

US-169/BUCK O'NEIL BRIDGE  
ENVIRONMENTAL STUDY

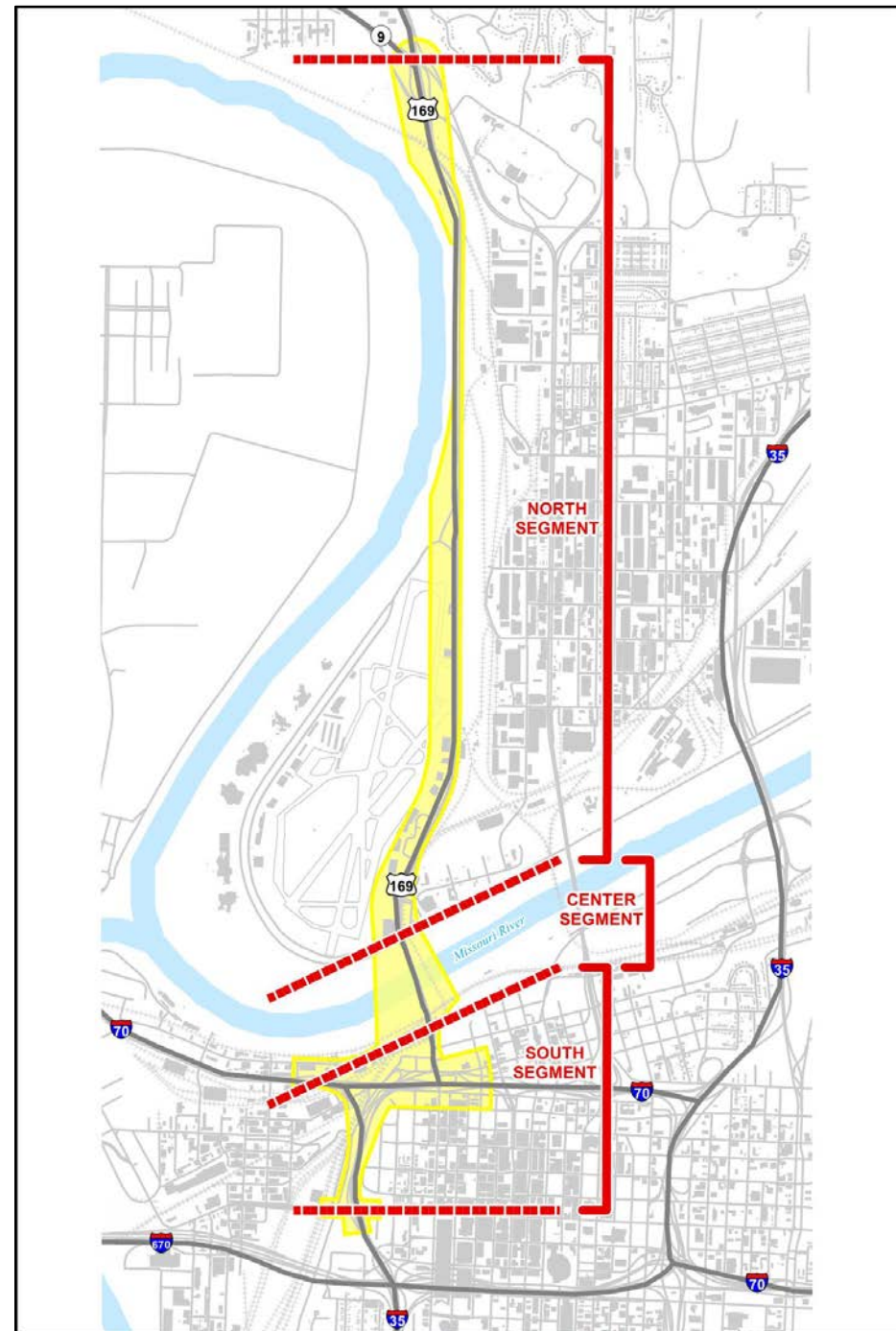




# US-169/BUCK O'NEIL BRIDGE

ENVIRONMENTAL STUDY

STUDY AREA



# US-169/BUCK O'NEIL BRIDGE

## ENVIRONMENTAL STUDY

- **Maintain Infrastructure**
- **Maintain reliable regional connection across the Missouri River that separates local and regional traffic and minimizes local traffic conflicts**
- **Improve operation and safety performance of crossing for all modes**



# US-169/BUCK O'NEIL BRIDGE

ENVIRONMENTAL STUDY

- **Maintain Infrastructure**
- **Maintain reliable regional connection across the Missouri River that **SERVICES** local and regional traffic and minimizes local traffic conflicts**
- **Improve operation and safety performance of crossing for all modes**

