

July 28, 2015

To: Plan Holders for Improvements to the
Rosecrans Memorial Aiport
St. Joseph, Missouri
MoDOT Project No. 13-012A-2
Air National Guard Project No. ULYB132006

Transmitted herewith is Addendum **No. 4** to the Issued for Bid Contract Documents, Specifications and Plans dated June 23, 2015 for Improvements to the Rosecrans Memorial Airport.

Schedule I: Replacement of Assault Strip Runway 13/31



Sincerely,

Jviation, Inc.

Ryan B. Lorton, P.E.
Project Manager

**ADDENDUM NO. 4
TO
CONTRACT DOCUMENTS, SPECIFICATIONS AND PLANS
FOR IMPROVEMENTS TO THE
ROSECRANS MEMORIAL AIRPORT
ST. JOSEPH, MISSOURI
MODOT PROJECT NO. 13-012A-2
AIR NATIONAL GUARD PROJECT NO. ULYB132006**

Bidders are informed that the above referenced Contract Documents, Specifications and Plans are modified as follows as of July 28, 2015:

1. CONTRACT DOCUMENTS

Section: Notice to Bidders
Subsection: Page 1-6
Revision: Add the following section at the end of the Notice to Bidders:

“Alternate Bid

In addition to the Schedule I bid, the Contractor shall include a lump sum additive or deductive cost to complete the work for the following alternate:

Alternate 1: Bond Breaker Fabric in Lieu of Choke Stone

For this alternate, the Contractor shall provide a lump sum add or deduct to the Schedule I bid amount to install Bond Breaker Fabric per the requirements of P-314 in lieu of the Choke Stone bond breaker as described in P-CS. The Bond Breaker Fabric as well as the Choke Stone are to be considered incidental the construction of the 6” Cement Treated Permeable Base Course as described in the Technical Specifications.”

July 28, 2015

To: Contract Documents,
Specifications, and Plans

MoDOT Project No. 13-012A-2

Air National Guard Project No. ULYB132006

Dated: June 23, 2015

Section: Section 80
 Subsection: 80-08 Failure to Complete on Time.
 Revision: Delete the Liquidated Damages Table in its entirety and replace with the following:

SCHEDULE I	LIQUIDATED DAMAGES COST	ALLOWED CONSTRUCTION TIME
Schedule I – Stage 1A	\$1,500 per calendar days(s)	120 calendar days
Schedule I – Stage 1B	\$1,500 per calendar days(s)	40 calendar days
Schedule I – Stage 2	\$1,500 if Runway 17/35 is not opened by 6:00 a.m. each day during Stage 2 and an additional \$1,500 for each hour that Runway 17/35 remains closed after 6:00 a.m. each day during Stage 2.	14 calendar days

The maximum construction time for the overall project is 120 calendar days. Work in Stage 1B and Stage 2 shall be performed concurrently with the work in Stage 1A.

Section: Part C Local Provisions
 Subsection: 20. Grade Control and Surface Tolerance
 Revision: Delete the first sentence in its entirety and replace with the following:
 “The Contractor will be required to provide a survey crew on site as necessary during the work to assure compliance with Section 100 of the General Provisions and to provide the following at a minimum.”

Section: Bid Proposal Form
 Subsection: Bid Proposal Summary
 Revision: Delete this page in its entirety and replace with the attached revised Bid Proposal Summary that includes Alternate No. 1 – Add/Deduct Fabric (P-314) in Lieu of Choke Stone (P-CS)

2. TECHNICAL SPECIFICATIONS

32 11 33: PERMEABLE CEMENT STABILIZED BASE COURSE AT AIRFIELDS AND ROADS

Page: Section 32 11 33 Page 10

Revision: 3.4.2 Choke Stone Bond Breaker

The following sentence has been added to the end of this section:

“In lieu of the Choke Stone Bond Breaker, a Bond Breaker Fabric meeting the requirements of P-314 provides an alternative to the use of Choke Stone as described in Alternate Bid No. 1. ”

32 11 33: PERMEABLE CEMENT STABILIZED BASE COURSE AT AIRFIELDS AND ROADS

Page: Section 32 11 33 Page 13

Revision: 3.8.2 Density Testing

Section has been deleted in its entirety and replaced with the following:

“Within each subplot in the field, one in-place density test shall be performed in accordance with ASTM D6938. The location of the test shall be randomly selected per ASTM D3665. The in-place density for each subplot comprising the lot shall be averaged and compared with the corresponding average lot density. Acceptance criteria for CTB density are provided in paragraph 3.8.8. All testing shall be done by the Engineer’s testing firm for acceptance determination.”

P-314: BOND BREAKER FABRIC

Page: All

Revision: Add Item P-314 Bond Breaker Fabric technical specification. This specification has been added based on Alternate No. 1 as described in this addendum.

See attached P-314 Bond Breaker Fabric technical specification.

MoDOT-609: ROCK DITCH LINER

Page: MoDOT-609-1

Revision: Section 609.62 Material

First sentence of the first paragraph has been deleted and replaced with the following:

“The material for rock lining shall consist of a predominantly one-sized, durable stone or shot rock. Broken concrete will not be allowed.”

3. PLAN SET

Sheet: G008
Title: Construction Safety Drawing – Schedule I, Stage 2
Revision: AIRPORT OPERATIONAL NOTES
Delete Note No. 2 in its entirety and replace with the following revised Note No. 2:
“CONTRACTOR SHALL RE-ESTABLISH THE RUNWAY SAFETY AREA PRIOR TO THE OPENING OF RUNWAY 17/35. THE CONTRACTOR WILL INCUR A \$1,500 LIQUIDATED DAMAGE IF RUNWAY 17/35 IS NOT OPENED BY 6:00 A.M. AND AN ADDITIONAL \$1,500 FOR EACH HOUR THAT RUNWAY 17/35 REMAINS CLOSED AFTER 6:00 A.M. THE REMOVAL OF THE LIGHTED “X”s AND BARRICADES SHALL BE COMPLETED PRIOR TO REOPENING THE RUNWAY.”
See attached revised sheet G008.

