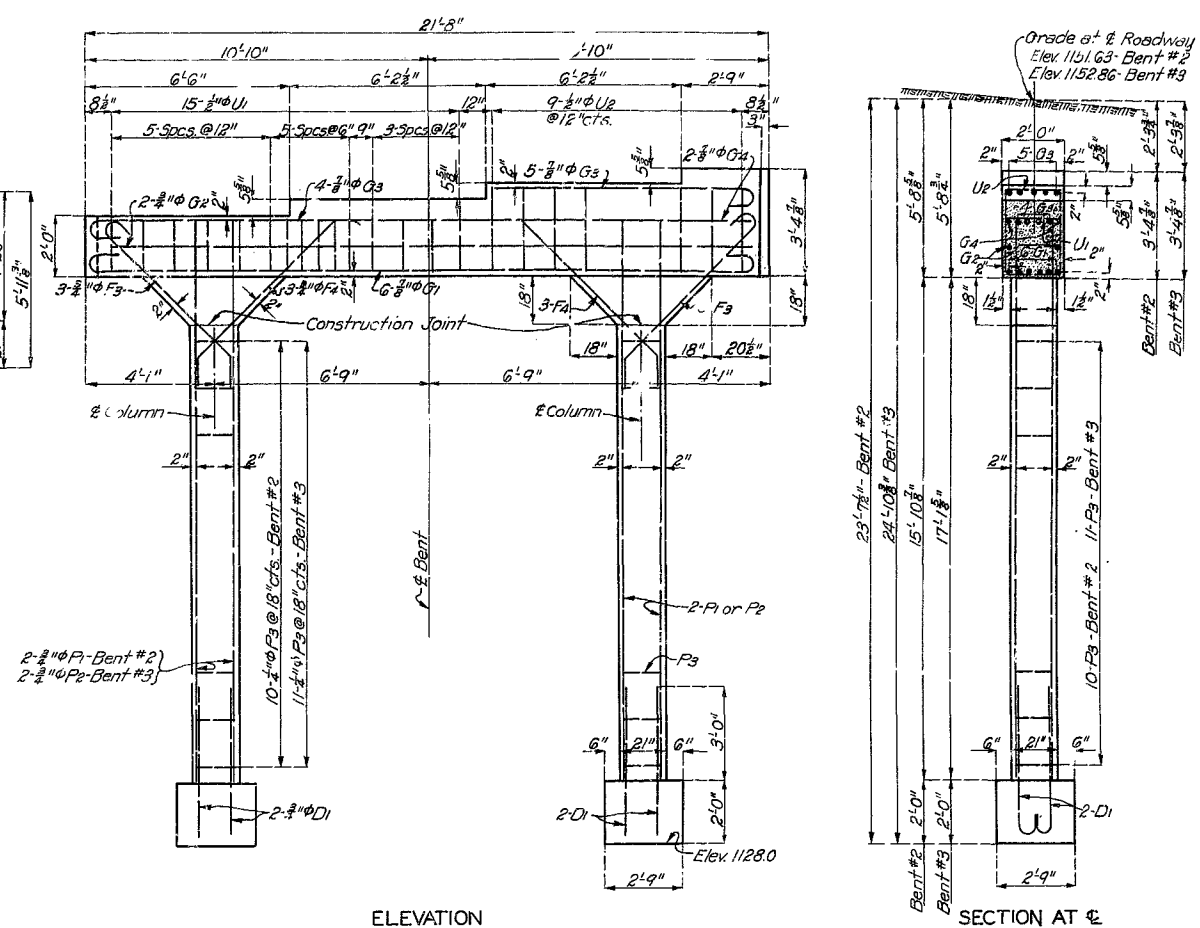
ABOUT 6.5 MILES N.E. OF WILLOW SPRINGS

PROJECT NO. S.J.-53 STA. 334+87.5

TEXAS COUNTY

SUBMITTED BY *M.R. Lark* DATE 11/18/32
APPROVED BY *T.H. Cutler* DATE 11/18/32
BRIDGE ENGINEER CHIEF ENGINEER

FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
5	MO.	SJ-53	19		



DETAILS OF BENTS NO. 2 & 3

	A_1	A_2	A_3	A_4	B_1	B_2	B_3	B_4
Bent#2	$15^{\circ}57'$	$73^{\circ}52'$	$73^{\circ}47'30''$	$73^{\circ}42'30''$	$76^{\circ}21'$	$76^{\circ}16'$	$76^{\circ}11'30''$	$76^{\circ}06'30''$
Bent#3	$73^{\circ}54'$	$73^{\circ}49'$	$73^{\circ}44'30''$	$73^{\circ}39'30''$	$76^{\circ}18'$	$76^{\circ}13'$	$76^{\circ}08'30''$	$76^{\circ}03'30''$

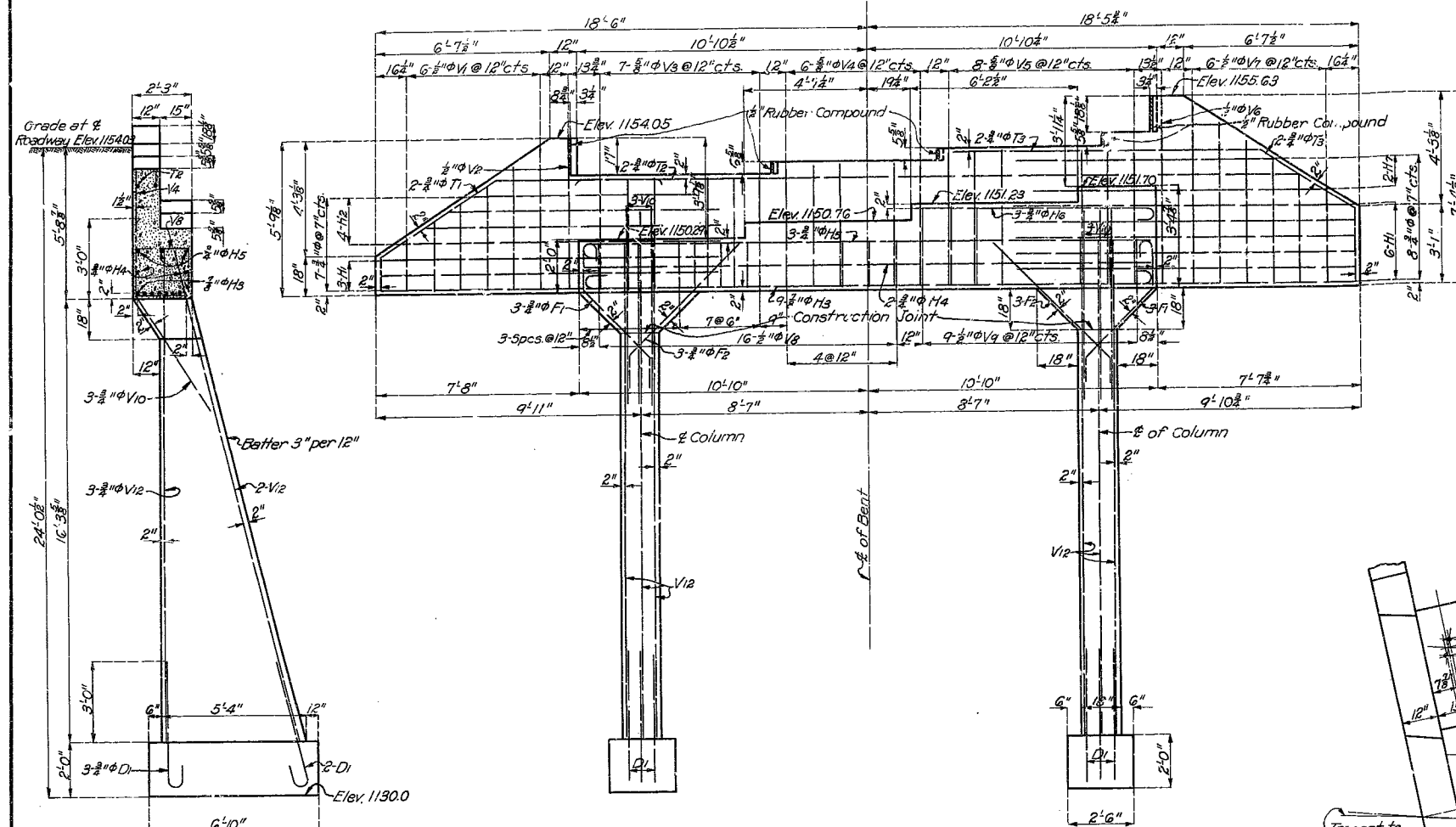
TEXAS COUNTY

Sheet No.2 of 4

S-528

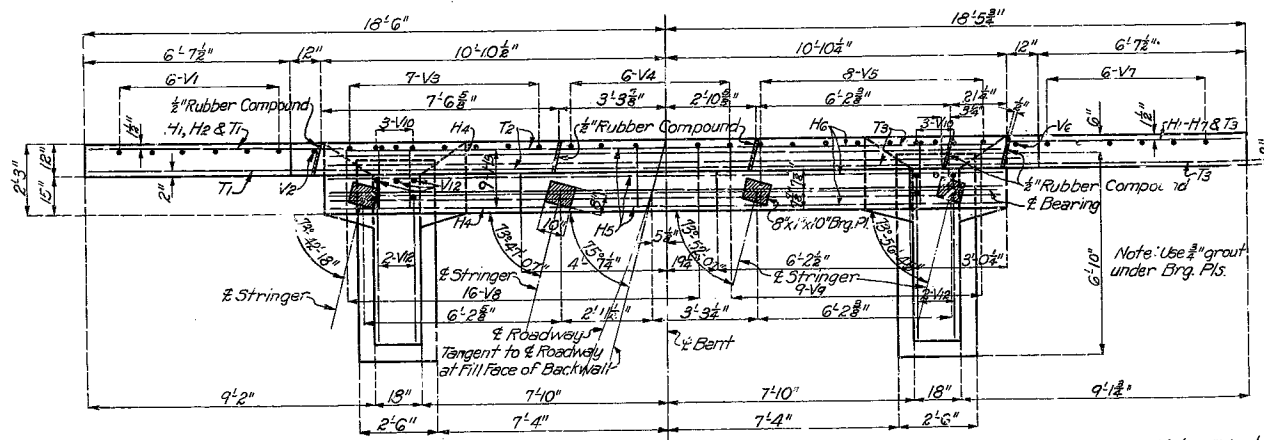
MISSOURI STATE HIGHWAY DEPARTMENT

FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
5	MO.	SJ-53	19		

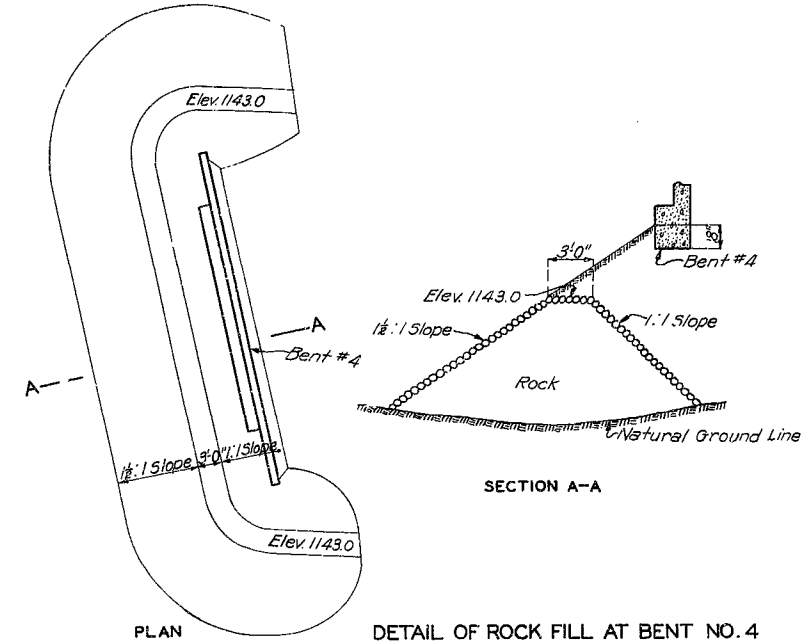


SECTION AT C

ELEVATION

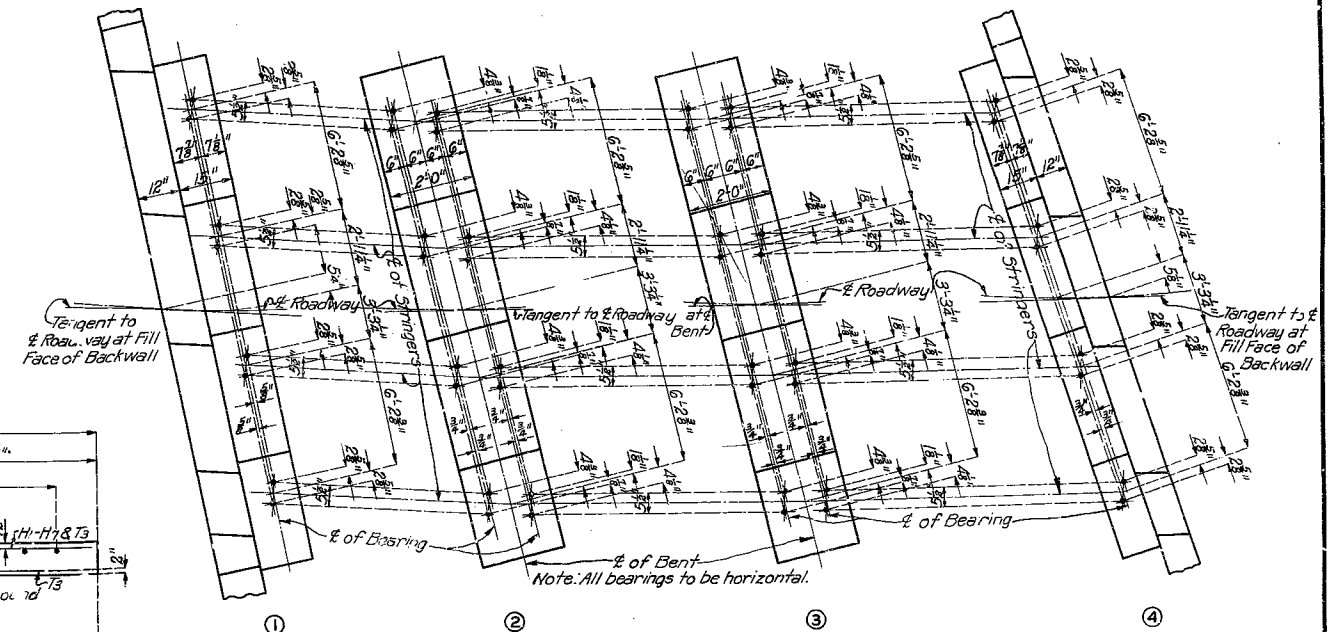


DETAILS OF END BENT NO. 4



PLAN

DETAIL OF ROCK FILL AT BENT NO. 4



ANCHOR BOLT PLAN

Note: This drawing is not to scale.
Follow dimensions.

BRIDGE OVER PINE CREEK

STATE ROAD FROM WILLOW SPRINGS TO ROUTE 17

ABOUT 6.5 MILES N.E. OF WILLOW SPRINGS

PROJECT NO. S.J.-53

STA. 33.4+ 87.5

TEXAS

COUNTY

S-528

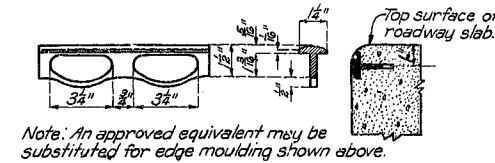
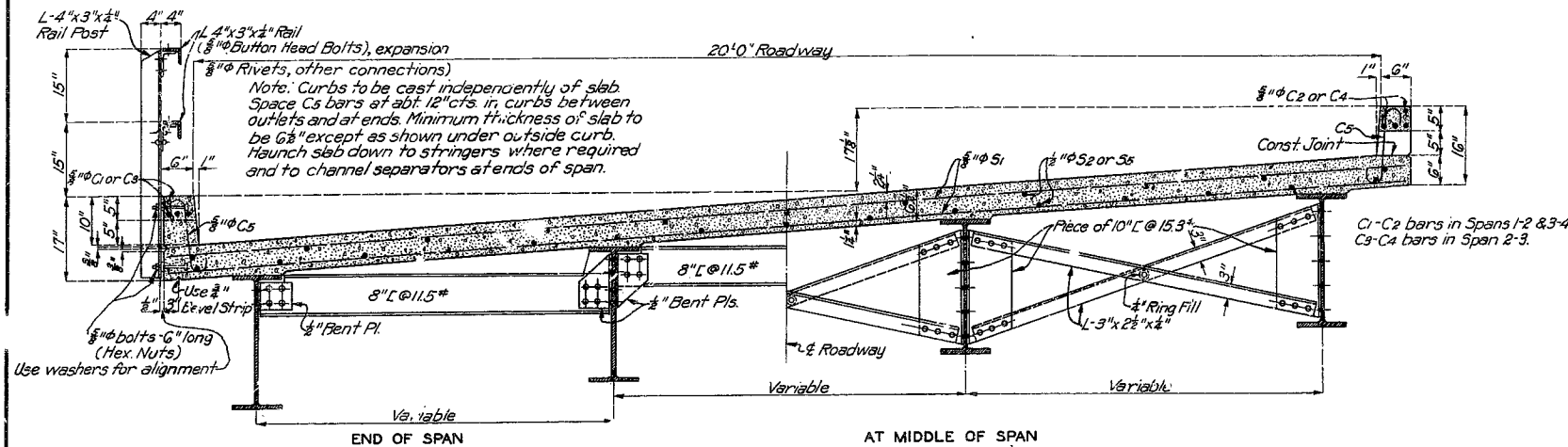
Drawn Nov. 1932 By N.W.R.
Traced Nov. 1932 By H.W.H.
Checked Nov. 1932 By I.B.

Sheet No. 3 of 4

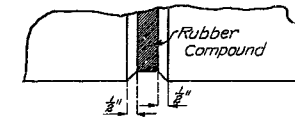
269

MISSOURI STATE HIGHWAY DEPARTMENT

FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
5	MO.	S.J.-53	19		

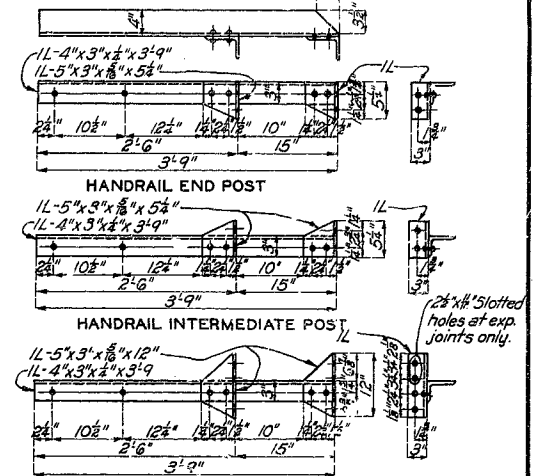


METALLIC EDGE Moulding

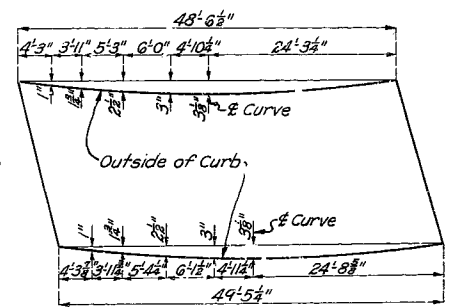


Note: Use bevel as shown for exposed faces of all joints consisting of rubber compound except at top surface of roadway slab. Use edging tool with 1/2" radius at top surface of roadway slab each side of rubber compound joint.

DETAILS OF BEVEL FOR RUBBER COMPOUND JOINTS

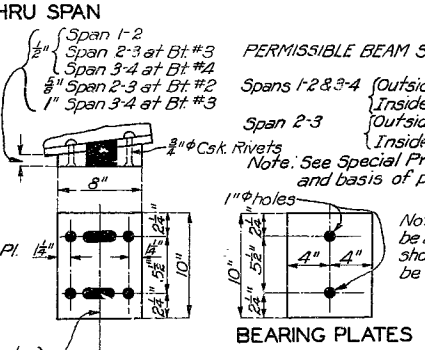
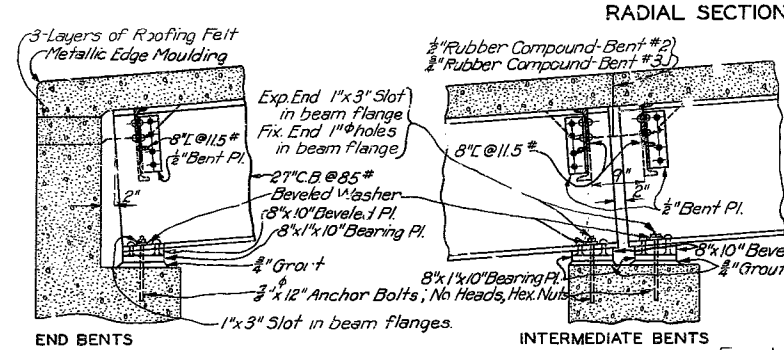


DETAILS OF HANDRAIL POSTS



Note: Floor slab to be brought to grade and dead load deflection taken care of by increasing slab thickness. Depth of slab at outside face of curb to be kept uniform and bottom surface of slab warped between curb and outside beam to obtain required thickness at beam. Payment will be allowed for additional concrete required for thickening slab. This additional concrete is included in Estimated Quantities.

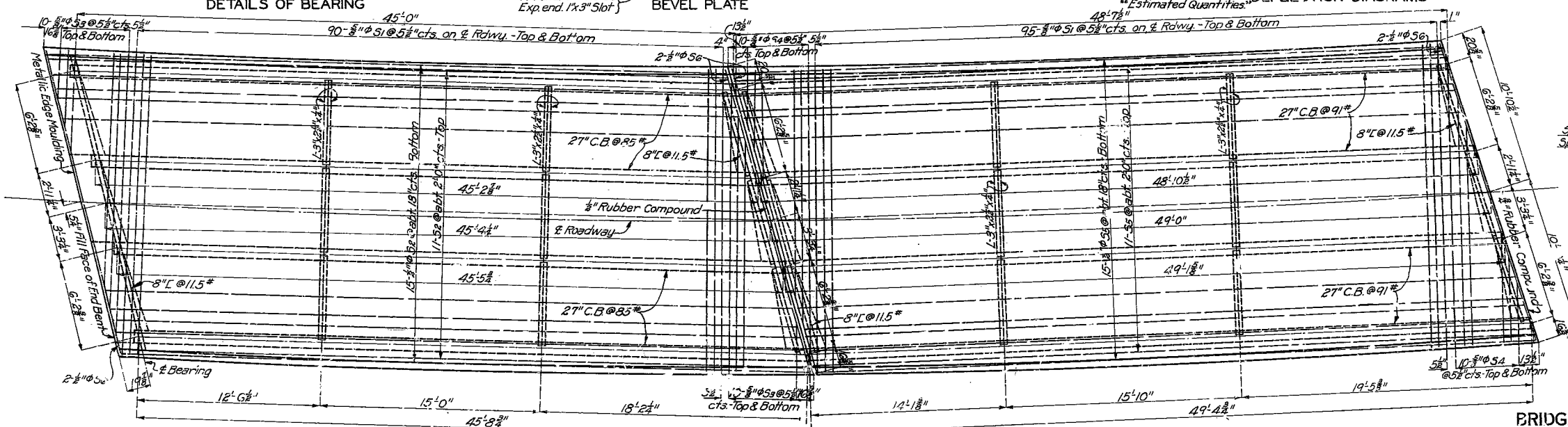
DEFLECTION DIAGRAMS



PERMISSIBLE BEAM SUBSTITUTIONS:

Spans 1-2 & 3-4 Outside Stringers 24" I @ 100" or 26" Beth. B @ 85"
Inside Stringers 24" I @ 105" or 26" Beth. B @ 85"
Span 2-3 Outside Stringers 24" I @ 105" or 28" Beth. B @ 91"
Inside Stringers 24" I @ 115" or 28" Beth. B @ 91"
Note: See Special Provisions in regard to permissible substitutions and basis of payment.

Note: Curb and outside edges of slab to be built parallel to roadway curve as shown by curb ordinates. All bearings to be horizontal.




BRIDGE OVER PINE CREEK

STATE ROAD FROM WILLOW SPRINGS TO ROUTE 17
ABOUT 8.5 MILES N.E. OF WILLOW SPRINGS
PROJECT NO. S.J.-53 STA. 334+87.5
TEXAS COUNTY

Drawn Oct. 1932 By N.W.R.
Traced Oct. 1932 By I.W.H.
Checked Nov. 1932 By I.B.


Note: This drawing is not to scale. Follow dimensions.