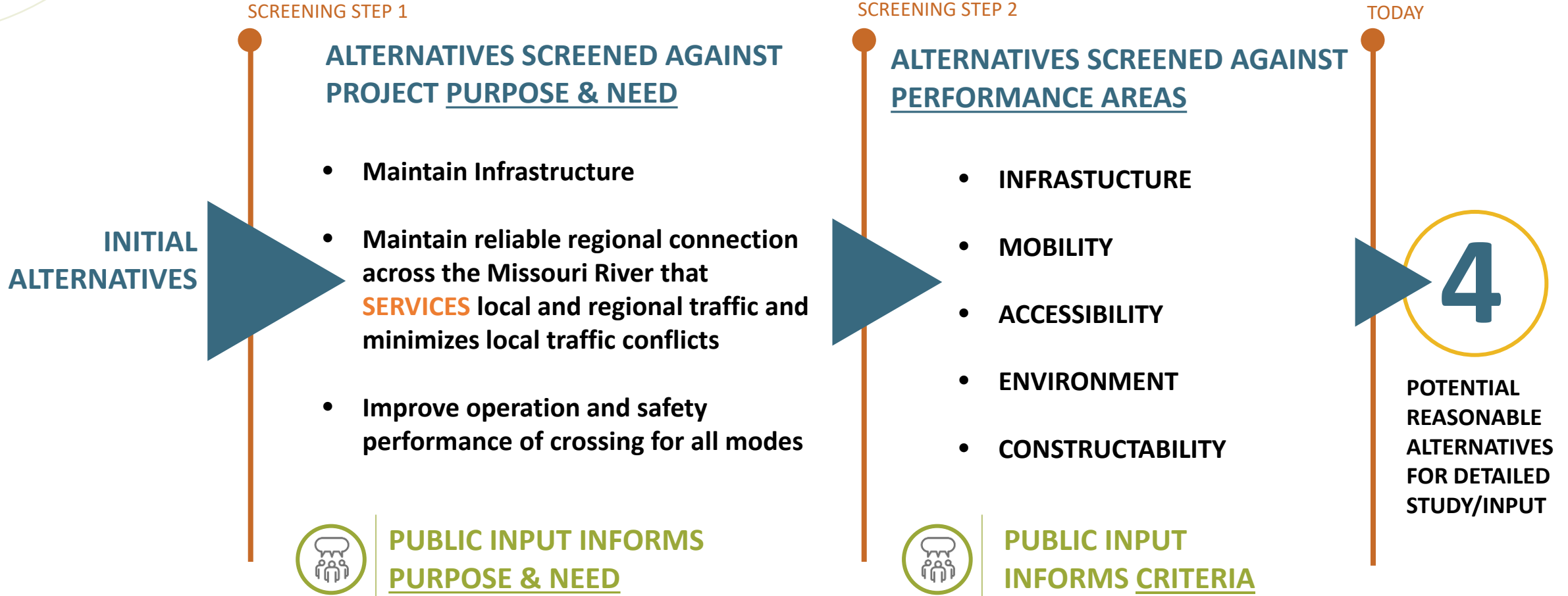
ADJUSTED PURPOSE  
& NEED



# US-169/BUCK O'NEIL BRIDGE

## ENVIRONMENTAL STUDY

### SCREENING



# US-169/BUCK O'NEIL BRIDGE

ENVIRONMENTAL STUDY

TODAY

4

POTENTIAL  
REASONABLE  
ALTERNATIVES  
FOR DETAILED  
STUDY/INPUT

1. No Build Alternative
2. West New Bridge Alternative
3. Central New Bridge Alternative
4. Adjacent New Bridge Alternative



**WE WANT TO HEAR FROM YOU**



SCREENING



MAINTAIN/REPAIR EXISTING CROSSING

# NO BUILD ALTERNATIVE

## PROS:

- EXISTING BUCK O'NEIL BRIDGE REMAINS IN PLACE
- NO NEW RIGHT OF WAY NEEDED
- NO CONSTRUCTION IMPACTS TO NATURAL FEATURES

## CONS:

- DOES NOT REPLACE OR IMPROVE AGING INFRASTRUCTURE
- DOES NOT IMPROVE LOCAL AND REGIONAL TRAFFIC CONNECTIONS
- DOES NOT ACCOMMODATE BICYCLES AND PEDESTRIANS - BRIDGE STRUCTURE CANNOT BE MODIFIED TO ACCOMMODATE BIKE/PED FACILITIES
- DOES NOT INCLUDE MAJOR REHABILITATION