Sheet: C350
Title: Storm, Underdrain and Erosion Control Details
Revision: DETAIL “PAVEMENT UNDERDRAIN DETAIL”
Delete the overbuild call-off note in its entirety and replace with the following revised note:
“7” OVERBUILD OF 6” AGGREGATE BASE COURSE FOR PAVEMENT SECTION.”
See attached revised sheet C350.

Sheet: C601
Title: Proposed Typical Sections
Revision: Revised the Bond Breaker call-offs on Typical Sections E and F to include:
“Alternate Bid No. 1: Proposed Bond Breaker Fabric (P-314)”
See attached revised sheet C601.

4. QUESTIONS

1. Will fabric be considered in lieu of the choke stone to serve as the bond breaker between the permeable cement stabilized base course and the PCC?

Answer: An Alternate Bid No. 1 for the use of fabric in lieu of choke stone has been added per this Addendum No. 4.

2. Under Part C Local Provisions Subsection 20. Grade Control and Surface Tolerance, is it the intent to have a survey crew on site at all times?

Answer: A survey crew shall be on-site as needed to meet the requirements of the Contract Documents. See the revised verbiage for Subsection 20 per this Addendum No. 4.

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3. What are the aggregate specifications for the Permeable Cement Stabilized Base Course? The specifications provide a gradation, PI, & LL, but no other quality specs? Can this be recycled crushed concrete?

Answer: The aggregates must meet the requirements of Specification 32 11 33 Permeable Cement Stabilized Base Course at Airfields and Roads. The use of recycled crushed concrete is not allowed.

4. The specifications for the Permeable Base also include a sliding scale for payment based on Average Dry Density. Density testing on a drainable base is highly variable and not very repeatable. This spec. looks like it was also to be used for a dense CTB. Should this density scale be used for the permeable base?

Answer: It is the intent of the specification to utilize the Sliding Pay Scale Factors for density as shown on page 15 of the Specification 32 11 33 Permeable Cement Stabilized Base Course at Airfields and Roads. However, Subsection 3.8.2 Density Testing has been modified per this Addendum No. 4.

5. The specification for Type 3 Rock Lining allows for the use of crushed recycled concrete. Was this intended to be allowed?

Answer: Technical Specification Section 609.60 Rock Ditch Liner has been revised per this Addendum No. 4 to eliminate the use of broken concrete as an allowable material for the rock lining.

6. In Section 32 13 11 - Concrete Pavement for Airfields; Paragraph 2.2.2.3 Size and Grading - Is the use of a sized #4 and sized #67 coarse aggregate required, or can you use one or the other?

Answer: The use of both ASTM C33, No. 4 and No. 67 to meet the coarseness and workability factor criteria is required.

7. Would the Airport consider an alternate haul route other than what is currently shown on the plans?

Answer: The Contractor shall use the haul route as indicated on the plans. See Plansheet G004, Note 9 regarding damage to existing roads.

****END OF ADDENDUM NO. 4****

BID PROPOSAL SUMMARY

Bidder Name:

SCHEDULE I - TOTAL

\$

ALTERNATE 1 - ADD /DEDUCT FABRIC (P-314) IN LIEU OF CHOKE STONE (P-CS)

\$

ITEM P-314 BOND BREAKER FABRIC

DESCRIPTION

314-1.1 This item shall consist of furnishing and installing a bond breaker over the 6" cement treated permeable base course as a bid alternate in lieu of the choke stone material per technical specification P-CS.

MATERIALS

314-2.1 Bond Breaker Fabric shall be a needle-punched nonwoven geotextile composed of polypropylene fibers conforming to the following test methods and physical properties.

Mechanical Properties	Test Method	Units	Roll Value
Mass per Unit Area	ASTM D 5261		
Minimum		oz/yd ²	13.3
Maximum		oz/yd ²	16.2
Thickness Under Load	ASTM D 5199		
0.29 psi		mm	3.0 min
2.9 psi		mm	2.5 min
29 psi		mm	1.0 min
Wide Width Tensile Strength	ASTM D 4595	lbs/ft	684 min
Wide Width Elongation	ASTM D 4595	%	130 max
Water Permeability in Normal Direction under load	ASTM D 5493		
2.9 psi (20 kPa)		m/s	1 x 10 ⁻⁴ min
In-plane Water Permeability	ASTM D 6574		
2.9 psi (20 kPa)		m/s	5 x 10 ⁻⁴ min
29 psi (200 kPa)		m/s	2 x 10 ⁻⁴ min
Alkali Resistance	EN 13249	%	96 min
UV Resistance (at 500 hours)	ASTM D 4355	% Strength Retained	60 min

Alternate bond breaker materials will be evaluated provided that a submittal is provided two weeks prior to the bid. The alternate bond breaker submittal must include a design signed and sealed by a professional engineer registered to practice in the country, state or province in which the project is located and a list of at least 5 projects where the product has been in ground for over a year under similar environmental conditions in which performance can be documented.

STORAGE

314-3.1 Bond breaker rolls shall be furnished with suitable wrapping to protect against moisture and extended ultraviolet exposure prior to placement.

314-3.2 Each roll shall be tagged or labeled with the name of the product and manufacturer. All rolls shall be tagged or labeled with the width, length, and unique roll number for quality tracking purposes. The tag or label shall be securely fastened to the outside of the roll.