		<div>Missouri Department of Transportation</div> <div>State Bridge Inspection Report</div>				<div>December 21, 2022</div> <div>10:26:45AM</div>			
COUNTY: TEXAS		DISTRICT: SE		CLASS: STATBR		FED-ID: 8748		BRIDGE: S0528	
GENERAL STRUCTURE INFORMATION							***BRIDGE INSPECTION INFORMATION***		
<div>ROUTE: MO137S</div> <div>FEATURE: PINE CR</div> <div>STATUS: P-POSTLOAD</div> <div>LOG MILE: 36.004</div> <div>DETOUR: 22.00 MILES</div> <div>NHS: NO</div> <div>BUILT: 1932</div> <div>REHAB:</div> <div>LOCATION: S 34 T 28 R 9 W</div> <div>LATITUDE: 37 3 21.24 (DMS)</div> <div>LONGITUDE: 91 54 31.56 (DMS)</div>		<div># SPANS: 3</div> <div>LANES ON: 2</div> <div>LANES UNDER: 0</div> <div>COMPASS DIRECTION: WEST to EAST</div> <div>DIRECTION OF TRAFFIC: 2-WAY TRAF</div> <div>FUNCTIONAL CLASS: RL-MAJOR COLLECTOR</div> <div>NBI OWNER: MODOT</div> <div>NBI MAINTAINED: MODOT</div> <div>MAINTENANCE DISTRICT: SE</div> <div>MAINTENANCE COUNTY: TEXAS</div> <div>SUB AREA: 7H51</div>		<div>PLACE CODE: 57476 PIERCE</div> <div>LENGTH: 143 FT 0 IN</div> <div>MAXIMUM SPAN: 49 FT 0 IN</div> <div>APPROACH ROADWAY: 20 FT 0 IN</div> <div>CURB TO CURB: 20 FT 0 IN</div> <div>OUT TO OUT: 21 FT 0 IN</div> <div>AADT: 550</div> <div>AADT YEAR: 2021</div> <div>AADT TRUCK: 7.1%</div> <div>FUTURE AADT: 770</div> <div>FUTURE AADT YEAR: 2041</div>		<div>DATE: 11/07/2022</div> <div>RESPONSIBILITY: DISTRICT</div> <div>FREQUENCY: 12</div> <div>CALCULATED INTERVAL**: 12</div> <div>TEAM LEADER: ED HESS</div> <div>ELEMENT: NO</div> <div>INSPECTOR 2:</div> <div>INSPECTOR 4:</div> <div>INSPECTOR 3:</div> <div>** When calculated interval exceeds the frequency, a justification comment per BIRM is required.</div>			
						<div>GENERAL INSPECTION COMMENTS</div>			
FRACTURE CRITICAL INSPECTION INFORMATION					***INDEPTH INSPECTION INFORMATION***				
<div>DATE:</div> <div>FREQUENCY:</div> <div>TEAM LEADER:</div> <div>INSPECTOR 2:</div> <div>RESPONSIBILITY:</div> <div>CALCULATED INTERVAL**:</div> <div>INSPECTOR 3:</div> <div>INSPECTOR 4:</div> <div>CATEGORY:</div> <div>NBI:</div> <div>METHOD:</div> <div>** When calculated interval exceeds the frequency, a justification comment per BIRM is required.</div>					<div>DATE:</div> <div>FREQUENCY:</div> <div>TEAM LEADER:</div> <div>INSPECTOR 2:</div> <div>RESPONSIBILITY:</div> <div>CALCULATED INTERVAL**:</div> <div>INSPECTOR 3:</div> <div>INSPECTOR 4:</div> <div>CATEGORY:</div> <div>NBI:</div> <div>METHOD:</div> <div>** When calculated interval exceeds the frequency, a justification comment per BIRM is required.</div>				
FRACTURE CRITICAL INSPECTION COMMENTS					INDEPTH INSPECTION COMMENTS				
SPECIAL INSPECTION INFORMATION					***UNDERWATER INSPECTION INFORMATION***				
<div>DATE: 06/13/2014</div> <div>FREQUENCY: 120</div> <div>TEAM LEADER:</div> <div>INSPECTOR 2: ED HESS</div> <div>RESPONSIBILITY: DISTRICT</div> <div>CALCULATED INTERVAL**:</div> <div>INSPECTOR 3:</div> <div>INSPECTOR 4:</div> <div>CATEGORY: CHANNEL CROSS SEC</div> <div>NBI: NO</div> <div>METHOD: WT TAPE</div> <div>** When calculated interval exceeds the frequency, a justification comment per BIRM is required.</div>					<div>DATE: 11/07/2022</div> <div>FREQUENCY: 60</div> <div>TEAM LEADER: ED HESS</div> <div>INSPECTOR 2:</div> <div>RESPONSIBILITY: DISTRICT</div> <div>CALCULATED INTERVAL**: 12</div> <div>INSPECTOR 3:</div> <div>INSPECTOR 4:</div> <div>CATEGORY: DRY</div> <div>NBI: NO</div> <div>METHOD: VISUAL</div> <div>** When calculated interval exceeds the frequency, a justification comment per BIRM is required.</div>				
SPECIAL INSPECTION COMMENTS					UNDERWATER INSPECTION COMMENTS				
OTHER SPECIAL INSPECTIONS					OTHER UNDERWATER INSPECTIONS				
<div>DATE</div> <div>FREQUENCY</div> <div>CATEGORY</div> <div>NBI</div> <div>CALCULATED INTERVAL</div> <div>RESPONSIBILITY</div> <div>METHOD</div>					<div>DATE</div> <div>FREQUENCY</div> <div>CATEGORY</div> <div>NBI</div> <div>CALCULATED INTERVAL</div> <div>RESPONSIBILITY</div> <div>METHOD</div>				

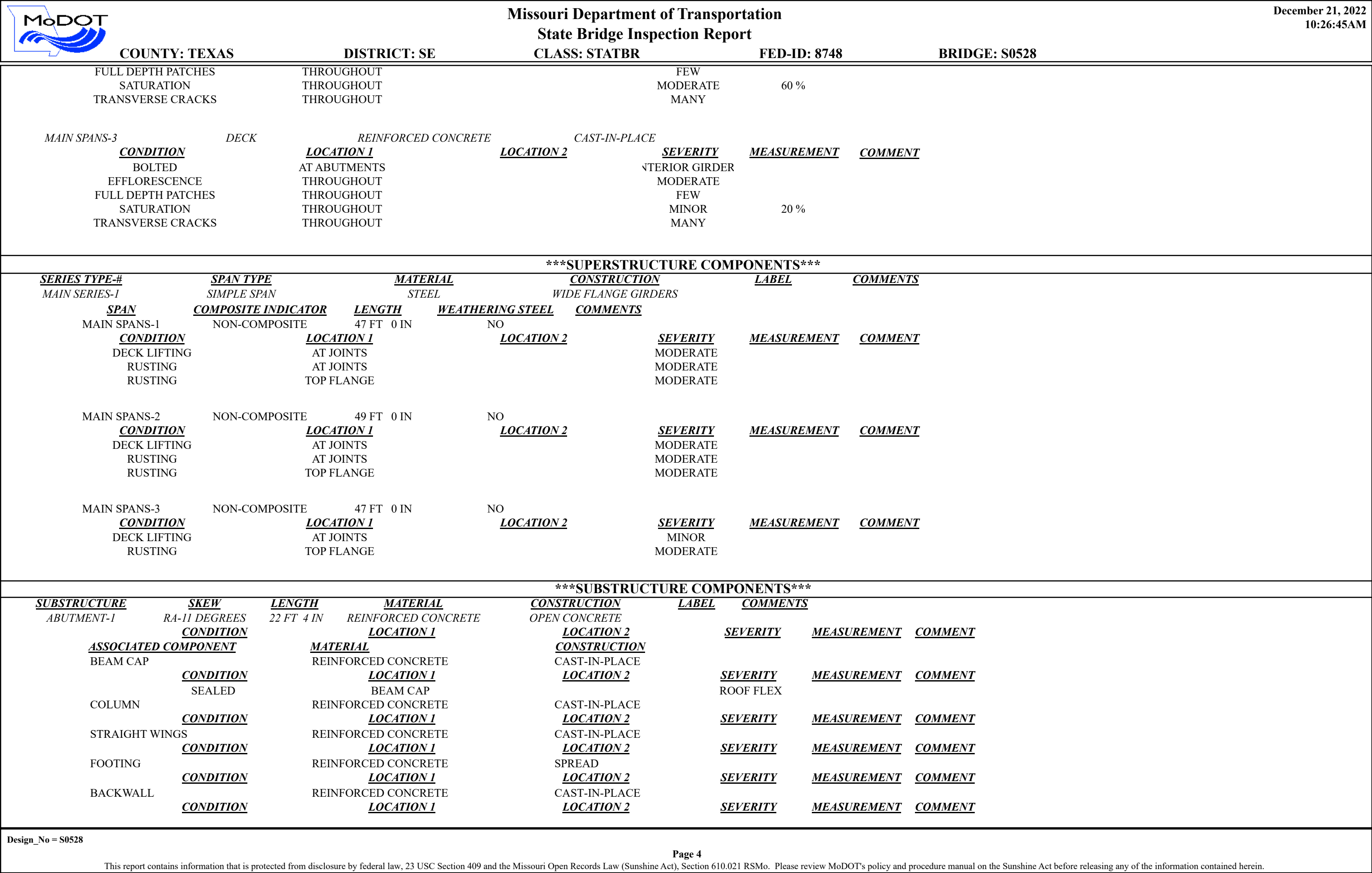
Design_No = S0528

Page 1

This report contains information that is protected from disclosure by federal law, 23 USC Section 409 and the Missouri Open Records Law (Sunshine Act), Section 610.021 RSMo. Please review MoDOT's policy and procedure manual on the Sunshine Act before releasing any of the information contained herein.

		Missouri Department of Transportation			December 21, 2022	
		State Bridge Inspection Report			10:26:45AM	
COUNTY: TEXAS		DISTRICT: SE	CLASS: STATBR	FED-ID: 8748	BRIDGE: S0528	
STRUCTURE POSTING						
APPROVED CATEGORY: S-7		TRUCKS OVER 18 TONS 15 MPH ON BRIDGE.				
Ton 1: 18		Ton 2:		Ton 3:		
COMMENTS:						
FIELD CATEGORY: S-7		TRUCKS OVER 18 TONS 15 MPH ON BRIDGE.				
Ton 1: 18		Ton 2:		Ton 3:		PROBLEM:
COMMENTS:		PROBLEM DIRECTION:				
GENERAL COMMENTS/MAJOR RATED ITEMS						
GENERAL COMMENTS: (BOWDEJ1, 01/06/2010)--(47'-49'-47') SMP WF GDR SPANS (CURVED)						
[ITEM 58] DECK: 4-POOR CONDITION		COMMENTS: (BRAWLK1, 11/04/2021)--DECK BOLTED, SATURATION THROUGH OUT				
RATING : 11/04/2021						
[ITEM 59] SUPER: 5-FAIR CONDITION		COMMENTS: (SHRUBM1, 12/23/2011)--RUSTING OF TOP & BOTTOM FLANGES				
RATING : 11/16/2015						
[ITEM 60] SUB: 4-POOR CONDITION		COMMENTS: (STEGEC, 05/02/2005)--TWO BEARING SEATS AND ONE CAP END REPAIRED ON SOUTH BENT BY DIV. REPAIR CREW (HR2) IN 4 / 2005				
RATING : 11/07/2022						
[ITEM 61] BANK/CHANNEL: 6-WIDESPREAD MINOR DAMAGE		COMMENTS: (DENNIB1, 11/05/2019)--MINOR EROSION ON BANK				
RATING : 11/05/2019						
[ITEM 113] SCOUR: 8-STABLE FOR CALCULATED		COMMENTS:				
RATING : 05/18/2001						
EVALUATION TYPE :						
[ITEM 71] WATERWAY ADEQUACY: DECK/APPRCH OVERTOP SLIGT		COMMENTS:				
RATING : 05/18/2001						
[ITEM 72] APPRRDWY ALIGNMENT: 6-SATISFACTORY		COMMENTS:				
RATING : 05/18/2001						
RAILING AND APPROACH PAVEMENT COMPONENTS AND RATINGS						
[ITEM 36A] BRIDGE RAILING RATING: DOESNT MEET CURRNT STND-0		RATING : 01/06/2010		COMMENTS:		
<u>MATERIAL</u>	<u>CONSTRUCTION</u>	<u>DIRECTION</u>	<u>COMMENTS</u>			
REINFORCED CONCRETE	CURB	BOTH				
STEEL	ANGLE-DOUBLE	BOTH				
[ITEM 36B] TRANSITION RAILING RATING: NOT PROVIDED-0		RATING : 05/18/2001		COMMENTS:		
[ITEM 36C] APPROACH RAILING RATING: NOT PROVIDED-0		RATING : 05/18/2001		COMMENTS:		
[ITEM 36D] RAIL END TREATMENT RATING: NOT PROVIDED-0		RATING : 05/18/2001		COMMENTS:		
Design_No = S0528						
Page 2						
This report contains information that is protected from disclosure by federal law, 23 USC Section 409 and the Missouri Open Records Law (Sunshine Act), Section 610.021 RSMo. Please review MoDOT's policy and procedure manual on the Sunshine Act before releasing any of the information contained herein.						

		Missouri Department of Transportation				December 21, 2022	
		State Bridge Inspection Report				10:26:45AM	
COUNTY: TEXAS		DISTRICT: SE		CLASS: STATBR		FED-ID: 8748	
						BRIDGE: S0528	
APPROACH PAVEMENT: *Overall condition assigned for each approach pavemenet component is shown below.							
<u>MATERIAL</u>		<u>CONSTRUCTION</u>		<u>DIRECTION</u>		<u>CONDITION*</u>	
ASPHALT		BITUMINOUS MAT		BOTH			
DRAINAGE, EXPANSION DEVICES, BANK/SLOPE, AND DECK PROTECTIVE COMPONENTS							
<u>DECK PROTECTIVE COMPONENTS:</u>							
<u>SERIES TYPE-#</u>		<u>COMPONENT</u>		<u>MATERIAL</u>		<u>CONSTRUCTION</u>	
MAIN SERIES-1		WEARING SURFACE		ASPHALT		BITUMINOUS SEAL COAT	
						<u>THICKNESS</u>	
						2.7 IN	
						<u>YEAR APPLIED</u>	
						2009	
						<u>MANUFACTURE</u>	
						<u>OVERALL CONDITION</u>	
						GOOD	
<u>COMMENT:</u> (BOWDEJ1, 04/28/2010)--SEALED IN 91- STRIPPING & MAT OVERLAY 2005 - CHIP SEAL OVER MAT OVERLAY 2009							
		<u>DECK PROTECTION</u>		<u>NOTAPPLICABLE</u>		<u>NONE</u>	
<u>COMMENT:</u>							
		<u>MEMBRANE</u>		<u>NOTAPPLICABLE</u>		<u>NONE</u>	
<u>COMMENT:</u>							
<u>DRAINAGE COMPONENTS:</u>							
		<u>COMPONENT</u>		<u>MATERIAL</u>		<u>CONSTRUCTION</u>	
		DRAINAGE		REINFORCED CONCRETE		CURB OUTLET	
						<u>DIRECTION</u>	
						<u>COMMENTS</u>	
<u>EXPANSION DEVICE COMPONENTS:</u>							
<u>SUB UNIT-#</u>		<u>SUB LABEL</u>		<u>COMPONENT</u>		<u>MATERIAL</u>	
BENT-2				CLOSED EXPANSION JOINT		FELT	
						<u>CONSTRUCTION</u>	
						FILLED JOINT	
						<u>GAP</u>	
						<u>YEAR APPLIED</u>	
						<u>MANUFACTURE</u>	
						<u>OVERALL CONDITION</u>	
<u>COMMENT:</u>							
BENT-3				CLOSED EXPANSION JOINT		FELT	
						<u>CONSTRUCTION</u>	
						FILLED JOINT	
<u>COMMENT:</u>							
<u>BANK/SLOPE PROTECTION COMPONENTS:</u>							
		<u>COMPONENT</u>		<u>MATERIAL</u>		<u>CONSTRUCTION</u>	
		BANK PROTECTION		ROCK		GROUTED	
						<u>DIRECTION</u>	
						BOTH	
						<u>COMMENTS</u>	
DECK COMPONENTS							
<u>SPAN TYPE-#</u>		<u>COMPONENT</u>		<u>MATERIAL</u>		<u>CONSTRUCTION</u>	
MAIN SPANS-1		DECK		REINFORCED CONCRETE		CAST-IN-PLACE	
						<u>COMMENTS</u>	
		<u>CONDITION</u>		<u>LOCATION 1</u>		<u>LOCATION 2</u>	
		BOLTED		AT ABUTMENTS			
		EFFLORESCENCE		THROUGHOUT			
		FULL DEPTH PATCHES		THROUGHOUT			
		SATURATION		THROUGHOUT			
		TRANSVERSE CRACKS		THROUGHOUT			
						<u>SEVERITY</u>	
						MODERATE	
						<u>MEASUREMENT</u>	
						60 %	
						<u>COMMENT</u>	
MAIN SPANS-2		DECK		REINFORCED CONCRETE		CAST-IN-PLACE	
						<u>COMMENTS</u>	
		<u>CONDITION</u>		<u>LOCATION 1</u>		<u>LOCATION 2</u>	
		EFFLORESCENCE		THROUGHOUT			
						<u>SEVERITY</u>	
						MODERATE	
						<u>MEASUREMENT</u>	
						<u>COMMENT</u>	
Design_No = S0528							
Page 3							
This report contains information that is protected from disclosure by federal law, 23 USC Section 409 and the Missouri Open Records Law (Sunshine Act), Section 610.021 RSMo. Please review MoDOT's policy and procedure manual on the Sunshine Act before releasing any of the information contained herein.							





Missouri Department of Transportation State Bridge Inspection Report

December 21, 2022
10:26:45AM

COUNTY: TEXAS

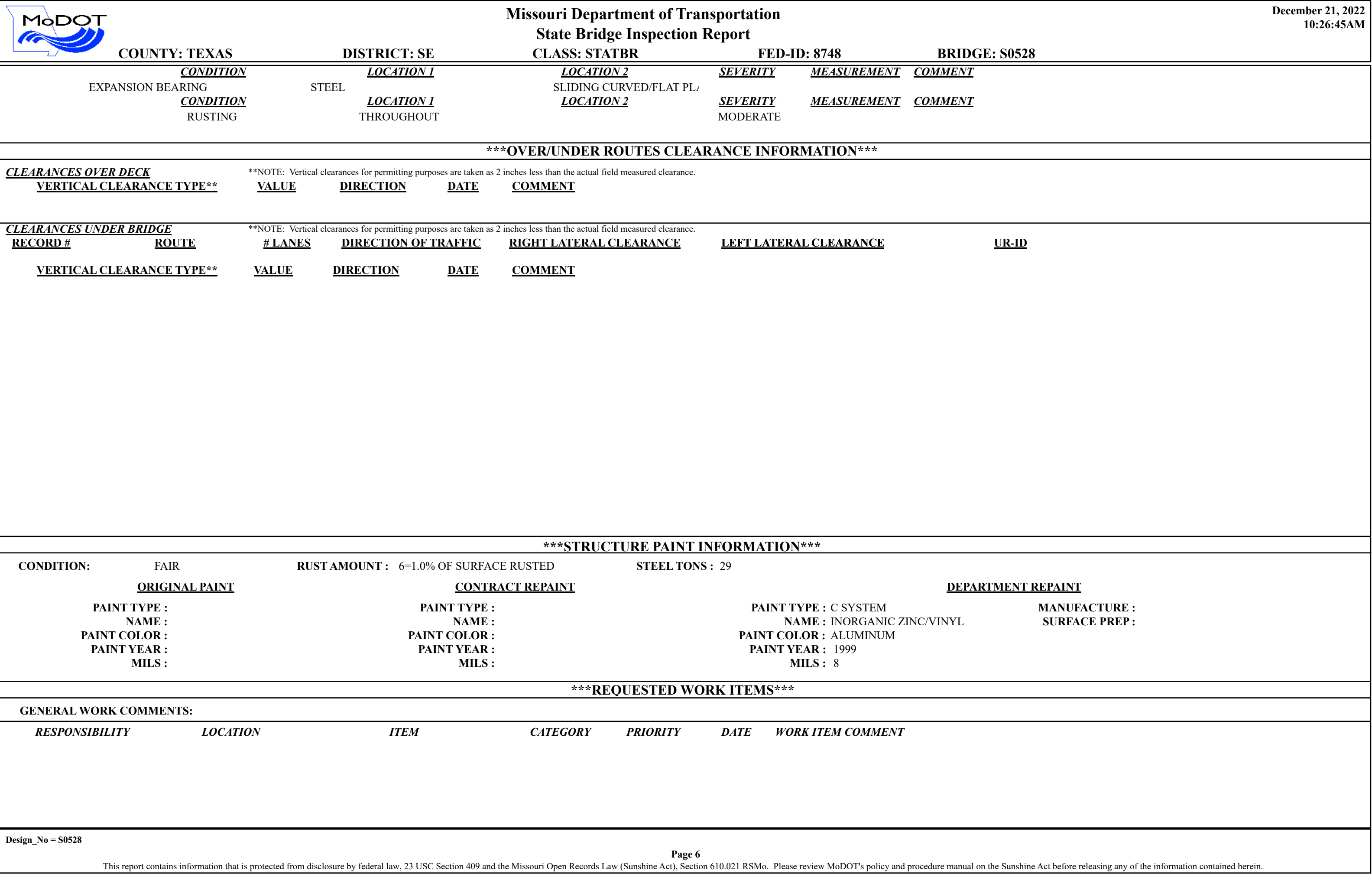
DISTRICT: SE


CLASS: STATBR

FED-ID: 8748

BRIDGE: S0528

EXPANSION BEARING			STEEL	SLIDING CURVED/FLAT PL.			
	<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
	RUSTING		THROUGHOUT		MODERATE		
BENT-2	RA-14 DEGREES	21 FT 8 IN	REINFORCED CONCRETE	MULTIPLE COLUMN			
	<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
<u>ASSOCIATED COMPONENT</u>			<u>MATERIAL</u>	<u>CONSTRUCTION</u>			
BEAM CAP			REINFORCED CONCRETE	CAST-IN-PLACE			
	<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
	HORIZONTAL CRACKS		RANDOM		MEDIUM		
	SEALED		BEAM CAP		ROOF FLEX		
COLUMN			REINFORCED CONCRETE	CAST-IN-PLACE			
	<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
	REBAR EXPOSED		RANDOM		MINOR		
	VERTICAL CRACKS		RANDOM		MODERATE		
FOOTING			REINFORCED CONCRETE	SPREAD			
	<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
EXPANSION BEARING			STEEL	SLIDING CURVED/FLAT PL.			
	<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
	RUSTING		THROUGHOUT		MODERATE		
BENT-3	RA-16 DEGREES	21 FT 8 IN	REINFORCED CONCRETE	MULTIPLE COLUMN			
	<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
<u>ASSOCIATED COMPONENT</u>			<u>MATERIAL</u>	<u>CONSTRUCTION</u>			
BEAM CAP			REINFORCED CONCRETE	CAST-IN-PLACE			
	<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
	REBAR EXPOSED		BOTTOM		MODERATE		
	SEALED		BEAM CAP		ROOF FLEX		
	SPALLS		BOTTOM		MODERATE		
COLUMN			REINFORCED CONCRETE	CAST-IN-PLACE			
	<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
	VERTICAL CRACKS		RANDOM		MODERATE		
FOOTING			REINFORCED CONCRETE	SPREAD			
	<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
EXPANSION BEARING			STEEL	SLIDING CURVED/FLAT PL.			
	<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
	RUSTING		THROUGHOUT		MODERATE		
ABUTMENT-4	RA-19 DEGREES	21 FT 8 IN	REINFORCED CONCRETE	OPEN CONCRETE			
	<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
<u>ASSOCIATED COMPONENT</u>			<u>MATERIAL</u>	<u>CONSTRUCTION</u>			
BEAM CAP			REINFORCED CONCRETE	CAST-IN-PLACE			
	<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
	SEALED		BEAM CAP		ROOF FLEX		
COLUMN			REINFORCED CONCRETE	CAST-IN-PLACE			
	<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
STRAIGHT WINGS			REINFORCED CONCRETE	CAST-IN-PLACE			
	<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
FOOTING			REINFORCED CONCRETE	SPREAD			
	<u>CONDITION</u>		<u>LOCATION 1</u>	<u>LOCATION 2</u>	<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
BACKWALL			REINFORCED CONCRETE	CAST-IN-PLACE			



		<div>Missouri Department of Transportation</div> <div>State Bridge Inspection Report</div>				<div>December 21, 2022</div> <div>10:26:45AM</div>																																																								
COUNTY: TEXAS		DISTRICT: SE		CLASS: STATBR		FED-ID: 8748		BRIDGE: S0528																																																						
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PROGRAM NOTES INFORMATION																																																														
<div><div>YEAR</div><div>2025</div></div> <div><div>PROJECT #</div><div>SE0027</div></div> <div><div>MONTH LET</div><div>1</div></div> <div><div>YEAR LET</div><div>2025</div></div> <div><div>ITEMS</div><div>REPLACE BRIDGE</div></div> <div><div>COMMENT</div></div>																																																														
COMPUTER GENERATED RATINGS AND DEFICIENCY ITEMS					***ADVANCED SIGN INFORMATION***																																																									
<div>NOTE: The items listed in this section are updated whenever computer edits are ran on a structure after the inspection updates have been entered in to TMS.</div> <table><tr><td><u>Rated Item</u></td><td><u>Rating</u></td><td><u>Rating Date</u></td></tr><tr><td>[Item 67] Structure Evaluation Rating:</td><td>2-BASICALLY INTOLRBLE REQ</td><td>2/17/2022</td></tr><tr><td>[Item 68] Deck Geometry Rating:</td><td>3-BASICALLY INTOL CORRECT</td><td>3/25/2002</td></tr><tr><td>[Item 69] Underclearance:</td><td>N-NOT APPLICABLE</td><td>5/18/2001</td></tr><tr><td>Sufficiency Rating:</td><td>4.0%</td><td>11/10/2022</td></tr><tr><td>Deficiency:</td><td>STRUCTURAL</td><td>4/2/2003</td></tr><tr><td>Funding Eligibility:</td><td>FULL</td><td>----</td></tr><tr><td>Estimated New Structure Length:</td><td>174 FT.</td><td>----</td></tr><tr><td>Estimated Structure Cost:</td><td>\$1,009,026</td><td>----</td></tr><tr><td>Estimated Total Project Cost:</td><td>\$1,513,539</td><td>----</td></tr><tr><td>Year of Cost Estimate:</td><td>2022</td><td>----</td></tr></table> <div>NOTE: The above structure length and cost estimates are computer generated using algorithmis in the TMS system. These algorithmis are generalized to use NBI items to come up with a new structure length and width to calculate a new area which is taken times a representative cost per square foot. The actual structure size and cost may vary significantly from these numbers once site specific engineering is done.</div>					<u>Rated Item</u>	<u>Rating</u>	<u>Rating Date</u>	[Item 67] Structure Evaluation Rating:	2-BASICALLY INTOLRBLE REQ	2/17/2022	[Item 68] Deck Geometry Rating:	3-BASICALLY INTOL CORRECT	3/25/2002	[Item 69] Underclearance:	N-NOT APPLICABLE	5/18/2001	Sufficiency Rating:	4.0%	11/10/2022	Deficiency:	STRUCTURAL	4/2/2003	Funding Eligibility:	FULL	----	Estimated New Structure Length:	174 FT.	----	Estimated Structure Cost:	\$1,009,026	----	Estimated Total Project Cost:	\$1,513,539	----	Year of Cost Estimate:	2022	----	<table><tr><td><div>SIGN #</div><div>1</div></td><td><div>SIGN TYPE</div></td><td><div>PROBLEM</div></td><td><div>PROBLEM DIRECTION</div></td></tr><tr><td colspan="4">***OUTFALL INSPECTION INFORMATION***</td></tr><tr><td><div># OUTFALLS:</div></td><td colspan="3"><div>INSPECTOR:</div></td></tr><tr><td><div>STATUS:</div></td><td colspan="3"><div>DATE:</div></td></tr><tr><td><div>NOTES:</div></td><td colspan="3"></td></tr></table>					<div>SIGN #</div> <div>1</div>	<div>SIGN TYPE</div>	<div>PROBLEM</div>	<div>PROBLEM DIRECTION</div>	***OUTFALL INSPECTION INFORMATION***				<div># OUTFALLS:</div>	<div>INSPECTOR:</div>			<div>STATUS:</div>	<div>DATE:</div>			<div>NOTES:</div>			
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Design_No = S0528

Page 7

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Missouri Department of Transportation
Bridge Inventory and Inspection System
Structural Inventory & Appraisal Sheet

December 21, 2022
10:20:37am

COUNTY : TEXAS BRIDGE : S0528 REVIEW STATUS : APPROVED NBI STATUS : T
RECORD TYPE : ROUTE CARRIED 'ON' STRUCT RUN DATE : 11/10/2022 SUBMITTAL YEAR : 2022

GENERAL STRUCTURE INFORMATION

1	State	MISSOURI
2	District	SE
3	County	TEXAS
8	Federal ID No.	8748
27	Year Built	1932
106	Year Reconstructed	0
42A	Type of Service On	HIGHWAY
21	Structure Maintenance	STATE HIGHWAY AGENCY
22	Structure Owner	STATE HIGHWAY AGENCY
33	Br. Median Code	NO MEDIAN
37	Historical Significance	HISTORICAL SIGNIF UNKNWN
101	Parallel Struc Desg	NONE EXISTS
103	Temporary Structure	NOT TEMPORARY
112	NBIS Bridge Length	YES

ROUTE DESIGNATION INFORMATION

5A	Record Type	ROUTE CARRIED 'ON' STRUCT
5B	Route Signing Prefix	MO
5C	Designated Level of Service	MAINLINE
5D	Route Number	00137
5E	Directional Suffix	NOT APPLICABLE
7	Facility Carried	MO 137 S
12	Base Hwy. Network	NO
13A	LRS Inventory Route No.	
13B	Subroute No.	
20	Toll Status	ON FREE ROAD
26	Functional Classification	07-RURAL MAJOR COLLECTOR
28A	Lanes on Structure	02
100	STRAHNET Designation	RTE NOT A DEFENSE HWY
104	National Highway System	NOT ON NHS
105	Federal Lands Highway	NOT APPLICABLE
110	Designated Nat. Network	NO

STRUCTURE LOCATION INFORMATION

4	Place	PIERCE
	Code	57476
9	Location	S 34 T 28 N R 9 W
11	Milepoint	36.21 miles
16	Latitude	37 D 3 M 21 S
17	Longitude	91 D 54 M 32 S

STRUCTURE TRAFFIC INFORMATION

29	AADT	550
30	AADT Year	2021
102	Direction of Traffic	2-WAY TRAFFIC
109	AADT Truck Percent	7%
114	Future AADT	770
115	Future AADT Year	2041

UNDERRECORD INFORMATION

6	Features Intersected	PINE CR
42B	Type of Service Under	WATERWAY
28B	Lanes Under Structure	00
54A	Vert. Clearance Ref.	N/A
54B	Vert. Clearance	0 Ft. 0 In.
55A	Rt. Lat Clear Ref.	N/A
55B	Rt. Lat Clearance	0 Ft. 0 In.
56	Left Lat Clearance	0 Ft. 0 In.
38	Navigation Control	PERMIT NOT REQ
39	Nav Vertical Clear	0 Ft. 0 In.
40	Nav Horizontal Clear	0 Ft. 0 In.
111	Nav. Pier Protection	
116	Nav. Cl. Vert. Clear	

STRUCTURE GEOMETRIC INFORMATION

10	Inventory Rte. Vert. Clear	99 Ft. 99 In.
19	By pass Detour Length	21.88 miles
32	Approach Roadway Width	20 Ft. 0 In.
34	Skew	15.00 Degrees
35	Struct. Flared	NO
47	Total Horiz. Clear	20 Ft. 0 In.
48	Maximum Span Length	48 Ft. 11 In.
49	Structure Length	143 Ft. 1 In.
50A	Left Curb/Sidewalk Width	0 Ft. 0 In.
50B	Right Curb/Sidewalk Width	0 Ft. 0 In.
51	Curb to Curb Br. Width	20 Ft. 0 In.
52	Deck Width (Out-Out)	20 Ft. 12 In.
53	Vert. Clearance Over Deck	99 Ft. 99 In.

