

Engineering Surveys and Services

Consulting Engineers, Scientists, and Land Surveyors
Analytical and Materials Laboratories

1775 West Main Street
Sedalia, Missouri 65301
Telephone 660-826-8618
Facsimile 660-826-6158

E-Mail ess@ESS-Inc.com
<http://www.ESS-Inc.com>

Addendum No.: Two (02)
Project: Downtown Streetscape Phase IIIa
Federal Project No. STP 5700(509)
Date: February 18, 2014
(Page 1 of 2 with attachments)

The following changes, additions, modifications, information and clarification are hereby made to the Contract Documents dated December 27, 2013, revised February 14, 2014, and shall be incorporated in preparation of all Proposals for the Work.

A. CHANGE IN DATE OF OPENING OF BIDS

- The opening of bids and bidding due date has been rescheduled to February 25, 2014, at 2:15 p.m. in the Mayor's Conference Room at the City of Sedalia Municipal Building. The original date for the opening of bids and bidding due date was February 13, 2014, at the same time and location.

B. PLAN SHEETS

The following civil siteplan sheets have been revised:

- Sheet 1 (Cover)

The following plan sheet has been added:

- Temporary Traffic Control Plan, Sheet C1.01

C. RESPONSE TO CONTRACTOR QUESTIONS

- See attached Memorandums from Ochsner Hare & Hare, dated February 10 and 13, 2014.
- See attached response from Timberlake Engineering.
- Coal Chute dimensions: the coal chutes encountered during the structural investigation had approximate dimensions as shown on the plans, with vertical depths of approximately 4 feet.
- Non-reinforced lean concrete used to fill coal chute voids shall have a minimum 28-day strength of 1500 pounds per square inch.

D. OTHER PLAN CLARIFICATIONS

- A permit from MoDOT will need to be obtained by the contractor prior to construction for the work in MoDOT right-of-way, as on the TTC plan. The MoDOT Kansas City District permit contact is Vernon Koch: (816) 607-2190.
- Brick Pavers shall match color, pattern, size, and material of pavers used in previous phases. For Contractor reference, pavers used in previous phases were Holland Stone pavers; Red Brown, 6 centimeters, at sidewalk corridors; Red Brown, 8 centimeters, at crosswalk borders; River Red, 8 centimeters, at crosswalk herringbone pattern.
- Mill & Overlay Detail: approved overlay fabrics include PETROMAT 4598 by PROPEX, US100P by US FABRICS, MIRAFI MPV500 by TENCATE, or approved equal.
- Concrete reinforcing fibers shall meet the requirements listed in ASTM C1116.
- All paving and sidewalk concrete shall meet or exceed the properties of the mix listed on the plans and specifications.

E. BID FORM

The revised Bid Form for the project is attached. Revisions include:

- Temporary Traffic Control (TTC) has been changed from a Lump Sum item to a breakdown of the traffic control devices according to the TTC plan.
- Handrail for Sidewalk ADA Ramp has been removed. The handrail at sidewalk ramps will be provided and installed by the City of Sedalia in accordance with Addendum One.
- Pavement Striping has been included as a line item.
- The quantity for Standard Duty Concrete Pavement has been updated.
- 4-inch conduit has been included as a line item.
- In addition to Reinforced Concrete Pipe, HDPE Pipe will be considered as an alternate for storm sewer pipe.
- An alternate in which the Contractor additionally salvages and palletizes the brick pavers underlying the existing asphalt pavement for future use by the City of Sedalia.

F. JOB SPECIAL PROVISIONS (JSPs)

Work Zone Traffic Management Plan (see attached) has been revised. Planholders shall include them in their project manuals.

G. TECHNICAL SPECIFICATIONS

The revised Technical Specifications for this project are attached. Planholders shall include them in their project manuals. Revisions include the following:

- S-4 TRAFFIC CONTROL, FLAGMEN, BARRICADES AND SIGNS

DOWNTOWN STREETSCAPE PHASE IIIa

SEDALIA, PETTIS COUNTY, MISSOURI

SITE PLAN

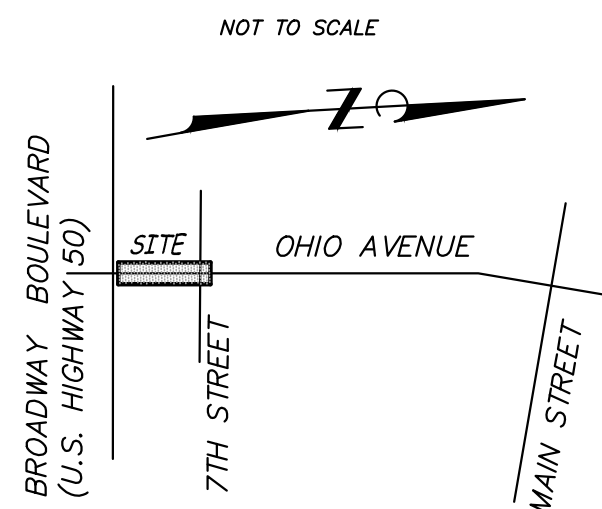
DECEMBER 27, 2013

REVISED: JANUARY 7, 2014

REVISED: FEBRUARY 14, 2014

FEDERAL PROJECT NO. STP 5700 (509)

SITE LOCATION MAP



UTILITY NOTES

THE LOCATIONS, SIZES, AND MATERIAL TYPES OF UNDERGROUND UTILITIES INDICATED ON THE PLAN, NOT VISIBLE OR APPARENT FROM THE SURFACE, ARE SHOWN IN THEIR APPROXIMATE LOCATIONS FROM A MISSOURI ONE CALL SYSTEM LOCATE, OR UTILITY COMPANY RECORDS AND WERE NOT VERIFIED IN THE FIELD.