314-3.3 Rolls shall be stored and handled in a manner to prevent damage.

314-3.4 After unloading, inspect rolls for defects and damage.

314-3.5 Bond breaker shall be stored per manufacturer's recommendations in a dry covered condition, free from dust, dirt and moisture.

314.3.6 Rolls shall be stored off ground, protected from precipitation, ultraviolet radiation, strong chemicals, sparks and flames, temperatures in excess of 71 °C (160 °F) and other environmental condition that could cause damage to bond breaker.

CONSTRUCTION METHODS

314-4.1 Base Course Preparation

a. Before installing fabric, all base course materials must be accepted for grade, and approved by the engineer for installation of fabric.

314-4.2 Bond Breaker Fabric Installation

a. In areas which will exposed to traffic, fabric shall not be installed until immediately prior to paving operations. But in no case shall fabric be installed more than 3 days prior to paving operations.

b. Bond Breaker Fabric shall be secured to underlying layer with pins or nails punched through 2 to 2.75 inch galvanized washers or discs every 6 feet or less. Additional fasteners can be used as needed to ensure that the fabric does not shift or fold before or during concrete placement.

c. Where it occurs, edges of the fabric shall overlap by 8 inches minimum.

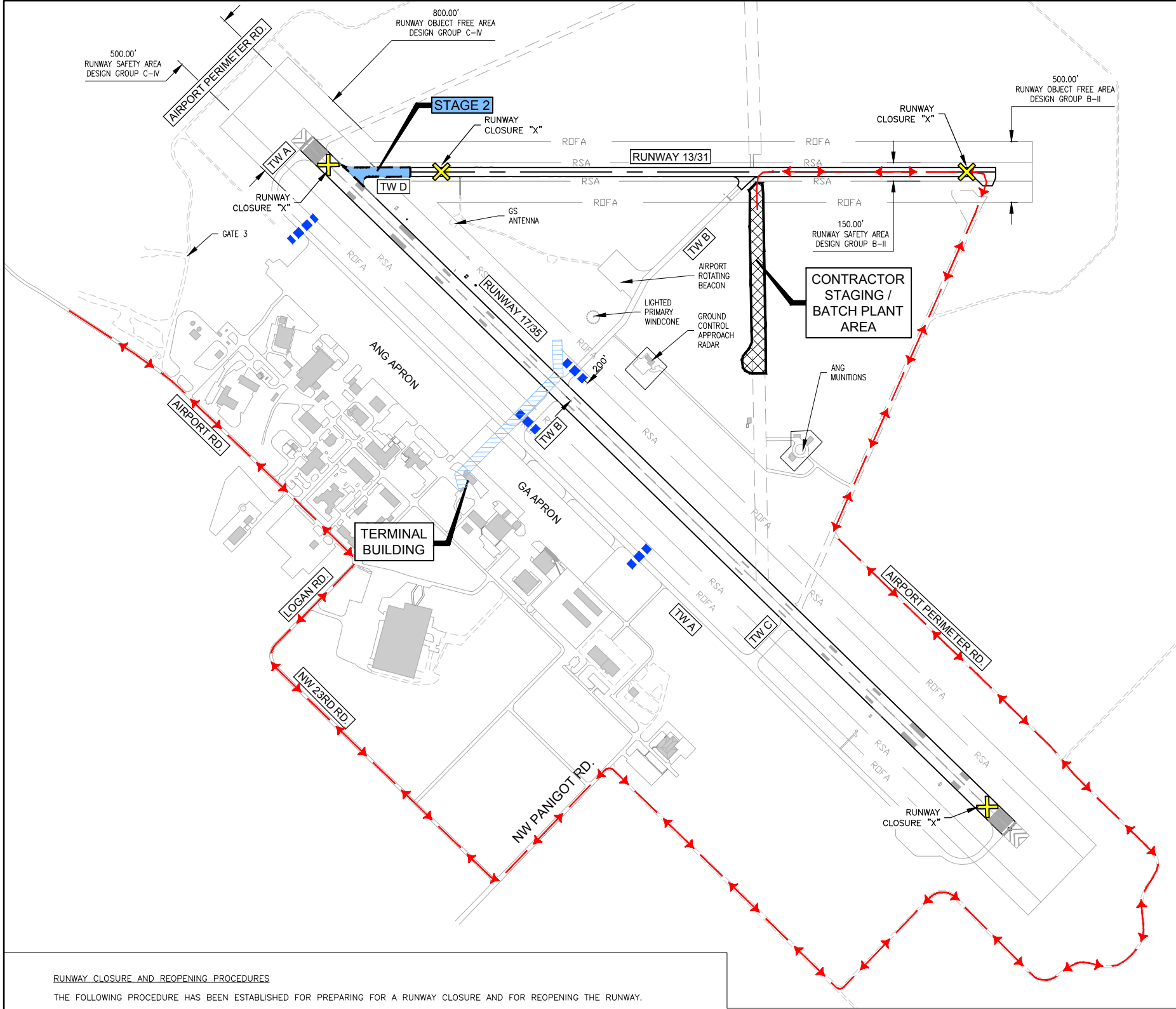
METHOD OF MEASUREMENT

314-5.1 Bond Breaker Fabric shall not be measured separately but shall be considered incidental to Item MO-307a – 6” Cement Treated Permeable Base Course.

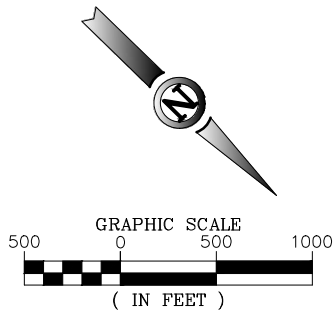
BASIS OF PAYMENT

314-6.1 No separate payment shall be made for Bond Breaker Fabric as it will be considered incidental to Item MO-307a – 6” Cement Treated Permeable Base Course.