Design_No = S0528 and Inventory_Appraisal_Submittal_Year = 2022



Missouri Department of Transportation
Bridge Inventory and Inspection System
Structural Inventory & Appraisal Sheet

December 21, 2022
10:20:37am

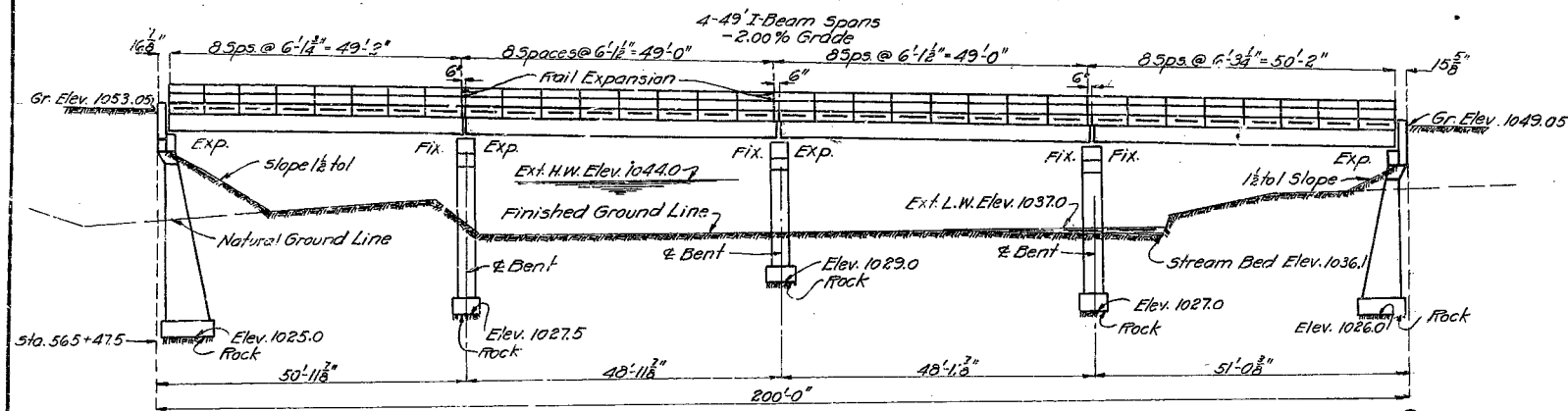
COUNTY : TEXAS BRIDGE : S0528 REVIEW STATUS : APPROVED NBI STATUS : T
RECORD TYPE : ROUTE CARRIED 'ON' STRUCT RUN DATE : 11/10/2022 SUBMITTAL YEAR : 2022

LOAD RATING AND POSTING INFORMATION				MATERIAL/CONSTRUCTION INFORMATION				
31	Design Load	H 10		43A	Main Struc. Mat type	STEEL		
41	Structure Status	POSTED FOR LOAD		43B	Main struc Constr. Type	STRINGER/MULTIBEAM - GRD		
63	Oper. Rating Meth.	ALLOWABLE STRESS		45	# of Main Spans	3		
64	Operating Rating	22 Tons.		44A	Appr Struc. Mat type	000		
65	Inventory Rating Meth	ALLOWABLE STRESS		44B	Appr Struc. Cnstr. type	000		
66	Inventory Rating	11 Tons.		46	# of Approach Span	0		
70	Bridge Posting Code	20.0-29.9% BELOW		107	Deck Mat/Constr.	1 CONCRETE CIP		
PROPOSED IMPROVEMENT INFORMATION				108A	Wear Surf Mat/Constr.	6 BITUMINOUS		
	Sufficiency Rating	4.0	Percent	108B	Membrane Mat/Constr.	0 NONE		
	Deficiency Rating	STRUCTURAL		108C	Deck Protect Mat/Constr.	0 NONE		
	Funding Eligibility	FULL		CONDITION RATING INFORMATION				
75A	Proposed Work	REPLACEMENT SUBSTND LOAD		58	Deck Cond. Rating	4		
75B	Work Done By	Contract		59	Superstructure Cond. Rating	5		
76	New Struc Length	173 Ft. 11 In.		60	Substructure Cond. Rating	4		
94	Struc Improve Cost	\$ 1,009,000		61	Channel /Channel Protection Cond. Rating	6		
95	Roadway Improve Cost	\$ 101,000		62	Culvert Cond. Rating	N		
96	Total Project Cost	\$ 1,514,000		INSPECTION INFORMATION				
97	Year of Cost Estimates	2022		90	Gen. Insp Date	11 / 22		
APPRAISAL RATING INFORMATION				91	Gen. Insp. Frequency	12	Months	
36A	Br. Rail App. Rating	DOES NOT MEET ACCEPT STND		92A	Frac. Critical Inspection	N	Months	
36B	Transition Rail App. Rating	DOES NOT MEET ACCEPT STND		93A	Frac. Critical Insp. Date			
36C	Approach Rail App. Rating	DOES NOT MEET ACCEPT STND		92B	Underwater Inspection	N	Months	
36D	Rail End Treat. App. Rating	DOES NOT MEET ACCEPT STND		93B	Underwater Insp. Date			
67	Struc Eval App. Rating	2		92C	Special Inspection	N	Months	
68	Deck Geometry App. Rating	3		93C	Special Inspection Date			
69	Underclearance App. Rating	N		BORDER BRIDGE INFORMATION				
71	Waterway Adeq. App. Rating	6		98	Neighboring State Code			
72	Approach Road App. Rating	6		98B	Neighboring State % Respon			
113	Scour Assess App. Rating	8		99	Neighboring State Struc. No.			
APPROVED POSTING INFORMATION				FIELD POSTING INFORMATION				
Approved Posting Category		S-7		Field Posting Category		S-7		
		Ton1	Ton2	Ton3		Ton1	Ton2	Ton3
Tonnage Values for Posting Sign		18		Tonnage Values for Posting Sign		18		
General Text for Posting Sign				General Text for Posting Sign				
TRUCKS OVER 18 TONS 15 MPH ON BRIDGE.				TRUCKS OVER 18 TONS 15 MPH ON BRIDGE.				

Design_No = S0528 and Inventory_Appraisal_Submittal_Year = 2022

MISSOURI STATE HIGHWAY DEPARTMENT

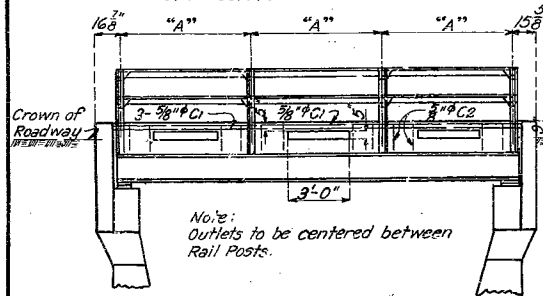
FED. ROAD DIST. NO.	STATE NO.	FED. AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
5	NO.	57-54	19		



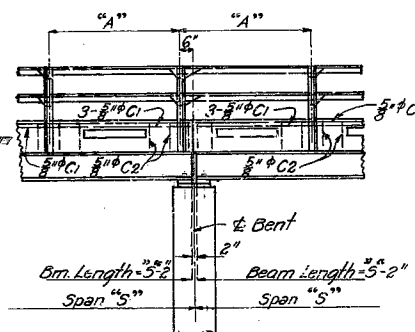
Note: Carry footings of all bents at least 18" into shale and other soft rock of 6" into solid hard rock.

GENERAL ELEVATION

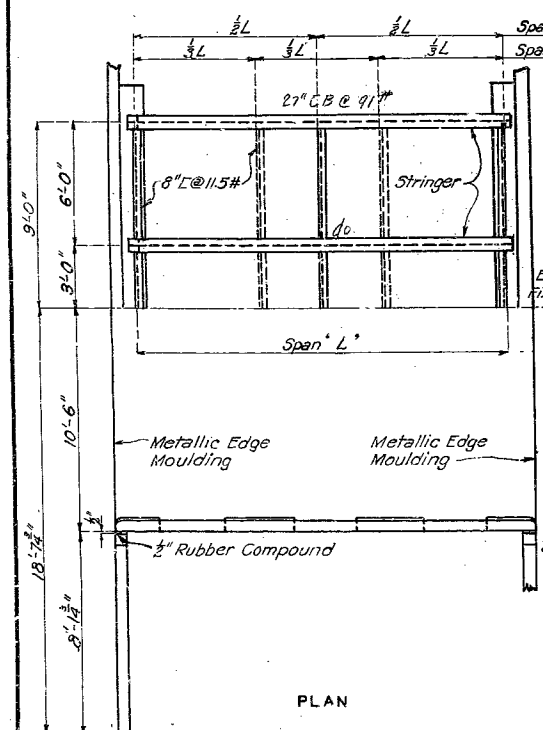
Note: Channel to be cleared above and below site approximately as shown in General Elevation.



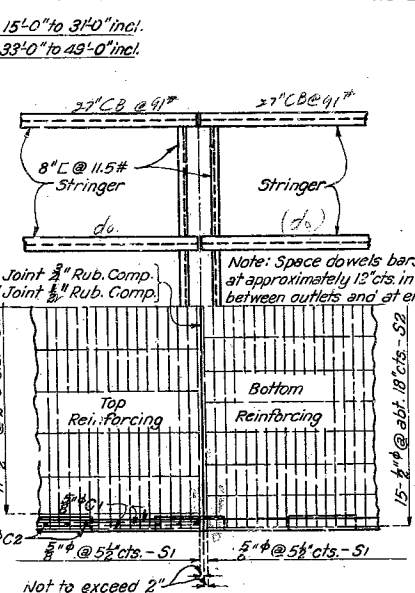
Note: Outlets to be centered between Rail Posts.



PART ELEVATION AT INT. BENT NO. 2



PLAN

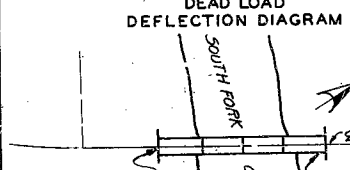


PART PLAN AT INT. BENT, SHOWING REINFORCING

HANDRAIL SPACING					
"A" DIMENSIONS					
SPAN "S"	NO. PANELS	SINGLE SPAN	1ST END SPAN	ALL INT. SPANS	LAST END SPAN
15'-0"	3	5'-5 1/2" abt.	5'-0 1/2" abt.	5'-0"	5'-4 1/2" abt.
17'-0"	3	6'-1 1/2" abt.	5'-8 1/2" abt.	5'-8"	6'-0 1/2" abt.
19'-0"	3	6'-9 1/2" abt.	6'-4 1/2" abt.	6'-4"	6'-8 1/2" abt.
21'-0"	4	5'-7"	5'-3 1/2" abt.	5'-3"	5'-6 1/2" abt.
23'-0"	4	6'-1"	5'-9 1/2" abt.	5'-9"	6'-6 1/2" abt.
25'-0"	4	6'-7"	6'-3 1/2" abt.	6'-3"	6'-6 1/2" abt.
27'-0"	5	5'-8"	5'-5 1/2" abt.	5'-5 1/2" abt.	5'-7 1/2" abt.
29'-0"	5	6'-0 1/2" abt.	5'-10" abt.	5'-9 1/2" abt.	6'-0 1/2" abt.
31'-0"	5	6'-5 1/2" abt.	6'-2 1/2" abt.	6'-2 1/2" abt.	6'-5 1/2" abt.
33'-0"	6	5'-8 1/2" abt.	5'-6 1/2" abt.	5'-6"	5'-8 1/2" abt.
35'-0"	6	6'-0 1/2" abt.	5'-10 1/2" abt.	5'-10"	6'-0 1/2" abt.
37'-0"	6	6'-4 1/2" abt.	6'-2 1/2" abt.	6'-2"	6'-4 1/2" abt.
39'-0"	7	5'-9 1/2" abt.	5'-7 1/2" abt.	5'-6 1/2" abt.	5'-8 1/2" abt.
41'-0"	7	6'-0 1/2" abt.	5'-10 1/2" abt.	5'-10 1/2" abt.	6'-0 1/2" abt.
43'-0"	7	6'-4"	6'-2"	6'-1 1/2" abt.	6'-3 1/2" abt.
45'-0"	8	5'-9 1/2" abt.	5'-7 1/2" abt.	5'-7 1/2" abt.	5'-9 1/2" abt.
47'-0"	8	6'-0 1/2" abt.	5'-10 1/2" abt.	5'-10 1/2" abt.	6'-0 1/2" abt.
49'-0"	8	6'-1 1/2" abt.	6'-1 1/2" abt.	6'-1 1/2" abt.	6'-3 1/2" abt.

DEFLECTION			
SPAN "S"	NO. PANELS	DEFLECTION	DEFLECTION
15'-0"	3	1/4"	1/4"
17'-0"	3	1/4"	1/4"
19'-0"	3	1/4"	1/4"
21'-0"	4	1/4"	1/4"
23'-0"	4	1/4"	1/4"
25'-0"	4	1/4"	1/4"
27'-0"	5	1/4"	1/4"
29'-0"	5	1/4"	1/4"
31'-0"	5	1/4"	1/4"
33'-0"	6	1/4"	1/4"
35'-0"	6	1/4"	1/4"
37'-0"	6	1/4"	1/4"
39'-0"	7	1/4"	1/4"
41'-0"	7	1/4"	1/4"
43'-0"	7	1/4"	1/4"
45'-0"	8	1/4"	1/4"
47'-0"	8	1/4"	1/4"
49'-0"	8	1/4"	1/4"

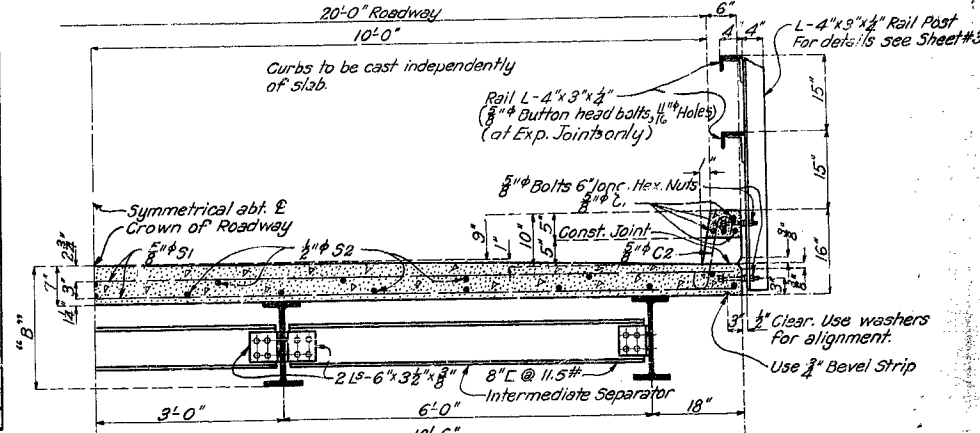
Note: Floor slab to be brought to grade and dead load deflection taken care of by increasing slab thickness. Depth of slab at outside face of curb to be kept uniform and bottom surface of slab warped between curb and outside beam to obtain required thickness at beam. Payment will be allowed for additional concrete required for thickening slab. This additional concrete is included in "Estimated Quantities."



LOCATION SKETCH

DIMENSION "B"	
SPAN "S"	"B"
15'-0"	20 1/2"
17'-0"	21"
19'-0"	22 1/2"
21'-0"	23"
23'-0"	23 1/2"
25'-0"	24"
27'-0"	24 1/2"
29'-0"	25"
31'-0"	25 1/2"
33'-0"	26"
35'-0"	26 1/2"
37'-0"	27"
39'-0"	27 1/2"
41'-0"	28"
43'-0"	28 1/2"
45'-0"	29"
47'-0"	29 1/2"
49'-0"	30"

Note: Top of channel separators at ends of each I-Beam Span to be flush with bottom of floor slab as shown in section thru end bent at E.



HALF SECTION THRU SPAN

TABLE OF STRINGERS					
SPAN "S"	PER PLANS		PERMISSIBLE SUBSTITUTIONS		
	INSIDE	OUTSIDE	CARNEGIE BEAMS	STANDARD I-BEAMS	BETHLEHEM BEAMS
15'-0"	14" @ 30"	14" @ 30"	12" @ 40.6"	12" @ 35.5"	14" @ 30"
17'-0"	14" @ 33"	14" @ 33"	15" @ 42.9"	15" @ 42.9"	14" @ 33"
19'-0"	16" @ 37"	16" @ 37"	15" @ 42.9"	15" @ 42.9"	16" @ 37"
21'-0"	16" @ 40"	16" @ 37"	15" @ 50"	15" @ 45"	16" @ 40"
23'-0"	16" @ 45"	16" @ 40"	18" @ 54.7"	18" @ 54.7"	16" @ 45"
25'-0"	18" @ 47"	18" @ 47"	18" @ 54.7"	18" @ 54.7"	18" @ 47"
27'-0"	18" @ 52"	18" @ 47"	18" @ 60"	18" @ 54.7"	18" @ 52"
29'-0"	20" @ 55"	20" @ 55"	20" @ 65.4"	20" @ 55"	20" @ 55"
31'-0"	21" @ 58"	21" @ 58"	20" @ 70"	20" @ 65.4"	22" @ 58"
33'-0"	21" @ 62"	21" @ 58"	20" @ 75"	20" @ 70"	22" @ 62"
35'-0"	21" @ 67"	21" @ 62"	20" @ 81.4"	20" @ 75"	22" @ 67"
37'-0"	24" @ 70"	24" @ 70"	24" @ 79.9"	24" @ 70"	24" @ 70"
39'-0"	24" @ 74"	24" @ 70"	24" @ 79.9"	24" @ 74"	24" @ 74"
41'-0"	24" @ 81"	24" @ 74"	24" @ 85"	24" @ 79.9"	24" @ 81"
43'-0"	24" @ 85"	24" @ 81"	24" @ 100"	24" @ 90"	26" @ 85"
45'-0"	27" @ 85"	27" @ 85"	24" @ 105.9"	24" @ 100"	26" @ 85"
47'-0"	27" @ 91"	27" @ 85"	24" @ 105.9"	24" @ 105.9"	28" @ 91"
49'-0"	27" @ 91"	27" @ 91"	24" @ 115"	24" @ 105.9"	28" @ 91"

GENERAL NOTES:

Loading: One 10 Ton Truck, 80% of weight on rear axle, 30% impact, 14'-0" wheel base, 6'-0" axle, 10' tire. Concrete in slab and curbs to be 1:2:4 mix, Class "B". All other concrete to be 1:2:4 mix, Class "B". Exposed edges to be beveled 1/4" where no other bevel is noted. Bridge excavation in accordance with Section 1 of Standard Specifications issued April 1, 1930, except that quantities paid for will be computed from extreme low water Elev. 1037.0, where existing ground line is below this elevation.

I-Beams with fastenings, spacers, handrail, handrail posts with fastenings, will be paid for as structural steel. Cost of metallic edge moulding will be included in unit bid price for concrete. Rivets 3/4", holes 1/2", except in handrail where rivets shall be 1/2" holes 1/4". Field connections riveted except as noted.

See Special Provisions in regard to permissible beam substitutions and basis of payment. Detail shop drawings shall be submitted to the State Highway Department in duplicate and shall be approved before steel is fabricated. Where rubber compound is specified on plans for use in partition and expansion joints, the pre-moulded joint shall be securely stitched to one face of concrete with copper wire. Paint: Shop, none; Field, contact surfaces with salted field connections one coat red lead. No other paint to be applied by contractor. All paint required will be furnished by the Missouri State Highway Department.

B.M. Elev. 1042.61 - Nail in rock of 30" water oak 80' L x 10" S x 11-1/2" W.

BRIDGE OVER SOUTH FORK OF JACKS FORK

STATE ROAD FROM WILLOW SPRINGS TO ROUTE 17 ABOUT 11 MILES N.E. OF WILLOW SPRINGS PROJECT NO. SJ-54 STA. 565 + 47.5 FINISHED

TEXAS COUNTY
SUBMITTED BY: N. R. Lark DATE: 11/17/32
APPROVED BY: T. H. Cutler DATE: 11/17/32
FINISHED

SHEET NO. 1 OF 4

S-529

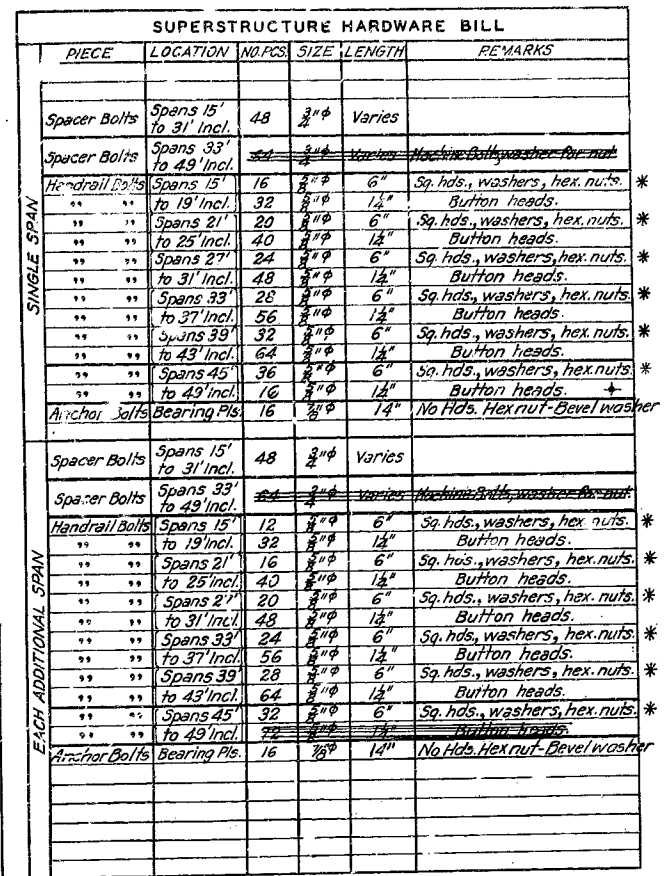
Designed Nov. 1929 By F.W.H.
Drawn Mar. 1930 By R.J.G.
Traced Dec. 1931 By R.J.G.
Checked Dec. 1931 By R.J.G.

Note: This drawing is not to scale. Follow dimensions.

Assembled Oct. 1932 By R.R.C.-H.E.U.
Checked Oct. 1932 By A.L.C.

272

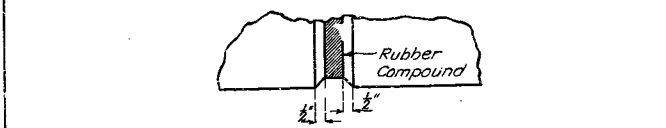
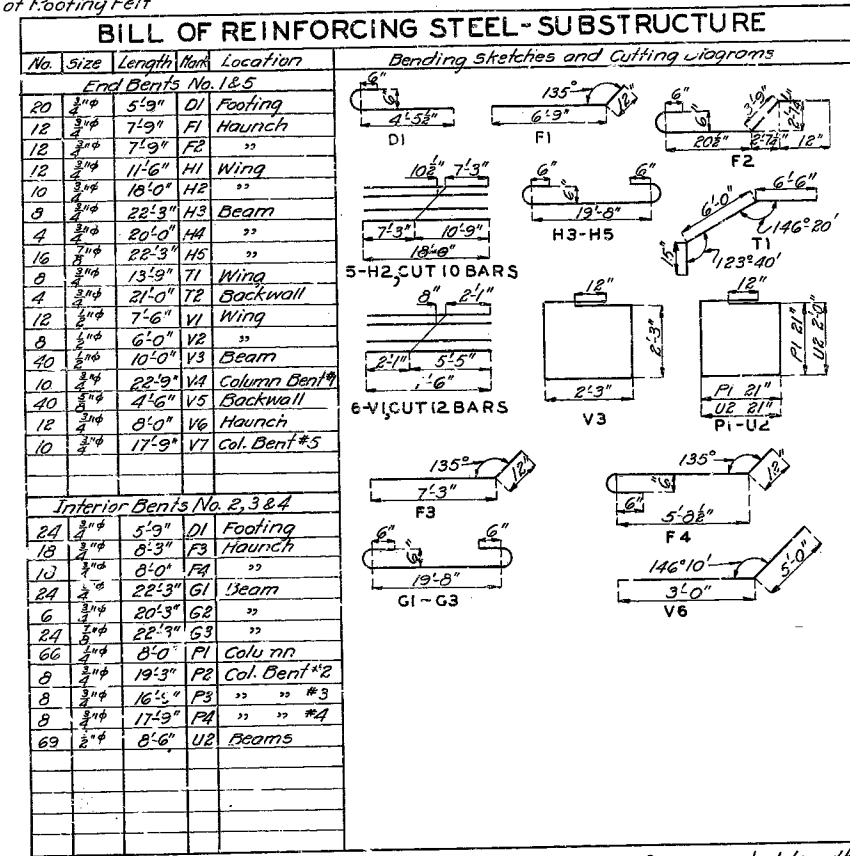
272



SECTION THRU INTERIOR BENT AT C

* 2 Washers plus washers for fill required for alignment of railposts.
 † Total for entire bridge.

* 2 Washers plus washers for fill required for alignment of railposts.
 † Total for entire bridge.



Note: Use bevel as shown for exposed faces of all joints consisting of rubber compound except at top surface of roadway slab.
Use edging tool with $\frac{3}{4}$ " radius at top surface of roadway each side of rubber compound joint.