WATER

SEDALIA WATER DEPARTMENT
PO BOX 806
SEDALIA, MISSOURI 65302
CONTACT: CHARLES BROSCH 660-826-1234
8" DI IN OHIO STREET
4" DI IN 7TH STREET
12" DI & 6" DI IN BROADWAY BOULEVARD AS SHOWN

ELECTRIC

KCP&L
921 PARKHURST DRIVE
SEDALIA, MISSOURI 65301
CONTACT: ERIC HARDING 877-729-8696
AS SHOWN

GAS

EMPIRE DISTRICT GAS COMPANY
1601 SEDALIA ROAD
SEDALIA, MISSOURI 65301
CONTACT: JIM HARGRAVE 660-827-3188
4" PLASTIC IN OHIO STREET
4" PLASTIC IN 7TH STREET

TELEPHONE

AT&T
220 E. 5TH
SEDALIA, MISSOURI 65301
CONTACT: STEPHEN HARLAN 816-325-5657
AS SHOWN

FIBER OPTIC

MISSOURI NETWORK ALLIANCE
800 NW CHIPMAN ROAD, SUITE 5750
LEE'S SUMMIT, MISSOURI 64063
CONTACT: 888-832-6662

TELEVISION

CHARTER COMMUNICATIONS
210 W. 7TH
SEDALIA, MISSOURI 65301
CONTACT: 888-438-2427
AS SHOWN

SANITARY SEWER

ALLIANCE WATER RESOURCES
901 E. THIRD STREET
SEDALIA, MISSOURI 65301
CONTACT: PHIL WEBSTER 660-827-7830
AS SHOWN

STORM SEWER

ALLIANCE WATER RESOURCES
901 E. THIRD STREET
SEDALIA, MISSOURI 65301
CONTACT: PHIL WEBSTER 660-827-7830
AS SHOWN

MoDOT

KANSAS CITY AREA
MARSHALL OFFICE
1593 WEST ARROW STREET
MARSHALL, MO 65340
CONTACT: MIKE McGRATH 660-886-2115
UNDERGROUND ELECTRIC TO SIGNALS ALONG
BROADWAY BOULEVARD STORM SEWERS ALONG
BROADWAY BOULEVARD



Call BEFORE you DIG
TOLL FREE
1-800-DIG-RITE
MISSOURI ONE-CALL SYSTEM, INC.

PROPERTY OWNER

CITY OF SEDALIA RIGHT-OF-WAY

ZONING NOTE

THIS PROPERTY IS ZONED "C-2" GENERAL BUSINESS DISTRICT

FLOOD PLAIN NOTE

THIS PROPERTY IS LOCATED IN ZONE "X" AREAS DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN, AS SHOWN BY FLOOD INSURANCE RATE MAP NUMBER 290283 0002C, DATED JANUARY 5, 1996.

OHIO AVENUE

FUNCTIONAL CLASSIFICATION= MINOR ARTERIAL
DESIGN SPEED = 20 M.P.H.
BEGIN PROJECT = STA. 0+49.1
END PROJECT = STA. 4+40.0
TOTAL PROJECT LENGTH = 390.9 FEET
R.O.W. WIDTH = 60 FEET

HATCH LEGEND

	DEMOLITION LIMITS
	NEW CONCRETE PAVEMENT
	BRICK INLAY
	NEW CONCRETE SIDEWALKS & DRIVEWAYS
	MILL AND OVERLAY EXISTING ASPHALT
	NEW CONCRETE PARKING LOT PAVEMENT
	COAL CHUTE TO BE FILLED IN