****END OF ITEM P-314****



SCHEDULE I - CONSTRUCTION STAGING NOTES - CONSTRUCTION STAGE 2 (14 CALENDAR DAYS)		
STAGE 2	MAJOR WORK TO BE COMPLETED	AIRPORT OPERATIONAL NOTES
ALL WORK IN STAGE 2 SHALL BE COMPLETED IN 14 CALENDAR DAYS AND SHALL BE PERFORMED BETWEEN THE HOURS OF 10:00 P.M. TO 6:00 A.M. ONLY. STAGE 2 WORK SHALL BE PERFORMED CONCURRENTLY WITH WORK IN STAGE 1. STAGE 2 ESTIMATED START DATE: SUMMER 2015 14 CALENDAR DAYS	STAGE 2 1. CRACK REPAIR 2. SEAL COAT 3. EDGE LIGHTING 4. PAVEMENT MARKINGS 5. AIRFIELD LIGHTING HOME RUN CONDUIT AND WIRE	1. THE CONTRACTOR SHALL HAVE ACCESS TO STAGE 2 WORK AREAS BETWEEN THE HOURS OF 10:00 P.M. TO 6:00 A.M. 2. CONTRACTOR SHALL RE-ESTABLISH THE RUNWAY SAFETY AREA PRIOR TO OPENING OF RUNWAY 17/35. THE CONTRACTOR WILL INCUR A \$1,500 LIQUIDATED DAMAGE IF RUNWAY 17/35 IS NOT OPENED BY 6:00 A.M. AND AN ADDITIONAL \$1,500 FOR EACH HOUR THAT RUNWAY 17/35 REMAINS CLOSED AFTER 6:00 A.M. THE REMOVAL OF THE LIGHTED "X"'S AND BARRICADES SHALL BE COMPLETED PRIOR TO REOPENING THE RUNWAY. 3. CONSTRUCTION BARRICADES SHALL BE INSTALLED AS DIRECTED BY ENGINEER FOR AREAS THAT ARE TEMPORARILY CLOSED TO TRAFFIC DURING PAVEMENT MARKING APPLICATION AND DRY TIME. 4. THE CONTRACTOR SHALL HAVE FLAGGERS AVAILABLE WHEN NEEDED TO DIRECT TRAFFIC. CLOSURES: 1. RUNWAY 13/31 CLOSED 2. TAXIWAY "D" CLOSED 3. TAXIWAY "C" CLOSED 4. TAXIWAY "B" CLOSED 5. RUNWAY 17/35 WILL BE CLOSED FROM 10:00 P.M. TO 6:00 A.M. 6. TAXIWAY "A" CLOSED SOUTH OF SOUTH EDGE OF AIR NATIONAL GUARD APRON AND NORTH OF GA APRON.
OTHER NOTES		CONSTRUCTION STAGING LEGEND
1. A MINIMUM OF 72 HOUR NOTICE SHOULD BE GIVEN TO THE AIRPORT FOR CONSTRUCTION ACCESS TO ALL STAGE 2 WORK AREAS IN ORDER FOR THE AIRPORT TO ISSUE A NOTAM. IF CLOSURES ARE NOT SCHEDULED PROPERLY AND IN WRITING, THE CONTRACTOR MAY NOT HAVE ACCESS TO THE DESIRED WORK AREA AND IN NO WAY IS THIS A VALIDATION FOR ADDITIONAL CALENDAR DAYS. 2. ATCT/CTAF - 126.9 MHZ (WHEN TOWER CLOSED) GROUND CONTROL - 121.9 MHZ (WHEN TOWER OPEN) 3. A SWEEPER OR OTHER APPROVED EQUIPMENT SHALL BE AVAILABLE AT ALL TIMES TO CLEAN DEBRIS FROM HAUL ROUTE OR AREAS ADJACENT TO CONSTRUCTION. 4. CONTRACTOR IS RESPONSIBLE FOR ANY DAMAGE CAUSED TO EXISTING AIRFIELD PAVEMENT, LIGHTS OR AREAS ALONG THE HAUL ROUTE. 5. THE CONTRACTOR SHALL ONLY PARK EQUIPMENT IN THE DESIGNATED STAGING AREA. 6. THE CONTRACTOR SHALL SUBMIT A DETAILED SCHEDULE.		<div><div></div>STAGE 2 (14 CALENDAR DAYS - NIGHT WORK)</div> <div><div></div>STAGE 2 ELECTRICAL (14 CALENDAR DAYS - NIGHT WORK)</div> <div><div></div>CONTRACTOR STAGING AREA</div> <div><div></div>HAUL ROUTE</div> <div><div></div>BARRICADES SEE DETAIL 1 ON SHEET G006.</div> <div><div></div>CLOSED RUNWAY "X" SEE LIGHTED "X" NOTES ON SHEET G006.</div> <div><div></div>CLOSED TAXIWAY "X" SEE DETAIL 2 ON SHEET G006.</div>



RUNWAY CLOSURE AND REOPENING PROCEDURES

THE FOLLOWING PROCEDURE HAS BEEN ESTABLISHED FOR PREPARING FOR A RUNWAY CLOSURE AND FOR REOPENING THE RUNWAY.