DETAILS OF BEVEL FOR RUBBER COMPOUND JOINTS

BRIDGE OVER SOUTH FORK OF JACKS FOR

STATE ROAD FROM WILLOW SPRINGS TO ROUTE 17

ABOUT 11 MILES N.E. OF WILLOW SPRINGS
PROJECT NO. SJ-S4 STA. 565 +47.5

TEXAS COUNTY

TEXAS COUNTY

FINISHED

S-52

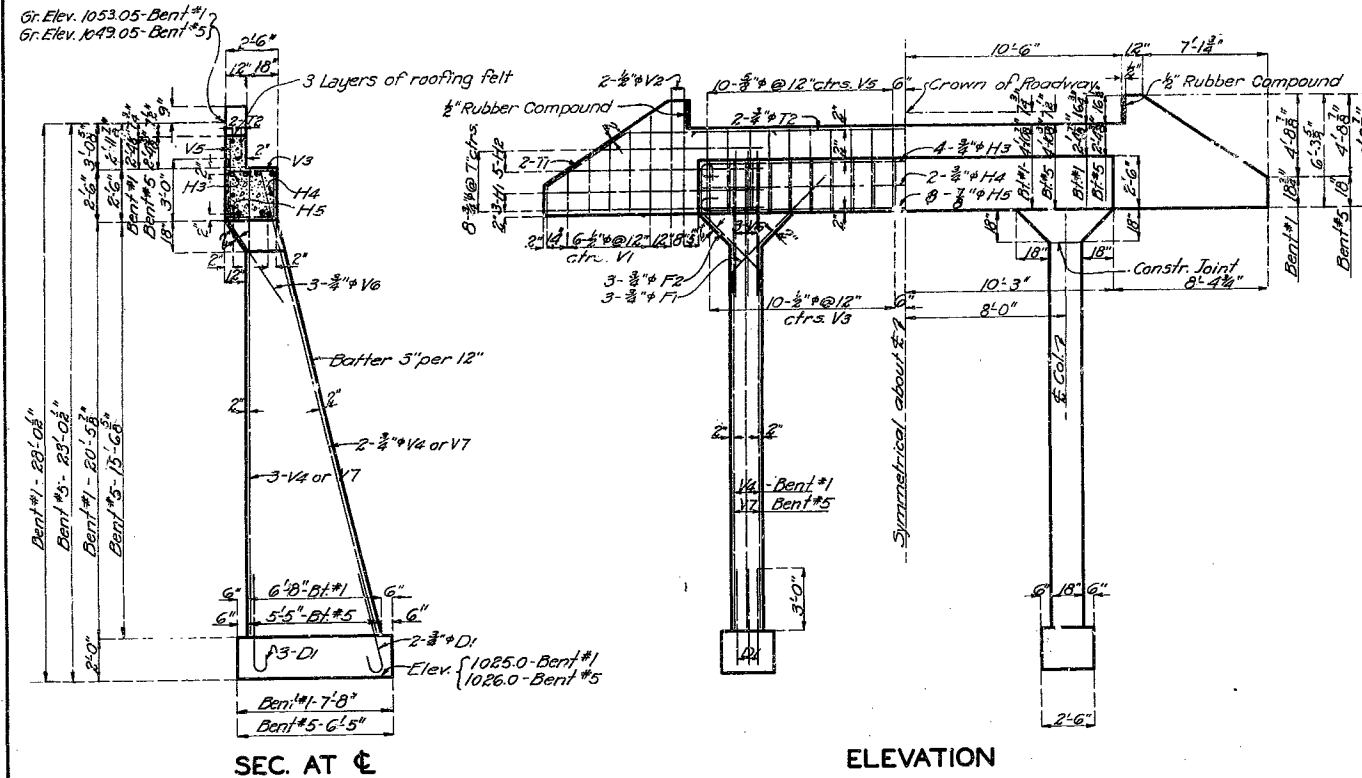
Note: Dimensions of bars are given along ϕ and are for computed lengths. Reinforcing bars $\frac{3}{4}$ " or over in diameter, which are bent to an angle greater than 90° , shall be of structural grade.

Sheet No. 3 of 4

15

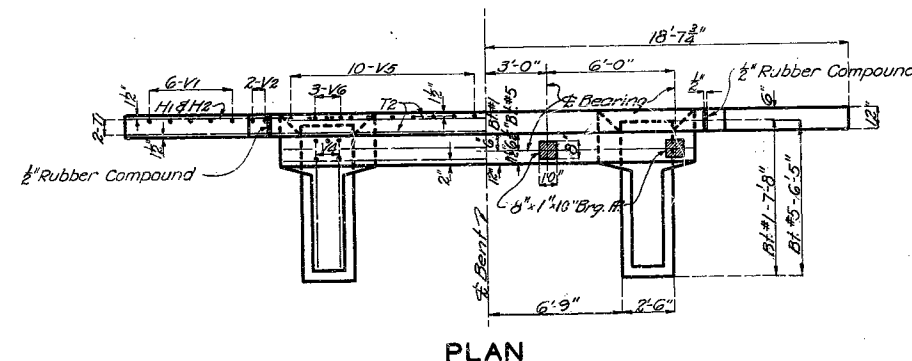
MISSOURI STATE HIGHWAY DEPARTMENT

FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
5	MO	51-54	19		



ELEVATION

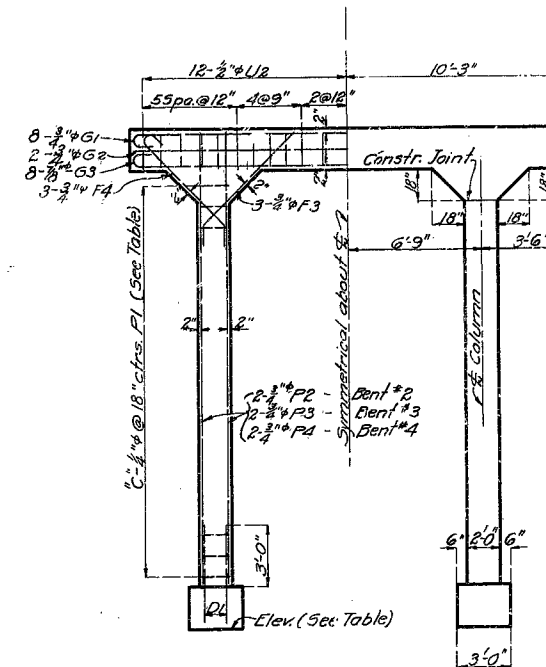
SEC. AT C



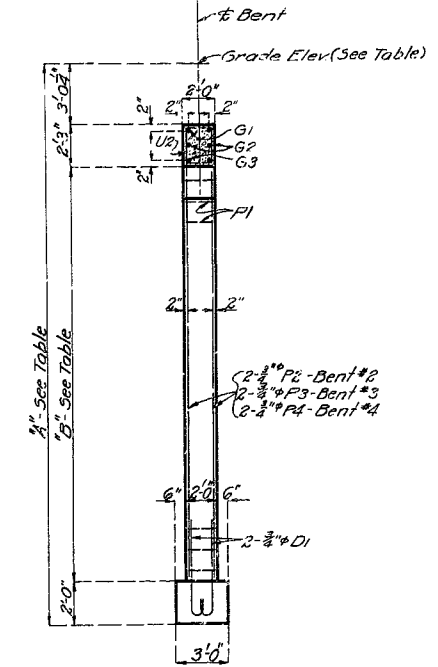
PLAN

DETAILS OF BENTS NO. 1 & 5

Note: This drawing is not to scale. Follow dimensions.

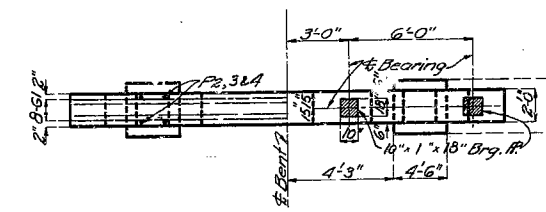


ELEVATION



SEC. AT C

TABLE OF DIMENSIONS					
Bent No.	Gr. Elev.	Top Elev.	Dim. A	Dim. B	No. C/C's
2	1052.03	1027.50	24'-6"	17'-3"	12
3	1051.05	1029.00	22'-0"	14'-9"	10
4	1050.07	1027.00	23'-0"	15'-3"	11



PLAN

DETAILS OF BENTS NO. 2, 3 & 4

BRIDGE OVER SOUTH FORK OF JACKS FORK
STATE ROAD FROM WILLOW SPRINGS TO ROUTE 17
ABOUT 11 MILES N.E. OF WILLOW SPRINGS
PROJECT NO. SJ-54 STA. 565+47.5 FINISHED
TEXAS COUNTY

Assembled Oct. 1932 by P.R.C.-H.E.U.
Checked Oct. 1932 by A.F.K.
Drawn Nov. 1930 by J.A.F.
Checked Aug. 1932 by P.H.S.

Sheet No. 3 of 4

S-529


MISSOURI STATE HIGHWAY DEPARTMENT

FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
5	MO.	57-54	19		

TABLE OF SPACERS				
SPAN	END SPACERS		INT. SPACERS	
	NO.	SIZE	NO.	SIZE
15'-0"	6	5-11/2"	3	5-11/2"
17'-0"	6	5-11/2"	3	5-11/2"
19'-0"	6	5-11/2"	3	5-11/2"
21'-0"	6	5-11/2"	3	5-11/2"
23'-0"	6	5-11/2"	3	5-11/2"
25'-0"	6	5-11/2"	3	5-11/2"
27'-0"	6	5-11/2"	3	5-11/2"
29'-0"	6	5-11/2"	3	5-11/2"
31'-0"	6	5-11/2"	3	5-11/2"
33'-0"	6	5-11/2"	3	5-11/2"
35'-0"	6	5-11/2"	3	5-11/2"
37'-0"	6	5-11/2"	3	5-11/2"
39'-0"	6	5-11/2"	3	5-11/2"
41'-0"	6	5-11/2"	3	5-11/2"
43'-0"	6	5-11/2"	3	5-11/2"
45'-0"	6	5-11/2"	3	5-11/2"
47'-0"	6	5-11/2"	3	5-11/2"
49'-0"	6	5-11/2"	3	5-11/2"

TABLE OF STRINGERS									
SPAN	NO.	SIZE	SINGLE SPAN		END SPAN		INT. SPAN		"C"
			"A"	"B"	"A"	"B"	"A"	"B"	
15'-0"	4	14" C.B. @ 30"	15'	15'	15'	15'	14'-11"	6"	5 1/2"
17'-0"	4	14" C.B. @ 30"	17'	17'	17'	17'	16'-11"	7"	5 1/2"
19'-0"	4	14" C.B. @ 30"	19'	19'	19'	19'	18'-11"	8"	6 1/2"
21'-0"	4	16" C.B. @ 37"	21'	21'	21'	21'	20'-11"	9"	6 1/2"
23'-0"	2	16" C.B. @ 37"	23'	23'	23'	23'	22'-11"	10"	6 1/2"
25'-0"	2	16" C.B. @ 40"	25'	25'	25'	25'	24'-11"	11"	7 1/2"
27'-0"	2	16" C.B. @ 40"	27'	27'	27'	27'	26'-11"	12"	7 1/2"
29'-0"	2	16" C.B. @ 45"	29'	29'	29'	29'	28'-11"	13"	8 1/2"
31'-0"	2	16" C.B. @ 45"	31'	31'	31'	31'	30'-11"	14"	9"
33'-0"	2	21" C.B. @ 58"	33'	33'	33'	33'	32'-11"	15"	9"
35'-0"	2	21" C.B. @ 58"	35'	35'	35'	35'	34'-11"	16"	9"
37'-0"	2	21" C.B. @ 62"	37'	37'	37'	37'	36'-11"	17"	10 1/2"
39'-0"	2	24" C.B. @ 70"	39'	39'	39'	39'	38'-11"	18"	10 1/2"
41'-0"	2	24" C.B. @ 74"	41'	41'	41'	41'	40'-11"	19"	10 1/2"
43'-0"	2	24" C.B. @ 81"	43'	43'	43'	43'	42'-11"	20"	10 1/2"
45'-0"	2	24" C.B. @ 85"	45'	45'	45'	45'	44'-11"	21"	12"
47'-0"	2	27" C.B. @ 91"	47'	47'	47'	47'	46'-11"	22"	12"
49'-0"	2	27" C.B. @ 97"	49'	49'	49'	49'	48'-11"	23"	12"


TABLE OF RAIL ANGLE DIMENSIONS																					
SPAN	MARK	"L"	"M"	"N"	"O"	"P"	"R"	"T"	"U"	"W"	"X"	"Y"	"Z"								
15'-0"	RA1	16'-7"	5'-0"	0	0	0	0	0	5'-5"	0	0	0	5'-0"								
17'-0"	"	18'-7"	5'-0"	0	0	0	0	0	6'-1"	0	0	0	5'-8"								
19'-0"	"	20'-7"	5'-0"	0	0	0	0	0	6'-9"	0	0	0	6'-4"								
21'-0"	"	22'-7"	5'-2"	0	0	0	5'-4"	2"	5'-7"	0	0	0	5'-2"								
23'-0"	"	24'-7"	5'-8"	0	0	0	5'-10"	2"	6'-1"	0	0	0	5'-8"								
25'-0"	"	26'-7"	6'-2"	0	0	0	6'-4"	2"	6'-7"	0	0	0	6'-2"								
27'-0"	"	28'-7"	5'-3"	0	0	0	5'-5"	2"	5'-8"	2"	5'-5"	0	5'-3"								
29'-0"	"	30'-7"	5'-7"	0	0	0	5'-10"	2"	6'-0"	2"	5'-10"	0	5'-7"								
31'-0"	"	32'-7"	6'-0"	0	0	0	6'-2"	2"	6'-3"	2"	6'-2"	0	6'-0"								
33'-0"	"	34'-7"	5'-5"	5'-5"	2"	0	0	0	2"	5'-6"	2"	5'-3"	5'-3"								
35'-0"	"	36'-7"	5'-7"	5'-9"	2"	0	0	0	2"	6'-0"	2"	5'-7"	5'-11"								
37'-0"	"	38'-7"	5'-11"	6'-1"	2"	0	0	0	2"	6'-4"	2"	5'-11"	5'-11"								
39'-0"	"	40'-7"	5'-6"	5'-6"	2"	0	0	2"	5'-5"	2"	5'-6"	2"	5'-4"								
41'-0"	"	42'-7"	5'-9"	5'-9"	2"	0	0	2"	6'-0"	2"	5'-9"	2"	5'-7"								
43'-0"	"	44'-7"	5'-11"	6'-1"	2"	0	0	2"	6'-4"	2"	6'-4"	2"	5'-11"								
45'-0"	"	46'-7"	5'-4"	5'-6"	2"	2"	5'-6"	2"	5'-9"	2"	5'-6"	2"	5'-4"								
47'-0"	"	48'-7"	5'-7"	5'-9"	2"	2"	5'-9"	2"	6'-0"	2"	5'-9"	2"	5'-7"								
49'-0"	"	50'-7"	5'-10"	6'-0"	2"	2"	6'-0"	2"	6'-3"	2"	6'-0"	2"	5'-10"								
15'-0"	RA2	15'-4"	4'-7"	0	0	0	0	0	5'-0"	0	0	0	4'-7"								
17'-0"	"	17'-4"	5'-3"	0	0	0	0	0	5'-8"	0	0	0	5'-5"								
19'-0"	"	19'-4"	5'-11"	0	0	0	0	0	6'-4"	0	0	0	5'-11"								
21'-0"	"	21'-4"	4'-10"	0	0	0	5'-0"	2"	5'-3"	0	0	0	4'-10"								
23'-0"	"	23'-4"	5'-4"	0	0	0	5'-6"	2"	5'-9"	0	0	0	5'-4"								
25'-0"	"	25'-4"	5'-10"	0	0	0	6'-0"	2"	6'-3"	0	0	0	5'-10"								
27'-0"	"	27'-4"	5'-0"	0	0	0	5'-2"	2"	5'-5"	2"	5'-2"	0	5'-0"								
29'-0"	"	29'-4"	5'-5"	0	0	0	5'-7"	2"	5'-10"	2"	5'-7"	0	5'-5"								
31'-0"	"	31'-4"	5'-9"	0	0	0	6'-0"	2"	6'-2"	2"	6'-0"	0	5'-9"								
33'-0"	"	33'-4"	5'-11"	5'-11"	2"	0	0	0	2"	5'-6"	2"	5'-6"	5'-6"								
35'-0"	"	35'-4"	5'-9"	5'-7"	2"	0	0	0	2"	5'-10"	2"	5'-9"	5'-9"								
37'-0"	"	37'-4"	5'-9"	5'-11"	2"	0	0	0	2"	6'-2"	2"	5'-9"	5'-9"								
39'-0"	"	39'-4"	5'-2"	5'-4"	2"	0	0	2"	5'-7"	2"	5'-4"	2"	5'-2"								
41'-0"	"	41'-4"	5'-8"	5'-7"	2"	0	0	2"	5'-10"	2"	5'-7"	2"	5'-8"								
43'-0"	"	43'-4"	5'-9"	5'-11"	2"	0	0	2"	6'-2"	2"	5'-11"	2"	5'-9"								
45'-0"	"	45'-4"	5'-2"	5'-5"	2"	2"	5'-5"	2"	5'-7"	2"	5'-5"	2"	5'-2"								
47'-0"	"	47'-4"	5'-8"	5'-8"	2"	2"	5'-8"	2"	5'-10"	2"	5'-8"	2"	5'-8"								
49'-0"	"	49'-4"	5'-9"	5'-11"	2"	2"	5'-11"	2"	6'-3"	2"	5'-11"	2"	5'-9"								
15'-0"	RA3	14'-11"	4'-5"	0	0	0	0	0	4'-9"	0	0	0	4'-7"								
17'-0"	"	16'-11"	5'-14"	0	0	0	0	0	5'-5"	0	0	0	5'-3"								
19'-0"	"	18'-11"	5'-9"	0	0	0	0	0	6'-14"	0	0	0	5'-11"								
21'-0"	"	20'-11"	4'-8"	0	0	0	5'-0"	2"	5'-0"	0	0	0	4'-10"								
23'-0"	"	22'-11"	5'-2"	0	0	0	5'-6"	2"	5'-6"	0	0	0	5'-4"								
25'-0"	"	24'-11"	5'-8"	0	0	0	6'-0"	2"	6'-0"	0	0	0	5'-10"								
27'-0"	"	26'-11"	4'-10"	0	0	0	5'-2"	2"	5'-2"	2"	5'-2"	0	4'-11"								
29'-0"	"	28'-11"	5'-3"	0	0	0	5'-6"	2"	5'-6"	2"	5'-6"	0	5'-4"								
31'-0"	"	30'-11"	5'-7"	0	0	0	5'-11"	2"	5'-11"	2"	5'-11"	0	5'-9"								
33'-0"	"	32'-11"	4'-11"	5'-3"	2"	0	0	0	2"	5'-8"	2"	5'-5"	5'-5"								
35'-0"	"	34'-11"	5'-3"	5'-7"	2"	0	0	0	2"	5'-7"	2"	5'-7"	5'-7"								
37'-0"	"	36'-11"	5'-7"	5'-11"	2"	0	0	0	2"	5'-14"	2"	5'-14"	5'-9"								
39'-0"	"	38'-11"	5'-0"	5'-4"	2"	0	0	2"	5'-4"	2"	5'-4"	2"	5'-11"								
41'-0"	"	40'-11"	5'-3"	5'-7"	2"	0	0	2"	5'-7"	2"	5'-7"	2"	5'-5"								
43'-0"	"	42'-11"	5'-7"	5'-11"	2"	0	0	2"	5'-10"	2"	5'-10"	2"	5'-6"								
45'-0"	"	44'-11"	5'-1"	5'-4"	2"	2"	5'-4"	2"	5'-4"	2"	5'-4"	2"	5'-2"								
47'-0"	"	46'-11"	5'-4"	5'-7"	2"	2"	5'-7"	2"	5'-7"	2"	5'-7"	2"	5'-3"								
49'-0"	"	48'-11"	5'-7"	5'-10"	2"	2"	5'-10"	2"	5'-10"	2"	5'-10"	2"	5'-8"								
15'-0"	RA4	16'-11"	4'-7"	0	0	0	0	0	5'-4"	0	0	0	4'-11"								
17'-0"	"	18'-11"	5'-3"	0	0	0	0	0	6'-0"	0	0	0	5'-7"								
19'-0"	"	20'-11"	5'-11"	0	0	0	0	0	6'-8"	0	0	0	6'-3"								
21'-0"	"	22'-11"	4'-9"	0	0	0	5'-3"	2"	5'-6"	0	0	0	5'-11"								
23'-0"	"	24'-11"	5'-3"	0	0	0	5'-9"	2"	6'-0"	0	0	0	5'-7"								
25'-0"	"	26'-11"	5'-9"	0	0	0	6'-3"	2"	6'-6"	0	0	0	6'-11"								
27'-0"	"	28'-11"	4'-0"	0	0	0	5'-4"	2"	5'-7"	2"	5'-4"	0	5'-2"								
29'-0"	"	30'-11"	5'-3"	0	0	0	5'-9"	2"	6'-0"	2"	5'-9"	0	5'-7"								
31'-0"	"	32'-11"	5'-7"	0	0	0	6'-3"	2"	6'-5"	2"	6'-2"	0	6'-0"								
33'-0"	"	34'-11"	4'-11"	5'-5"	2"	0	0	0	2"	5'-6"	2"	5'-3"	5'-3"								
35'-0"	"	36'-11"	5'-8"	5'-9"	2"	0	0	0	2"	6'-0"	2"	5'-7"	5'-11"								
37'-0"	"	38'-11"	5'-7"	6'-1"	2"	0	0	0	2"	6'-4"	2"	5'-11"	5'-11"								
39'-0"	"	40'-11"	4'-11"	5'-6"	2"	0	0	2"	5'-8"	2"	5'-6"	2"	5'-7"								
41'-0"	"	42'-11"	5'-3"	5'-9"	2"	0	0	2"	6'-0"	2"	5'-9"	2"	5'-11"								
43'-0"	"	44'-11"	5'-6"	6'-1"	2"	0	0	2"	6'-3"	2"	6'-0"	2"	5'-7"								
45'-0"	"	46'-11"	5'-0"	5'-6"	2"	2"	5'-6"	2"	5'-9"	2"	5'-6"	2"	5'-4"								
47'-0"	"	48'-11"	5'-3"	5'-9"	2"	2"	5'-9"	2"	6'-2"	2"	5'-9"	2"	5'-7"								
49'-0"	"	50'-11"	5'-6"	6'-0"	2"	2"	6'-0"	2"	6'-3"	2"	6'-0"	2"	5'-10"								


		<div>Missouri Department of Transportation</div> <div>State Bridge Inspection Report</div>				<div>December 21, 2022</div> <div>10:28:39AM</div>			
COUNTY: TEXAS		DISTRICT: SE		CLASS: STATBR		FED-ID: 8749		BRIDGE: S0529	
GENERAL STRUCTURE INFORMATION							***BRIDGE INSPECTION INFORMATION***		
<div>ROUTE: MO137S</div> <div>FEATURE: S FK JACKS FK RVR</div> <div>STATUS: P-POSTLOAD</div> <div>LOG MILE: 31.678</div> <div>DETOUR: 22.00 MILES</div> <div>NHS: NO</div> <div>BUILT: 1932</div> <div>REHAB:</div> <div>LOCATION: S 19 T 28 R 8 W</div> <div>LATITUDE: 37 5 23.65 (DMS)</div> <div>LONGITUDE: 91 51 51.55 (DMS)</div>		<div># SPANS: 4</div> <div>LANES ON: 1</div> <div>LANES UNDER: 0</div> <div>COMPASS DIRECTION: SOUTH to NORTH</div> <div>DIRECTION OF TRAFFIC: 1-LN/2-WAY</div> <div>FUNCTIONAL CLASS: RL-MAJOR COLLECTOR</div> <div>NBI OWNER: MODOT</div> <div>NBI MAINTAINED: MODOT</div> <div>MAINTENANCE DISTRICT: SE</div> <div>MAINTENANCE COUNTY: TEXAS</div> <div>SUB AREA: 7H51</div>		<div>PLACE CODE: 57476 PIERCE</div> <div>LENGTH: 200 FT 0 IN</div> <div>MAXIMUM SPAN: 51 FT 0 IN</div> <div>APPROACH ROADWAY: 20 FT 0 IN</div> <div>CURB TO CURB: 20 FT 0 IN</div> <div>OUT TO OUT: 21 FT 0 IN</div> <div>AADT: 204</div> <div>AADT YEAR: 2021</div> <div>AADT TRUCK: 10.3%</div> <div>FUTURE AADT: 286</div> <div>FUTURE AADT YEAR: 2041</div>		<div>DATE: 11/07/2022</div> <div>RESPONSIBILITY: DISTRICT</div> <div>FREQUENCY: 12</div> <div>CALCULATED INTERVAL**: 12</div> <div>TEAM LEADER: ED HESS</div> <div>ELEMENT: NO</div> <div>INSPECTOR 2:</div> <div>INSPECTOR 4:</div> <div>INSPECTOR 3:</div> <div>** When calculated interval exceeds the frequency, a justification comment per BIRM is required.</div>			
						GENERAL INSPECTION COMMENTS			
FRACTURE CRITICAL INSPECTION INFORMATION					***INDEPTH INSPECTION INFORMATION***				
<div>DATE:</div> <div>FREQUENCY:</div> <div>TEAM LEADER:</div> <div>INSPECTOR 2:</div> <div>** When calculated interval exceeds the frequency, a justification comment per BIRM is required.</div>					<div>RESPONSIBILITY:</div> <div>CALCULATED INTERVAL**:</div> <div>INSPECTOR 3:</div> <div>INSPECTOR 4:</div> <div>CATEGORY:</div> <div>NBI:</div> <div>METHOD:</div> <div>** When calculated interval exceeds the frequency, a justification comment per BIRM is required.</div>				
FRACTURE CRITICAL INSPECTION COMMENTS					INDEPTH INSPECTION COMMENTS				
SPECIAL INSPECTION INFORMATION					***UNDERWATER INSPECTION INFORMATION***				
<div>DATE: 07/07/2020</div> <div>FREQUENCY: 72</div> <div>TEAM LEADER: ED HESS</div> <div>INSPECTOR 2:</div> <div>** When calculated interval exceeds the frequency, a justification comment per BIRM is required.</div>					<div>RESPONSIBILITY: DISTRICT</div> <div>CALCULATED INTERVAL**: 73</div> <div>INSPECTOR 3:</div> <div>INSPECTOR 4:</div> <div>CATEGORY: CHANNEL CROSS SEC</div> <div>NBI: NO</div> <div>METHOD: WT TAPE</div> <div>** When calculated interval exceeds the frequency, a justification comment per BIRM is required.</div>				
SPECIAL INSPECTION COMMENTS					UNDERWATER INSPECTION COMMENTS				
OTHER SPECIAL INSPECTIONS					OTHER UNDERWATER INSPECTIONS				
<div><div>DATE</div><div>FREQUENCY</div><div>CATEGORY</div><div>NBI</div><div>CALCULATED INTERVAL</div><div>RESPONSIBILITY</div><div>METHOD</div></div> <div>01/27/2016</div> <div>99</div> <div>QUALITY ASSURANCE</div> <div>NO</div> <div></div> <div>DISTRICT</div> <div></div>					<div><div>DATE</div><div>FREQUENCY</div><div>CATEGORY</div><div>NBI</div><div>CALCULATED INTERVAL</div><div>RESPONSIBILITY</div><div>METHOD</div></div> <div></div> <div></div> <div></div> <div></div> <div></div> <div></div> <div></div>				

Design_No = S0529

Page 1

This report contains information that is protected from disclosure by federal law, 23 USC Section 409 and the Missouri Open Records Law (Sunshine Act), Section 610.021 RSMo. Please review MoDOT's policy and procedure manual on the Sunshine Act before releasing any of the information contained herein.

