

April 26, 2017

To: Plan Holders for Improvements to the  
Northwest Missouri Regional Airport  
Maryville, Missouri  
MoDOT Project No. 16-002A-1

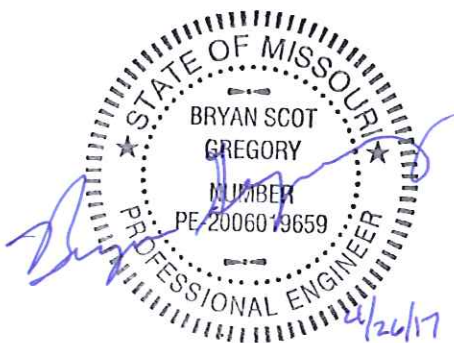
Transmitted herewith is Addendum No. 1 to the Issued for Bid Contract Documents, Specifications, and Plans dated March 28, 2017 for improvements to the Northwest Missouri Regional Airport Schedule I, Apron Reconstruction and Schedule II, Runway Safety Area Grading, including:

- Pre-Bid Conference Minutes
- Questions asked during the pre-bid are answered within the Pre-Bid Minutes
- The Pre-Bid meeting sign-in sheet
- Modified Sheet C200 showing the layout of the Aircraft Tie-Down Anchors
- Modified Sheet C750 showing the apron tie-down anchor details.

As a reminder, bids are due Monday, May 1<sup>st</sup>, 2017 at 2:00 PM.

Sincerely,

Jviation, Inc.



Bryan Gregory, P.E.

Project Engineer

## NORTHWEST MISSOURI REGIONAL AIRPORT

MoDOT PROJECT NO. 16-002A-1

DATE: April 13, 2017

TIME: 11:00 A.M.

**PRE-BID CONFERENCE Meeting Minutes – Agenda in black; Discussion in red**

### 1. RECORDING OF ATTENDEES

A. Recording of attendees, firm represented, address, phone number and email.

See attached sign-in sheet

### 2. INTRODUCTIONS & PROJECT DESCRIPTION

A. Airport Representatives

- Greg McDanel – City Manager
- Ryan Heiland – Assistant City Manager

B. Airport Engineering – Jviation, Inc.

- Bryan Gregory – Project Manager

C. MoDOT Aviation

- Kyle LePage/Darrell Goth – Project Manager(s)

D. Project Schedules

- Schedule I – Apron Reconstruction
- Schedule II – Runway Safety Area Grading

E. Work Items

Schedule I – Apron Reconstruction

- Mobilization ..... 1 LS
- Full Depth PCC Pavement Removal ..... 7,292 SY
- Overexcavation and Replacement ..... 300 SY
- Unclassified Excavation ..... 1,500 CY
- 9-Inch Fly Ash Treated Subgrade ..... 7,292 SY
- Class C Fly Ash ..... 440 TON
- 6-Inch Crushed Aggregate Base Course ..... 7,292 SY
- Stabilization Fabric ..... 7,292 SY
- 6” Portland Cement Concrete Pavement ..... 7,292 SY

- Airport Taxiway Pavement Marking (Yellow) .....105 SF
- Airport Pavement Marking (Black) .....210 SF
- Aircraft Tiedown Anchors..... 33 EA
- 12” Reinforced Concrete Pipe ..... 210 LF
- Existing Storm Pipe Removal ..... 210 LF
- Aircraft Rated Inlet .....1 EA
- Existing Inlet Removal.....1 EA
- Seeding with Hydromulch .....0.5 AC

Schedule II – Reconstruct Connecting Taxiway

- Mobilization ..... 1 LS
- Overexcavation and Replacement ..... 850 SY
- Unclassified Excavation ..... 3,000 CY
- Silt Fence ..... 1,500 LF
- Ditch Check .....3 EA
- Culvert Protection.....2 EA
- Install 6-Inch Non-Perforated Polyethylene Pipe ..... 450 LF
- Existing 4” PVC Underdrain Outlet Pipe Removal..... 405 LF
- Install 6-Inch Underdrain Outfall..... 11 EA
- Adjust Existing Runway Power Cable ..... 85 LF
- Adjust Existing PAPI Power Cable..... 80 LF
- Adjust Runway/Taxiway Stake Mounted Edge Light ..... 16 EA
- Seeding with Hydromulch .....3.5 AC

Contractors often over-build (add width to) the flyash treated subgrade, base course, and stabilization fabric items in order to accommodate paving equipment. While this is encouraged, such over-build is not considered for payment and costs should be included in other items.