- PREPARATION FOR RUNWAY CLOSURE
 - CONTRACTOR TO NOTIFY THE ENGINEER THAT A RUNWAY CLOSURE WILL BE REQUIRED WITH A MINIMUM NOTICE OF 72 HOURS.
 - CONTRACTOR SHALL TAKE WEATHER FORECASTS INTO CONSIDERATION SO THAT ALL ANTICIPATED WORK CAN BE COMPLETED WITHIN THE ALLOCATED CLOSURE TIMES.
 - ENGINEER SHALL COORDINATE WITH STJ STAFF.
 - STJ STAFF WILL ISSUE A NOTAM FOR THE RUNWAY CLOSURE.
 - NIGHT TIME CLOSURES ARE LIMITED BETWEEN 10PM AND 6AM.
 - MEET ONSITE 30 MINUTES PRIOR TO CLOSURE TO DISCUSS CLOSURE WORK AND PREPARE FOR MOBILIZATION.
- ACCESS TO CLOSED RUNWAY
 - NO PERSONNEL, EQUIPMENT, OR MATERIAL WILL BE ALLOWED ON THE RUNWAY OR IN THE SAFETY AREA PRIOR TO 10PM.
 - STJ STAFF WILL NOTIFY THE ENGINEER THAT THE LAST FLIGHT HAS LANDED, THE RUNWAY IS CLEAR, AND THE NOTAM CLOSURE IS IN PLACE.
 - ENGINEER WILL NOTIFY THE CONTRACTOR, WHO WILL PLACE THE LIGHTED RCMS OVER THE RUNWAY NUMBERS ON BOTH ENDS.
 - THE RUNWAY LIGHTS SHALL BE DEACTIVATED DURING THE CLOSURE PERIOD.
 - THE CONTRACTOR WILL HAVE ACCESS TO THE ENTIRE SAFETY AREA DURING THE CLOSURE PERIOD.

RUNWAY CLOSURE AND REOPENING PROCEDURES (CONT.)

- REOPENING A CLOSED RUNWAY
 - CONTRACTOR SHALL BE RESPONSIBLE FOR INSPECTING THE ENTIRE RUNWAY AND TAXIWAY SYSTEM TO ENSURE THAT FOD IS CLEANED UP.
 - THE SAFETY AREA MUST BE GRADED TO MEET ALL SAFETY CRITERIA.
 - ALL SURFACE MARKINGS SHALL BE COMPLETED PRIOR TO OPENING TO AIRFIELD TRAFFIC.
 - INSTALL TAPERS, IF NECESSARY, AT DROPS IN PAVEMENT.
 - REACTIVATE RUNWAY LIGHTS.
 - CONTRACTOR TO REMOVE LIGHTED RCMS PRIOR TO 5:30AM.
 - ALL PERSONNEL, EQUIPMENT, OR MATERIAL SHALL BE CLEAR OF THE RUNWAY AND THE SAFETY AREA PRIOR TO 5:30AM.
 - CONTRACTOR SHALL NOTIFY ENGINEER WHEN THEY ARE CLEAR OF THE RUNWAY PRIOR TO 5:30AM.
 - THE ENGINEER SHALL NOTIFY THE STJ STAFF THE RUNWAY IS CLEAR.
 - STJ STAFF WILL INSPECT THE AIRFIELD PAVEMENT AND SAFETY AREA STARTING AT 5:30AM.
 - THE CONTRACTOR SHALL BE PRESENT DURING THE INSPECTION AND SHALL IMMEDIATELY REMEDY ANY SAFETY CONCERNS THAT STJ STAFF IDENTIFIES.
 - STJ STAFF WILL CALL CLEAR THE RUNWAY FOR AIRFIELD TRAFFIC.

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A I R P O R T
CITY OF ST. JOSEPH, MISSOURI

DES: D.W.C.	ISSUE RECORD			
	NO.	BY	DATE	DESCRIPTION
DR: D.W.C.	1	R.B.L.	06/23/15	ISSUED FOR BID
	2	R.B.L.	07/28/15	ADDENDUM NO. 4
CH: C.L.G.				
APP: R.B.L.				

REPLACEMENT OF
ASSAULT STRIP
RUNWAY 13/31

CONSTRUCTION SAFETY DRAWING -
SCHEDULE I, STAGE 2

JVIATION PROJ. NO.
2013.STJ.01

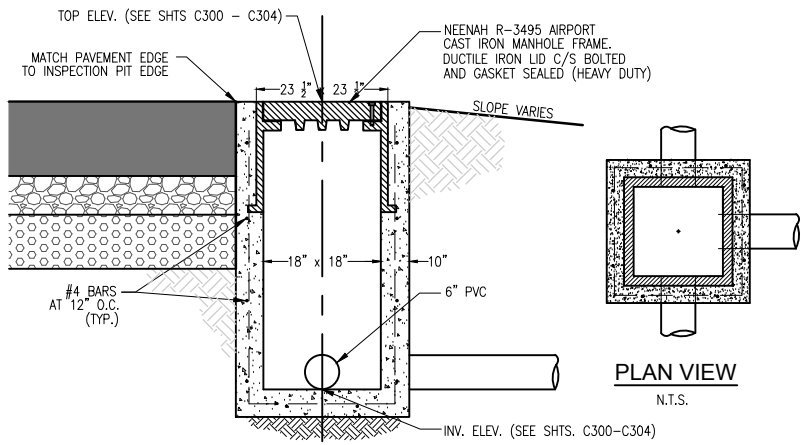
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06/23/15