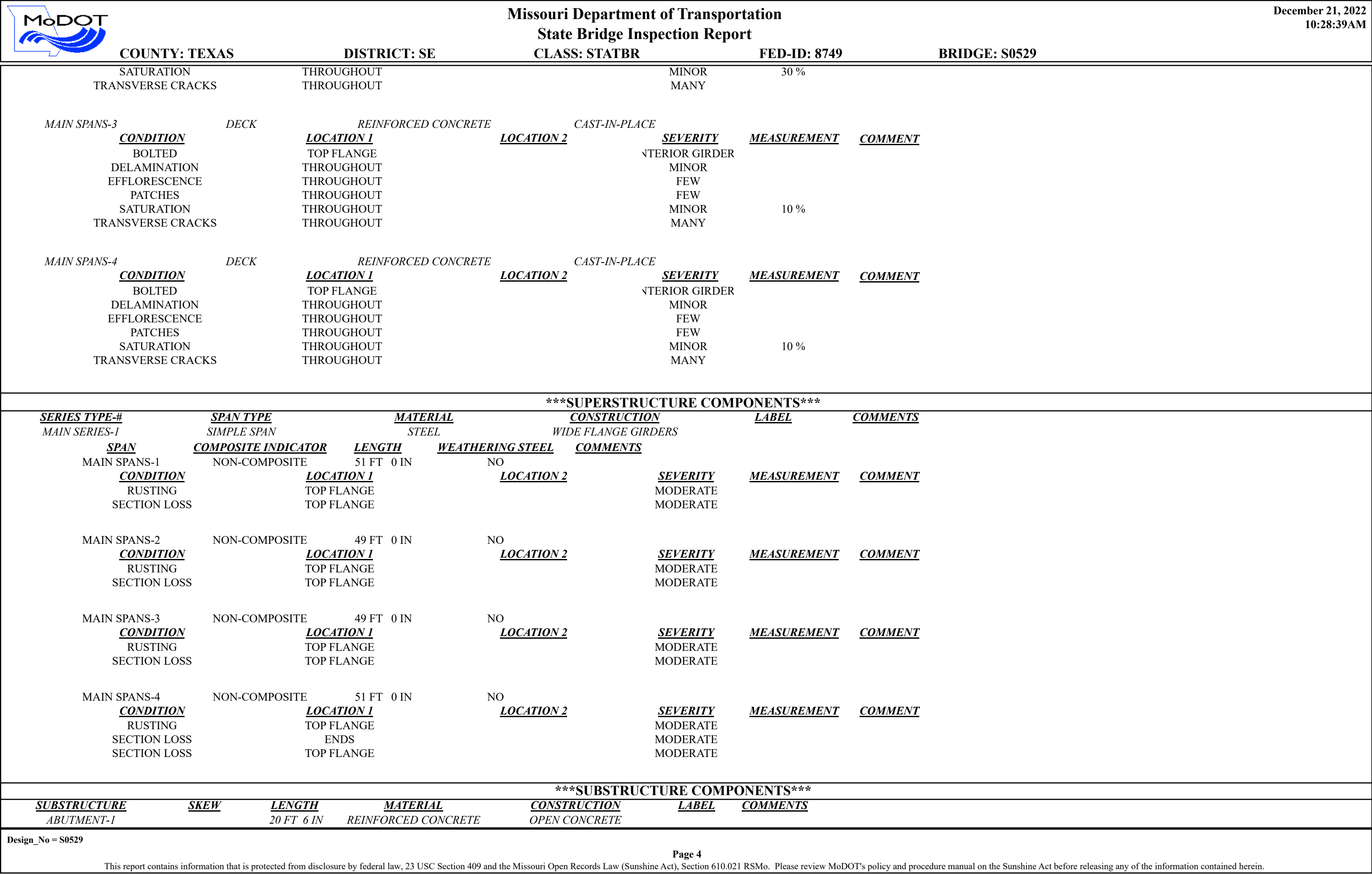
		<div>Missouri Department of Transportation</div> <div>State Bridge Inspection Report</div>			<div>December 21, 2022</div> <div>10:28:39AM</div>	
COUNTY: TEXAS		DISTRICT: SE	CLASS: STATBR	FED-ID: 8749	BRIDGE: S0529	
STRUCTURE POSTING						
APPROVED CATEGORY: S-17		CL OF BR AND TRK OVR 16 T 15MPH ON BR EXCPT SNGL E UNT TRKS WT LMT 22 T OTHR TRKS WT LMT 39 T.				
Ton 1: 16		Ton 2: 22	Ton 3: 39			
COMMENTS:						
FIELD CATEGORY: S-17		CL OF BR AND TRK OVR 16 T 15MPH ON BR EXCPT SNGL E UNT TRKS WT LMT 22 T OTHR TRKS				
Ton 1: 16		Ton 2: 22	Ton 3: 39	PROBLEM:	PROBLEM DIRECTION:	
COMMENTS:						
GENERAL COMMENTS/MAJOR RATED ITEMS						
GENERAL COMMENTS: (BOWDEJ1, 08/28/2008)--(51'-49'-49'-51') SMP WF GDR SPANS						
[ITEM 58] DECK: 4-POOR CONDITION			COMMENTS: (BRAWLK1, 11/04/2021)--PATCHINGD & SATURATION			
RATING : 11/04/2021						
[ITEM 59] SUPER: 5-FAIR CONDITION			COMMENTS: (DENNIB1, 11/13/2013)--RUSTING TOP FLANGES			
RATING : 11/16/2015						
[ITEM 60] SUB: 6-SATISFACTORY CONDITION			COMMENTS: (DENNIB1, 11/05/2019)--DIAG. CRACKS			
RATING : 11/05/2019						
[ITEM 61] BANK/CHANNEL: 5-MAJOR DAMAGE			COMMENTS: (DENNIB1, 11/29/2017)--EROSION ON BOTH SLOPES			
RATING : 11/29/2017						
[ITEM 113] SCOUR: 8-STABLE FOR CALCULATED			COMMENTS:			
RATING : 05/18/2001						
EVALUATION TYPE :						
[ITEM 71] WATERWAY ADEQUACY: DECK/APPRCH OVERTOP SLIGT			COMMENTS:			
RATING : 05/18/2001						
[ITEM 72] APPRRDWY ALIGNMENT: 6-SATISFACTORY			COMMENTS:			
RATING : 05/18/2001						
RAILING AND APPROACH PAVEMENT COMPONENTS AND RATINGS						
[ITEM 36A] BRIDGE RAILING RATING: DOESNT MEET CURRNT STND-0			RATING : 01/06/2010	COMMENTS:		
<u>MATERIAL</u>	<u>CONSTRUCTION</u>	<u>DIRECTION</u>	<u>COMMENTS</u>			
REINFORCED CONCRETE	CURB	BOTH				
STEEL	ANGLE-DOUBLE	BOTH				
[ITEM 36B] TRANSITION RAILING RATING: NOT PROVIDED-0			RATING : 05/18/2001	COMMENTS:		
[ITEM 36C] APPROACH RAILING RATING: NOT PROVIDED-0			RATING : 05/18/2001	COMMENTS:		
[ITEM 36D] RAIL END TREATMENT RATING: NOT PROVIDED-0			RATING : 05/18/2001	COMMENTS:		
Design_No = S0529						
<div>Page 2</div> <div>This report contains information that is protected from disclosure by federal law, 23 USC Section 409 and the Missouri Open Records Law (Sunshine Act), Section 610.021 RSMo. Please review MoDOT's policy and procedure manual on the Sunshine Act before releasing any of the information contained herein.</div>						

		Missouri Department of Transportation				December 21, 2022	
		State Bridge Inspection Report				10:28:39AM	
COUNTY: TEXAS		DISTRICT: SE		CLASS: STATBR		FED-ID: 8749	
						BRIDGE: S0529	
APPROACH PAVEMENT: *Overall condition assigned for each approach pavemenet component is shown below.							
<u>MATERIAL</u>		<u>CONSTRUCTION</u>		<u>DIRECTION</u>		<u>CONDITION*</u>	
ASPHALT		BITUMINOUS MAT		BOTH			
DRAINAGE, EXPANSION DEVICES, BANK/SLOPE, AND DECK PROTECTIVE COMPONENTS							
<u>DECK PROTECTIVE COMPONENTS:</u>							
<u>SERIES TYPE-#</u>		<u>COMPONENT</u>		<u>MATERIAL</u>		<u>CONSTRUCTION</u>	
MAIN SERIES-1		WEARING SURFACE		ASPHALT		BITUMINOUS SEAL COAT	
						<u>THICKNESS</u>	
						.3 IN	
						<u>YEAR APPLIED</u>	
						2008	
						<u>MANUFACTURE</u>	
						<u>OVERALL CONDITION</u>	
						FAIR	
<u>COMMENT:</u>							
		DECK PROTECTION		NOTAPPLICABLE		NONE	
<u>COMMENT:</u>							
		MEMBRANE		NOTAPPLICABLE		NONE	
<u>COMMENT:</u>							
<u>DRAINAGE COMPONENTS:</u>							
		<u>COMPONENT</u>		<u>MATERIAL</u>		<u>CONSTRUCTION</u>	
		DRAINAGE		REINFORCED CONCRETE		CURB OUTLET	
						<u>DIRECTION</u>	
						<u>COMMENTS</u>	
<u>EXPANSION DEVICE COMPONENTS:</u>							
<u>SUB UNIT-#</u>		<u>SUB LABEL</u>		<u>COMPONENT</u>		<u>MATERIAL</u>	
						<u>CONSTRUCTION</u>	
						<u>GAP</u>	
						<u>YEAR APPLIED</u>	
						<u>MANUFACTURE</u>	
						<u>OVERALL CONDITION</u>	
<u>COMMENT:</u>							
<u>BANK/SLOPE PROTECTION COMPONENTS:</u>							
		<u>COMPONENT</u>		<u>MATERIAL</u>		<u>CONSTRUCTION</u>	
		BANK PROTECTION		ROCK		GROUTED	
						<u>DIRECTION</u>	
						BOTH	
						<u>COMMENTS</u>	
DECK COMPONENTS							
<u>SPAN TYPE-#</u>		<u>COMPONENT</u>		<u>MATERIAL</u>		<u>CONSTRUCTION</u>	
MAIN SPANS-1		DECK		REINFORCED CONCRETE		CAST-IN-PLACE	
						<u>COMMENTS</u>	
<u>CONDITION</u>		<u>LOCATION 1</u>		<u>LOCATION 2</u>		<u>SEVERITY</u>	
BOLTED		TOP FLANGE				NTERIOR GIRDER	
DELAMINATION		THROUGHOUT				MINOR	
EFFLORESCENCE		THROUGHOUT				FEW	
PATCHES		THROUGHOUT				MODERATE	
SATURATION		THROUGHOUT				MINOR	
TRANSVERSE CRACKS		THROUGHOUT				30 %	
						MANY	
MAIN SPANS-2		DECK		REINFORCED CONCRETE		CAST-IN-PLACE	
						<u>COMMENTS</u>	
<u>CONDITION</u>		<u>LOCATION 1</u>		<u>LOCATION 2</u>		<u>SEVERITY</u>	
BOLTED		TOP FLANGE				NTERIOR GIRDER	
DELAMINATION		THROUGHOUT				MINOR	
EFFLORESCENCE		THROUGHOUT				FEW	
PATCHES		THROUGHOUT				MODERATE	

Design_No = S0529

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MoDOT

Missouri Department of Transportation

State Bridge Inspection Report

December 21, 2022

10:28:39AM

COUNTY: TEXAS

DISTRICT: SE

CLASS: STATBR

FED-ID: 8749

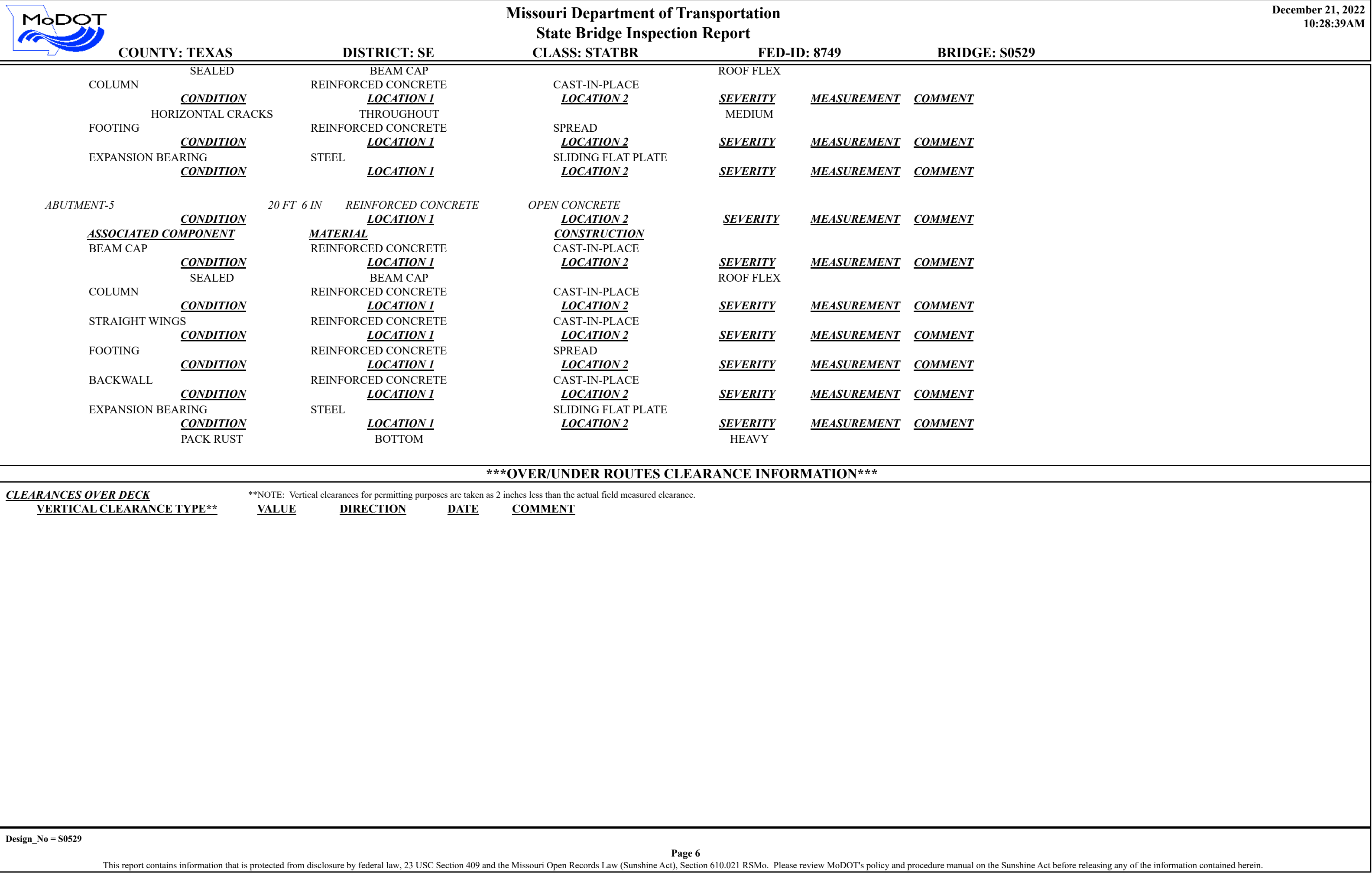
BRIDGE: S0529

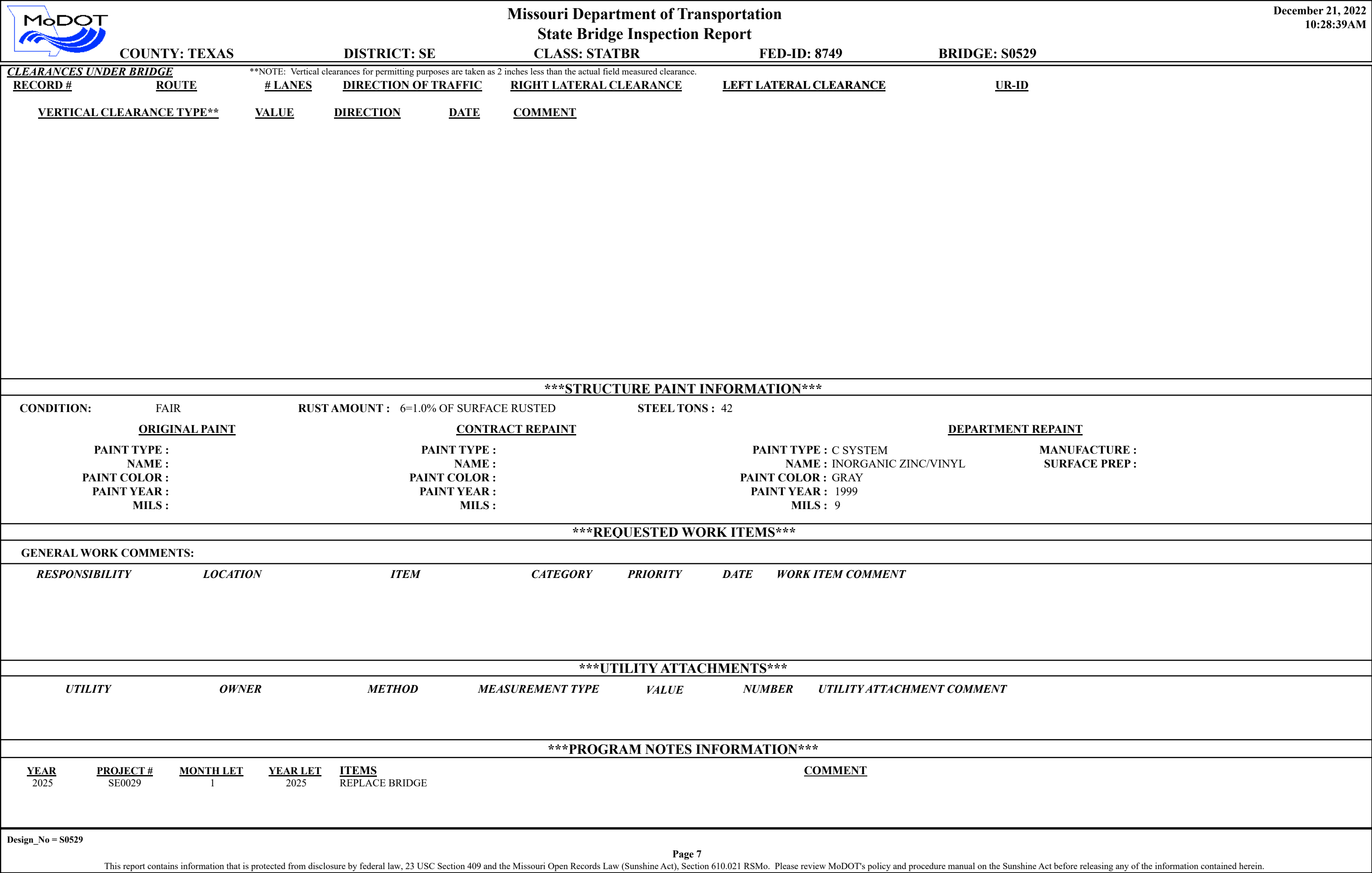
	<u>CONDITION</u>		<u>LOCATION 1</u>		<u>LOCATION 2</u>		<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
	<u>ASSOCIATED COMPONENT</u>		<u>MATERIAL</u>		<u>CONSTRUCTION</u>				
	BEAM CAP		REINFORCED CONCRETE		CAST-IN-PLACE				
	<u>CONDITION</u>		<u>LOCATION 1</u>		<u>LOCATION 2</u>		<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
	SEALED		BEAM CAP				ROOF FLEX		
COLUMN			REINFORCED CONCRETE		CAST-IN-PLACE				
	<u>CONDITION</u>		<u>LOCATION 1</u>		<u>LOCATION 2</u>		<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
STRAIGHT WINGS			REINFORCED CONCRETE		CAST-IN-PLACE				
	<u>CONDITION</u>		<u>LOCATION 1</u>		<u>LOCATION 2</u>		<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
	BROKEN		TOP				OFF		
FOOTING			REINFORCED CONCRETE		SPREAD				
	<u>CONDITION</u>		<u>LOCATION 1</u>		<u>LOCATION 2</u>		<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
BACKWALL			REINFORCED CONCRETE		CAST-IN-PLACE				
	<u>CONDITION</u>		<u>LOCATION 1</u>		<u>LOCATION 2</u>		<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
EXPANSION BEARING			STEEL		SLIDING FLAT PLATE				
	<u>CONDITION</u>		<u>LOCATION 1</u>		<u>LOCATION 2</u>		<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
	PACK RUST		BOTTOM				HEAVY		
BENT-2		20 FT 6 IN	REINFORCED CONCRETE		MULTIPLE COLUMN				
	<u>CONDITION</u>		<u>LOCATION 1</u>		<u>LOCATION 2</u>		<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
	<u>ASSOCIATED COMPONENT</u>		<u>MATERIAL</u>		<u>CONSTRUCTION</u>				
	BEAM CAP		REINFORCED CONCRETE		CAST-IN-PLACE				
	<u>CONDITION</u>		<u>LOCATION 1</u>		<u>LOCATION 2</u>		<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
	SEALED		BEAM CAP				ROOF FLEX		
COLUMN			REINFORCED CONCRETE		CAST-IN-PLACE				
	<u>CONDITION</u>		<u>LOCATION 1</u>		<u>LOCATION 2</u>		<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
HIGH STEEL SPALLS			RANDOM				FEW		
FOOTING			REINFORCED CONCRETE		SPREAD				
	<u>CONDITION</u>		<u>LOCATION 1</u>		<u>LOCATION 2</u>		<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
EXPANSION BEARING			STEEL		SLIDING FLAT PLATE				
	<u>CONDITION</u>		<u>LOCATION 1</u>		<u>LOCATION 2</u>		<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
BENT-3		20 FT 6 IN	REINFORCED CONCRETE		MULTIPLE COLUMN				
	<u>CONDITION</u>		<u>LOCATION 1</u>		<u>LOCATION 2</u>		<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
	<u>ASSOCIATED COMPONENT</u>		<u>MATERIAL</u>		<u>CONSTRUCTION</u>				
	BEAM CAP		REINFORCED CONCRETE		CAST-IN-PLACE				
	<u>CONDITION</u>		<u>LOCATION 1</u>		<u>LOCATION 2</u>		<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
DIAGONAL CRACKS			ENDS				MODERATE		
SEALED			BEAM CAP				ROOF FLEX		
COLUMN			REINFORCED CONCRETE		CAST-IN-PLACE				
	<u>CONDITION</u>		<u>LOCATION 1</u>		<u>LOCATION 2</u>		<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
FOOTING			REINFORCED CONCRETE		SPREAD				
	<u>CONDITION</u>		<u>LOCATION 1</u>		<u>LOCATION 2</u>		<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
EXPANSION BEARING			STEEL		SLIDING FLAT PLATE				
	<u>CONDITION</u>		<u>LOCATION 1</u>		<u>LOCATION 2</u>		<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
BENT-4		20 FT 6 IN	REINFORCED CONCRETE		MULTIPLE COLUMN				
	<u>CONDITION</u>		<u>LOCATION 1</u>		<u>LOCATION 2</u>		<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>
	<u>ASSOCIATED COMPONENT</u>		<u>MATERIAL</u>		<u>CONSTRUCTION</u>				
	BEAM CAP		REINFORCED CONCRETE		CAST-IN-PLACE				
	<u>CONDITION</u>		<u>LOCATION 1</u>		<u>LOCATION 2</u>		<u>SEVERITY</u>	<u>MEASUREMENT</u>	<u>COMMENT</u>


Design_No = S0529

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<div><div>Missouri Department of Transportation</div><div>State Bridge Inspection Report</div></div>			December 21, 2022 10:28:39AM																																																	
COUNTY: TEXAS			DISTRICT: SE		CLASS: STATBR		FED-ID: 8749		BRIDGE: S0529																																											
COMPUTER GENERATED RATINGS AND DEFICIENCY ITEMS						***ADVANCED SIGN INFORMATION***																																														
<div>NOTE: The items listed in this section are updated whenever computer edits are ran on a structure after the inspection updates have been entered in to TMS.</div> <table><tr><td><u>Rated Item</u></td><td><u>Rating</u></td><td><u>Rating Date</u></td></tr><tr><td>[Item 67] Structure Evaluation Rating:</td><td>2-BASICALLY INTOLRBLE REQ</td><td>2/17/2022</td></tr><tr><td>[Item 68] Deck Geometry Rating:</td><td>4-MEETS MINIMUM TOLERABLE</td><td>3/25/2002</td></tr><tr><td>[Item 69] Underclearance:</td><td>N-NOT APPLICABLE</td><td>5/18/2001</td></tr><tr><td>Sufficiency Rating:</td><td>43.2%</td><td>3/8/2022</td></tr><tr><td>Deficiency:</td><td>STRUCTURAL</td><td>10/7/2019</td></tr><tr><td>Funding Eligibility:</td><td>FULL</td><td>----</td></tr><tr><td>Estimated New Structure Length:</td><td>131 FT.</td><td>----</td></tr><tr><td>Estimated Structure Cost:</td><td>\$1,211,367</td><td>----</td></tr><tr><td>Estimated Total Project Cost:</td><td>\$1,817,051</td><td>----</td></tr><tr><td>Year of Cost Estimate:</td><td>2022</td><td>----</td></tr></table> <div>NOTE: The above structure length and cost estimates are computer generated using algorithms in the TMS system. These algorithms are generalized to use NBI items to come up with a new structure length and width to calculate a new area which is taken times a representative cost per square foot. The actual structure size and cost may vary significantly from these numbers once site specific engineering is done.</div>						<u>Rated Item</u>	<u>Rating</u>	<u>Rating Date</u>	[Item 67] Structure Evaluation Rating:	2-BASICALLY INTOLRBLE REQ	2/17/2022	[Item 68] Deck Geometry Rating:	4-MEETS MINIMUM TOLERABLE	3/25/2002	[Item 69] Underclearance:	N-NOT APPLICABLE	5/18/2001	Sufficiency Rating:	43.2%	3/8/2022	Deficiency:	STRUCTURAL	10/7/2019	Funding Eligibility:	FULL	----	Estimated New Structure Length:	131 FT.	----	Estimated Structure Cost:	\$1,211,367	----	Estimated Total Project Cost:	\$1,817,051	----	Year of Cost Estimate:	2022	----	<table><tr><td>SIGN #</td><td>SIGN TYPE</td><td>PROBLEM</td><td>PROBLEM DIRECTION</td></tr><tr><td>1</td><td></td><td></td><td></td></tr></table>						SIGN #	SIGN TYPE	PROBLEM	PROBLEM DIRECTION	1			
<u>Rated Item</u>	<u>Rating</u>	<u>Rating Date</u>																																																		
[Item 67] Structure Evaluation Rating:	2-BASICALLY INTOLRBLE REQ	2/17/2022																																																		
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STATUS:	DATE:																																																			
NOTES:																																																				



Missouri Department of Transportation
Bridge Inventory and Inspection System
Structural Inventory & Appraisal Sheet

December 21, 2022
10:24:11am

COUNTY : TEXAS BRIDGE : S0529 REVIEW STATUS : APPROVED NBI STATUS : T
RECORD TYPE : ROUTE CARRIED 'ON' STRUCT RUN DATE : 11/10/2022 SUBMITTAL YEAR : 2022

GENERAL STRUCTURE INFORMATION			ROUTE DESIGNATION INFORMATION		
1	State	MISSOURI	5A	Record Type	ROUTE CARRIED 'ON' STRUCT
2	District	SE	5B	Route Signing Prefix	MO
3	County	TEXAS	5C	Designated Level of Service	MAINLINE
8	Federal ID No.	8749	5D	Route Number	00137
27	Year Built	1932	5E	Directional Suffix	NOT APPLICABLE
106	Year Reconstructed	0	7	Facility Carried	MO 137 S
42A	Type of Service On	HIGHWAY	12	Base Hwy. Network	NO
21	Structure Maintenance	STATE HIGHWAY AGENCY	13A	LRS Inventory Route No.	
22	Structure Owner	STATE HIGHWAY AGENCY	13B	Subroute No.	
33	Br. Median Code	NO MEDIAN	20	Toll Status	ON FREE ROAD
37	Historical Significance	HISTORICAL SIGNIF UNKNWN	26	Functional Classification	07-RURAL MAJOR COLLECTOR
101	Parallel Struc Desg	NONE EXISTS	28A	Lanes on Structure	01
103	Temporary Structure	NOT TEMPORARY	100	STRAHNET Designation	RTE NOT A DEFENSE HWY
112	NBIS Bridge Length	YES	104	National Highway System	NOT ON NHS
			105	Federal Lands Highway	NOT APPLICABLE
			110	Designated Nat. Network	NO
STRUCTURE LOCATION INFORMATION			STRUCTURE TRAFFIC INFORMATION		
4	Place	PIERCE	29	AADT	204
	Code	57476	30	AADT Year	2021
9	Location	S 19 T 28 N R 8 W	102	Direction of Traffic	ONE LANE BRIDGE FOR 2-WAY
11	Milepoint	31.86 miles	109	AADT Truck Percent	10%
16	Latitude	37 D 5 M 24 S	114	Future AADT	286
17	Longitude	91 D 51 M 52 S	115	Future AADT Year	2041
UNDERRECORD INFORMATION			STRUCTURE GEOMETRIC INFORMATION		
6	Features Intersected	S FK JACKS FK RVR	10	Inventory Rte. Vert. Clear	99 Ft. 99 In.
42B	Type of Service Under	WATERWAY	19	By pass Detour Length	21.88 miles
28B	Lanes Under Structure	00	32	Approach Roadway Width	20 Ft. 0 In.
54A	Vert. Clearance Ref.	N/A	34	Skew	0.00 Degrees
54B	Vert. Clearance	0 Ft. 0 In.	35	Struct. Flared	NO
55A	Rt. Lat Clear Ref.	N/A	47	Total Horiz. Clear	20 Ft. 0 In.
55B	Rt. Lat Clearance	0 Ft. 0 In.	48	Maximum Span Length	50 Ft. 10 In.
56	Left Lat Clearance	0 Ft. 0 In.	49	Structure Length	200 Ft. 2 In.
38	Navigation Control	PERMIT NOT REQ	50A	Left Curb/Sidewalk Width	0 Ft. 0 In.
39	Nav Vertical Clear	0 Ft. 0 In.	50B	Right Curb/Sidewalk Width	0 Ft. 0 In.
40	Nav Horizontal Clear	0 Ft. 0 In.	51	Curb to Curb Br. Width	20 Ft. 0 In.
111	Nav. Pier Protection		52	Deck Width (Out-Out)	20 Ft. 12 In.
116	Nav. Cl. Vert. Clear		53	Vert. Clearance Over Deck	99 Ft. 99 In.