**3. BID OPENING**

- A. Date: Monday, May 1, 2017 at 2:00 P.M. (CST)
- B. Location: City of Maryville  
City Hall, City Council Chambers  
415 N. Market Street  
Maryville, MO 64468
- C. Bid Bond: 5% of bid amount – Section 2, Instructions to Bidders, Paragraph 4 and Section 20-10 Bid Guaranty.
- D. Contract Proposal: Proposal Form starts on page B-1. Submit pages B-1 through B-21 with bid.

E. Award: Notice to Bidders, Section 1, Page 2 and Section 30 Award and Execution of Contract.

#### **4. DBE GOALS**

A. 5% of Contract Amount. Good Faith Effort documentation will be required if not meeting the goal. Document all conversations with sub-contractors in trying to meet the goal.

#### **5. ESTIMATED CRITICAL CONTRACT DATES**

A. Contract Award: Summer 2017 (ESTIMATED) –The signature of the bidder on the proposal form indicates that within thirty (30) calendar days from acceptance of its bid offer it will execute a contract with the Owner and, if indicated in this solicitation, furnish a project specific Certificate of Insurance naming the Owner as Additional Insured, furnish Performance and Payment Bonds and any other documents required by the Contract Documents.

B. Award date will depend on receipt of federal funds. It is possible that federal funds will not be awarded until August or September. Bids can be held up to 120 calendar days. If a July 2017 start is delayed due to funding, construction may be delayed until 2018, pending conversations with the successful bidder.

C. Notice to Proceed: Summer 2017 (ESTIMATED), Refer to Section 80-2.

Project Time: 75 Calendar Days for Schedule I. Schedule II is 10 Calendar Days and is concurrent with Schedule I for a total of 75 Calendar Days if all Schedules are awarded. Notice to Proceed, Refer to Section 80-08 for more information.

#### **6. BONDING**

A. Payment Bond: 100% of Bid Amount, Refer to Section 30-05

B. Performance Bond: 100% of Bid Amount, Refer to Section 30-05

#### **7. BUY AMERICAN REQUIREMENTS**

A. See Section 4 – Supplementary Provisions, Part A Federal and State Provisions for Buy American Requirements. Buy American Certification required to be submitted as part of the Bid Proposal Form.

#### **8. INSURANCE REQUIREMENTS**

Refer to Section 4, Part C Local Provisions, Paragraph 11 for Contractor's Liability Insurance.

A. The Contractor shall pay for and maintain during the life of this contract adequate Workmen's Compensation, Public Liability and Property Damage Insurance. The Contractor is charged with the responsibility for adequate and proper coverage for all his subcontract operations. Contractor shall furnish to the Sponsor satisfactory proof of carriage of the insurance required.

#### **9. FEDERAL AND STATE WAGE RATES (DAVIS BACON ACT)**

- A. The higher of the Federal wage and State wage rates are required for this project for work completed under Schedules I and II. Refer to Section 4, Part D Federal and State Wage Rates.
- B. If Missouri Annual Wage Order AWO24 becomes effective on or before April 21<sup>st</sup>, it will be substituted as an addendum to the project. Otherwise, AWO23 will be in effect for the project.

As of April 24, 2017, Missouri Annual Wage Order No. 23 is still in effect. While modifications 6 and 7 to the Federal General Decision Number MO170001 have been published, no changes were identified in Nodaway County. Therefore, the addendum will not include any wage rate changes.

**10. ENGINEER’S FIELD OFFICE**

- A. See Section 60-05.

**11. LIQUIDATED DAMAGES**

- A. As compensation for non-use, the Contractor shall be assessed a liquidated damage of **\$750/calendar day(s)** for each day that the work remains uncompleted beyond the contract period.