SHEET NAME
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SHEET NO.
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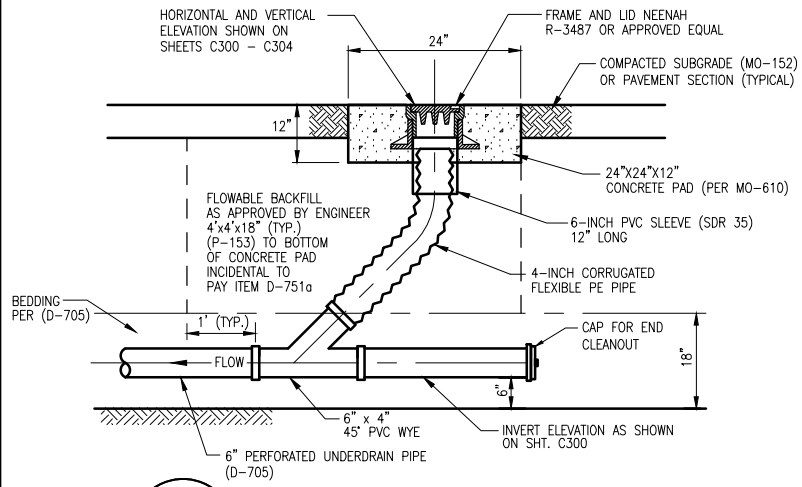
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OR UNDER THE SUPERVISION OF:

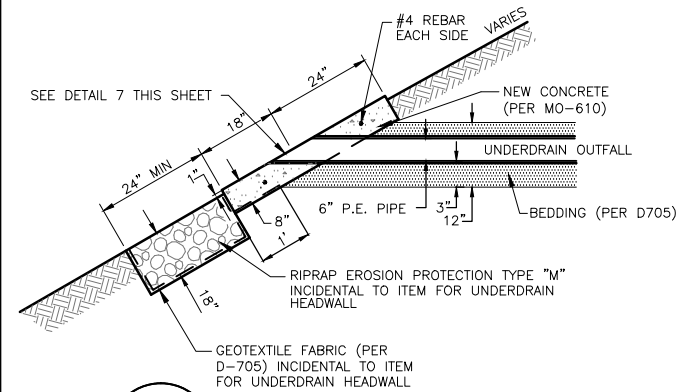
RYAN B. LORTON PE-2004017211 06/23/15
NAME REG. NO. DATE
FOR AND ON BEHALF OF JVIATION, INC.



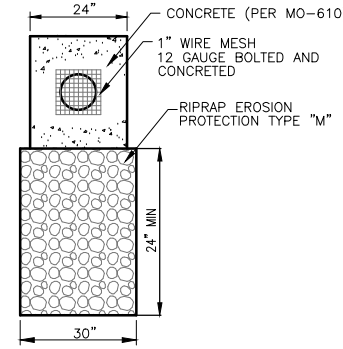
1 INSPECTION PIT DETAIL FOR UNDERDRAIN
C300 - C304
N.T.S.



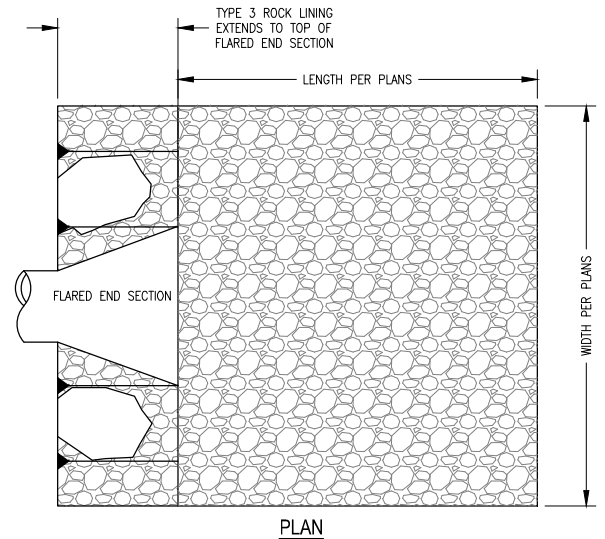
2 UNDER DRAIN CLEANOUT DETAIL
C300 - C304
N.T.S.



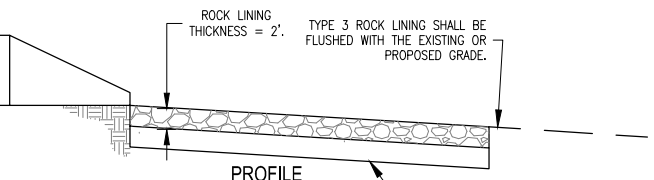
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C300 - C304
N.T.S.



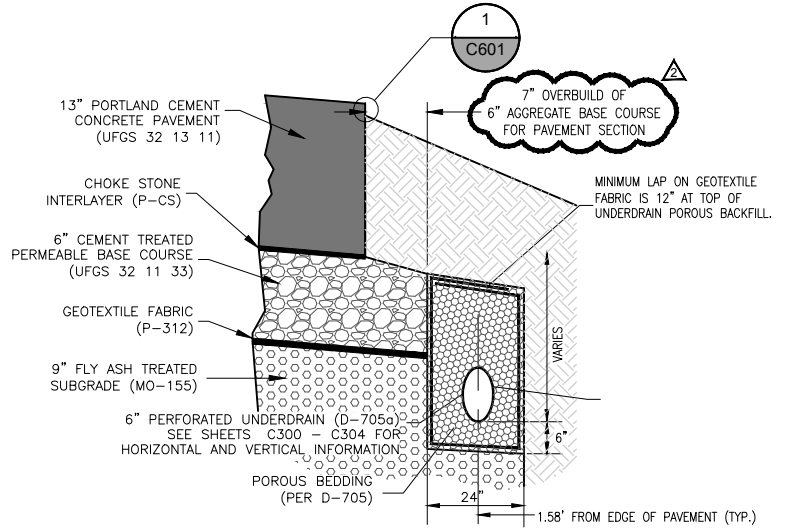
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C350
N.T.S.