CONSTRUCTION NOTES

- THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE UTILITY COMPANIES, AND WHERE POSSIBLE MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CALL THE APPROPRIATE UTILITY COMPANIES AT LEAST 72 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATION OF UTILITIES. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RELOCATE ALL EXISTING UTILITIES WHICH CONFLICT WITH THE PROPOSED IMPROVEMENTS SHOWN ON THE PLANS.
- CONSTRUCTION SHALL COMPLY WITH ALL APPLICABLE GOVERNING CODES AND BE CONSTRUCTED TO SAME, INCLUDING APPLICABLE CITY OF SEDALIA AND/OR MISSOURI DEPARTMENT OF TRANSPORTATION SPECIFICATIONS. ALL CONTRACTORS SHALL FULLY COMPLY WITH THE PROVISIONS OF THE FEDERAL OCCUPATIONAL SAFETY AND HEALTH ACT.
- IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT ALL IDENTIFIED PROPERTY CORNERS, LAND SURVEY CORNERS, AND ACCESSORIES. THE CONTRACTOR SHALL CAUSE THE CORNERS AND ACCESSORIES TO BE REFERENCED BY A LICENSED LAND SURVEYOR, AND ANY SUCH CORNER OR ACCESSORIES DUBBED OR DESTROYED DURING CONSTRUCTION SHALL BE RESET BY THE SURVEYOR AT THE ORIGINAL LOCATION, AND FILE THE RESTORATIONS AND MONUMENT DOCUMENTS AS THE LAW REQUIRES.
- ALL TRAFFIC CONTROL DEVICES AND TEMPORARY TRAFFIC CONTROL PLANS SHALL BE IN ACCORDANCE WITH THE MOST RECENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
- CONTRACTOR SHALL SUBMIT SHOP DRAWINGS TO ENGINEERING SURVEYS AND SERVICES FOR REVIEW FOR ALL MATERIALS BEFORE ORDERING.
- ALL DIMENSIONS ARE TO BACK OF CURB, FACE OF SIDEWALK, OR EDGE OF PAVEMENT, UNLESS OTHERWISE NOTED.
- CONTRACTOR SHALL ADJUST ALL GRATES, MANHOLES, VALVE BOXES, ETC. TO MATCH FINISH GRADES, AS REQUIRED TO ASSURE A SMOOTH FIT AND CONTINUOUS GRADE.
- CONTRACTOR SHALL ASSURE POSITIVE DRAINAGE AWAY FROM ALL BUILDINGS FOR ALL LANDSCAPED AND PAVED AREAS.
- THE CONTRACTOR SHALL MAINTAIN PROPER DRAINAGE DURING CONSTRUCTION SUCH THAT STRUCTURES, BASEMENTS, AND DRIVEWAYS ARE NOT FLOODED.
- CONTRACTOR SHALL INSTALL EROSION CONTROL AS SHOWN ON PLANS.
- NO CONSTRUCTION SHALL BEGIN UNTIL ALL EROSION CONTROL MEASURES HAVE BEEN INSTALLED AND APPROVAL HAS BEEN RECEIVED FROM ALL GOVERNING AUTHORITIES.
- THE EROSION CONTROL PLAN SHALL NOT BE CONSIDERED ALL INCLUSIVE AS THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS TO PREVENT SEDIMENT FROM LEAVING THE SITE.
- ADDITIONAL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE INSTALLED IF DEEMED NECESSARY BY ON SITE INSPECTION.
- THE CONTRACTOR SHALL REMOVE ALL EXISTING PAVEMENT AND DRAINAGE STRUCTURES IN RIGHT-OF-WAY WITHIN PROJECT LIMITS UNLESS SHOWN OTHERWISE ON THE PLANS. THIS SHALL BE INCLUDED IN THE BID ITEM "REMOVAL OF IMPROVEMENTS". SOME SPECIFIC AREAS ARE HATCHED IN THE PLANS TO BETTER CLARIFY DEMOLITION LIMITS.
- TO INSURE THAT THE WORK WILL PROCEED CONTINUOUSLY THROUGH THE SUCCEEDING OPERATIONS TO ITS COMPLETION WITH THE LEAST POSSIBLE INTERFERENCE TO TRAFFIC AND INCONVENIENCE TO THE PUBLIC, THE CONTRACTOR SHALL SUBMIT TO THE ENGINEER FOR REVIEW A COMPLETE SCHEDULE OF THE PROPOSED CONSTRUCTION PROCEDURE, STATING THE SEQUENCE AND TIME FRAME IN WHICH VARIOUS OPERATIONS OF WORK ARE TO BE PERFORMED. THE INITIAL SCHEDULE SHALL BE SUBMITTED 7 DAYS PRIOR TO THE PRE-CONSTRUCTION CONFERENCE. SEE MODOT SPECIFICATIONS SECTION 108.4 FOR FORM & CONTENT REQUIREMENTS OF PROGRESS SCHEDULE. ALL CONSTRUCTION PHASING SHALL BE DONE IN ACCORDANCE WITH THE LATEST VERSION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES. THIS SCHEDULE SHALL BE UPDATED MONTHLY AND SUBMITTED TO THE ENGINEER TO CORRECT FOR ANY CHANGES IN THE SCHEDULE. CONTRACTOR SHALL ALSO SUBMIT TRAFFIC CONTROL PLANS IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES. THIS PLAN SHALL BE UPDATED TO REFLECT THE CURRENT WORK ACTIVITIES.
- THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR MAINTAINING & COORDINATING SAFE & ACCESSIBLE ROUTES (MIN. 6' WIDE) TO ALL BUSINESSES WITHIN CONSTRUCTION LIMITS. THE CONTRACTOR IS ALSO RESPONSIBLE FOR SAFETY OF ALL PERSONS & PROPERTY DURING CONSTRUCTION. THESE REQUIREMENTS APPLY CONTINUOUSLY AND ARE NOT LIMITED TO NORMAL WORKING HOURS.
- CONTRACTOR SHALL REMOVE ALL TRASH, DEBRIS, TREES & BRUSH AND OTHER MATERIAL CREATED AS A RESULT OF THE CONSTRUCTION WORK AND THE SITE SHALL BE RETURNED TO ITS ORIGINAL CONDITION.
- CONTRACTOR SHALL NOT ADVANCE TRENCH EXCAVATION BEYOND AMOUNT THAT CAN ACCOMMODATE PIPE INSTALLATION AND BACKFILLING AT THE END OF EACH DAY.
- PRIOR TO FILL PLACEMENT, IT IS RECOMMENDED THAT THE PAVEMENT SUBGRADE BE PROOF-ROLLED WITH A RUBBER-TRED PIECE OF CONSTRUCTION EQUIPMENT SUCH AS A FULLY LOADED, TANDEN-AXLE DUMP TRUCK TO HELP IDENTIFY ANY SOFT OR UNSUITABLE AREAS. AREAS IDENTIFIED AS UNSUITABLE SHOULD BE SCARIFIED, MOISTURE CONDITIONED AND RECOMPACTED TO ENGINEERED FILL SPECIFICATIONS OR OVEREXCAVATED AND RECONSTRUCTED WITH ENGINEERED FILL.
- ENGINEERED FILL SHOULD BE FREE OF FROZEN SOIL, ORGANICS, RUBBISH, LARGE ROCKS, WOOD, OR OTHER DELETERIOUS MATERIAL. COHESIVE FILLS SHOULD BE UNIFORMLY COMPACTED TO AT LEAST 95 PERCENT OF THE "STANDARD" MAXIMUM DRY DENSITY AND BE WITHIN -2 TO +4 PERCENT OF OPTIMUM MOISTURE CONTENT AS DESCRIBED BY ASTM D998. GRANULAR FILLS SHOULD BE UNIFORMLY COMPACTED TO AT LEAST 95 PERCENT OF THE "STANDARD" MAXIMUM DRY DENSITY AND SHOULD BE DRIER THAN +4 PERCENT OF OPTIMUM MOISTURE CONTENT. PLACE FILL MATERIAL IN LOOSE LIFTS NOT TO EXCEED 8 INCHES IN THICKNESS.
- FILL AND SUBGRADE CONSTRUCTION SHOULD NOT BE STARTED ON FOUNDATION SOIL, PARTIALLY COMPLETED FILL, OR SUBGRADES THAT CONTAIN FROST OR ICE. FILL SHOULD NOT BE CONSTRUCTED USING FROZEN SOIL. FROZEN SOIL SHOULD BE REMOVED PRIOR TO PLACING FILL MATERIAL.
- ALL UTILITY TRENCHES UNDER PAVEMENT SHALL BE BACKFILLED WITH COMPACTED GRANULAR FILL. ALL GRANULAR FILL WITH A MOISTURE DENSITY RELATION USED FOR TRENCH BACKFILL SHALL BE COMPACTED TO 95 PERCENT OF THE MAXIMUM DRY DENSITY AS DETERMINED BY THE STANDARD PROCTOR, ASTM D-698. THE MOISTURE CONTENT SHOULD BE HIGH ENOUGH TO PROVIDE FOR PROPER COMPACTION BUT LOW ENOUGH TO PREVENT UNDUE PUMPING.
- ALL TRENCHING, PIPE LAYING, AND BACKFILLING SHALL BE IN ACCORDANCE WITH FEDERAL OSHA REGULATIONS. BACKFILL OF TRENCHES THROUGH ANY IMPROVED AREAS, SUCH AS STREET, DRIVES OR PARKING LOTS SHALL BE COMPACTED TO MINIMUM 95% STANDARD PROCTOR DENSITY (ASTM D-698).
- FIELD DENSITY TESTS SHOULD BE CONDUCTED IN ACCORDANCE WITH ASTM D6938 (NUCLEAR METHODS) OR ASTM D 1556 (SAND CONE METHOD). FIELD DENSITY TESTS SHOULD BE PERFORMED AT THE RATE OF ONE TEST PER 2,500 SQUARE FEET PER LIFT BENEATH PAVEMENTS, SIDEWALKS, AND OTHER POTENTIAL STRUCTURAL AREAS WITH A MINIMUM OF 3 TESTS PER LIFT AND ONE TEST PER 50 LINEAL FEET PER LIFT FOR TRENCH BACKFILL. ALL COMPACTION SHALL BE OBSERVED BY THE ENGINEER. ALL MATERIALS TESTING WILL BE PAID FOR BY THE CITY OF SEDALIA.
- SIDEWALKS AND STREET CROSSINGS SHALL MEET ALL A.D.A. REQUIREMENTS AND NOT EXCEED 2% CROSS SLOPE.
- CONCRETE DRIVEWAY APRONS SHALL BE CONSTRUCTED AS PER CURRENT CITY OF SEDALIA SPECIFICATIONS. CONTRACTOR SHALL OBTAIN PERMIT FROM CITY TO WORK WITHIN STREET RIGHT-OF-WAY.
- ALL STORM SEWER PIPES AND INLETS SHALL MEET HEAVY DUTY TRAFFIC (HS20) LOADING AND BE INSTALLED ACCORDINGLY.
- CONCRETE STORM SEWER INLETS & JUNCTION BOXES SHALL BE INSTALLED PER THE CITY OF SEDALIA SPECIFICATIONS AND AS DETAILED IN THESE PLANS.
- ALL HDPE PIPE AND FITTINGS USED IN CITY R/W SHALL BE WATER TIGHT SMOOTH INTERIOR PIPE. ALL HDPE PIPE AND FITTINGS USED IN PRIVATE STORM SEWERS SHALL BE SOIL TIGHT SMOOTH INTERIOR PIPE. INSTALLATION SHALL FOLLOW THE "EMBEDMENT OF PLASTIC STORM SEWER PIPE" DETAIL.
- PVC PIPE MAY BE USED IN LIEU OF HDPE FOR DIAMETERS LESS THAN 15". PVC PIPE SHALL BE SDR 35 OR GREATER, AS REQUIRED BY DEPTH.
- ALL STRUCTURE CONNECTIONS SHALL BE WATERTIGHT.
- ALL CONCRETE STORM STRUCTURES SHALL HAVE A SMOOTH UNIFORM POURED CONCRETE INVERT FROM INVERT IN TO INVERT OUT.
- PIPE LENGTHS ARE GIVEN FROM CENTER OF STRUCTURE OR DOWNSTREAM END OF FLARED END SECTIONS.
- CONTRACTOR SHALL COORDINATE WITH ALL UTILITY COMPANIES FOR THE UTILITY INSTALLATIONS WITHIN THE PROJECT LIMITS THAT ARE UNDER SEPARATE CONTRACT.
- SUBSTANCES REGULATED BY FEDERAL LAW UNDER THE RESOURCE CONSERVATION AND RECOVERY ACT (RCRA) OR THE COMPREHENSIVE ENVIRONMENTAL RESPONSE COMPENSATION AND LIABILITY ACT (CERCLA) WHICH ARE TRANSPORTED, STORED OR USED FOR MAINTENANCE, CLEANING OR REPAIRS SHALL BE MANAGED ACCORDING TO THE PROVISIONS OF RCRA AND CERCLA.
- ALL PAINTS, SOLVENTS, PETROLEUM PRODUCTS AND PETROLEUM WASTE PRODUCTS (EXCEPT FUELS) AND STORAGE CONTAINERS (SUCH AS DRUMS, CANS OR CARTONS) SHALL BE STORED SUCH THAT THESE MATERIALS ARE NOT EXPOSED TO STORM WATER. SUFFICIENT PRACTICES OF SPILL PREVENTION, CONTROL AND/OR MANAGEMENT SHALL BE PROVIDED TO PREVENT ANY SPILLS OF THESE POLLUTANTS FROM ENTERING A WATER OF THE STATE. ANY CONTAINMENT SYSTEM USED TO IMPLEMENT THIS REQUIREMENT SHALL BE CONSTRUCTED OF MATERIALS COMPATIBLE WITH THE SUBSTANCES CONTAINED AND SHALL ALSO PREVENT THE CONTAMINATION OF GROUNDWATER.
- THE APPLICANT SHALL NOTIFY BY TELEPHONE AND IN WRITING THE DEPARTMENT OF NATURAL RESOURCES, WATER POLLUTION CONTROL PROGRAM, POST OFFICE BOX 176, JEFFERSON CITY, MO 65102, 1-800-361-4827, OF ANY OIL SPILLS OR IF HAZARDOUS SUBSTANCES ARE FOUND DURING THE PROSECUTION OF WORK UNDER THIS PERMIT.
- THE CONTRACTOR SHALL COORDINATE WITH THE CITY OF SEDALIA FOR ALLOWED STAGING AREAS.
- THE CONTRACTOR SHALL PROVIDE CONSTRUCTION STAKING.
- IF A CONFLICT EXISTS FOR THE STANDARDS AND SPECIFICATIONS BETWEEN THE CIVIL PLANS AND THAT OF THE CITY AND/OR MoDOT, THE MORE STRINGENT SHALL GOVERN.