Design_No = S0529 and Inventory_Appraisal_Submittal_Year = 2022



Missouri Department of Transportation
Bridge Inventory and Inspection System
Structural Inventory & Appraisal Sheet

December 21, 2022
10:24:11am

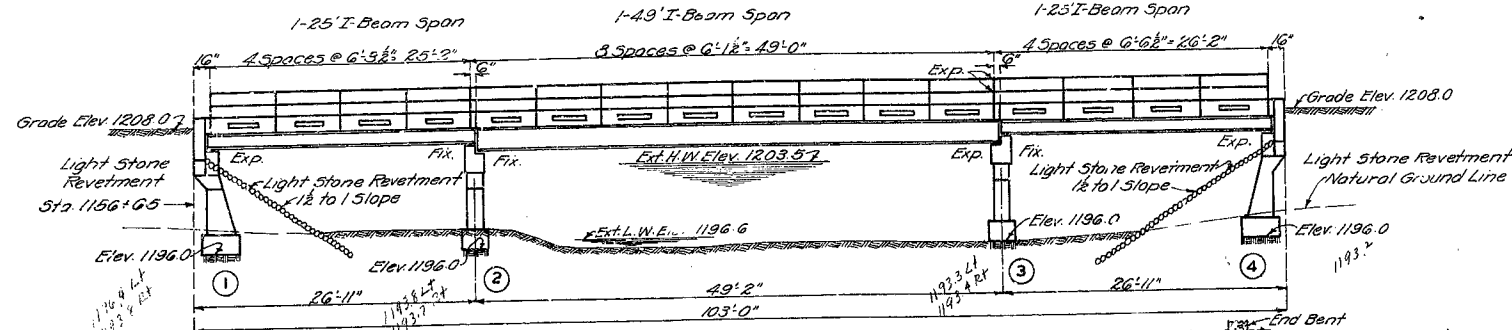
COUNTY : TEXAS BRIDGE : S0529 REVIEW STATUS : APPROVED NBI STATUS : T
RECORD TYPE : ROUTE CARRIED 'ON' STRUCT RUN DATE : 11/10/2022 SUBMITTAL YEAR : 2022

LOAD RATING AND POSTING INFORMATION				MATERIAL/CONSTRUCTION INFORMATION					
31	Design Load	H 10		43A	Main Struc. Mat type	STEEL			
41	Structure Status	POSTED FOR LOAD		43B	Main struc Constr. Type	STRINGER/MULTIBEAM - GRD			
63	Oper. Rating Meth.	LOAD FACTOR		45	# of Main Spans	4			
64	Operating Rating	17 Tons.		44A	Appr Struc. Mat type	000			
65	Inventory Rating Meth	LOAD FACTOR		44B	Appr Struc. Cnstr. type	000			
66	Inventory Rating	10 Tons.		46	# of Approach Span	0			
70	Bridge Posting Code	30.0-39.9% BELOW		107	Deck Mat/Constr.	1 CONCRETE CIP			
PROPOSED IMPROVEMENT INFORMATION				108A	Wear Surf Mat/Constr.	6 BITUMINOUS			
	Sufficiency Rating	43.2	Percent	108B	Membrane Mat/Constr.	0 NONE			
	Deficiency Rating	STRUCTURAL			108C	Deck Protect Mat/Constr.	0 NONE		
	Funding Eligibility	FULL			CONDITION RATING INFORMATION				
75A	Proposed Work	REPLACEMENT SUBSTND LOAD		58	Deck Cond. Rating	4			
75B	Work Done By	Contract		59	Superstructure Cond. Rating	5			
76	New Struc Length	232 Ft. 11 In.		60	Substructure Cond. Rating	6			
94	Struc Improve Cost	\$ 1,211,000		61	Channel /Channel Protection Cond. Rating	5			
95	Roadway Improve Cost	\$ 121,000		62	Culvert Cond. Rating	N			
96	Total Project Cost	\$ 1,817,000		INSPECTION INFORMATION					
97	Year of Cost Estimates	2022		90	Gen. Insp Date	11 / 22			
APPRAISAL RATING INFORMATION				91	Gen. Insp. Frequency	12	Months		
36A	Br. Rail App. Rating	DOES NOT MEET ACCEPT STND		92A	Frac. Critical Inspection	N	Months		
36B	Transition Rail App. Rating	DOES NOT MEET ACCEPT STND		93A	Frac. Critical Insp. Date				
36C	Approach Rail App. Rating	DOES NOT MEET ACCEPT STND		92B	Underwater Inspection	N	Months		
36D	Rail End Treat. App. Rating	DOES NOT MEET ACCEPT STND		93B	Underwater Insp. Date				
67	Struc Eval App. Rating	2		92C	Special Inspection	N	Months		
68	Deck Geometry App. Rating	4		93C	Special Inspection Date				
69	Underclearance App. Rating	N		BORDER BRIDGE INFORMATION					
71	Waterway Adeq. App. Rating	6		98	Neighboring State Code				
72	Approach Road App. Rating	6		98B	Neighboring State % Respon				
113	Scour Assess App. Rating	8		99	Neighboring State Struc. No.				
APPROVED POSTING INFORMATION				FIELD POSTING INFORMATION					
Approved Posting Category		S-17		Field Posting Category		S-17			
		Ton1	Ton2	Ton3		Ton1	Ton2	Ton3	
Tonnage Values for Posting Sign		16	22	39	Tonnage Values for Posting Sign		16	22	39
General Text for Posting Sign					General Text for Posting Sign				
CL OF BR AND TRK OVR 16 T 15MPH ON BR EXCPT SNGL E UNT					CL OF BR AND TRK OVR 16 T 15MPH ON BR EXCPT SNGL E UNT				
TRKS WT LMT 22 T OTHR TRKS WT LMT 39 T.					TRKS WT LMT 22 T OTHR TRKS WT LMT 39 T.				

Design_No = S0529 and Inventory_Appraisal_Submittal_Year = 2022

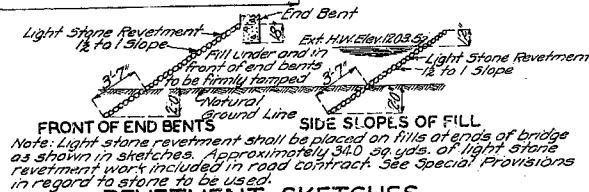
MISSOURI STATE HIGHWAY DEPARTMENT

FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
5	MO	57-56	19		



Note: Carry footings at least 18" into shale and other soft rock or 6" into solid hard rock.

GENERAL ELEVATION



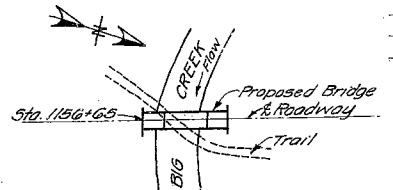
REVENMENT SKETCHES

HANDRAIL SPACING				
"A" DIMENSIONS				
SPAN "S"	NO. PANELS	SINGLE SPAN	1ST END SPAN	LAST END SPAN
15'-0"	3	5'-5 1/2" abt.	5'-0 1/2" abt.	5'-0"
17'-0"	3	6'-1 1/2" abt.	5'-8 1/2" abt.	5'-8"
19'-0"	3	6'-9 1/2" abt.	6'-4 1/2" abt.	6'-4"
21'-0"	4	5'-7" abt.	5'-3 1/2" abt.	5'-3"
23'-0"	4	6'-1" abt.	5'-9 1/2" abt.	5'-9"
25'-0"	4	6'-5 1/2" abt.	6'-3 1/2" abt.	6'-3"
27'-0"	5	5'-8" abt.	5'-5 1/2" abt.	5'-5 1/2"
29'-0"	5	6'-0 1/2" abt.	5'-10" abt.	6'-0 1/2"
31'-0"	5	6'-5 1/2" abt.	6'-2 1/2" abt.	6'-2 1/2"
33'-0"	6	5'-8 1/2" abt.	5'-6 1/2" abt.	5'-6 1/2"
35'-0"	6	6'-0 1/2" abt.	5'-10 1/2" abt.	5'-10 1/2"
37'-0"	6	6'-4 1/2" abt.	6'-2 1/2" abt.	6'-2 1/2"
39'-0"	7	5'-9 1/2" abt.	5'-7 1/2" abt.	5'-7 1/2"
41'-0"	7	6'-0 1/2" abt.	5'-10 1/2" abt.	6'-0 1/2"
43'-0"	7	6'-4" abt.	6'-1 1/2" abt.	6'-1 1/2"
45'-0"	8	5'-9 1/2" abt.	5'-7 1/2" abt.	5'-7 1/2"
47'-0"	8	6'-0 1/2" abt.	5'-10 1/2" abt.	6'-0 1/2"
49'-0"	8	6'-5 1/2" abt.	6'-3 1/2" abt.	6'-3 1/2"

DEFLECTION	
SPAN "S"	"C" "D"
15'-0"	1/8"
17'-0"	1/8"
19'-0"	1/8"
21'-0"	1/8"
23'-0"	1/8"
25'-0"	1/8"
27'-0"	1/8"
29'-0"	1/8"
31'-0"	1/8"
33'-0"	1/8"
35'-0"	1/8"
37'-0"	1/8"
39'-0"	1/8"
41'-0"	1/8"
43'-0"	1/8"
45'-0"	1/8"
47'-0"	1/8"
49'-0"	1/8"

Note: Floor slab to be brought to grade and dead load deflection taken care of by increasing slab thickness. Depth of slab at outside face of curb to be kept uniform and bottom surface of slab warped between curb and outside beam to obtain required thickness at beam. Payment will be allowed for additional concrete required for thickening slab. This additional concrete is included in "Estimated Quantities".

DEFLECTION DIAGRAM

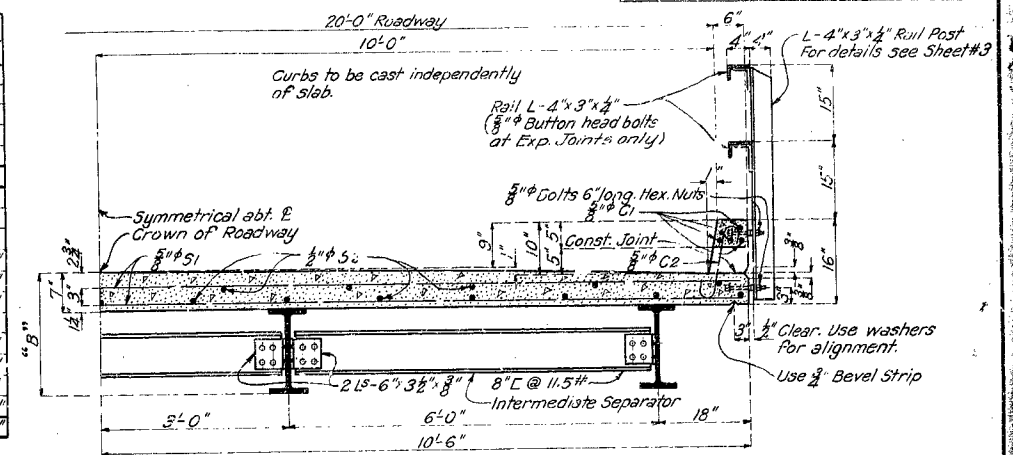


LOCATION SKETCH

DIMENSION "B"	
SPAN "S"	"B"
15'-0"	20 1/2"
17'-0"	21"
19'-0"	22 1/2"
21'-0"	23"
23'-0"	23 1/2"
25'-0"	24 1/2"
27'-0"	25 1/2"
29'-0"	26 1/2"
31'-0"	27 1/2"
33'-0"	28 1/2"
35'-0"	29 1/2"
37'-0"	30 1/2"
39'-0"	31 1/2"
41'-0"	32 1/2"
43'-0"	33 1/2"
45'-0"	34 1/2"
47'-0"	35 1/2"
49'-0"	36 1/2"

Note: Top of channel separators at ends of each I-Beam Span to be flush with bottom of floor slab as shown in section thru end bent at E.

Note: Depth of outside stringers will in some cases be a fraction of an inch less than that of inside stringers and in order to keep bottom of slab horizontal it will be necessary to haunch slab down to top of outside stringers.



HALF SECTION THRU SPAN

SPAN "S"	TABLE OF STRINGERS			
	PER PLANS		PERMISSIBLE SUBSTITUTIONS	
	CARNEGIE BEAMS	STANDARD I-BEAMS	BETHLEHEM BEAMS	
	Inside	Outside	Inside	Outside
15'-0"	14" @ 30"	14" @ 30"	12" @ 35"	14" @ 30"
17'-0"	14" @ 33"	14" @ 33"	15" @ 42 1/2"	14" @ 33"
19'-0"	16" @ 37"	16" @ 37"	15" @ 42 1/2"	16" @ 37"
21'-0"	16" @ 40"	16" @ 37"	15" @ 45"	16" @ 40"
23'-0"	16" @ 45"	16" @ 40"	18" @ 54 1/2"	16" @ 45"
25'-0"	18" @ 47"	18" @ 47"	18" @ 54 1/2"	18" @ 47"
27'-0"	18" @ 52"	18" @ 47"	18" @ 60"	18" @ 52"
29'-0"	20" @ 55"	20" @ 55"	20" @ 65 1/2"	20" @ 55"
31'-0"	21" @ 58"	21" @ 58"	20" @ 70"	22" @ 58"
33'-0"	21" @ 62"	21" @ 58"	20" @ 75"	22" @ 62"
35'-0"	21" @ 67"	21" @ 62"	20" @ 81 1/2"	22" @ 67"
37'-0"	24" @ 70"	24" @ 70"	24" @ 79 1/2"	24" @ 70"
39'-0"	24" @ 74"	24" @ 70"	24" @ 79 1/2"	24" @ 74"
41'-0"	24" @ 81"	24" @ 74"	24" @ 85"	24" @ 81"
43'-0"	24" @ 85"	24" @ 81"	24" @ 100"	26" @ 85"
45'-0"	27" @ 85"	27" @ 85"	24" @ 105 1/2"	28" @ 85"
47'-0"	27" @ 91"	27" @ 85"	24" @ 105 1/2"	28" @ 91"
49'-0"	27" @ 91"	27" @ 91"	24" @ 115"	28" @ 91"

GENERAL NOTES:

Loading: One 10 Ton Truck, 40% of weight on rear axle, 30% impact, 14'-0" wheel base, 6'-0" gage, 10" tire. All concrete to be 1:2 1/2 mix "X". Exposed edges to be beveled 1/4" where no other bevel is noted.

Bridge excavation in accordance with Section I of Standard Specifications issued April 1, 1930, except that quantities paid for will be computed from Ext. L.W. Elev. 1196.6 where existing ground line is below this elevation. I-Beams with fastenings, spacers, handrail, handrail posts with fastenings, will be paid for as structural steel. Cost of metallic edge moulding will be included in unit bid price for concrete. Rivets 3/4" holes 1/2", except in handrail where rivets shall be 5/8" holes 1/2". Field connections riveted except as noted.

Detail shop drawings shall be submitted to the State Highway Department in duplicate and shall be approved before steel is fabricated. Where rubber compound is specified on plans for use in partition and expansion joints, the pre-moulded joint shall be securely stitched to one face of concrete with copper wire. Paint: Shop, no. 1: Field, contact surfaces with boiled field connections are coat red lead. No other paint to be applied by contractor. All paint required will be furnished by the Missouri State Highway Department.

B.M. Elev. 1203.18 - Nail in Root 18" Twin Walnut 60 Ft. Sta. 1157+70.

BRIDGE OVER BIG CREEK

STATE ROAD FROM WILLOW SPRINGS TO ROUTE 17
ABOUT 22 MILES NORTH OF WILLOW SPRINGS
PROJECT NO. 33-S6 STA. 1156+65

TEXAS COUNTY

SUBMITTED BY *N.R. Lock* DATE *11/18/32*
APPROVED BY *T.H. Carter* DATE *11/18/32*
FINISHED

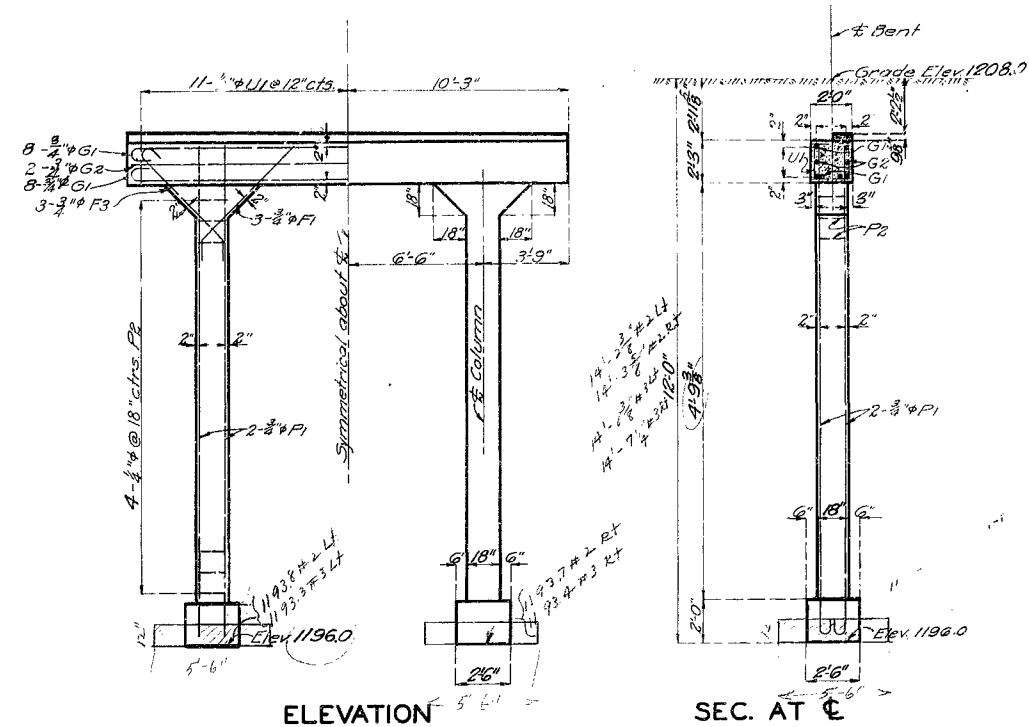
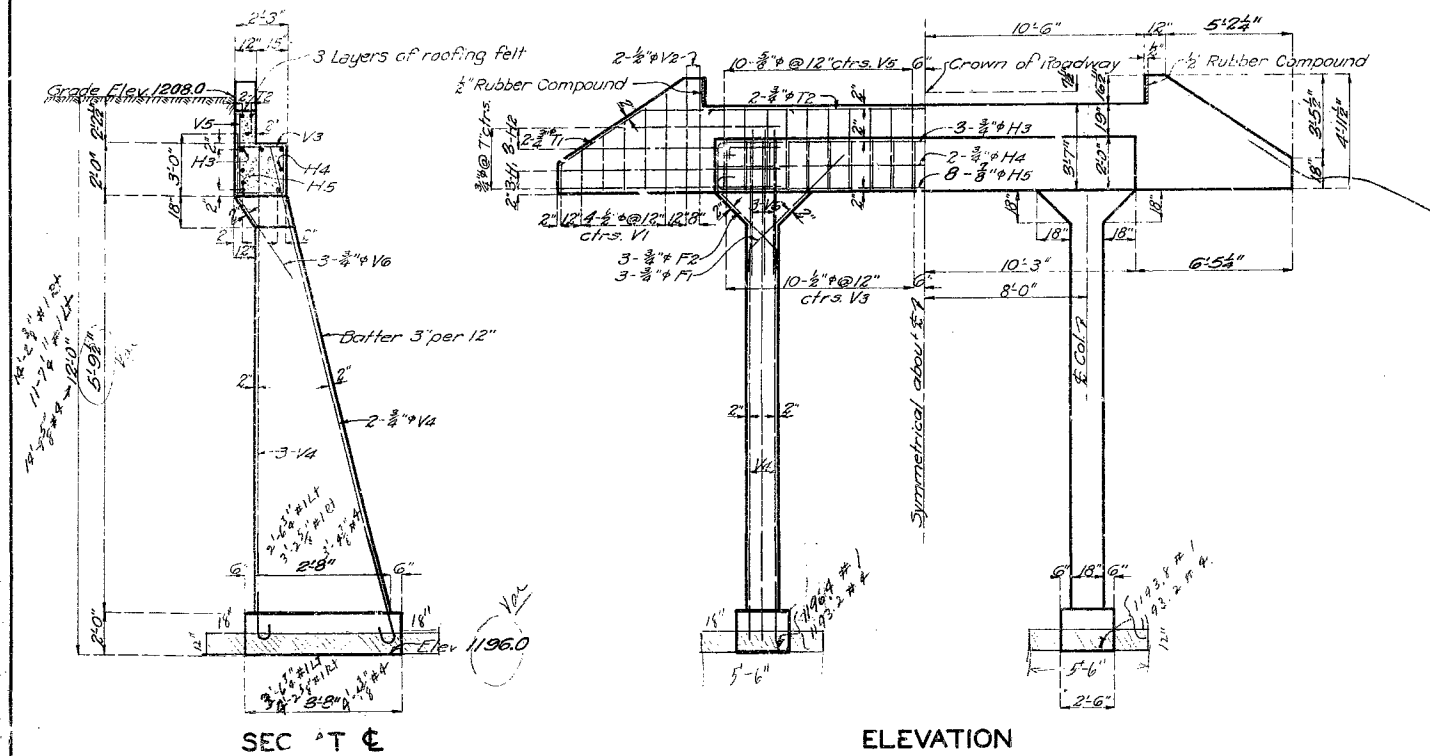
S-607

Designed Nov. 1929 By F.W.H.
Drawn Mar. 1930 By R.J.G.
Traced Dec. 1931 By R.J.G.
Checked Dec. 1931 By R.J.G.
Assembled Nov. 1932 By L.K.S.-G.W.
Checked Nov. 1932 By R.J.G.

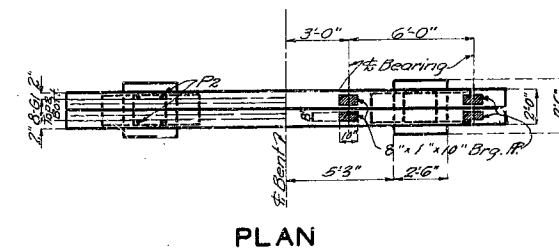
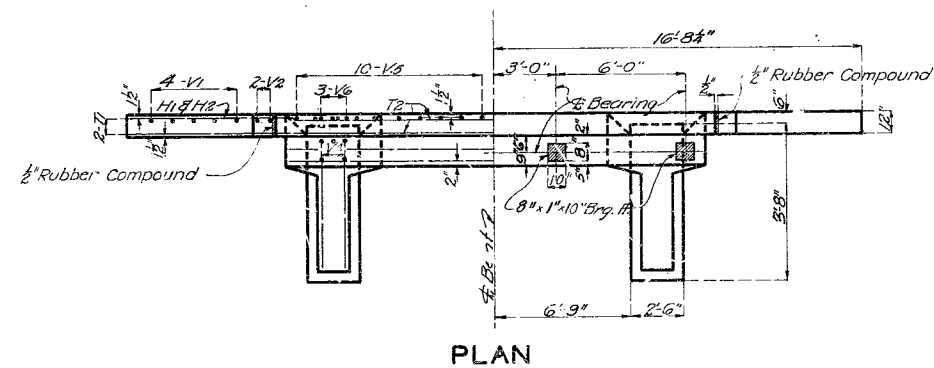
Sheet No. 1 of 2

10-15-32

FED. ROAD DIST. NO.	STATE	FED. ID PROJ. NO.	FISCAL YEAR	STATUS	DATE
55-5	MO	57-56	1966		



*Note: This drawing is not to scale.
Follow dimensions.*



DETAILS OF BENTS NO. 2 & 3

DETAILS OF BENTS NO. 1 & 4

BRIDGE OVER BIG CREEK

STATE ROAD FROM WILLOW SPRINGS TO ROUTE 17

ABOUT 22 MILES NORTH OF WILLOW SPRINGS, FINISHED

PROJECT NO. SJ-S6 STA 1156 + 65

TEXAS COUNTY

FINISHED

COUNTY

Assembled Nov. 1932 by L.K.S.
Checked Nov. 1932 by PAB.
Drawn Nov. 1930 by C.A.F.
Checked Aug. 1932 by P.H.S.

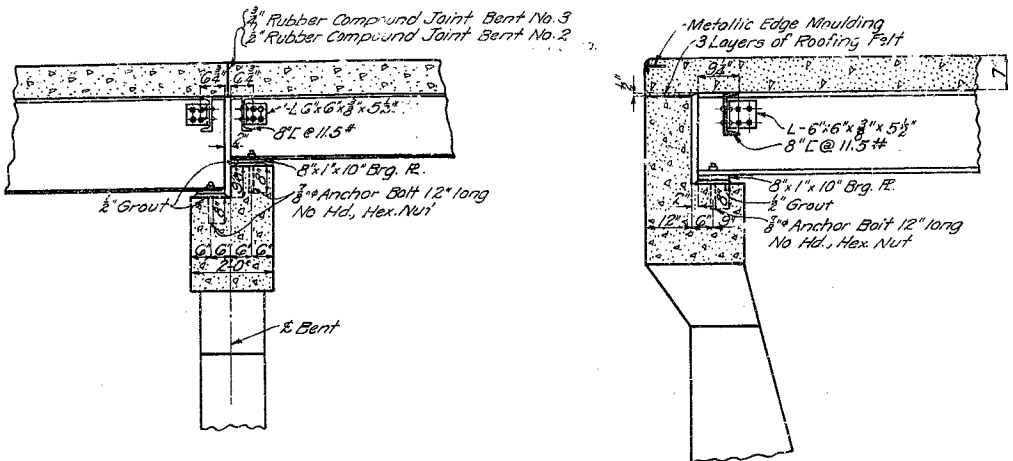
Sheet No. 2 of 4.