NEW RIVER CROSSING – BRIDGE ON WEST ALIGNMENT

# WEST ALTERNATIVE

RAMPS AT 5<sup>TH</sup> & 6<sup>TH</sup> STREET, DIRECT CONNECT TO I-35

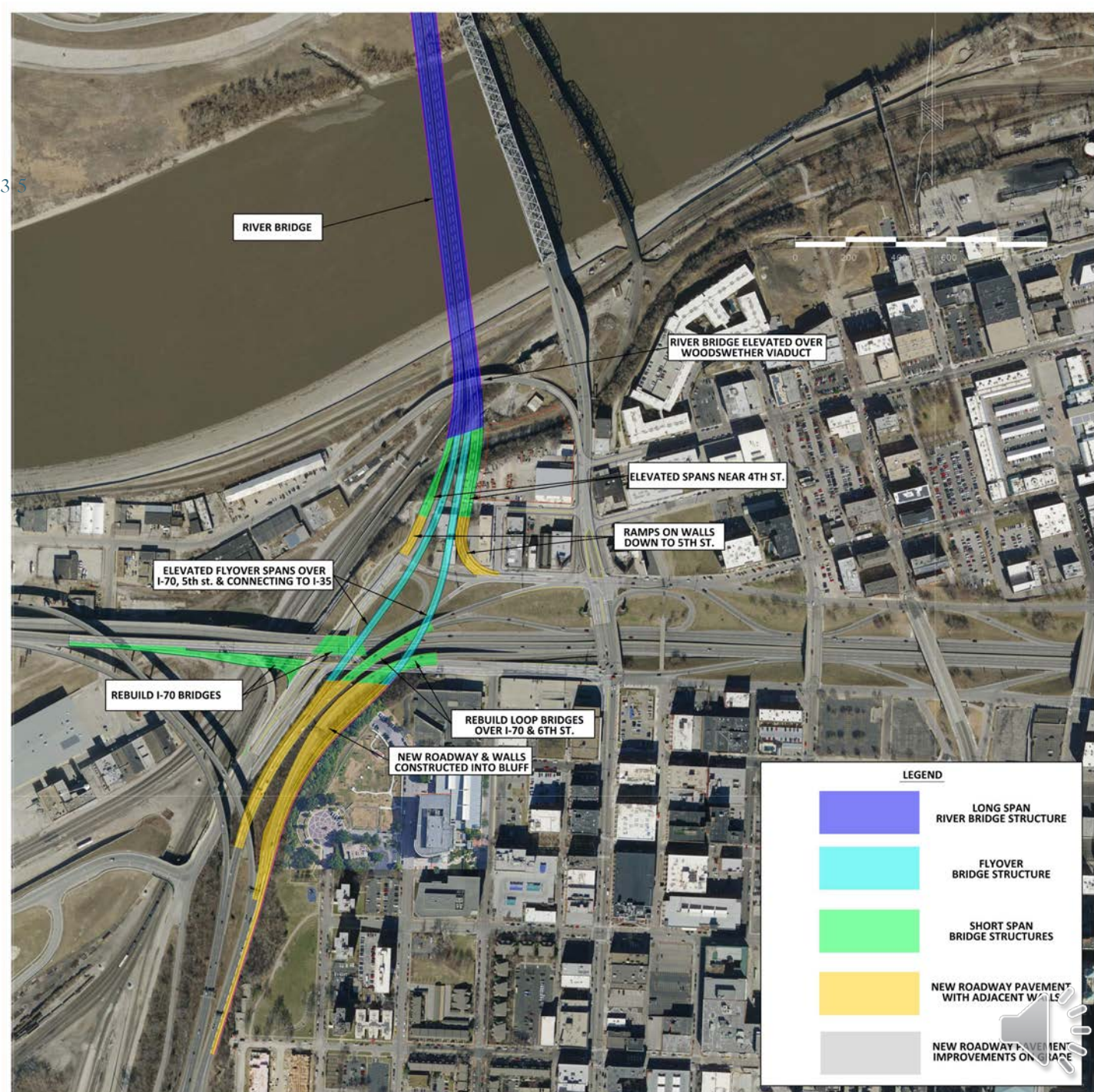
## PROS:

- PROVIDES NEW RIVER BRIDGE
- ACCOMMODATES BICYCLES AND PEDESTRIANS
- IMPROVES AIRPORT AND DOWNTOWN ACCESS
- PROVIDES DIRECT CONNECTIONS TO I-35
- IMPROVES COMMUNITY CONNECTIVITY
- MINIMIZES AMOUNT OF NEW RIGHT OF WAY NEEDED
- MINIMIZES CONSTRUCTION CLOSURE DURATION FOR US-169 & BROADWAY