LEGEND

E	PROPERTY LINE
T	ELECTRIC LINE
TV	TELEPHONE LINE
FO	FIBER OPTIC LINE
UE	UNDERGROUND ELECTRIC LINE
UT	UNDERGROUND TELEPHONE LINE
S	SANITARY SEWER LINE
SS	STORM SEWER LINE
G	GAS LINE
W	WATER LINE
X	FENCE
- - - - -	EXISTING CONTOUR
+	ANCHOR
⊕	IRON
△ CP#	CONTROL POINT
DI	DUCTILE IRON PIPE
FH	FIRE HYDRANT
GM	GAS METER
GV	GAS VALVE
LS	LIGHT STANDARD
PVC	POLYVINYL CHLORIDE PIPE
RCP	REINFORCED CONCRETE PIPE
RD	ROOF DRAIN
TW	TOP OF WALL
UP	UTILITY POLE
URD	UNDERGROUND ROOF DRAIN
VCP	VITRIFIED CLAY PIPE
WM	WATER METER
WV	WATER VALVE
---	DOORWAY
---	SILT FENCE
750	FINISH CONTOUR
800	TOP OF CURB ELEVATION
49.5	TOP OF PAVEMENT ELEVATION
49.5	FINISH GRADE ELEVATION
SS	PROPOSED STORM SEWER
RW STA:	ROADWAY STATIONING
PIPE STA:	STORM SEWER PIPE STATIONING
LS	LIGHT STANDARD (SEE LIGHTING PLANS)

BENCH MARK

BM - MISSOURI GEOGRAPHIC REFERENCE SYSTEM MONUMENT PE-28 LOCATED 38 FEET WEST OF U.S. HIGHWAY 65, AND 200 FEET NORTH OF LIBERTY PARK BOULEVARD. ELEVATION = 839.89
TBM - NORTHEAST TOP FLANGE BOLT OF THE FIRE HYDRANT LOCATED AT THE NORTHWEST CORNER OF THE INTERSECTION OF OHIO STREET AND 7TH STREET. ELEVATION = 907.03