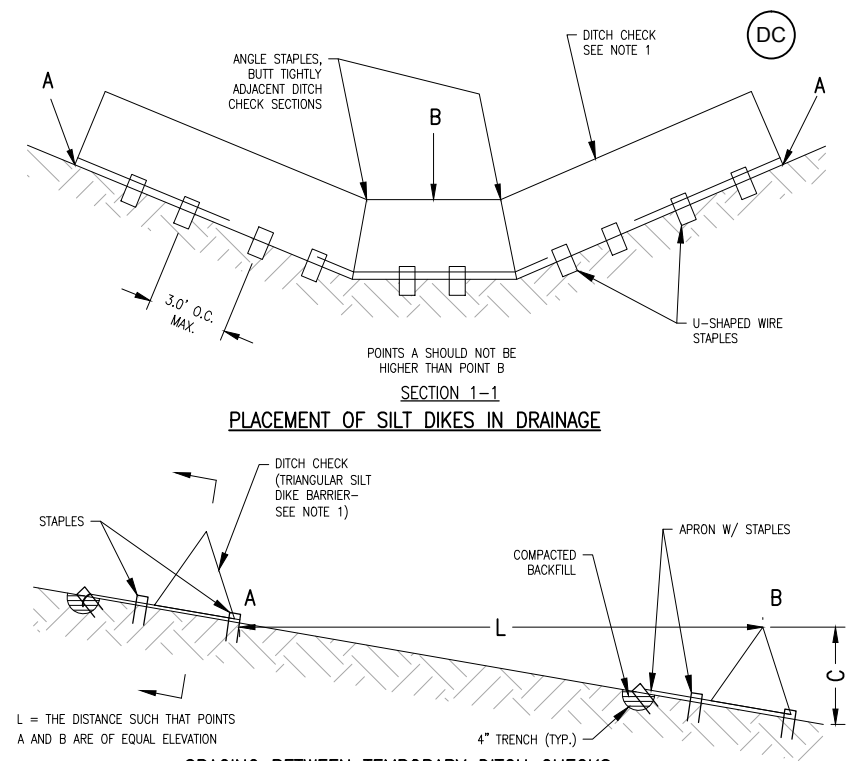
5 PAVEMENT UNDERDRAIN DETAIL
C350
N.T.S.



6 LOW TAIL WATER RIPRAP DETAIL
C300 - C304
N.T.S.



7 CULVERT PROTECTION EROSION CONTROL LOG
C350
N.T.S.



8 DITCH CHECKS
C350
N.T.S.

- NOTES:
- COIR LOGS, GEORIDGE OR SEDIMENT STOP FILTRATION SYSTEM MAY BE USED IN LIEU OF TRIANGULAR SILT DIKE BARRIER IF APPROVED BY THE ENGINEER.
 - INSPECTION OF SILT DIKES SHALL BE AT LEAST ONCE PER WEEK AND AFTER RAIN EVENTS IN EXCESS OF HALF AN INCH (1/2") PER DAY OR EQUAL SNOW MELT. REPAIR OR REPLACEMENT OF DITCH CHECK SHALL BE MADE PROMPTLY AS NEEDED.
 - REMOVE SEDIMENT WHEN SEDIMENT DEPTH AT THE DITCH CHECK IS APPROXIMATELY EQUAL TO ONE-HALF OF THE DIKE'S HEIGHT (0.5C).
 - SILT DIKES SHALL BE REMOVED UPON COMPLETION OF CONSTRUCTION AND ONLY WHEN DIRECTED BY THE VILLAGE ENGINEER.

REVISED:01-01-13

NOT TO SCALE
TEMPORARY
DITCH CHECK
DETAIL

ISSUED FOR BID

THESE DRAWINGS ARE FOR BIDDING ONLY AND ARE NOT INTENDED FOR CONSTRUCTION, OR PERMIT PURPOSES. THEY WERE PREPARED BY OR UNDER THE SUPERVISION OF:

RYAN B. LORTON PE-2004017211 06/23/15
NAME REG. NO. DATE
FOR AND ON BEHALF OF JVIATION, INC.

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CITY OF ST. JOSEPH, MISSOURI

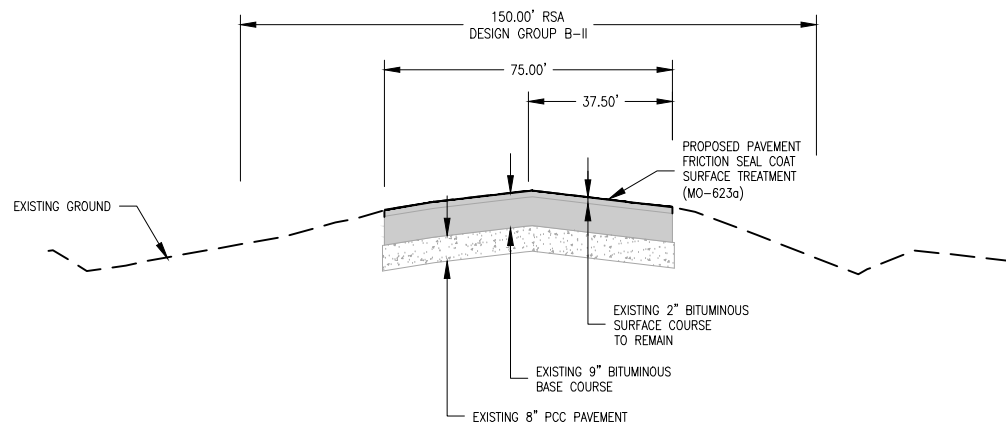
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REPLACEMENT OF
ASSAULT STRIP
RUNWAY 13/31