## CONS:

- REMOVES EXISTING BUCK O'NEIL BRIDGE
- TEMPORARY CLOSURES ALONG I-70 REQUIRED DURING CONSTRUCTION

ESTIMATED CONSTRUCTION COST \$230-\$250 MILLION





NEW RIVER CROSSING – BRIDGE ON CENTRAL ALIGNMENT

# CENTRAL ALTERNATIVE

RAMPS AT BROADWAY, DIRECT CONNECT TO I-35

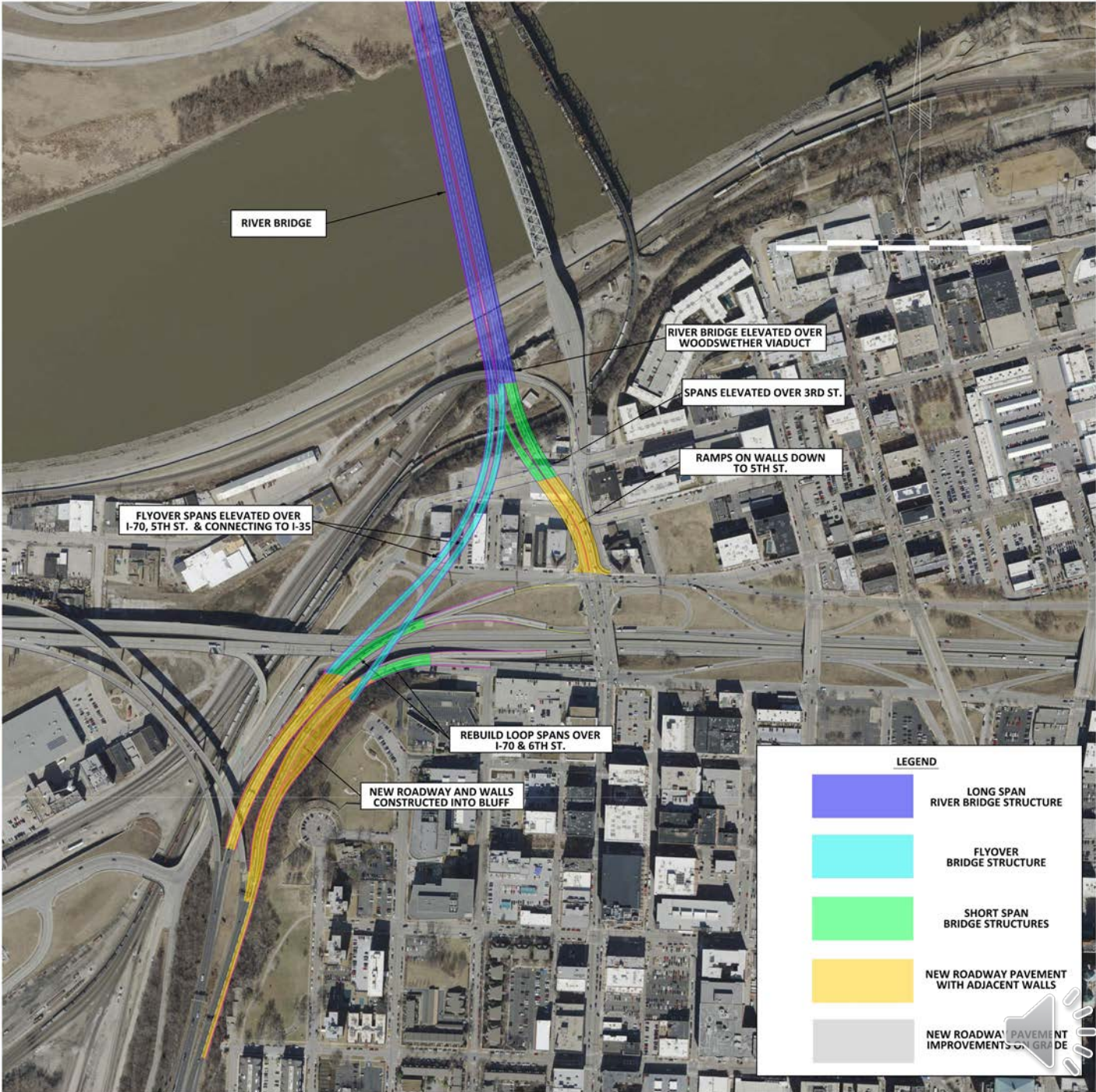
## PROS:

- PROVIDES NEW RIVER BRIDGE
- ACCOMMODATES BICYCLES AND PEDESTRIANS
- IMPROVES AIRPORT AND DOWNTOWN ACCESS
- PROVIDES DIRECT CONNECTIONS TO I-35
- PARTIALLY IMPROVES COMMUNITY CONNECTIVITY

## CONS:

- REMOVES EXISTING BUCK O'NEIL BRIDGE
- ADDITIONAL RIGHT OF WAY NEEDED
- TEMPORARY CLOSURES ALONG US-169 AND I-70 REQUIRED FOR A MODERATE TIME PERIOD DURING CONSTRUCTION