SURVEY CONTROL POINTS

POINT #	NORTH	EAST	ELEVATION	DESCRIPTION
CP50	1046464.11	1432599.85	904.78	MAG NAIL
CP51	1046172.46	1432526.67	907.41	MAG NAIL
CP52	1046584.94	1432547.88	905.29	DRILL HOLE
CP53	1046725.66	1432555.28	905.34	DRILL HOLE
CP54	1046703.68	1432894.70	911.10	DRILL HOLE
CP55	1046388.67	1432877.11	909.46	MAG NAIL
CP56	1046283.86	1432590.01	905.91	DRILL HOLE
CP139	1046022.61	1432687.05	911.57	DRILL HOLE

APPROVED BY THE CITY OF SEDALIA:

BILL BECK
PUBLIC WORKS DIRECTOR

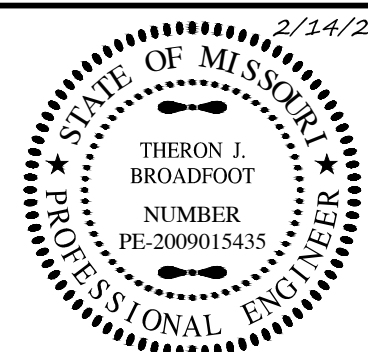
SHEET INDEX

1	COVER
--	TOPOGRAPHIC SURVEY (SHEETS 1-2)
--	RIGHT OF WAY PLAN (SHEET 1 OF 1)
5	DEMOLITION PLAN
6	DEMOLITION PLAN
7	OVERALL SITE, STRIPING, & SIGNAGE PLAN
8	OHIO AVE. - STREET PLAN & PROFILE & EROSION CONTROL PLAN
9	OHIO AVE. - STREET PLAN & PROFILE & EROSION CONTROL PLAN
10	7TH STREET - STREET PLAN & PROFILE
11	LAYOUT PLAN
12	LAYOUT PLAN
13	STORM SEWER PROFILES
14	DETAILS
15	DETAILS
16	DETAILS
17	DRAINAGE AREA MAP
C1.01	TEMPORARY TRAFFIC CONTROL PLAN
--	GATEWAY PLANS (SHEETS 1-10)
--	LIGHTING PLANS (SHEETS 1-2)

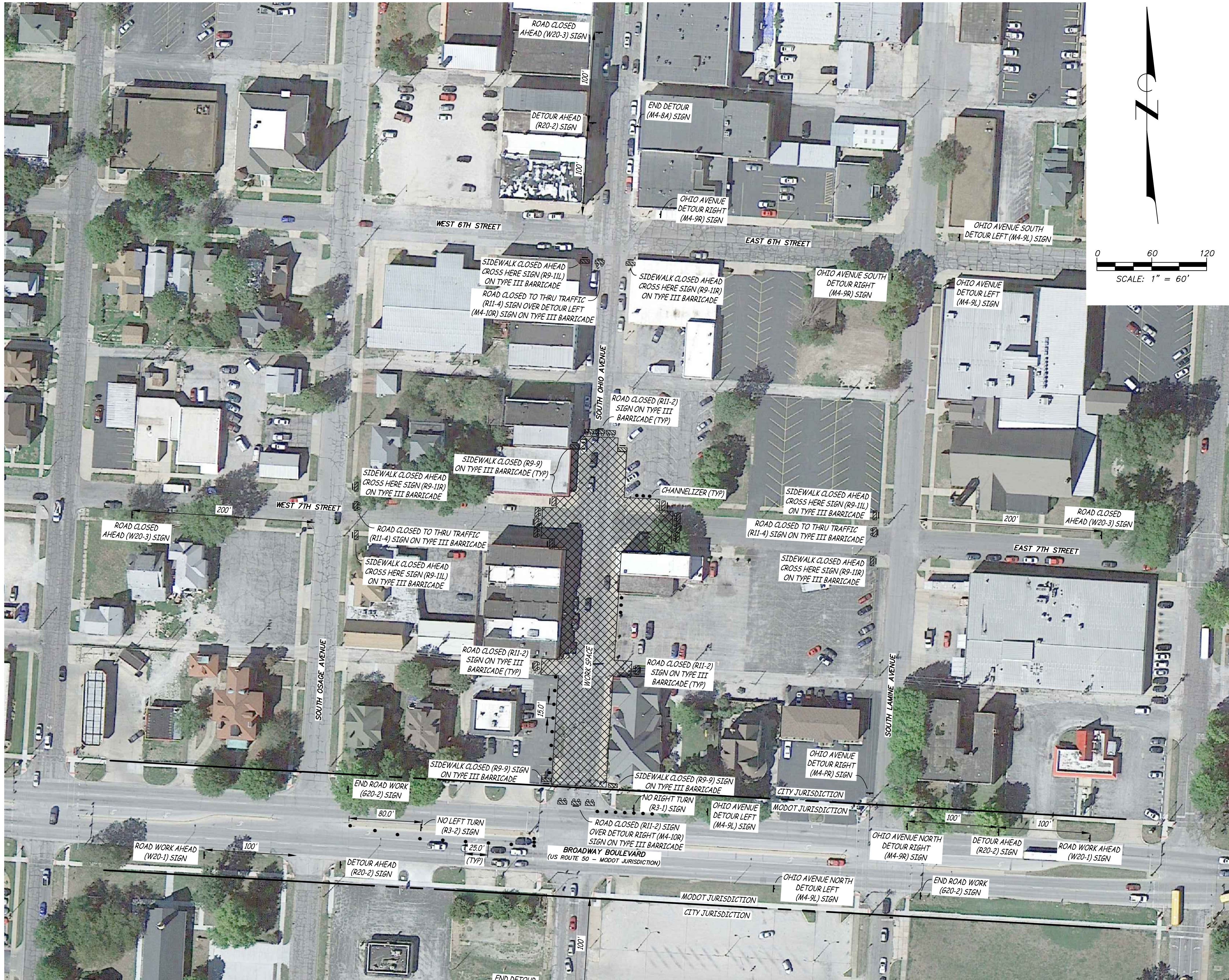
DOWNTOWN STREETSCAPE PHASE IIIa

SHEET 1 OF 17

THIS DOCUMENT WAS INTENDED TO BE DIGITALLY SIGNED. IF SIGNATURE AND VALIDATION ARE NOT PRESENT THIS MEDIA SHOULD NOT BE CONSIDERED A CERTIFIED DOCUMENT.