SECTION 80-08 FAILURE TO COMPLETE ON TIME.

SCHEDULE	LIQUIDATED DAMAGES COST	ALLOWED CONSTRUCTION TIME
Schedule I	\$750/Calendar Day(s)	75 Calendar Days
Schedule II	10 Days Concurrent with the 75 Calendar Days provided in Schedule I	

The maximum construction time for the overall project is 75 calendar days.

Please see the Phasing Sheets, G008 through G010, of the Construction Drawings for more information on the scheduling/sequencing of work.

- B. Permitting the Contractor to continue and finish the work or any part of it after the time fixed for its completion, or after the date to which the time for completion may have been extended, will in no way operate as a waiver on the part of the Owner of any of its rights under the contract.
- C. The Engineer and City have attempted to estimate the amount of time needed for construction management services by the Engineer. If unusually long working days or extended working days are required, reimbursement for unscheduled employment of the engineer may be required as shown in Section 4 of the Contract Documents, under “Liquidated Damages”.

**12. PROJECT PHASING**

- A. Phase 1: The work in this phase includes all Schedule I reconstruction of the northeast apron pavement and subsidiary items associated with this new pavement. This work does not include the new pavement in front of the fuel farm.

- B. Phase 2: The work in this phase surrounds Runway end 32. Phase 2 work will be completed in 10 Calendar Days concurrently with Schedule I. Runway 14/32 will be closed for the duration of Schedule II – Phase 2.

### 13. MISCELLANEOUS

- A. Construction Materials: Sales Tax Exemption. Refer to Section 4, Part C Local Provisions, Paragraph 13 Sales and Use Taxes.
- B. Survey Requirements – Refer to Section 50-06, Construction Layout and Stakes.
- C. Contractor Access: Off Hawk Road as shown on the Phasing sheets.
- D. Acceptance Testing: Responsibility of Engineer.
- E. Adjusting Lights- will require more effort than simply pulling them up or pushing them down. Hand-digging around the lights should be anticipated.
- F. Waste Area- Contractor shall dispose of all waste materials offsite. See “Material Supply and Disposal” notes on Sheet G004.
- G. Haul Route- Sheet G004, “Site Access and Staging” notes. The Contractor shall be responsible for any damage to existing facilities or roads. Repairs shall be made at no additional cost to the Sponsor.
- H. Airport radios shall be required and must be monitored, especially during the closure of Runway 14/32.
- I. Questions will be taken via written format only to Jviation, Inc. until **Monday, April 24, 2017 at 5:00 p.m. (CST)**. E-mail comments will be accepted to [Bryan.Gregory@jviation.com](mailto:Bryan.Gregory@jviation.com). Any last addendum items should be issued no later than Wednesday, April 26<sup>th</sup>.

### 14. QUESTIONS

Q1: How long will it take to review the survey after subgrade and base course completion?

A1: If the survey review time falls within the critical path, coordinate closely with the Engineer’s on-site representative and/or project manager. Reasonable efforts will be made to review the survey within 1 to 3 working days, unless prior coordination has occurred. The Engineer’s on-site representative can often help identify ways to make this review quicker if it falls in the critical path. This may include accompanying the survey crew or requesting a “hard copy” of the survey to enable quicker review.

Q2: Regarding the 5% DBE Goal – it appears much of the DBE work is in Schedule II. What happens if Schedule II is not awarded?

A2: In coordination with MoDOT External Civil Rights Division, the DBE goal applies to the entire project. The 5% goal will apply to whatever schedules are awarded. Regarding the situation presented in the question, if Schedule I were awarded and not Schedule II, the Contractor would be required to either provide for 5% DBE work on Schedule I or provide good faith effort documentation showing why that is not possible. This situation is not anticipated and we have received no indication that the Sponsor won't be able to award both phases at this time.

Q3: Does the entire project "Contract Documents and Specifications" book need to be returned with a bid?

A3: It is only required to return Pages B-1 through B-21 (along with any required attachments identified on those pages).

Q4: Does the City want the concrete removed from the apron?