ESTIMATED CONSTRUCTION COST \$210-\$230 MILLION



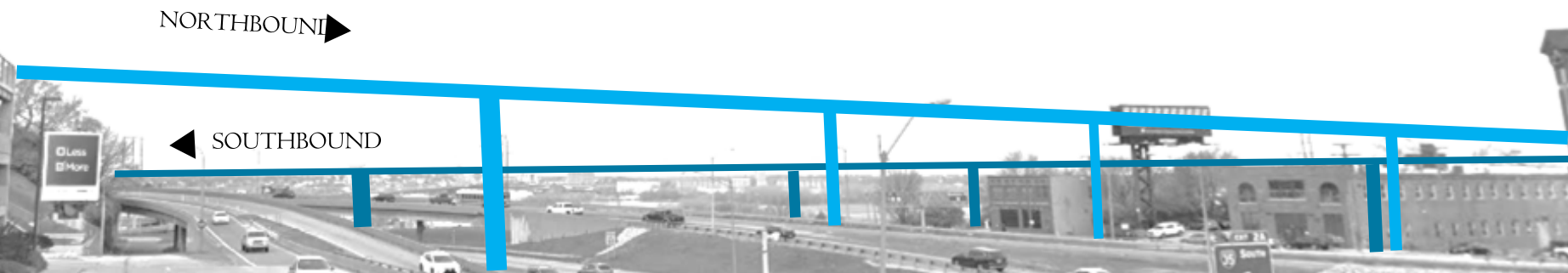
NEW RIVER CROSSING – WEST OR CENTRAL ALIGNMENT

# WEST OR CENTRAL ALTERNATIVE

## DIRECT CONNECT TO I-35

LOOKING WEST FROM 6<sup>TH</sup> STREET AND BROADWAY

Options





# NEW RIVER CROSSING – BRIDGE ON ADJACENT ALIGNMENT ADJACENT ALTERNATIVE

## OPTION 1

CAPACITY IMPROVEMENTS AT 5TH & BROADWAY  
NO DIRECT CONNECT TO I-35

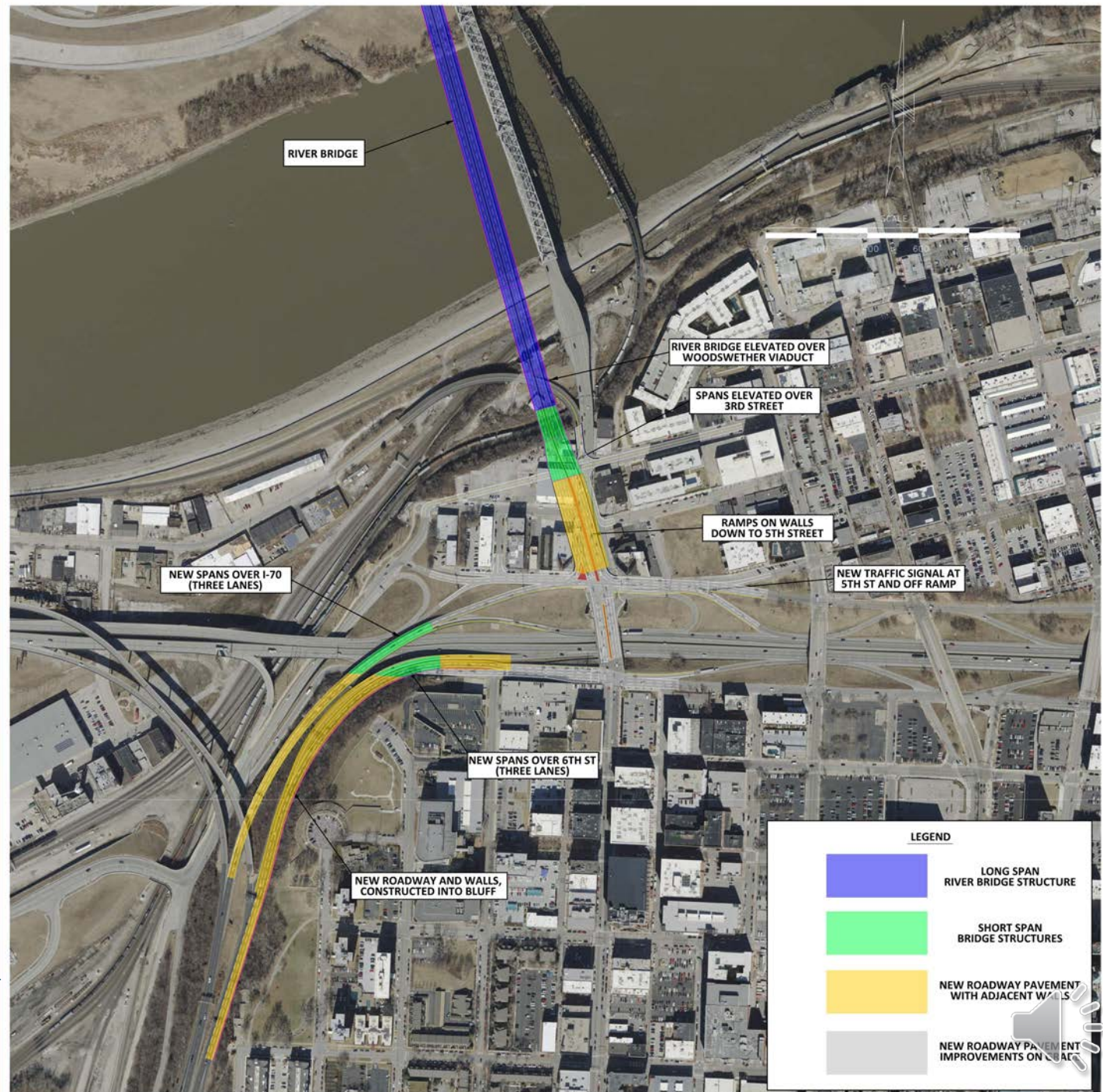
### PROS:

- PROVIDES NEW RIVER BRIDGE
- ACCOMMODATES BICYCLES AND PEDESTRIANS
- IMPROVES DOWNTOWN ACCESS
- MINIMIZES AMOUNT OF NEW RIGHT OF WAY NEEDED

### CONS:

- REMOVES EXISTING BUCK O'NEIL BRIDGE
- NO DIRECT CONNECTIONS TO I-35
- DOES NOT IMPROVE COMMUNITY CONNECTIVITY
- TEMPORARY CLOSURES ALONG US-169 AND BROADWAY REQUIRED FOR A MODERATE TIME PERIOD DURING CONSTRUCTION