Theron J. Broadfoot
PROFESSIONAL ENGINEER
PE-2009015435



LEGEND	
	RIGHT-OF-WAY LINE
	TYPE III BARRICADE
	WORK SPACE
	CHANNELIZER

DESIGN DATA

DESIGN SPEED (SOUTH OHIO AVENUE) = 20 MPH
(BROADWAY BOULEVARD) = 35 MPH



NOTE: STREET NAME(S) AS SHOWN PER PLAN

M4-9L
"DETOUR" W/
LEFT ARROW SIGN

NOT TO SCALE



NOTE: STREET NAME(S) AS SHOWN PER PLAN

M4-9R
"DETOUR" W/
RIGHT ARROW SIGN

NOT TO SCALE



G20-2
"END ROAD WORK" SIGN

NOT TO SCALE



M4-8A
"END DETOUR" SIGN

NOT TO SCALE



M4-10R
"DETOUR RIGHT
ARROW" SIGN

NOT TO SCALE



M4-10L
"DETOUR LEFT
ARROW" SIGN

NOT TO SCALE



R3-1
"NO RIGHT TURN" SIGN

NOT TO SCALE



R3-2
"NO LEFT TURN" SIGN

NOT TO SCALE



R9-9
"SIDEWALK CLOSED" SIGN

NOT TO SCALE



R9-11aL
"SIDEWALK CLOSED
AHEAD" SIGN

NOT TO SCALE



R9-11aR
"SIDEWALK CLOSED
AHEAD" SIGN

NOT TO SCALE



R11-2
"ROAD CLOSED" SIGN

NOT TO SCALE



R11-4
"ROAD CLOSED TO
THRU TRAFFIC" SIGN

NOT TO SCALE



R20-2
"DETOUR AHEAD" SIGN

NOT TO SCALE



W20-1
"ROAD WORK AHEAD" SIGN

NOT TO SCALE

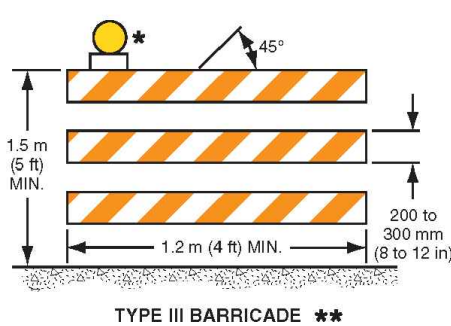


W20-3
"ROAD CLOSED AHEAD" SIGN

NOT TO SCALE

TEMPORARY TRAFFIC GENERAL NOTES

- ALL TRAFFIC CONTROL DEVICES AND TEMPORARY TRAFFIC CONTROL PLANS SHALL BE IN ACCORDANCE WITH THE MOST RECENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
- THE CONTRACTOR IS SOLELY RESPONSIBLE FOR JOB SITE, WORKER, AND TRAFFIC SAFETY.
- CONTRACTOR MAY MODIFY THE TEMPORARY TRAFFIC CONTROL PLAN TO ACCOMMODATE THEIR CONSTRUCTION MEANS, METHODS, SEQUENCE, AND TECHNIQUES OR PROCEDURES.
 - CONTRACTOR SHALL SUBMIT ALL PROPOSED MODIFICATIONS TO THE TEMPORARY TRAFFIC CONTROL PLAN THAT AFFECT TRAFFIC CONTROL DEVICES ON THE U.S. ROUTE 50 RIGHT-OF-WAY TO MODOT FOR MODOT'S APPROVAL, IN WRITING, PRIOR TO IMPLEMENTATION.
 - CONTRACTOR SHALL SUBMIT ALL PROPOSED MODIFICATIONS TO THE TEMPORARY TRAFFIC CONTROL PLAN THAT AFFECT TRAFFIC CONTROL DEVICES OUTSIDE THE U.S. ROUTE 50 RIGHT-OF-WAY TO THE CITY OF SEDALIA FOR THE CITY'S APPROVAL, IN WRITING, PRIOR TO IMPLEMENTATION.
- CONTRACTOR SHALL DESIGNATE A TRAINED PERSON AT THE PROJECT LEVEL WHO HAS THE PRIMARY RESPONSIBILITY, WITH SUFFICIENT AUTHORITY, FOR IMPLEMENTING THE TEMPORARY TRAFFIC CONTROL PLAN AND OTHER SAFETY AND MOBILITY ASPECTS OF THE PROJECT. CONTRACTOR SHALL SUBMIT PROOF THAT THEIR TRAINED PERSON HAS SUCCESSFULLY PASSED MODOT'S ADVANCED WORK ZONE COURSE OR APPROVED EQUIVALENT TRAINING.
- CONTRACTOR SHALL MONITOR TRAFFIC FLOW THROUGH THE WORK ZONE AND MAKE ADJUSTMENTS AS REQUIRED AND WITH THE APPROVAL OF THE AUTHORITY HAVING JURISDICTION TO THE TRAFFIC CONTROL PLAN.



TYPE III BARRICADE **

* WARNING LIGHTS
** RAIL STRIPE WIDTHS SHALL BE 150 MM (6 IN.), EXCEPT THAT 100 MM (4 IN.) WIDE STRIPES MAY BE USED IF RAIL LENGTHS ARE LESS THAN 900 MM (36 IN.). THE SIDES OF BARRICADES FACING TRAFFIC SHALL HAVE RETROREFLECTIVE RAIL FACES.

TYPE III BARRICADE

NOT TO SCALE

THIS DOCUMENT WAS INTENDED TO BE DIGITALLY SIGNED. IF SIGNATURE AND VALIDATION ARE NOT PRESENT THIS MEDIA SHOULD NOT BE CONSIDERED A CERTIFIED DOCUMENT.

TEMPORARY TRAFFIC CONTROL PLAN

DOWNTOWN STREETScape PH III A

SEDALIA, MISSOURI

Design: TJB

Drawn: BR

Checked: TJB

Scale

1" = 60'

Date

FEBRUARY 14, 2014

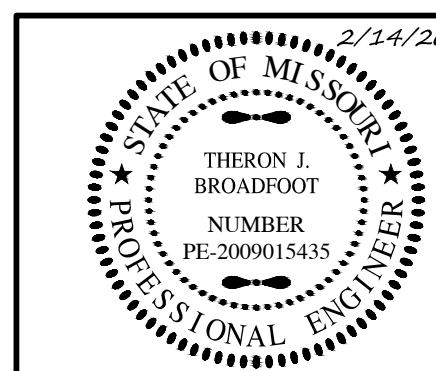
Job

3557

Revised

Sheet

C1.01



Theron J. Broadfoot
PROFESSIONAL ENGINEER
PE-2009015435

Attn.: Abby McMullin
Engineering Surveys and Services
1775 West Main Street
Sedalia, MO 65301

From: Korey P. Schulz, RLA, ASLA

Date: 02/10/2014
Project: Sedalia Downtown Streetscape Phase IIIa
Project No.: 13-027

The following are the responses to the questions that you forwarded from the Sedalia Downtown Streetscape Phase IIIa pre-bid meeting.