A4: The City will gladly accept concrete that is free of reinforcement and broken down into 1' to 2' maximum size chunks. In this situation, coordination with the Engineer's on-site representative should occur to identify a safe place for a stockpile of this material. The Sponsor will coordinate the hauling of the material.

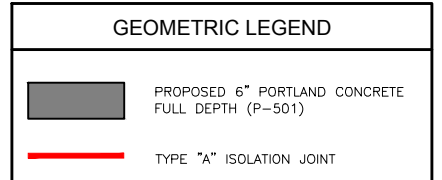
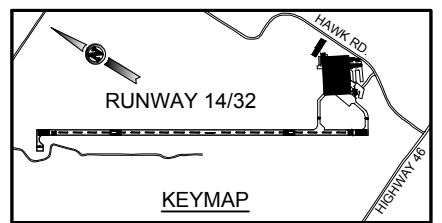
We are including with this addendum a revised Plan Sheet C200, Geometric Plan for the Apron Area. This sheet has been updated to include the Aircraft Tiedown Anchor locations.

Please remember to acknowledge receipt of the addendum on Page B-4 of the Contract Documents before returning it as part of your bid.

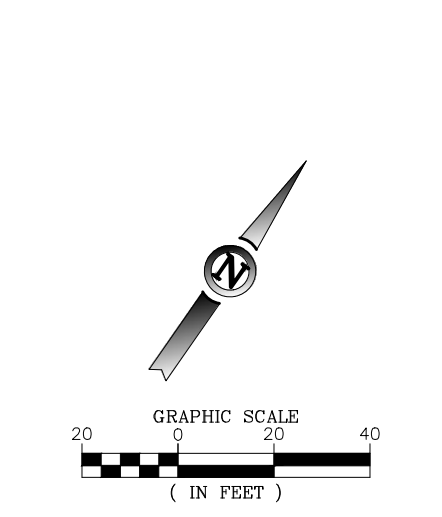
## **15. PROJECT SITE TOUR**







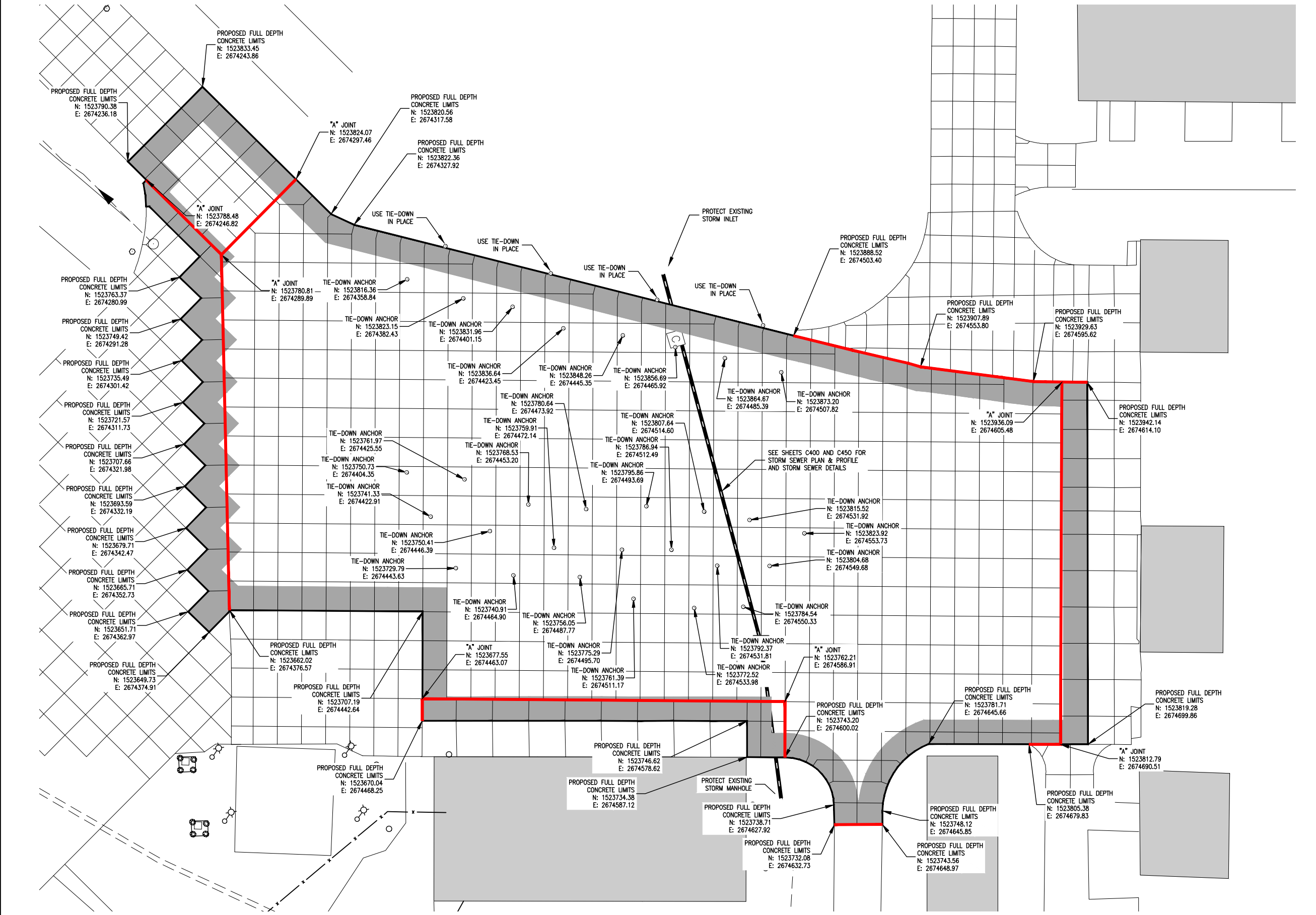
- NOTES**
1. ANY PAVEMENT DAMAGED DURING REMOVAL OUTSIDE THE PROPOSED REMOVAL LIMITS SHALL BE SQUARED OFF TO THE SATISFACTION OF THE ENGINEER. ALL COSTS ASSOCIATED WITH THE ADDITIONAL REMOVAL AND RECONSTRUCTION SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