ESTIMATED CONSTRUCTION COST \$180-\$200 MILLION





# NEW RIVER CROSSING – BRIDGE ON ADJACENT ALIGNMENT ADJACENT ALTERNATIVE

## OPTION 2

CAPACITY IMPROVEMENTS AT 5TH & BROADWAY  
FUTURE DIRECT CONNECT TO I-35

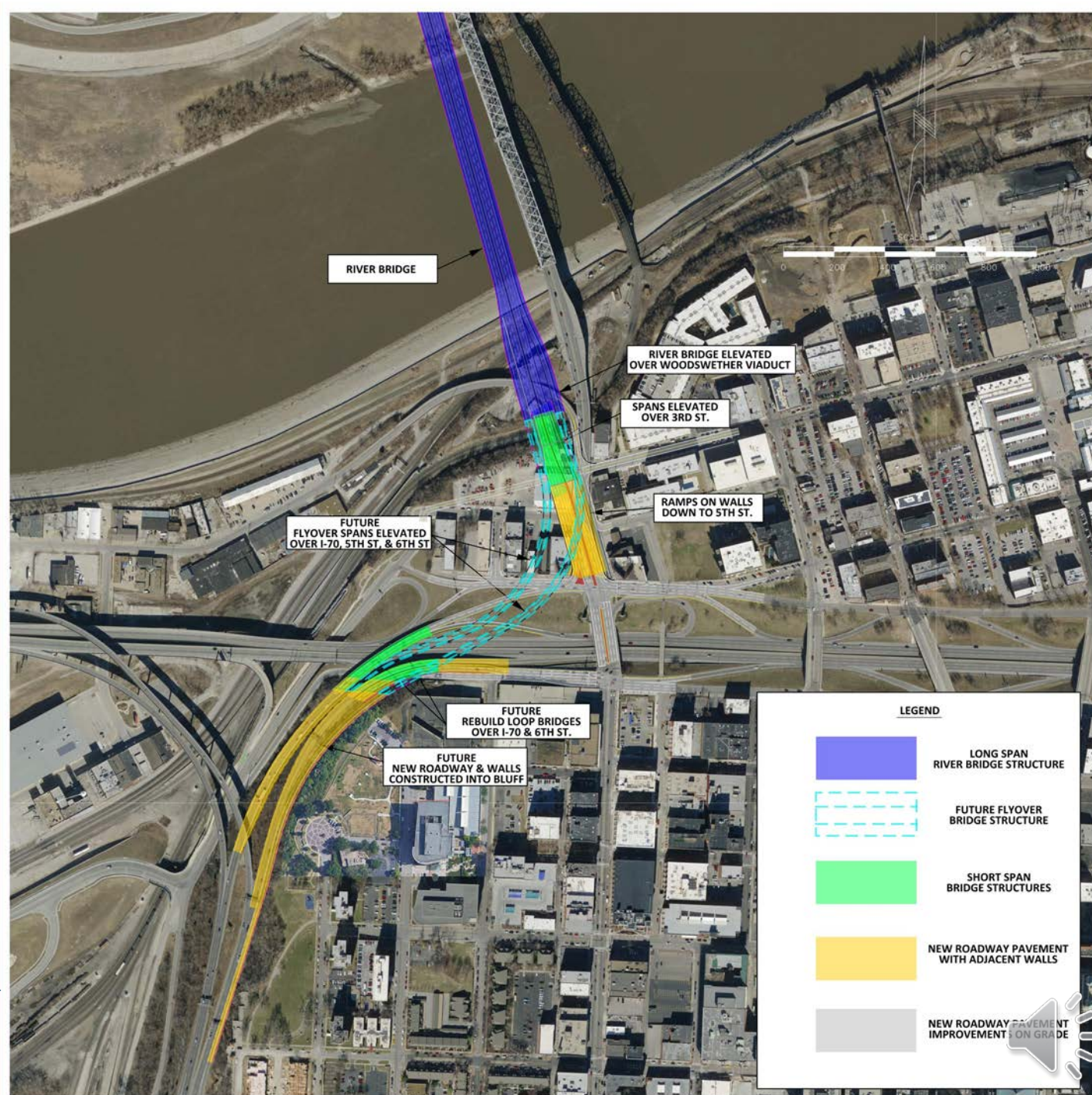
### PROS:

- PROVIDES NEW RIVER BRIDGE
- ACCOMMODATES BICYCLES AND PEDESTRIANS
- IMPROVES AIRPORT AND DOWNTOWN ACCESS
- PROVIDES FOR FUTURE DIRECT CONNECTIONS TO I-35
- MINIMIZES AMOUNT OF NEW RIGHT OF WAY NEEDED

### CONS:

- REMOVES EXISTING BUCK O'NEIL BRIDGE
- DOES NOT IMPROVE COMMUNITY CONNECTIVITY
- MODERATE CONSTRUCTION CLOSURE DURATION FOR US-169 & BROADWAY

ESTIMATED CONSTRUCTION COST \$180-\$200 MILLION  
INITIALLY, ADDITIONAL FUTURE COSTS TO PROVIDE  
DIRECT CONNECT RAMPS TO I-35