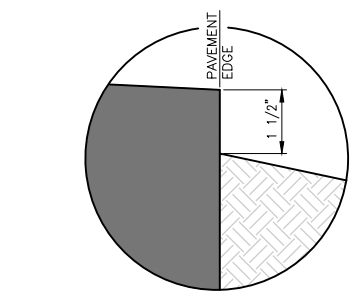
STORM, UNDERDRAIN AND
EROSION CONTROL DETAILS

JVIATION PROJ. NO. 2013.STJ.01
DATE: 06/23/15
SHEET NAME C350
SHEET NO. 36 of 100

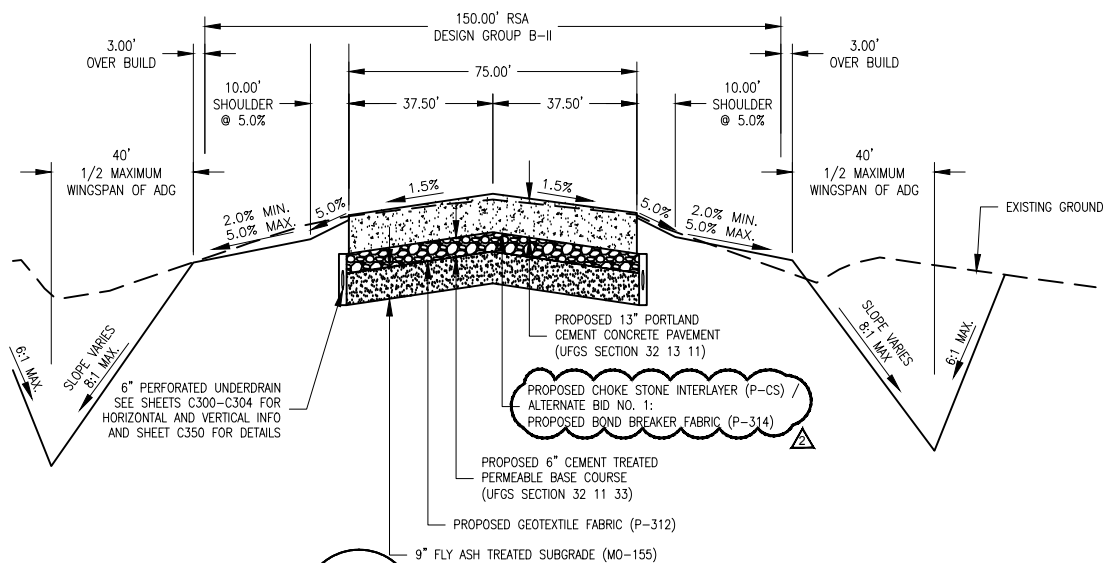
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Jul 28, 2015 12:21pm
dconway



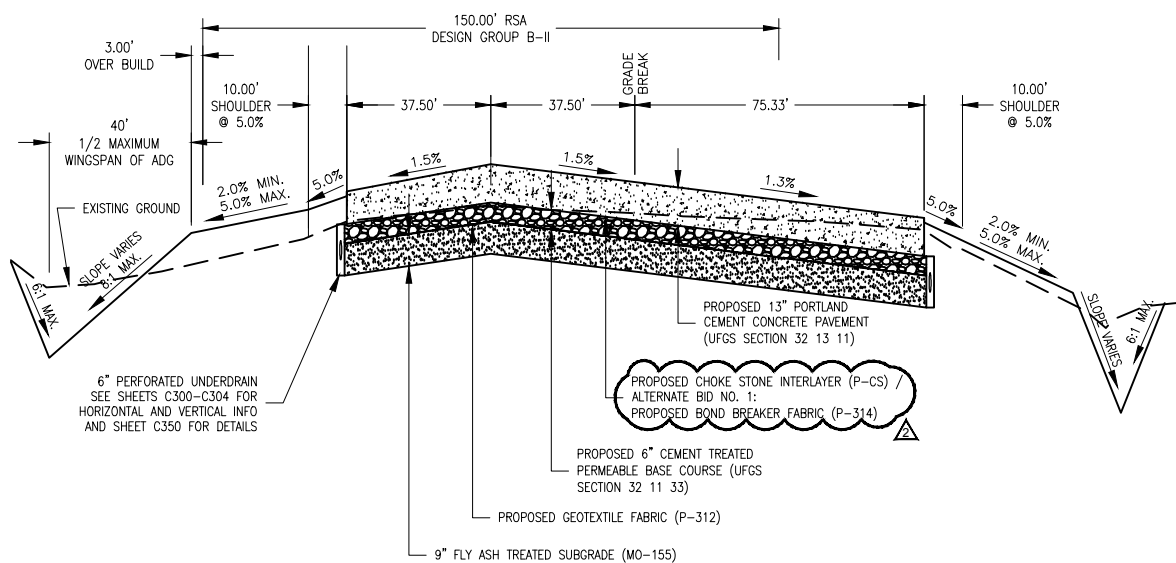
D
C200 - C204
TYPICAL SECTION D
STA 4+94 TO 10+00
N.T.S.



1
C460
TYPICAL PAVEMENT EDGE
N.T.S.



E
C200 - C204
TYPICAL SECTION E
STA 10+00 TO 12+50
(TRANSITIONING CROSS SLOPE),
STA 12+50 TO 56+00
(1.50% CROSS SLOPE)
N.T.S.



F
C200 - C204
TYPICAL SECTION F
STA 56+00 TO 57+97

ISSUED FOR BID

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RYAN B. LORTON PE-2004017211 06/23/15
NAME REG. NO. DATE
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ASSAULT STRIP
RUNWAY 13/31

PROPOSED TYPICAL SECTIONS

JVIATION PROJ. NO.
2013.STJ.01

DATE:
06/23/15

SHEET NAME
C601
SHEET NO.
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