S-607

9-17-32

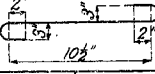
MISSOURI STATE HIGHWAY DEPARTMENT

FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
5	MO.	33-56	19		



SECTION THRU INT. BENT AT C

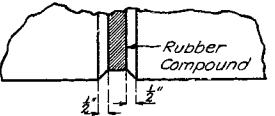
SECTION THRU END BENT AT E

BILL OF REINFORCING STEEL - SUPERSTR.														
NO.	SIZE	LENGTH	MARK	LOCATION	BENDING SKETCH									
"A"	8" #	"B"	C1	Curb										
"C"	8" #	2'-0"	C2	"										
"D"	8" #	20'-9"	S1	Slab										
"E"	8" #	"F"	S2	"										
SINGLE SPAN														
SPAN	"A"	"B"	"C"	"D"	"E"	"F"	"A"	"B"	"C"	"D"	"E"	"F"		
15'-0"	8	24	26	26	8	22	26	8	14'-9"	20	64	26	14'-9"	
17'-0"	8	28	26	26	8	24	26	8	16'-9"	20	74	26	16'-9"	
19'-0"	8	28	26	26	8	28	26	8	18'-9"	28	84	26	18'-9"	
21'-0"	8	30	26	26	8	28	26	8	20'-9"	26	92	26	20'-9"	
23'-0"	8	36	26	26	8	34	26	8	22'-9"	32	100	26	22'-9"	
25'-0"	8	36	26	26	8	26'-9"	38	118	26	26'-9"	32	118	26	26'-9"
27'-0"	8	36	26	26	8	34	26	8	26'-9"	32	118	26	26'-9"	
29'-0"	8	44	26	26	8	42	26	8	28'-9"	40	126	26	28'-9"	
31'-0"	8	44	26	26	8	44	26	8	30'-9"	44	134	26	30'-9"	
33'-0"	16	52	52	16	50	52	16	17'-6"	48	146	52	17'-6"		
35'-0"	16	52	52	16	50	52	16	18'-6"	48	154	52	18'-6"		
37'-0"	16	52	52	16	50	52	16	19'-6"	48	164	52	19'-6"		
39'-0"	16	60	52	16	58	52	16	20'-6"	56	172	52	20'-6"		
41'-0"	16	60	52	16	58	52	16	21'-6"	56	180	52	21'-6"		
43'-0"	16	60	52	16	58	52	16	22'-6"	56	190	52	22'-6"		
45'-0"	16	68	52	16	66	52	16	23'-6"	64	198	52	23'-6"		
47'-0"	16	68	52	16	66	52	16	24'-6"	64	204	52	24'-6"		
49'-0"	16	68	52	16	66	52	16	25'-6"	64	214	52	25'-6"		

Note: Reinforcing bars in each span to be billed and tagged separately.

SUPERSTRUCTURE HARDWARE BILL												
PIECE	LOCATION	NO. PCS.	SIZE	LENGTH	REMARKS							
or Bolts	Spans 15' to 31' Incl.	48	3/4"	Varies	bolts, washer for nut							
Spacer Bolts	Spans 33' to 49' Incl.	64	3/4"	Varies	bolts, washer for nut							
Handrail Bolts	Spans 15'	16	3/4"	6"	Sq. hds., washers, hex. nuts							
"	to 19' Incl.	32	3/4"	1 1/2"	Button heads							
"	Spans 21'	20	3/4"	6"	Sq. hds., washers, hex. nuts							
"	to 25' Incl.	40	3/4"	1 1/2"	Button heads							
"	Spans 27'	24	3/4"	6"	Sq. hds., washers, hex. nuts							
"	to 31' Incl.	48	3/4"	1 1/2"	Button heads							
"	Spans 33'	28	3/4"	6"	Sq. hds., washers, hex. nuts							
"	to 37' Incl.	56	3/4"	1 1/2"	Button heads							
"	Spans 39'	32	3/4"	6"	Sq. hds., washers, hex. nuts							
"	to 43' Incl.	64	3/4"	1 1/2"	Button heads							
"	Spans 45'	36	3/4"	6"	Sq. hds., washers, hex. nuts							
"	to 49' Incl.	72	3/4"	1 1/2"	Button heads							
Bolts	Bearing Pls.	16	3/4"	12"	Sq. heads, no threads							
Spacer Bolts	Spans 15' to 31' Incl.	48	3/4"	Varies	bolts, washer for nut							
Spacer Bolts	Spans 33' to 49' Incl.	64	3/4"	Varies	bolts, washer for nut							
Handrail Bolts	Spans 15'	12	3/4"	6"	Sq. hds., washers, hex. nuts							
"	to 19' Incl.	32	3/4"	1 1/2"	Button heads							
"	Spans 21'	16	3/4"	6"	Sq. hds., washers, hex. nuts							
"	to 25' Incl.	40	3/4"	1 1/2"	Button heads							
"	Spans 27'	20	3/4"	6"	Sq. hds., washers, hex. nuts							
"	to 31' Incl.	48	3/4"	1 1/2"	Button heads							
"	Spans 33'	24	3/4"	6"	Sq. hds., washers, hex. nuts							
"	to 37' Incl.	56	3/4"	1 1/2"	Button heads							
"	Spans 39'	28	3/4"	6"	Sq. hds., washers, hex. nuts							
"	to 43' Incl.	64	3/4"	1 1/2"	Button heads							
"	Spans 45'	32	3/4"	6"	Sq. hds., washers, hex. nuts							
"	to 49' Incl.	72	3/4"	1 1/2"	Button heads							
Bolts	Bearing Pls.	16	3/4"	12"	Sq. heads, no threads							

* 2 Washers plus washers for fill required for alignment of railposts.



Note: Use bevel as shown for exposed faces of all joints consisting of rubber compound except at top surface of roadway slab. Use edging tool with 1/4" radius at top surface of roadway slab each side of rubber compound joint.

DETAILS OF BEVEL FOR RUBBER COMPOUND JOINTS

BRIDGE OVER BIG CREEK

STATE ROAD FROM WILLOW SPRINGS TO ROUTE 17
ABOUT 22 MILES NORTH OF WILLOW SPRINGS FINISHED
PROJECT NO. SJ-56 STA. 1156+65

TEXAS COUNTY
FINISHED

Note: This drawing is not to scale. Follow dimensions.

MISSOURI STATE HIGHWAY DEPARTMENT

FED. ROAD DIST. NO.	SHEET NO.	FED. AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
5	MO	SJ-S6	19		

TABLE OF SPACERS

SPAN "S"	NO.	END SPACERS "V"	INT. SPACERS "V"
15'-0"	6	5'-11 1/2"	3 5'-11 1/2"
17'-0"	6	5'-11 1/2"	3 5'-11 1/2"
19'-0"	6	5'-11 1/2"	3 5'-11 1/2"
21'-0"	6	5'-11 1/2"	3 5'-11 1/2"
23'-0"	6	5'-11 1/2"	3 5'-11 1/2"
25'-0"	6	5'-11 1/2"	3 5'-11 1/2"
27'-0"	6	5'-11 1/2"	3 5'-11 1/2"
29'-0"	6	5'-11 1/2"	3 5'-11 1/2"
31'-0"	6	5'-11 1/2"	3 5'-11 1/2"
33'-0"	6	5'-11 1/2"	3 5'-11 1/2"
35'-0"	6	5'-11 1/2"	6 5'-11 1/2"
37'-0"	6	5'-11 1/2"	6 5'-11 1/2"
39'-0"	6	5'-11 1/2"	6 5'-11 1/2"
41'-0"	6	5'-11 1/2"	6 5'-11 1/2"
43'-0"	6	5'-11 1/2"	6 5'-11 1/2"
45'-0"	6	5'-11 1/2"	6 5'-11 1/2"
47'-0"	6	5'-11 1/2"	6 5'-11 1/2"
49'-0"	6	5'-11 1/2"	6 5'-11 1/2"

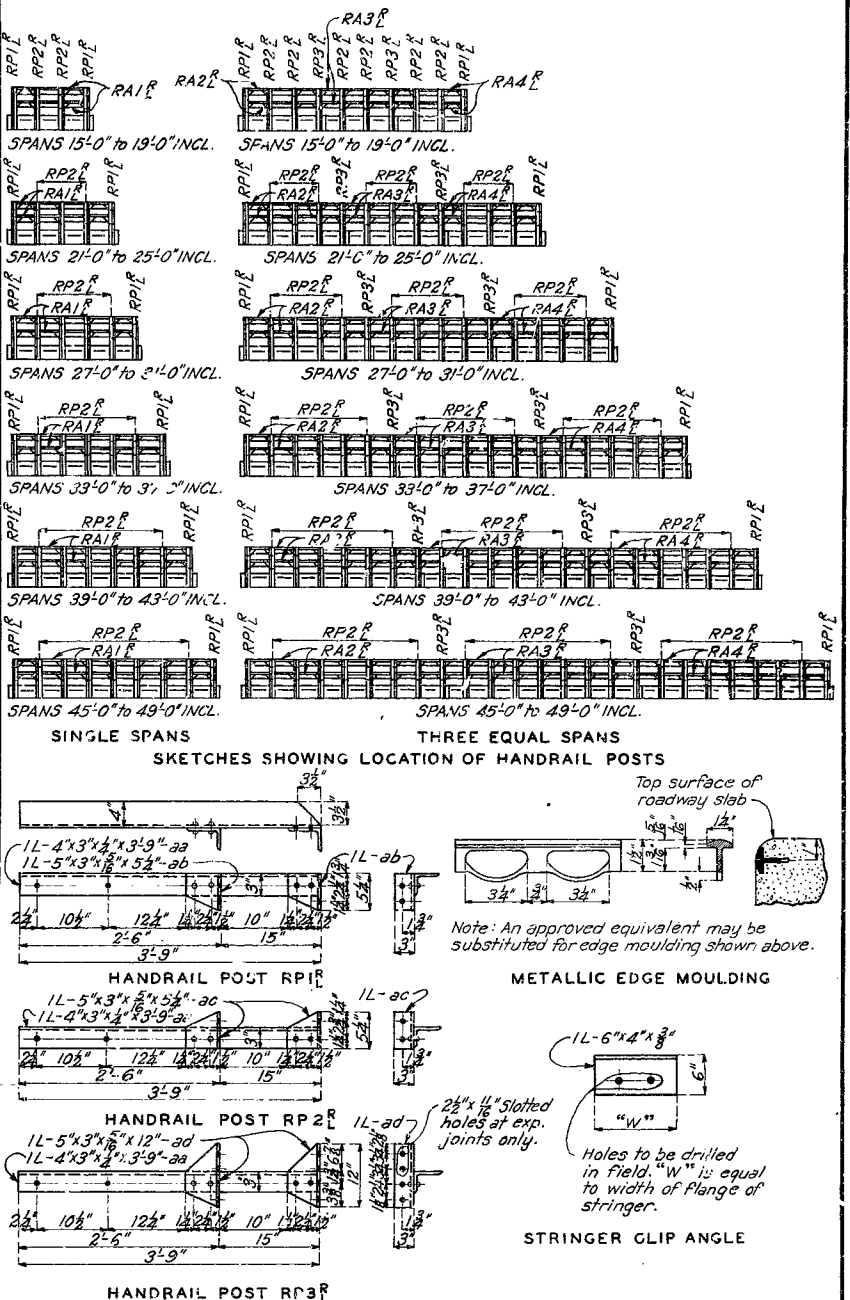
TABLE OF STRINGERS


SPAN	NO.	SIZE	SINGLE SPAN	END SPAN	INT. SPAN	"A"	"B"	"C"
15'-0"	4	14" C.B. @ 30"	15'-8"	6'-8"	15'-8"	6'-8"	14'-11"	6'-8"
17'-0"	4	14" C.B. @ 33"	17'-8"	7'-8"	17'-8"	7'-8"	16'-11"	7'-8"
19'-0"	4	16" C.B. @ 37"	19'-8"	8'-8"	19'-8"	8'-8"	18'-11"	8'-8"
21'-0"	2	16" C.B. @ 37"	21'-8"	9'-8"	21'-8"	9'-8"	20'-11"	9'-8"
23'-0"	2	16" C.B. @ 40"	23'-8"	10'-8"	23'-8"	10'-8"	22'-11"	10'-8"
25'-0"	2	16" C.B. @ 45"	25'-8"	11'-8"	25'-8"	11'-8"	24'-11"	11'-8"
27'-0"	2	18" C.B. @ 47"	27'-8"	12'-8"	27'-8"	12'-8"	26'-11"	12'-8"
29'-0"	2	18" C.B. @ 52"	29'-8"	13'-8"	29'-8"	13'-8"	28'-11"	13'-8"
31'-0"	4	20" C.B. @ 55"	31'-8"	14'-8"	31'-8"	14'-8"	30'-11"	14'-8"
33'-0"	2	21" C.B. @ 58"	33'-8"	15'-8"	33'-8"	15'-8"	32'-11"	15'-8"
35'-0"	2	21" C.B. @ 62"	35'-8"	16'-8"	35'-8"	16'-8"	34'-11"	16'-8"
37'-0"	4	24" C.B. @ 70"	37'-8"	18'-8"	37'-8"	18'-8"	36'-11"	18'-8"
39'-0"	2	24" C.B. @ 74"	39'-8"	19'-8"	39'-8"	19'-8"	38'-11"	19'-8"
41'-0"	2	24" C.B. @ 74"	41'-8"	20'-8"	41'-8"	20'-8"	40'-11"	20'-8"
43'-0"	2	24" C.B. @ 81"	43'-8"	21'-8"	43'-8"	21'-8"	42'-11"	21'-8"
45'-0"	4	27" C.B. @ 85"	45'-8"	22'-8"	45'-8"	22'-8"	44'-11"	22'-8"
47'-0"	4	27" C.B. @ 91"	47'-8"	23'-8"	47'-8"	23'-8"	46'-11"	23'-8"
49'-0"	4	27" C.B. @ 91"	49'-8"	24'-8"	49'-8"	24'-8"	48'-11"	24'-8"


TABLE OF RAIL ANGLE DIMENSIONS

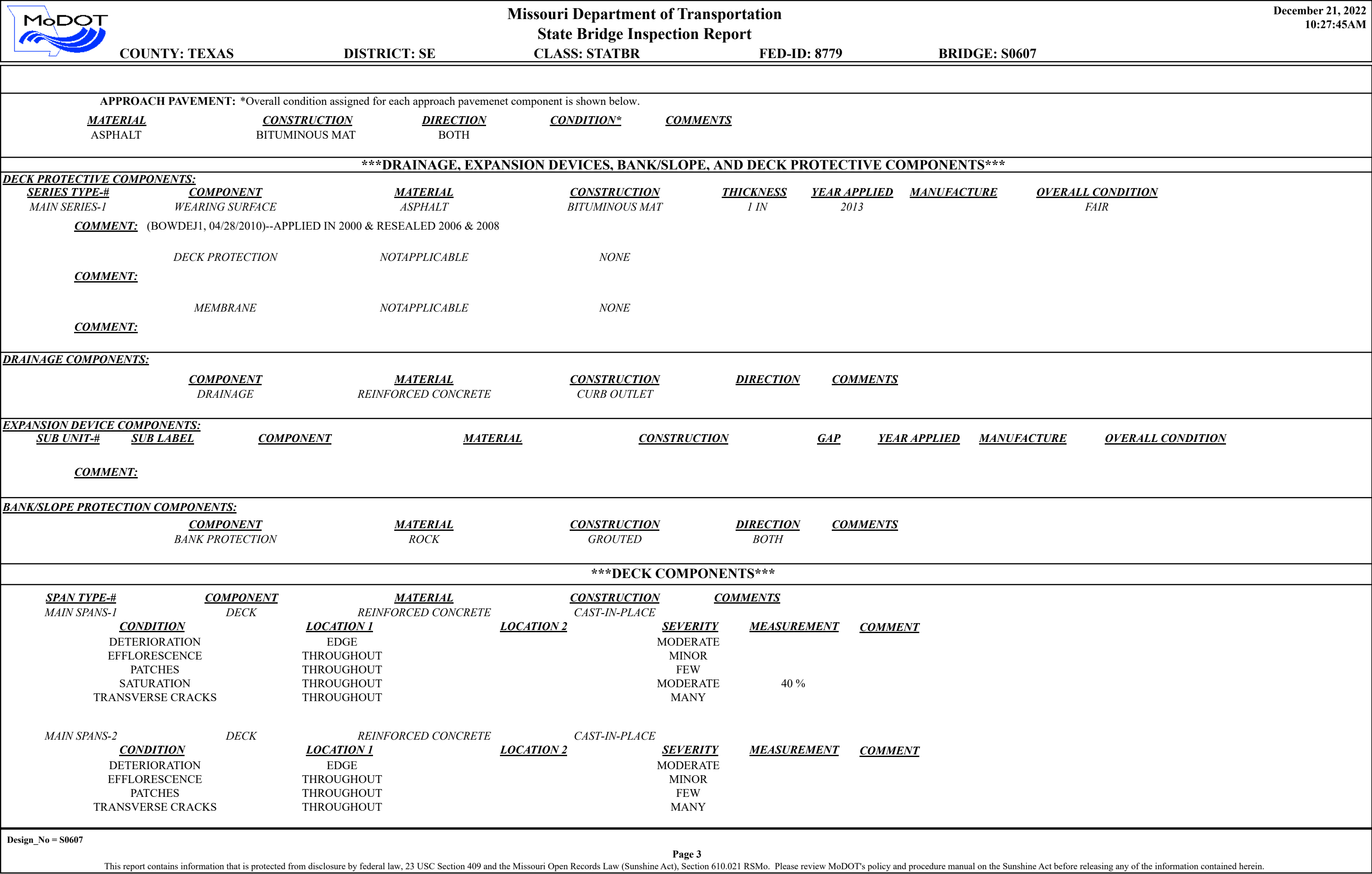
SPAN	MARK	"L"	"M"	"N"	"O"	"P"	"Q"	"R"	"S"	"T"	"U"	"V"	"W"	"X"	"Y"	"Z"
15'-0"	RA1	16'-7 1/2"	5'-0 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	5'-0 1/2"
17'-0"	"	18'-7 1/2"	5'-0 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	5'-0 1/2"
19'-0"	"	20'-7 1/2"	5'-0 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	5'-0 1/2"
21'-0"	"	22'-7 1/2"	5'-0 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	5'-0 1/2"
23'-0"	"	24'-7 1/2"	5'-0 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	5'-0 1/2"
25'-0"	"	26'-7 1/2"	5'-0 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	5'-0 1/2"
27'-0"	"	28'-7 1/2"	5'-0 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	5'-0 1/2"
29'-0"	"	30'-7 1/2"	5'-0 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	5'-0 1/2"
31'-0"	"	32'-7 1/2"	5'-0 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	5'-0 1/2"
33'-0"	"	34'-7 1/2"	5'-0 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	5'-0 1/2"
35'-0"	"	36'-7 1/2"	5'-0 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	5'-0 1/2"
37'-0"	"	38'-7 1/2"	5'-0 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	5'-0 1/2"
39'-0"	"	40'-7 1/2"	5'-0 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	5'-0 1/2"
41'-0"	"	42'-7 1/2"	5'-0 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	5'-0 1/2"
43'-0"	"	44'-7 1/2"	5'-0 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	5'-0 1/2"
45'-0"	"	46'-7 1/2"	5'-0 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	5'-0 1/2"
47'-0"	"	48'-7 1/2"	5'-0 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	5'-0 1/2"
49'-0"	"	50'-7 1/2"	5'-0 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	5'-0 1/2"
15'-0"	RA2	15'-4 1/2"	4'-7 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-7 1/2"
17'-0"	"	17'-4 1/2"	4'-7 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-7 1/2"
19'-0"	"	19'-4 1/2"	4'-7 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-7 1/2"
21'-0"	"	21'-4 1/2"	4'-7 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-7 1/2"
23'-0"	"	23'-4 1/2"	4'-7 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-7 1/2"
25'-0"	"	25'-4 1/2"	4'-7 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-7 1/2"
27'-0"	"	27'-4 1/2"	4'-7 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-7 1/2"
29'-0"	"	29'-4 1/2"	4'-7 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-7 1/2"
31'-0"	"	31'-4 1/2"	4'-7 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-7 1/2"
33'-0"	"	33'-4 1/2"	4'-7 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-7 1/2"
35'-0"	"	35'-4 1/2"	4'-7 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-7 1/2"
37'-0"	"	37'-4 1/2"	4'-7 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-7 1/2"
39'-0"	"	39'-4 1/2"	4'-7 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-7 1/2"
41'-0"	"	41'-4 1/2"	4'-7 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-7 1/2"
43'-0"	"	43'-4 1/2"	4'-7 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-7 1/2"
45'-0"	"	45'-4 1/2"	4'-7 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-7 1/2"
47'-0"	"	47'-4 1/2"	4'-7 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-7 1/2"
49'-0"	"	49'-4 1/2"	4'-7 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-7 1/2"
15'-0"	RA3	14'-11"	4'-5 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-5 1/2"
17'-0"	"	16'-11"	4'-5 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-5 1/2"
19'-0"	"	18'-11"	4'-5 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-5 1/2"
21'-0"	"	20'-11"	4'-5 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-5 1/2"
23'-0"	"	22'-11"	4'-5 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-5 1/2"
25'-0"	"	24'-11"	4'-5 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-5 1/2"
27'-0"	"	26'-11"	4'-5 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-5 1/2"
29'-0"	"	28'-11"	4'-5 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-5 1/2"
31'-0"	"	30'-11"	4'-5 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-5 1/2"
33'-0"	"	32'-11"	4'-5 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-5 1/2"
35'-0"	"	34'-11"	4'-5 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-5 1/2"
37'-0"	"	36'-11"	4'-5 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-5 1/2"
39'-0"	"	38'-11"	4'-5 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-5 1/2"
41'-0"	"	40'-11"	4'-5 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-5 1/2"
43'-0"	"	42'-11"	4'-5 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-5 1/2"
45'-0"	"	44'-11"	4'-5 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-5 1/2"
47'-0"	"	46'-11"	4'-5 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-5 1/2"
49'-0"	"	48'-11"	4'-5 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-5 1/2"
15'-0"	RA4	15'-1 1/2"	4'-7 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-7 1/2"
17'-0"	"	17'-1 1/2"	4'-7 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-7 1/2"
19'-0"	"	19'-1 1/2"	4'-7 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-7 1/2"
21'-0"	"	21'-1 1/2"	4'-7 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-7 1/2"
23'-0"	"	23'-1 1/2"	4'-7 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-7 1/2"
25'-0"	"	25'-1 1/2"	4'-7 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-7 1/2"
27'-0"	"	27'-1 1/2"	4'-7 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-7 1/2"
29'-0"	"	29'-1 1/2"	4'-7 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-7 1/2"
31'-0"	"	31'-1 1/2"	4'-7 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-7 1/2"
33'-0"	"	33'-1 1/2"	4'-7 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-7 1/2"
35'-0"	"	35'-1 1/2"	4'-7 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-7 1/2"
37'-0"	"	37'-1 1/2"	4'-7 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-7 1/2"
39'-0"	"	39'-1 1/2"	4'-7 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-7 1/2"
41'-0"	"	41'-1 1/2"	4'-7 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-7 1/2"
43'-0"	"	43'-1 1/2"	4'-7 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-7 1/2"
45'-0"	"	45'-1 1/2"	4'-7 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-7 1/2"
47'-0"	"	47'-1 1/2"	4'-7 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-7 1/2"
49'-0"	"	49'-1 1/2"	4'-7 1/2"	0	0	0	0	0	0	0	0	0	0	0	0	4'-7 1/2"

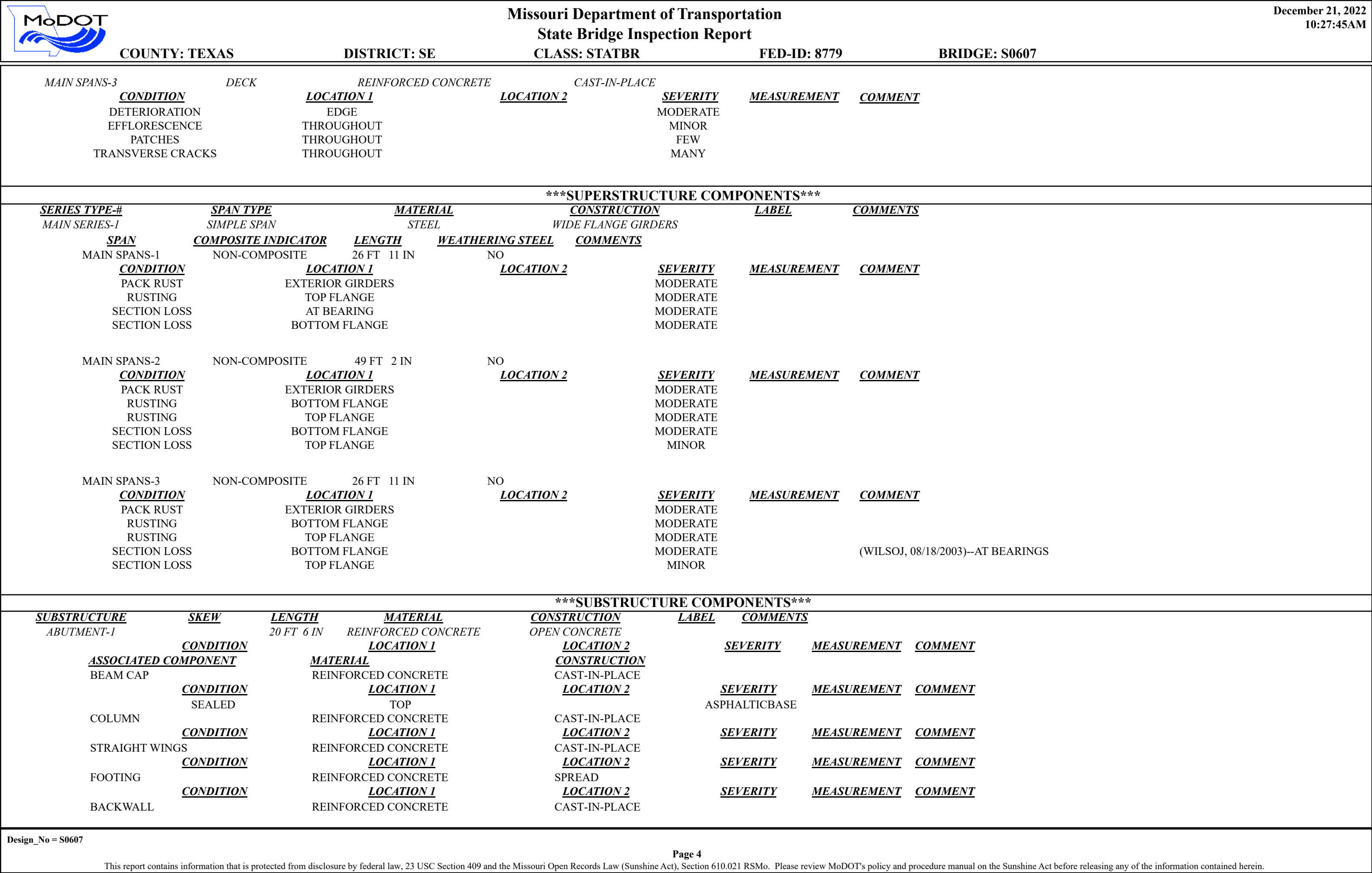
DETAIL OF RAIL ANGLES RA1 TO RA4 INCL.