  2. CONTRACTOR SHALL LOCATE AND PROTECT ALL EXISTING UTILITIES AND ELECTRICAL SYSTEMS DURING CONSTRUCTION.
  3. CONTRACTOR SHALL PROTECT ALL EXISTING UTILITIES. CONTRACTOR SHALL COORDINATE ALL UTILITY LOCATES.
  4. ANY PAINT DAMAGED OUTSIDE OF REMOVAL LIMITS TO BE RESTORED TO ORIGINAL CONDITIONS AT THE EXPENSE OF THE CONTRACTOR.
  5. IF EXISTING PAVEMENT IS DAMAGED BY HAUL OPERATIONS, THE CONTRACTOR SHALL REPAIR PAVEMENT AT NO ADDITIONAL COST TO THE SPONSOR.
  6. SEE DETAIL 2 ON SHEET C750 FOR AIRCRAFT TIE-DOWN ANCHOR DETAILS



**ISSUE FOR BID**

THESE DRAWINGS ARE FOR BIDDING PURPOSES ONLY. THEY WERE PREPARED BY OR UNDER THE SUPERVISION OF:

BRYAN S. GREGORY PE-2006019659 03/28/2017  
 NAME REG. NO. DATE  
 FOR AND ON BEHALF OF JVIATION, INC.



NORTHWEST MISSOURI  
 REGIONAL AIRPORT  
 MARYVILLE, MISSOURI

ISSUE RECORD				
NO.	BY	DATE	DESCRIPTION	
1	B.S.G.	03/28/2017	ISSUED FOR BID	
2	B.S.G.	04/26/2017	BID ADDENDUM #1	

APRON RECONSTRUCTION  
 AND RUNWAY  
 SAFETY AREA GRADING

GEOMETRIC PLAN APRON AREA			SHEET NAME
MODOT PROJ. NO. 16-002A-1	JVIATION PROJ. NO. EVU-LOC-16-01	DATE: 03/28/2017	C200
			SHEET NO. 14 of 29