Please let me know if you have any questions.

1. Please explain the procedure of the Gateway mock up.
There are two mock-ups referred to in the downtown gateway construction documents. The light mock up as stated on sheet L401 note #6 and the masonry mock up as stated on sheet L404 Section 1.4C.
 - *Light mock up – The light mock up shall consist of providing one (1) of the column uplights and one (1) of the striplight uplighting fixtures as specified for review with the fabricated steel structure, prior to being delivered to the site. Lights shall have temporary power provided to them in the proposed location for review of lighting affect. After review and approval the remaining column uplights and striplight uplighting fixtures shall be ordered. The ingrade uplighting does not have to be mocked up. All submittal reviews shall be completed prior to mock up.*
 - *Masonry mock up – The masonry mock up shall consist of building a sample panel of the column showing the different materials, brick veneer courses, joints and construction methods. The sample panel can be one of the columns in its final location, but the contractor will be liable for any costs for items not approved that will need to be removed and reconstructed. All submittal reviews shall be completed prior to mock up.*
2. Is there a detail for the Gateway arch base plate?
The steel base plate shall be provided per detail 1/L900. The steel fabricator and electrical contractor shall coordinate required penetrations for light fixtures and conduits. The steel shop drawings shall detail the fabrication of the steel base plate that has been coordinated with all contractors.
3. What is “receptacle 2” at the top of the Gateway referring to?
The “receptacle 2” refers to the two electrical receptacles that are shown at the top of the columns recessed into the steel tubing. Contractors shall reference detail 1/L401 and electrical plans sheet MEP2.0.

4. Do we need to include remote access to control the Gateway lighting?

There is not remote access to the control. The control shall be per the plans, located within the secured access panel.

Signed: Korey P. Schulz, RLA, ASLA

Ochsner Hare & Hare, L.L.C.

Copy to: OHH files 13-027

Attn.: Abby McMullin
Engineering Surveys and Services
1775 West Main Street
Sedalia, MO 65301
From: Korey P. Schulz, RLA, ASLA

Date: 02/13/2014
Project: Sedalia Downtown Streetscape Phase IIIa
Project No.: 13-027

The following are the responses to the additional questions that you forwarded from the Sedalia Downtown Streetscape Phase IIIa contractors.

Please let me know if you have any questions.

1. The plan calls for the steel letters to be 1" thick material. The letters may possibly weigh anywhere around 65-85 #'s each pending the letter, that's extremely heavy. Is there a reason that they have the letters so heavy?
 - *The letters are to be 1" thick, but they are not intended to be solid steel material. The letters should have a front and back face with full depth sides matching the font as shown in the plans. Channel letters are not acceptable.*
2. Are the "pin mounts" to be removable or welded to the back side of the letter? The pin mount design will need to accommodate the heavy letters possibly making the stand-offs need to be more than one inch if they are to be removable or to allow for enough room to paint in between the letter and the arch frame.
 - *The letters are to be welded to the back side of the letter*
3. Could there be a change in size of thickness for the letters?
 - *The thickness of the letters should not be changed.*

Signed: Korey P. Schulz, RLA, ASLA

Ochsner Hare & Hare, L.L.C.

Copy to: OHH files 13-027



Sedalia Streetscape Addendum #1

1. *Is there a separate controller and meter for the light standards from the Gateway feature?*
No the pole lights are controlled by a photocell mounted on the pole.
2. *Should there be a riser on the utility pole in the alley?*
Refer to electric riser on sheet MEP2
3. *Who is furnishing the anchor bolts for the street lights?*
These are supplied and installed by contractor. This also applies to the pull boxes.
4. *What is "receptacle 2" referring to on top of the Gateway?*
This is one of two receptacles. There is another outlet located on the other side. See gateway signage drawings.

BID FOR UNIT PRICE CONTRACTS (CONTINUED)

Bidder agrees to perform all the site work described in the specifications and shown on the plans, for the following unit price:

General

	description	quantity	unit	unit price	extension
1	Mobilization & Bonds	1	LS		
2	Traffic Control	1	LS		
3	Demolition	1	LS		
4	Site Layout (Construction Staking)	1	LS		
5	Remove and Replace Unsuitable Soil	300	CY		
6	Proof-roll Subgrade Parking	86	SY		
Subtotal:					

Sediment/Erosion Control

	description	quantity	unit	unit price	extension
7	Construction Entrance/Exit	1	EA		
8	Concrete Washout (including maintenance)	1	EA		
9	Inlet Protection	9	EA		
Subtotal:					

Storm Water Improvements

	description	quantity	unit	unit price	extension
10	4" HDPE (roof drain)	27	LF		
11	8" HDPE (roof drain)	94	LF		
12	15" RCP	289	LF		
13	18" RCP	86	LF		
14	24" RCP	34	LF		
15	Curb Inlet Type A 4'X3'	2	EA		
16	Single Combination Curb Inlet 3'X2'	2	EA		
17	Double Combination Curb Inlet 6'X2'	4	EA		
18	Junction Box 3'X3'	1	EA		
Subtotal:					

Pavement

	description	quantity	unit	unit price	extension
19	Standard Duty Concrete (including base)	100	SY		
20	Concrete Coal Chutes Backfill	1	LS		
21	Brick Pavers In Street Pavement	203	SY		
22	Brick Pavers Inlay Sidewalk Behind Curb	110	SY		
23	Subgrade Fabric	1,525	SY		

24	8" Fiber Reinforced Concrete Pavement (including base)	1,525	SY		
25	Asphalt Mill & Overlay	236	SY		
26	Concrete Sidewalk (width varies)	620	SY		
27	Sidewalk ADA Ramp (Brdwy Blvd & Ohio Ave. intersection)	4	EA		
28	Sidewalk ADA Ramp (pavestone ADA paver detect. warning)	8	EA		
29	24" Curb & Gutter	821	LF		
30	Concrete Drive Apron	91	SY		
31	6" White Solid Line Crosswalk	96	LF		
32	Roadway Marking 4" Double Solid Yellow	57	LF		
33	Roadway Marking 4" Single Solid White	72	LF		
34	24" Stop Bar	6	EA		
35	Painted Directional Arrow	2	EA		
36	R1-1 "STOP" Sign (incl. installation with decorative pole)	4	EA		
				Subtotal:	