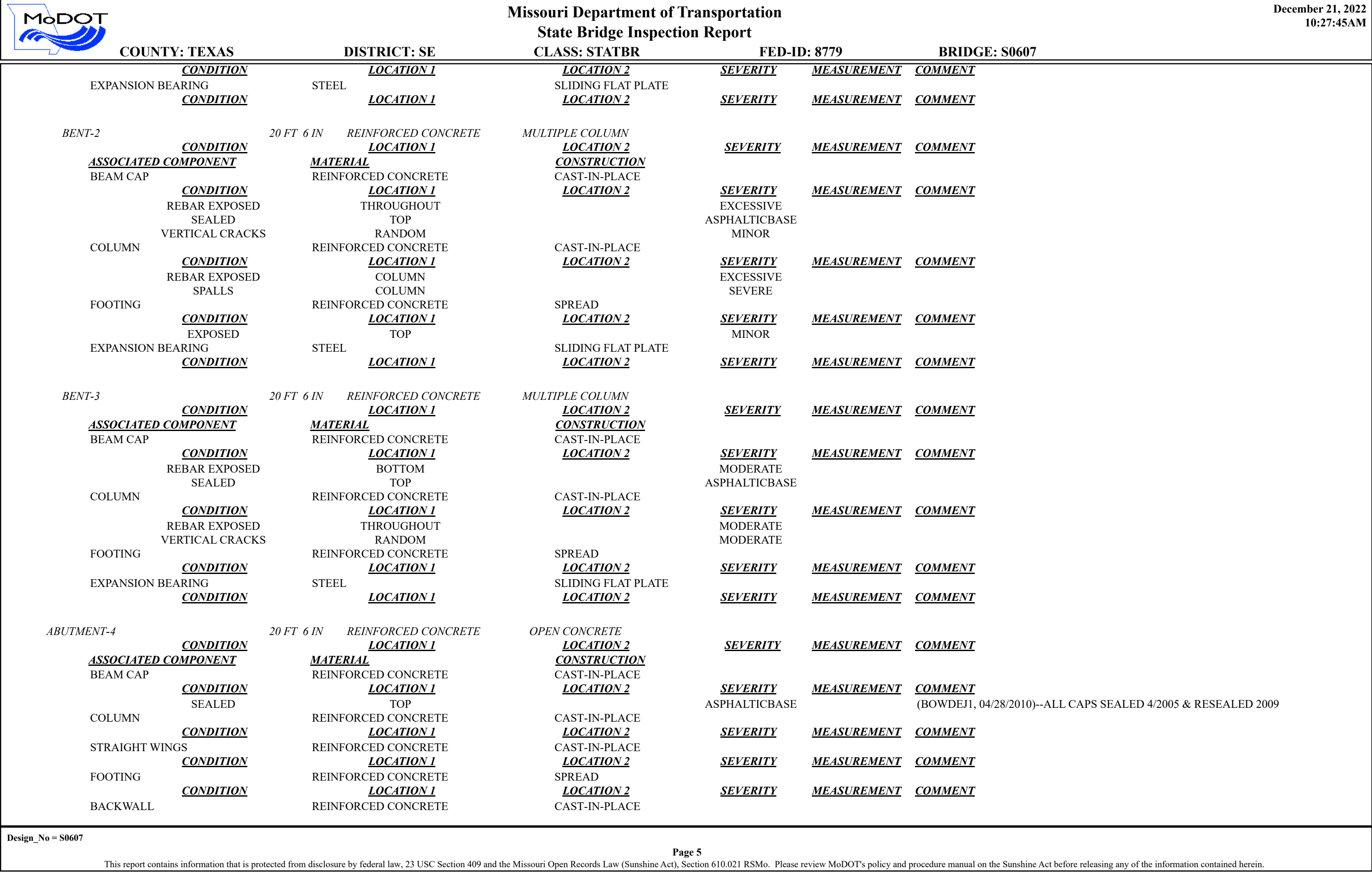



		<div>Missouri Department of Transportation</div> <div>State Bridge Inspection Report</div>				<div>December 21, 2022</div> <div>10:27:45AM</div>													
COUNTY: TEXAS		DISTRICT: SE		CLASS: STATBR		FED-ID: 8779		BRIDGE: S0607											
GENERAL STRUCTURE INFORMATION							***BRIDGE INSPECTION INFORMATION***												
<div>ROUTE: MO137S</div> <div>FEATURE: BIG CR</div> <div>STATUS: P-POSTLOAD</div> <div>LOG MILE: 20.474</div> <div>DETOUR: 16.00 MILES</div> <div>NHS: NO</div> <div>BUILT: 1932</div> <div>REHAB:</div> <div>LOCATION: S 5 T 29 R 8 W</div> <div>LATITUDE: 37 13 58.72 (DMS)</div> <div>LONGITUDE: 91 51 .27 (DMS)</div>		<div># SPANS: 3</div> <div>LANES ON: 1</div> <div>LANES UNDER: 0</div> <div>COMPASS DIRECTION: SOUTH to NORTH</div> <div>DIRECTION OF TRAFFIC: 1-LN/2-WAY</div> <div>FUNCTIONAL CLASS: RL-MAJOR COLLECTOR</div> <div>NBI OWNER: MODOT</div> <div>NBI MAINTAINED: MODOT</div> <div>MAINTENANCE DISTRICT: SE</div> <div>MAINTENANCE COUNTY: TEXAS</div> <div>SUB AREA: 7H46</div>		<div>PLACE CODE: 55820 OZARK</div> <div>LENGTH: 103 FT 0 IN</div> <div>MAXIMUM SPAN: 49 FT 2 IN</div> <div>APPROACH ROADWAY: 20 FT 0 IN</div> <div>CURB TO CURB: 20 FT 0 IN</div> <div>OUT TO OUT: 21 FT 0 IN</div> <div>AADT: 335</div> <div>AADT YEAR: 2021</div> <div>AADT TRUCK: 9.0%</div> <div>FUTURE AADT: 469</div> <div>FUTURE AADT YEAR: 2041</div>		<div>DATE: 11/07/2022</div> <div>RESPONSIBILITY: DISTRICT</div> <div>FREQUENCY: 12</div> <div>CALCULATED INTERVAL**: 12</div> <div>TEAM LEADER: ED HESS</div> <div>ELEMENT: NO</div> <div>INSPECTOR 2:</div> <div>INSPECTOR 4:</div> <div>INSPECTOR 3:</div> <div>** When calculated interval exceeds the frequency, a justification comment per BIRM is required.</div>													
						<div>GENERAL INSPECTION COMMENTS</div>													
FRACTURE CRITICAL INSPECTION INFORMATION					***INDEPTH INSPECTION INFORMATION***														
<div>DATE:</div> <div>FREQUENCY:</div> <div>TEAM LEADER:</div> <div>INSPECTOR 2:</div> <div>** When calculated interval exceeds the frequency, a justification comment per BIRM is required.</div>					<div>RESPONSIBILITY:</div> <div>CALCULATED INTERVAL**:</div> <div>INSPECTOR 3:</div> <div>INSPECTOR 4:</div> <div>CATEGORY:</div> <div>NBI:</div> <div>METHOD:</div> <div>** When calculated interval exceeds the frequency, a justification comment per BIRM is required.</div>														
<div>FRACTURE CRITICAL INSPECTION COMMENTS</div>					<div>INDEPTH INSPECTION COMMENTS</div>														
SPECIAL INSPECTION INFORMATION					***UNDERWATER INSPECTION INFORMATION***														
<div>DATE: 06/13/2014</div> <div>FREQUENCY: 120</div> <div>TEAM LEADER:</div> <div>INSPECTOR 2: ED HESS</div> <div>** When calculated interval exceeds the frequency, a justification comment per BIRM is required.</div>					<div>RESPONSIBILITY: DISTRICT</div> <div>CALCULATED INTERVAL**:</div> <div>INSPECTOR 3:</div> <div>INSPECTOR 4:</div> <div>CATEGORY: CHANNEL CROSS SEC</div> <div>NBI: NO</div> <div>METHOD: WT TAPE</div>					<div>DATE: 11/07/2022</div> <div>FREQUENCY: 60</div> <div>TEAM LEADER: ED HESS</div> <div>INSPECTOR 2:</div> <div>** When calculated interval exceeds the frequency, a justification comment per BIRM is required.</div>					<div>RESPONSIBILITY: DISTRICT</div> <div>CALCULATED INTERVAL**: 12</div> <div>INSPECTOR 3:</div> <div>INSPECTOR 4:</div> <div>CATEGORY: DRY</div> <div>NBI: NO</div> <div>METHOD: VISUAL</div>				
<div>SPECIAL INSPECTION COMMENTS</div>					<div>UNDERWATER INSPECTION COMMENTS</div>														
OTHER SPECIAL INSPECTIONS					OTHER UNDERWATER INSPECTIONS														
<div>DATE</div> <div>FREQUENCY</div> <div>CATEGORY</div> <div>NBI</div> <div>CALCULATED INTERVAL</div> <div>RESPONSIBILITY</div> <div>METHOD</div>					<div>DATE</div> <div>FREQUENCY</div> <div>CATEGORY</div> <div>NBI</div> <div>CALCULATED INTERVAL</div> <div>RESPONSIBILITY</div> <div>METHOD</div>														
Design_No = S0607																			
<div>Page 1</div> <div>This report contains information that is protected from disclosure by federal law, 23 USC Section 409 and the Missouri Open Records Law (Sunshine Act), Section 610.021 RSMo. Please review MoDOT's policy and procedure manual on the Sunshine Act before releasing any of the information contained herein.</div>																			


		Missouri Department of Transportation			December 21, 2022	
		State Bridge Inspection Report			10:27:45AM	
COUNTY: TEXAS		DISTRICT: SE	CLASS: STATBR	FED-ID: 8779	BRIDGE: S0607	
STRUCTURE POSTING						
APPROVED CATEGORY: S-17		CL OF BR AND TRK OVR 16 T 15MPH ON BR EXCPT SNGL E UNT TRKS WT LMT 22 T OTHR TRKS WT LMT 39 T.				
Ton 1: 16		Ton 2: 22		Ton 3: 39		
COMMENTS:						
FIELD CATEGORY: S-17		CL OF BR AND TRK OVR 16 T 15MPH ON BR EXCPT SNGL E UNT TRKS WT LMT 22 T OTHR TRKS				
Ton 1: 16		Ton 2: 22		Ton 3: 39		PROBLEM:
COMMENTS:		PROBLEM DIRECTION:				
GENERAL COMMENTS/MAJOR RATED ITEMS						
GENERAL COMMENTS: (BOWDEJ1, 08/28/2008)--(26'-49'-26') SMP WF GDR SPANS						
[ITEM 58] DECK: 4-POOR CONDITION			COMMENTS: (DENNIB1, 11/13/2013)--EDGE DETERIORATION			
RATING : 11/07/2022						
[ITEM 59] SUPER: 5-FAIR CONDITION			COMMENTS: (DENNIB1, 11/13/2013)--TOP FLANGE RUSTING			
RATING : 05/18/2001						
[ITEM 60] SUB: 3-SERIOUS CONDITION			COMMENTS: (DENNIB1, 11/16/2015)--VERTICLE CRACKS & SPALL AT BEARING			
RATING : 11/07/2022						
[ITEM 61] BANK/CHANNEL: 6-WIDESPREAD MINOR DAMAGE			COMMENTS:			
RATING : 05/18/2001						
[ITEM 113] SCOUR: 8-STABLE FOR CALCULATED			COMMENTS:			
RATING : 05/18/2001						
EVALUATION TYPE :						
[ITEM 71] WATERWAY ADEQUACY: DECK/APPRCH OVERTOP SLIGT			COMMENTS:			
RATING : 05/18/2001						
[ITEM 72] APPRRDWY ALIGNMENT: 8-VERYGOOD			COMMENTS:			
RATING : 05/18/2001						
RAILING AND APPROACH PAVEMENT COMPONENTS AND RATINGS						
[ITEM 36A] BRIDGE RAILING RATING: DOESNT MEET CURRNT STND-0						
RATING : 01/06/2010		COMMENTS:				
<u>MATERIAL</u>		<u>CONSTRUCTION</u>		<u>DIRECTION</u>		<u>COMMENTS</u>
REINFORCED CONCRETE		CURB		BOTH		
<u>CONDITION</u>		<u>LOCATION 1</u>		<u>LOCATION 2</u>		<u>SEVERITY</u>
DETERIORATION		THROUGHOUT				MODERATE
STEEL		ANGLE-DOUBLE		BOTH		
[ITEM 36B] TRANSITION RAILING RATING: NOT PROVIDED-0						
RATING : 05/18/2001		COMMENTS:				
[ITEM 36C] APPROACH RAILING RATING: NOT PROVIDED-0						
RATING : 05/18/2001		COMMENTS:				
[ITEM 36D] RAIL END TREATMENT RATING: NOT PROVIDED-0						
RATING : 05/18/2001		COMMENTS:				
Design_No = S0607						
Page 2						
This report contains information that is protected from disclosure by federal law, 23 USC Section 409 and the Missouri Open Records Law (Sunshine Act), Section 610.021 RSMo. Please review MoDOT's policy and procedure manual on the Sunshine Act before releasing any of the information contained herein.						







		Missouri Department of Transportation				December 21, 2022	
		State Bridge Inspection Report				10:27:45AM	
COUNTY: TEXAS		DISTRICT: SE		CLASS: STATBR		FED-ID: 8779	
						BRIDGE: S0607	
<u>CONDITION</u>		<u>LOCATION 1</u>		<u>LOCATION 2</u>		<u>SEVERITY</u>	
EXPANSION BEARING		STEEL		SLIDING FLAT PLATE			
<u>CONDITION</u>		<u>LOCATION 1</u>		<u>LOCATION 2</u>		<u>SEVERITY</u>	
						<u>MEASUREMENT</u>	
						<u>COMMENT</u>	
OVER/UNDER ROUTES CLEARANCE INFORMATION							
<u>CLEARANCES OVER DECK</u>		**NOTE: Vertical clearances for permitting purposes are taken as 2 inches less than the actual field measured clearance.					
<u>VERTICAL CLEARANCE TYPE**</u>		<u>VALUE</u>		<u>DIRECTION</u>		<u>DATE</u>	
						<u>COMMENT</u>	
<u>CLEARANCES UNDER BRIDGE</u>		**NOTE: Vertical clearances for permitting purposes are taken as 2 inches less than the actual field measured clearance.					
<u>RECORD #</u>		<u>ROUTE</u>		<u># LANES</u>		<u>DIRECTION OF TRAFFIC</u>	
						<u>RIGHT LATERAL CLEARANCE</u>	
						<u>LEFT LATERAL CLEARANCE</u>	
						<u>UR-ID</u>	
<u>VERTICAL CLEARANCE TYPE**</u>		<u>VALUE</u>		<u>DIRECTION</u>		<u>DATE</u>	
						<u>COMMENT</u>	
STRUCTURE PAINT INFORMATION							
CONDITION:		FAIR		RUST AMOUNT : 7 = .2% OF SURFACE RUSTED		STEEL TONS : 17	
<u>ORIGINAL PAINT</u>				<u>CONTRACT REPAINT</u>		<u>DEPARTMENT REPAINT</u>	
PAINT TYPE :		PAINT TYPE :		PAINT TYPE : C SYSTEM		MANUFACTURE :	
NAME :		NAME :		NAME : INORGANIC ZINC/VINYL		SURFACE PREP :	
PAINT COLOR :		PAINT COLOR :		PAINT COLOR : ALUMINUM			
PAINT YEAR :		PAINT YEAR :		PAINT YEAR : 2000			
MILS :		MILS :		MILS : 8			
REQUESTED WORK ITEMS							
GENERAL WORK COMMENTS:							
<u>RESPONSIBILITY</u>		<u>LOCATION</u>		<u>ITEM</u>		<u>CATEGORY</u>	
REGIONAL		ABUTMENT-COLUMN		REPAIR COLUMN OR SHAFT		SUBSTRUCTURE	
						<u>PRIORITY</u>	
						2	
						<u>DATE</u>	
						11/28/2017	
						<u>WORK ITEM COMMENT</u>	
						(DENNIB1, 11/05/2019)--REPAIR ALL COLUMNS IN BENTS 2 & 3 & CAP @ BENT 2	
UTILITY ATTACHMENTS							
Design_No = S0607							
Page 6							
This report contains information that is protected from disclosure by federal law, 23 USC Section 409 and the Missouri Open Records Law (Sunshine Act), Section 610.021 RSMo. Please review MoDOT's policy and procedure manual on the Sunshine Act before releasing any of the information contained herein.							

		<div>Missouri Department of Transportation</div> <div>State Bridge Inspection Report</div>				<div>December 21, 2022</div> <div>10:27:45AM</div>																																																						
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<div>UTILITY</div> <div>OWNER</div> <div>METHOD</div> <div>MEASUREMENT TYPE</div> <div>VALUE</div> <div>NUMBER</div> <div>UTILITY ATTACHMENT COMMENT</div>																																																												
PROGRAM NOTES INFORMATION																																																												
<div><div>YEAR</div><div>2025</div></div> <div><div>PROJECT #</div><div>SE0028</div></div> <div><div>MONTH LET</div><div>1</div></div> <div><div>YEAR LET</div><div>2025</div></div> <div><div>ITEMS</div><div>REPLACE BRIDGE</div></div> <div><div>COMMENT</div></div>																																																												
COMPUTER GENERATED RATINGS AND DEFICIENCY ITEMS					***ADVANCED SIGN INFORMATION***																																																							
<div>NOTE: The items listed in this section are updated whenever computer edits are ran on a structure after the inspection updates have been entered in to TMS.</div> <table><tr><td><div>Rated Item</div></td><td><div>Rating</div></td><td><div>Rating Date</div></td></tr><tr><td>[Item 67] Structure Evaluation Rating:</td><td>3-BASICALLY INTOL CORRECT</td><td>11/10/2022</td></tr><tr><td>[Item 68] Deck Geometry Rating:</td><td>4-MEETS MINIMUM TOLERABLE</td><td>11/13/2019</td></tr><tr><td>[Item 69] Underclearance:</td><td>N-NOT APPLICABLE</td><td>5/18/2001</td></tr><tr><td>Sufficiency Rating:</td><td>34.4%</td><td>11/10/2022</td></tr><tr><td>Deficiency:</td><td>STRUCTURAL</td><td>11/9/2021</td></tr><tr><td>Funding Eligibility:</td><td></td><td>----</td></tr><tr><td>Estimated New Structure Length:</td><td></td><td>----</td></tr><tr><td>Estimated Structure Cost:</td><td></td><td>----</td></tr><tr><td>Estimated Total Project Cost:</td><td></td><td>----</td></tr><tr><td>Year of Cost Estimate:</td><td></td><td>----</td></tr></table> <div>NOTE: The above structure length and cost estimates are computer generated using algorithms in the TMS system. These algorithms are generalized to use NBI items to come up with a new structure length and width to calculate a new area which is taken times a representative cost per square foot. The actual structure size and cost may vary significantly from these numbers once site specific engineering is done.</div>					<div>Rated Item</div>	<div>Rating</div>	<div>Rating Date</div>	[Item 67] Structure Evaluation Rating:	3-BASICALLY INTOL CORRECT	11/10/2022	[Item 68] Deck Geometry Rating:	4-MEETS MINIMUM TOLERABLE	11/13/2019	[Item 69] Underclearance:	N-NOT APPLICABLE	5/18/2001	Sufficiency Rating:	34.4%	11/10/2022	Deficiency:	STRUCTURAL	11/9/2021	Funding Eligibility:		----	Estimated New Structure Length:		----	Estimated Structure Cost:		----	Estimated Total Project Cost:		----	Year of Cost Estimate:		----	<table><tr><td><div>SIGN #</div></td><td><div>SIGN TYPE</div></td><td><div>PROBLEM</div></td><td><div>PROBLEM DIRECTION</div></td></tr><tr><td>1</td><td>B - ONE LANE BRIDGE</td><td></td><td></td></tr><tr><td>2</td><td>YIELD TO ONCOMING TRAFFIC</td><td></td><td></td></tr></table> <div>***OUTFALL INSPECTION INFORMATION***</div> <table><tr><td><div># OUTFALLS:</div></td><td><div>INSPECTOR:</div></td></tr><tr><td><div>STATUS:</div></td><td><div>DATE:</div></td></tr><tr><td><div>NOTES:</div></td><td></td></tr></table>					<div>SIGN #</div>	<div>SIGN TYPE</div>	<div>PROBLEM</div>	<div>PROBLEM DIRECTION</div>	1	B - ONE LANE BRIDGE			2	YIELD TO ONCOMING TRAFFIC			<div># OUTFALLS:</div>	<div>INSPECTOR:</div>	<div>STATUS:</div>	<div>DATE:</div>	<div>NOTES:</div>	
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Missouri Department of Transportation
Bridge Inventory and Inspection System
Structural Inventory & Appraisal Sheet

December 21, 2022
10:21:52am

COUNTY : TEXAS BRIDGE : S0607 REVIEW STATUS : APPROVED NBI STATUS : T
RECORD TYPE : ROUTE CARRIED 'ON' STRUCT RUN DATE : 11/10/2022 SUBMITTAL YEAR : 2022

GENERAL STRUCTURE INFORMATION			ROUTE DESIGNATION INFORMATION		
1	State	MISSOURI	5A	Record Type	ROUTE CARRIED 'ON' STRUCT
2	District	SE	5B	Route Signing Prefix	MO
3	County	TEXAS	5C	Designated Level of Service	MAINLINE
8	Federal ID No.	8779	5D	Route Number	00137
27	Year Built	1932	5E	Directional Suffix	NOT APPLICABLE
106	Year Reconstructed	0	7	Facility Carried	MO 137 S
42A	Type of Service On	HIGHWAY	12	Base Hwy. Network	NO
21	Structure Maintenance	STATE HIGHWAY AGENCY	13A	LRS Inventory Route No.	
22	Structure Owner	STATE HIGHWAY AGENCY	13B	Subroute No.	
33	Br. Median Code	NO MEDIAN	20	Toll Status	ON FREE ROAD
37	Historical Significance	HISTORICAL SIGNIF UNKNWN	26	Functional Classification	07-RURAL MAJOR COLLECTOR
101	Parallel Struc Desg	NONE EXISTS	28A	Lanes on Structure	01
103	Temporary Structure	NOT TEMPORARY	100	STRAHNET Designation	RTE NOT A DEFENSE HWY
112	NBIS Bridge Length	YES	104	National Highway System	NOT ON NHS
			105	Federal Lands Highway	NOT APPLICABLE
			110	Designated Nat. Network	NO
STRUCTURE LOCATION INFORMATION			STRUCTURE TRAFFIC INFORMATION		
4	Place	OZARK	29	AADT	335
	Code	55820	30	AADT Year	2021
9	Location	S 5 T 29 N R 8 W	102	Direction of Traffic	ONE LANE BRIDGE FOR 2-WAY
11	Milepoint	20.59 miles	109	AADT Truck Percent	9%
16	Latitude	37 D 13 M 59 S	114	Future AADT	469
17	Longitude	91 D 51 M 0 S	115	Future AADT Year	2041
UNDERRECORD INFORMATION			STRUCTURE GEOMETRIC INFORMATION		
6	Features Intersected	BIG CR	10	Inventory Rte. Vert. Clear	99 Ft. 99 In.
42B	Type of Service Under	WATERWAY	19	By pass Detour Length	16.25 miles
28B	Lanes Under Structure	00	32	Approach Roadway Width	20 Ft. 0 In.
54A	Vert. Clearance Ref.	N/A	34	Skew	0.00 Degrees
54B	Vert. Clearance	0 Ft. 0 In.	35	Struct. Flared	NO
55A	Rt. Lat Clear Ref.	N/A	47	Total Horiz. Clear	20 Ft. 0 In.
55B	Rt. Lat Clearance	0 Ft. 0 In.	48	Maximum Span Length	49 Ft. 3 In.
56	Left Lat Clearance	0 Ft. 0 In.	49	Structure Length	103 Ft. 0 In.
38	Navigation Control	PERMIT NOT REQ	50A	Left Curb/Sidewalk Width	0 Ft. 0 In.
39	Nav Vertical Clear	0 Ft. 0 In.	50B	Right Curb/Sidewalk Width	0 Ft. 0 In.
40	Nav Horizontal Clear	0 Ft. 0 In.	51	Curb to Curb Br. Width	20 Ft. 0 In.
111	Nav. Pier Protection		52	Deck Width (Out-Out)	20 Ft. 12 In.
116	Nav. Cl. Vert. Clear		53	Vert. Clearance Over Deck	99 Ft. 99 In.

Design_No = S0607 and Inventory_Appraisal_Submittal_Year = 2022



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LOAD RATING AND POSTING INFORMATION			MATERIAL/CONSTRUCTION INFORMATION		
31	Design Load	H 10	43A	Main Struc. Mat type	STEEL
41	Structure Status	POSTED FOR LOAD	43B	Main struc Constr. Type	STRINGER/MULTIBEAM - GRD
63	Oper. Rating Meth.	LOAD FACTOR	45	# of Main Spans	3
64	Operating Rating	22 Tons.	44A	Appr Struc. Mat type	000
65	Inventory Rating Meth	LOAD FACTOR	44B	Appr Struc. Cnstr. type	000
66	Inventory Rating	14 Tons.	46	# of Approach Span	0
70	Bridge Posting Code	10.0-19.9% BELOW	107	Deck Mat/Constr.	1 CONCRETE CIP
PROPOSED IMPROVEMENT INFORMATION			108A	Wear Surf Mat/Constr.	6 BITUMINOUS
Sufficiency Rating 34.4 Percent			108B	Membrane Mat/Constr.	0 NONE
Deficiency Rating STRUCTURAL			108C	Deck Protect Mat/Constr.	0 NONE
Funding Eligibility FULL			CONDITION RATING INFORMATION		
75A	Proposed Work	REPLACEMENT SUBSTND LOAD	58	Deck Cond. Rating	4
75B	Work Done By	Contract	59	Superstructure Cond. Rating	5
76	New Struc Length	131 Ft. 3 In.	60	Substructure Cond. Rating	3
94	Struc Improve Cost	\$ 681,000	61	Channel /Channel Protection Cond. Rating	6
95	Roadway Improve Cost	\$ 68,000	62	Culvert Cond. Rating	N
96	Total Project Cost	\$ 1,022,000	INSPECTION INFORMATION		
97	Year of Cost Estimates	2022	90	Gen. Insp Date	11 / 22
APPRAISAL RATING INFORMATION			91	Gen. Insp. Frequency	12 Months
36A	Br. Rail App. Rating	DOES NOT MEET ACCEPT STND	92A	Frac. Critical Inspection	N Months
36B	Transition Rail App. Rating	DOES NOT MEET ACCEPT STND	93A	Frac. Critical Insp. Date	
36C	Approach Rail App. Rating	DOES NOT MEET ACCEPT STND	92B	Underwater Inspection	N Months
36D	Rail End Treat. App. Rating	DOES NOT MEET ACCEPT STND	93B	Underwater Insp. Date	
67	Struc Eval App. Rating	3	92C	Special Inspection	N Months
68	Deck Geometry App. Rating	4	93C	Special Inspection Date	
69	Underclearance App. Rating	N	BORDER BRIDGE INFORMATION		
71	Waterway Adeq. App. Rating	6	98	Neighboring State Code	
72	Approach Road App. Rating	8	98B	Neighboring State % Respon	
113	Scour Assess App. Rating	8	99	Neighboring State Struc. No.	
APPROVED POSTING INFORMATION			FIELD POSTING INFORMATION		
Approved Posting Category S-17			Field Posting Category S-17		
Ton1 Ton2 Ton3			Ton1 Ton2 Ton3		
Tonnage Values for Posting Sign 16 22 39			Tonnage Values for Posting Sign 16 22 39		
General Text for Posting Sign			General Text for Posting Sign		
CL OF BR AND TRK OVR 16 T 15MPH ON BR EXCPT SNGL E UNT TRKS WT LMT 22 T OTHR TRKS WT LMT 39 T.			CL OF BR AND TRK OVR 16 T 15MPH ON BR EXCPT SNGL E UNT TRKS WT LMT 22 T OTHR TRKS WT LMT 39 T.		

Design_No = S0607 and Inventory_Appraisal_Submittal_Year = 2022