# NEW RIVER CROSSING – BRIDGE ON ADJACENT ALIGNMENT ADJACENT ALTERNATIVE

## OPTION 3

RAMPS AT BROADWAY, DIRECT CONNECT TO I-35

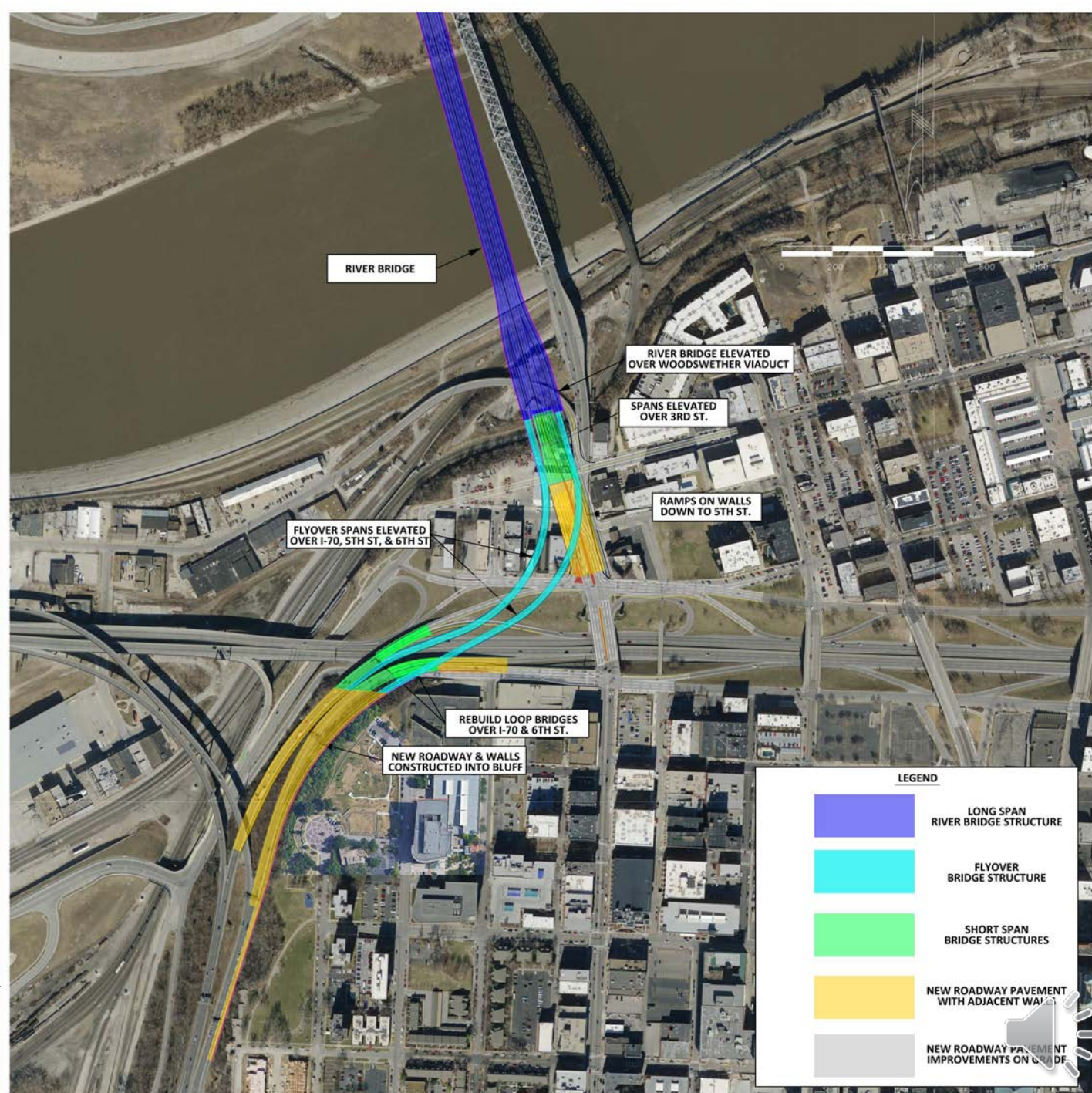
### PROS:

- PROVIDES NEW RIVER BRIDGE
- ACCOMMODATES BICYCLES AND PEDESTRIANS
- IMPROVES AIRPORT AND DOWNTOWN ACCESS
- PROVIDES DIRECT CONNECTIONS TO I-35
- PARTIALLY IMPROVES COMMUNITY CONNECTIVITY
- MINIMIZES AMOUNT OF NEW RIGHT OF WAY NEEDED