Temporary Traffic Control

	description	quantity	unit	unit price	extension
37	Type III Barricade	29	EA		
38	Channelizer	32	EA		
39	Road Work Ahead Sign (W20-1)	2	EA		
40	Detour Ahead Sign (R20-2)	4	EA		
41	End Road Work Sign (G20-2)	2	EA		
42	Ohio Ave North Detour Sign (M4-9R)	4	EA		
43	Ohio Ave North Detour Sign (M4-9L)	4	EA		
44	Detour Left Sign (M4-10L)	1	EA		
45	Detour Right Sign (M4-10R)	1	EA		
46	End Detour Sign (M4-8A)	2	EA		
47	Road Closed Ahead Sign (W20-3)	4	EA		
48	Sidewalk Closed Ahead Sign (R9-11R)	3	EA		
49	Sidewalk Closed Ahead Sign (R9-11L)	3	EA		
50	Sidewalk Closed Sign (R9-9)	8	EA		
51	No Right Turn Sign (R3-1)	1	EA		
52	No Left Turn Sign (R3-2)	1	EA		
53	Road Closed To Thru Traffic Sign (R11-4)	3	EA		
54	Road Closed Sign (R11-2)	4	EA		
				Subtotal:	

Alternate - HDPE in Lieu of RCP

	description	quantity	unit	unit price	extension
55	15" HDPE	289	LF		
56	18" HDPE	86	LF		
57	24" HDPE	34	LF		
Subtotal:					

Alternate - Salvage and Palletize Brick Pavers**Underlying Existing Asphalt Pavement**

	description	quantity	unit	unit price	extension
58	Salvage & Palletize Brick Pavers	1	LS		
Subtotal:					

Gateway

	description	quantity	unit	unit price	extension
59	Gateway	1	LS		
Subtotal:					

Lighting

	description	quantity	unit	unit price	extension
60	Street Light Poles and Bases – Install Only (provided by KCP&L)	10	EA		
61	Pull Boxes (18"X24")	6	EA		
62	2" Conduit	596	LF		
63	3" Conduit	824	LF		
64	4" Conduit	412	LF		
Subtotal:					

Total General:	
Total Sediment/Erosion Control:	
Total Storm Water Improvements:	
Total Pavement:	
Total Temporary Traffic Control:	
Total Alternate - HDPE in Lieu of RCP:	
Total Alternate - Salvage and Palletize Brick Pavers:	
Total Gateway:	
Total Lighting:	
GRAND TOTAL	

WORK ZONE TRAFFIC MANAGEMENT PLAN JSP-02-06A

1.0 Description. Work zone traffic management shall be in accordance with applicable portions of Division 100 and Division 600 of the Standard Specifications, and specifically as follows.

2.0 Traffic Management Schedule.

2.1 Traffic management schedules shall be submitted to the engineer for review prior to the start of work and prior to any revisions to the traffic management schedule. The traffic management schedule shall include the proposed traffic control measures, hours traffic control will be in place, and work hours.

2.2 The contractor shall notify the engineer prior to lane closures or shifting traffic onto detours.

2.3 The engineer shall be notified as soon as practical of any postponement due to weather, material or other circumstances.

2.4 In order to ensure minimal traffic interference, the contractor shall schedule lane closures for the absolute minimum amount of time required to complete the work. Lanes shall not be closed until material is available for continuous construction and the contractor is prepared to diligently pursue the work until the closed lane is opened to traffic.

2.5 Traffic Congestion. The contractor shall, upon approval of the engineer, take proactive measures to reduce traffic congestion in the work zone.

2.5.1 Traffic Delay. The contractor shall be responsible for maintaining the existing traffic flow through the job site during construction. If disruption of the traffic flow occurs and traffic is backed up in queues of 15 minute delays or longer, then the contractor shall review the construction operations which contributed directly to disruption of the traffic flow and make adjustments to the operations to prevent the queues from occurring again.

2.5.2 Traffic Safety.

2.5.2.1 Where traffic queues routinely extend to within 1000 feet (300 m) of the ROAD WORK AHEAD, or similar, sign on a divided highway or to within 500 feet (150 m) of the ROAD WORK AHEAD, or similar, sign on an undivided highway, the contractor shall extend the advance warning area, as approved by the engineer.

2.5.2.2 When a traffic queue extends to within 1000 feet (300 m) of the ROAD WORK AHEAD, or similar, sign on a divided highway or to within 500 feet (150 m) of the ROAD WORK AHEAD, or similar, sign on an undivided highway due to non-recurring congestion, the contractor shall deploy a means of providing advance warning of the traffic congestion, as approved by the engineer. The warning location shall be no less than 1000 feet (300 m) and no more than 0.5 mile (0.8 km) in advance of the end of the traffic queue on divided highways and no less than 500 feet (150 m) and no more than 0.5 mile (0.8 km) in advance of the end of the traffic queue on undivided highways.

3.0 Work Hour Restrictions.

3.1 There are three major summer holiday periods: Memorial Day, Independence Day, and Labor Day. All lanes shall be scheduled to be open to traffic during these holiday periods, from 12:00 noon on the last working day preceding the holiday until 9:00 a.m. on the first working day subsequent to the holiday.

3.2 The contractor shall not perform any construction operation on the roadway, roadbed or active lanes, including the hauling of material within the project limits, during restricted periods, holiday periods or other special events specified in the contract documents.

3.3 Working hours restrictions will be in effect from 7:00 p.m. to 7:00 a.m.

3.4 Deleted.

4.0 Detours and Lane Closures.

4.1 Deleted.

4.2 At least one lane of traffic in each direction on State Highway 50 shall be maintained at all times except for brief intervals of time required when the movement of the contractor's equipment will seriously hinder the safe movement of traffic. Periods during which the contractor will be allowed to halt traffic will be designated by the engineer.

5.0 Basis of Payment. No direct payment will be made to the contractor to recover the cost of equipment, labor, materials or time required to fulfill the above provisions, unless specified elsewhere in the contract document.

S-4 TRAFFIC CONTROL, FLAGMEN, BARRICADES AND SIGNS

The Contractor shall provide temporary traffic control in accordance with the Temporary Traffic Control Plan (Sheet C1.01) and Temporary Traffic Control Plan Notes. All temporary traffic control shall comply with the requirements of MoDOT and the current Manual on Uniform Traffic Control Devices.

All trenching, excavation and other construction work shall be made in a manner to cause the least interruption to traffic. No separate payment will be made for maintenance of traffic. All cost pertaining thereto shall be included in the contract unit prices for items as listed in the Bid Form. No separate payment will be made for the maintenance of traffic control devices.