### CONS:

- REMOVES EXISTING BUCK O'NEIL BRIDGE
- MODERATE CONSTRUCTION CLOSURE DURATION FOR US-169 & BROADWAY

ESTIMATED CONSTRUCTION COST \$210-\$230 MILLION





SAME ACCESS IMPROVEMENTS FOR ALL BUILD ALTERNATIVES

# NORTH SEGMENT

AT AIRPORT AND HARLEM ROAD

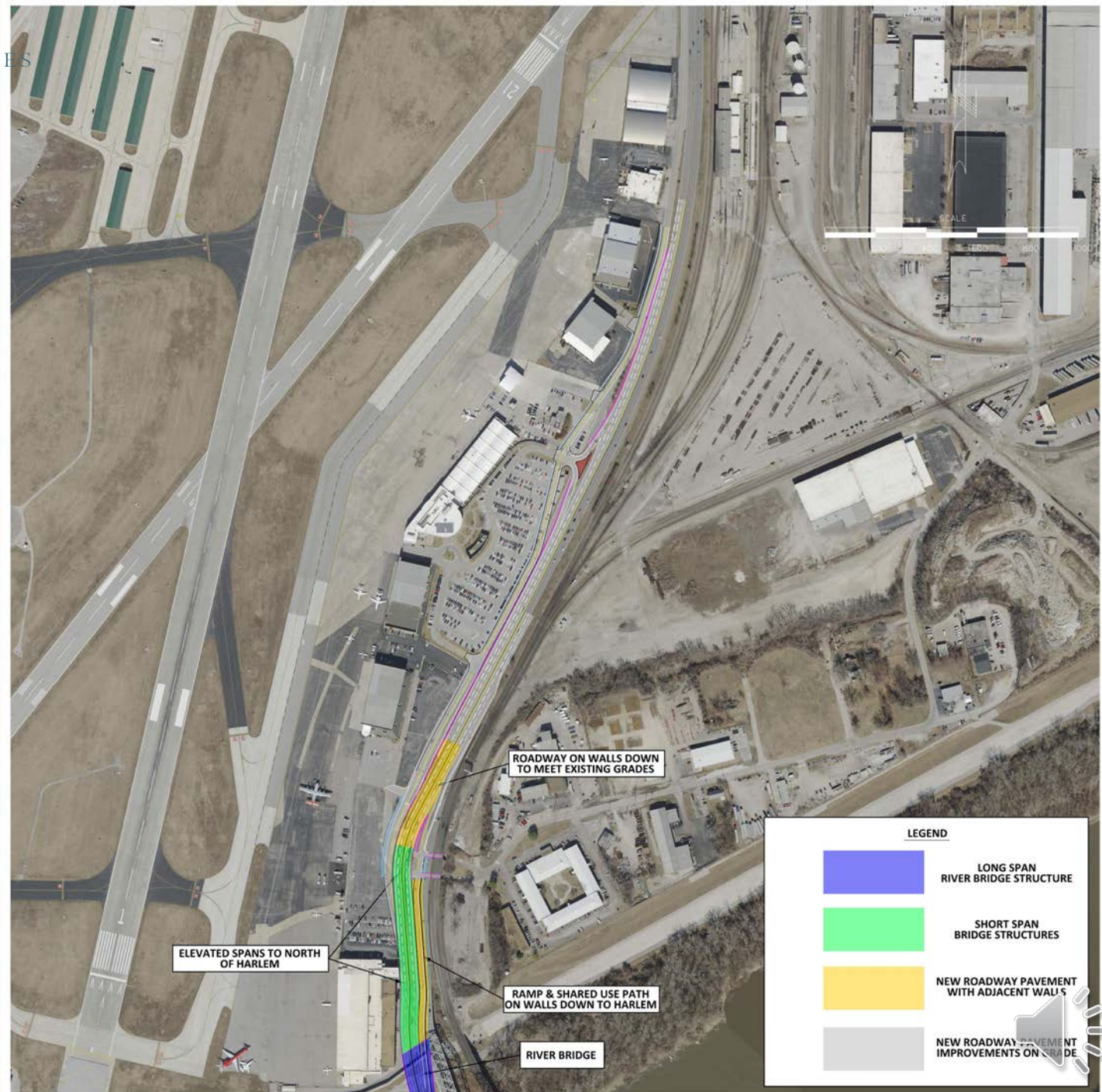
## PROS:

- IMPROVES ACCESS FOR TENANTS AND TRAVELERS AT BOTH THE NORTH AND HARLEM ROAD ENTRANCES
- AVOID IMPACTS TO THE HISTORIC T&WA BUILDING
- AVOID ENCROACHMENT INTO RAILROAD RIGHT OF WAY

## CONS:

- MAY REDUCE THE NUMBER OF PARKING SPACES AVAILABLE AT CERTAIN BUILDINGS
- LIMITED LANE CLOSURES ALONG US-169 DURING CONSTRUCTION

*MoDOT, KCMO, and the Kansas City Aviation Department are still reviewing options to improve access. Improvements to the Airport will also require approval by the Federal Aviation Administration.*





# NEXT STEPS

TIMELINE:

- VIRTUAL MEETING, AUGUST 2019 – OBTAIN INPUT ON WHICH ALTERNATIVES TO STUDY IN DETAIL
- ONGOING 2019 – DETAILED ENVIRONMENTAL STUDIES FOR REASONABLE ALTERNATIVES – TRAFFIC ANALYSIS, NOISE ANALYSIS, IMPACT EVALUATION, SECTION 106 AND 4(f) CONSULTATION, AND AGENCY COORDINATION
- FALL 2019 – ENVIRONMENTAL DOCUMENT RELEASED AND PUBLIC HEARING
- EARLY 2020 – ENVIRONMENTAL DECISION ISSUED BY FHWA



**WE WANT TO HEAR FROM YOU!**

**LEAVE A COMMENT AND TELL US WHAT YOU THINK  
ABOUT THE ALTERNATIVES PRESENTED TODAY**





An aerial sketch of a city and river. The city is shown in the background with various buildings and structures. A river flows through the foreground, with a bridge crossing it. The bridge is a truss bridge with a steel structure. The river is blue, and the surrounding land is green with trees. There are colored overlays on the image: a red band across the top, an orange band below it, and a green band below that. The word "Thanks." is written in large white letters across the center of the image.

# Thanks.



US-169/BUCK O'NEIL BRIDGE  
ENVIRONMENTAL STUDY

